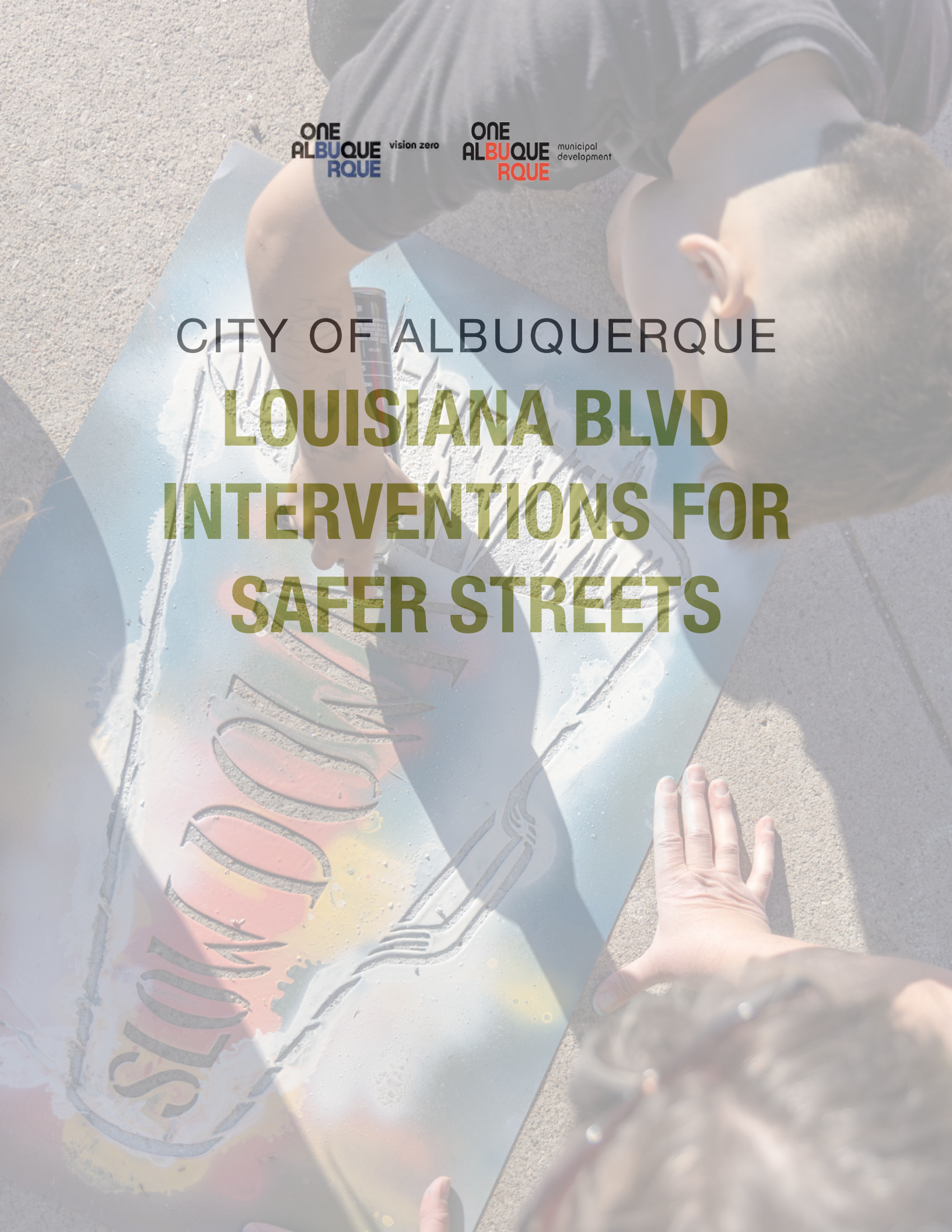


CITY OF ALBUQUERQUE **LOUISIANA BLVD INTERVENTIONS FOR SAFER STREETS**



MAY 2023

LOUISIANA BLVD INTERVENTIONS FOR SAFER STREETS OUTREACH REPORT

Prepared for
CITY OF ALBUQUERQUE

Prepared by
groundworkstudio



ACKNOWLEDGEMENTS

COMMUNITY PARTNERS

CABQ Vision Zero
CABQ Department of Municipal Development
International District Healthy Communities Coalition
Prebyterian Healthcare; Healthy Here
Together 4 Brothers
East Central Ministries
Casa Feliz; Greater Albuquerque Housing Partnership
New Mexico Asian Families Center
Van Buren Middle School



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INTRODUCTION

Community members of Albuquerque's International District have worked for decades to make their neighborhood safer for everyone. This project sought community input on ways in which the City can respond with interventions to slow traffic and reduce fatal and serious injury crashes. The intermediate interventions proposed provide space for collaboration, integration of neighborhood history, culture and art, neighborhood self-determination and long-term safety solutions.

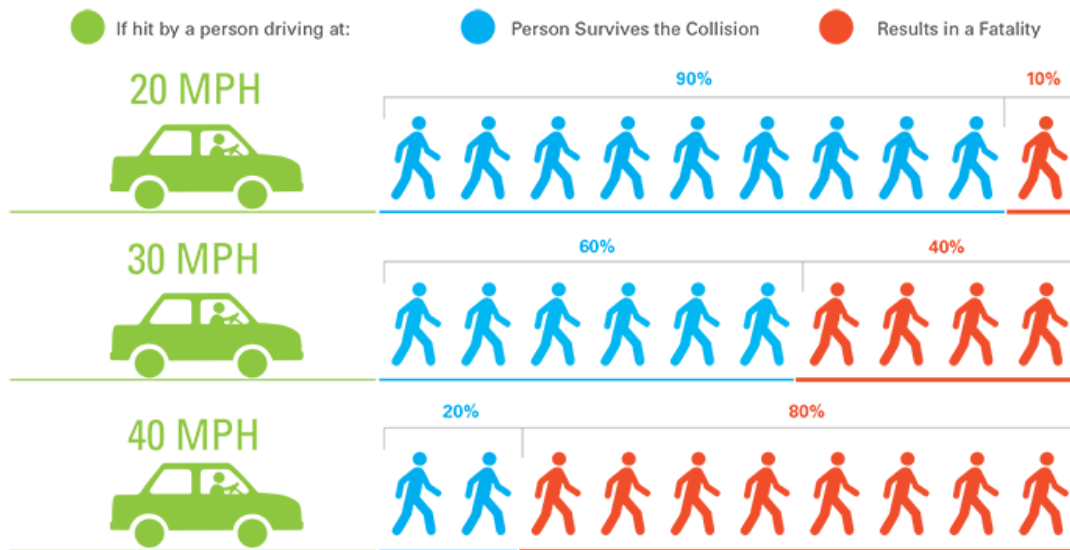
In 2021, the City engaged Bohannon Huston Inc. and Groundwork Studio to assist with community engagement and design for safety improvements on Louisiana Blvd. This report summarizes the engagement process and outcomes.

BACKGROUND

The City of Albuquerque's Vision Zero resolution aims to make streets safe for everyone regardless of age, ability, or travel mode by eliminating all traffic fatalities and serious injuries by 2040. Traffic crashes resulted in 92 fatalities and 5,592 injuries in Albuquerque in 2019 alone, and injury and fatality crashes have increased over the past decade.^[i]

Pedestrian fatalities and injuries in Albuquerque's International District neighborhood have been a major concern for residents for years. In 2020, the City of Albuquerque conducted a Road Safety Audit (RSA) to pinpoint the most dangerous areas along Louisiana Blvd between Gibson Blvd and Lomas Blvd and provide possible intermediate and long term solutions to reduce traffic fatalities and injuries. The Pedestrian Road Safety Assessment Report can be found at <https://www.cabq.gov/vision-zero/documents/final-abq-rsa-report-12oct2020.pdf>.

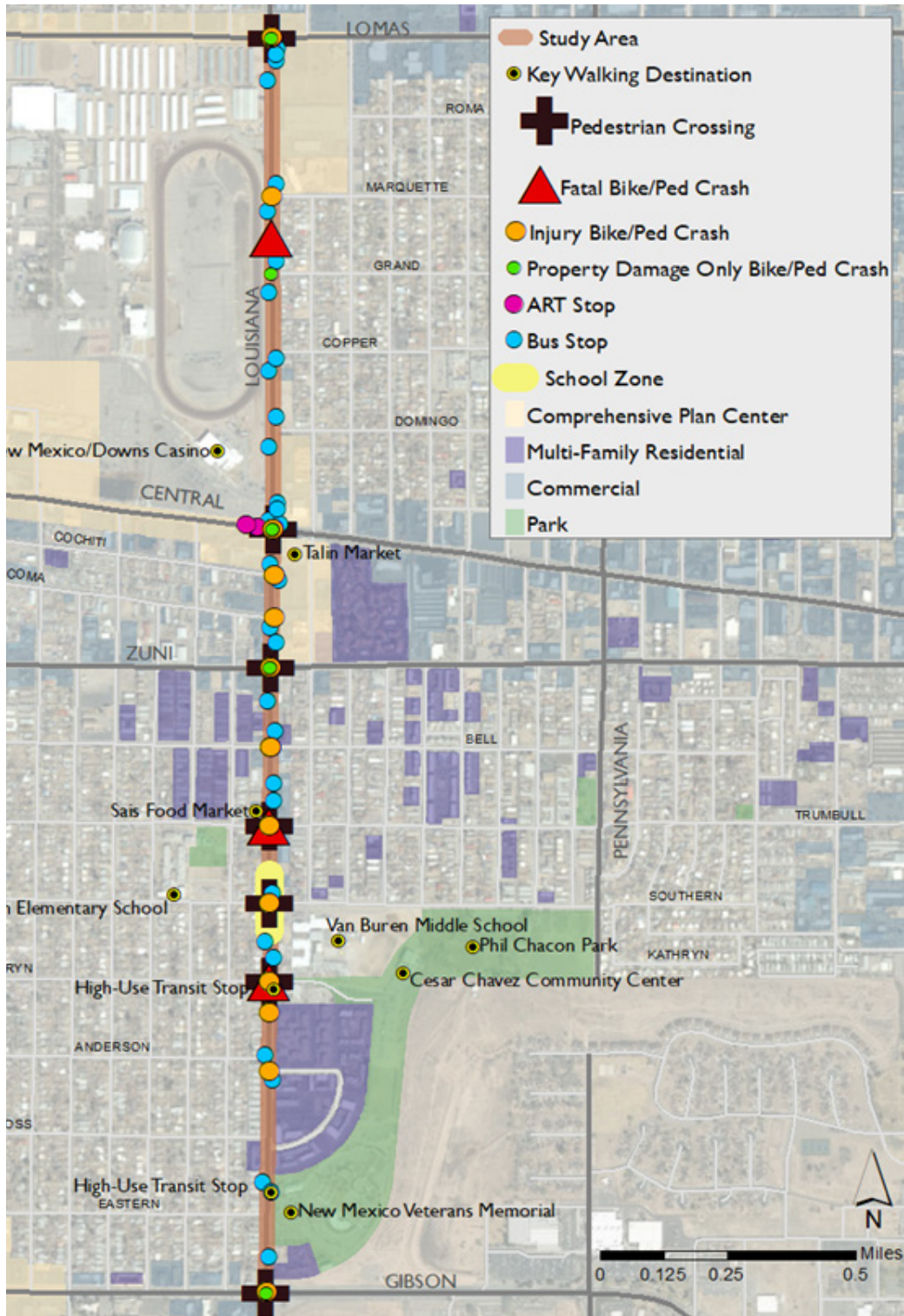
[i] NMDOT. (2020). 2019 Community Report Albuquerque. Retrieved from https://gps.unm.edu/gps_assets/tru_data/



Vision Zero Action Plan San Francisco MTA <https://view.joomag.com/vision-zero-san-francisco/0685197001423594455?short>

Traffic calming interventions increase safety for all roadway users by encouraging and influencing drivers to slow down. Slower driving significantly increases the chances of surviving a crash as shown by the above diagram: pedestrians have a 90% chance of surviving a crash with a vehicle moving at 20 mph and only a 20% chance of surviving a crash with vehicle speeds of 40 mph. Driver speeds can be reduced by implementing design elements that slow traffic, such as adding medians and pedestrian refuges, reducing the number of travel lanes, and narrowing the roadway by adding curb extensions.

This effort sought neighborhood input regarding possible traffic calming treatments on the Louisiana corridor between Gibson Blvd and Lomas Blvd in the City of Albuquerque. The project collaborated with International District Neighborhood residents to evaluate which interventions can best improve neighborhood safety and quality of life. Generally, these treatments can be installed in areas with high rates of traffic fatalities to address safety issues while long-term solutions are developed and funding for larger-scale projects is procured.



Study Area with Bike and Pedestrian Crash Sites; BHI 2021

ENGAGEMENT PROCESS & OUTCOMES

STAKEHOLDERS

Due to the disproportionate number of bike and pedestrian crashes along the Louisiana Blvd corridor within the International District, community members continue to voice concern and propose solutions to improve neighborhood safety. This project is a response to those efforts. The neighborhood outreach seeks to inform the City's plan for interventions that will slow traffic and reduce crashes along this corridor. Outreach efforts encouraged community participation in developing design solutions and messaging that reflects and supports the cultures and characteristics of the neighborhood. The participation process was multifaceted and involved several activities designed to engage stakeholder groups. These activities and the results are summarized in the following pages.

International District Healthy Communities Coalition (IDHCC):

Residents of Albuquerque's International District have faced pedestrian safety problems for over a decade. According to a series of articles by KUNM's Marisa DeMarco in 2019, one in five crashes in Bernalillo County happen in this neighborhood. The International District Healthy Communities Coalition (IDHCC), is one of the most active groups organizing local residents to improve pedestrian safety. Leaders in the IDHCC point to speeding drivers, poorly lit crosswalks, narrow sidewalks and broken streetlights, as major sources of danger in the International District. Most tragic are pedestrian fatalities that have happened in the ID, such as an 8 year old boy in 2017, who was struck by a speeding driver while crossing the street. In 2019, IDHCC raised funds and installed 24 solar street lights in the International District.

Groundwork Studio coordinated with the IDHCC through attending their meetings and inviting members to participate in various outreach events.

East Central Ministries:

East Central Ministries is a core organization in the International District, serving the most marginalized of Albuquerque residents; most specifically recent immigrants and refugees. ECM assisted by hosting the first community design workshop in the Spring of 2022.

Casa Feliz Apartments, an affordable housing complex in the International District run by the Greater Albuquerque Housing Partnership, also allowed Groundwork Studio to host a community design workshop and volunteer “build for safety” day at their location. In both of these instances, hosting community workshops in the neighborhood, close to the Louisiana corridor, was key to engagement.

Together 4 Brothers (T4B), has been active in working towards transit equity in the International District and other city neighborhoods. Their mission aligns closely with this project's goals. T4B provided assistance gathering neighborhood feedback as well as conducting a walk and bike audit.

Van Buren Middle School is a neighborhood anchor, located along the Louisiana corridor. Groundwork Studio worked with their Community Schools' Coordinator to promote volunteer events throughout the year. The community sidewalk safety stenciling activity was held during the school's Multicultural Family Night in April 2023.

Asian Family Services serves the significant population of Asian Americans, as well as immigrants and refugees native to Asian countries living in the International District. Asian Family Services translated the project outreach messages on bus shelter interactive installations into eight languages: Vietnamese, Chinese (Simplified), Chinese (Traditional), Dari, Korean, Japanese, Thai, and Farsi.

The Greater Albuquerque Active Transportation Committee, or GAATC, is a coalition of groups advocating for improved and more accessible public transit and people powered transit in the Albuquerque region. Groundwork Studio and BHI presented this project and its goals at their monthly meeting in the fall of 2021.

STORYMAP WEBSITE

Groundwork Studio created a Storymap through ESRI's platform. Their project team shared background information, visuals, and advertised upcoming events on this site. They also communicated project updates to the public and included the following information through the website;

- Updates and photos of recent project community engagement
- Upcoming event announcements and email sign up
- Community outreach results
- Project background and neighborhood history
- The 2020 Louisiana Blvd Pedestrian Road Safety Assessment Report
- Visuals and explanations of near-term, medium-term and long-term safety interventions
- Stakeholder group information
- The Mid-Region Metropolitan Planning Organization's map of high fatality corridors/locations
- Resources and links to other pedestrian and bicycle related creative placemaking projects

Possible Interventions

Pedestrian Refuge Islands



Curb Extensions & Bump-outs




Volunteer for Ciglovia Introduction Community Feedback on Safety I... Background Possible Interventions Neighborhood and Project Back... Involved Stakeholders and Comm...

ROAD RECONFIGURATION (Near-Term)


BENEFITS

- Reduces crossing distance for pedestrians
- Can reduce vehicle speeds
- Buffers sidewalk from travel lanes
- Improves quality of community life by allowing for safer pedestrian use

Source: Albuquerque Road Safety Audit



Before



Louisiana Blvd Interventions for Safer Streets website

CIQLOVÍA BIKE & WALK AUDIT

CiQlovía 2021

Open street events, also known as ciclovías, are events in which specific streets are closed off to vehicular traffic and opened up to people to walk, bike, play, and reimagine our largest public space...streets. They are also an opportunity to re-envision roadways to include more pedestrian and bike infrastructure. The term “ciclovía” means cycleway or bike path. The event takes over city streets and parking spaces with pop-up installations, making them more pedestrian and bicycle friendly. Community organizers plan and implement Albuquerque’s version, CiQlovía, annually with cycling activities, temporary street installations, health hubs, games, music, and demonstrations reimagining safer outdoor public spaces.

In October of 2021, one CiQlovía hub was located in the International District, in the courtyard of the Casa Feliz Apartments. A polling activity was set up to gather feedback from community residents. Preference boards with images of pedestrian refuges, brightly painted crosswalks, curb bumpouts, chicanes, buffer lanes, bus stop shelters, pavement murals, street signage, midblock crossings, delineators, temporary curbs and community based art were displayed through example images. (See figures 1 and 2). CiQlovía participants were asked to place dots on the images they liked best to improve safety on Louisiana Blvd.

The most popular design preferences were bike lane delineators, buffered bike lanes, curb bumpouts, pavement murals, bus stop shelters and wide mid-block crossings. See Appendix A for the complete outreach responses.



CiQlovía, Fall 2021; Casa Feliz



Figure 1: Preference Board at CiQlovia



Figure 2: Preference Board at CiQlovia

Together for Brothers Bike & Walk Audit

Together for Brothers (T4B) hosted a community bike and walk audit in collaboration with CiQlovia. They had fifteen participants complete the audit and tool, commenting on comfort, biking, traffic, streetscaping and lighting. The tool and responses included scaled questions as well as open ended questions. The results demonstrated a general consensus that there was a need to slow traffic and also reduce the size of lanes on Louisiana to make it safer for bikers, pedestrians, people in wheelchairs and others.



Together for Brothers participants noting their opinions on preference boards.



Together for Brothers on their walk and bike audit of the International District.



Louisiana and Eastern photo of current conditions



Louisiana and Trumbull photo of current conditions



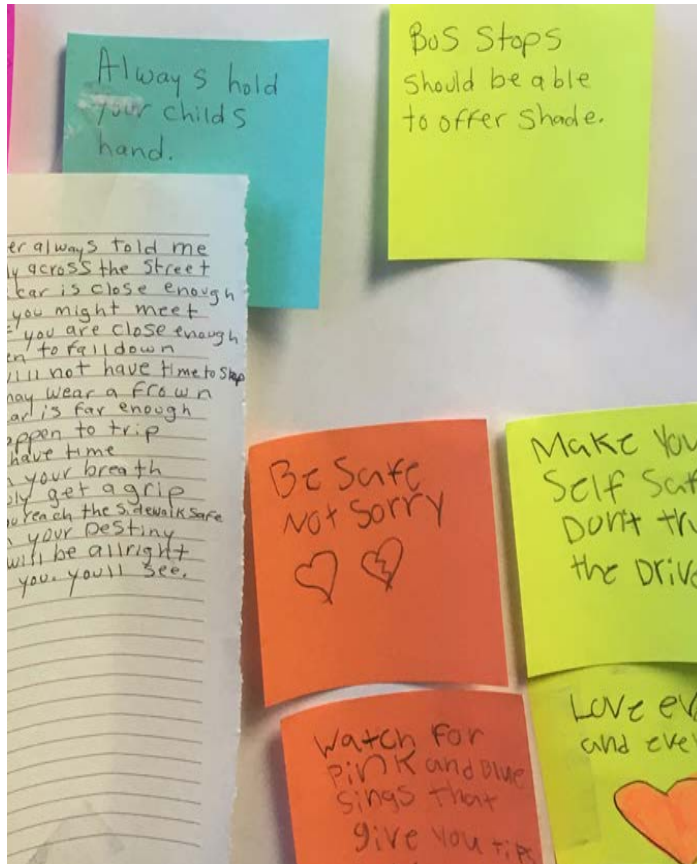
Louisiana and Eastern; photo realistic rendering with interventions



Louisiana and Trumbull photo realistic rendering with interventions

Photo-realistic renderings were also created of two sections of Louisiana Blvd at Eastern and Trumbull. At Eastern, adding a median, buffered bike lane and restriping the pavement would slow vehicles and allow for safer biking and walking. The Trumbull rendering above shows the addition of an artistically painted median, a buffered bike lane and a restriped road. While the interventions shown in these renderings do not reflect the final re-stripping design, nor the Manual on Uniform Traffic Control Devices, they allowed the public to re-imagine how safety interventions could transform the corridor to a more vibrant, walkable space.

DESIGN WORKSHOP SERIES



Engaging the International District in Design

In order to develop community-based messaging around neighborhood identity and safety concerns, members of the community were invited to attend a series of design workshops. The first Saturday workshop used poetry creation as a vehicle for identifying important cultural and historical messages and symbols to be incorporated in the Louisiana Blvd Interventions. It was held in the neighborhood at East Central Ministries. During the following Saturday workshop at Casa Feliz, participants revisited themes from their poems, such as significant community landmarks, important childhood memories, familiar sayings, and notable surroundings. They then split into groups in which members collaborated on design interventions for safety they would like to see in the International District. On their posters,



group members noted their guiding values, physical intervention ideas, the materials needed and cut out photos from magazines as a way to illustrate their visions. Participants were compensated for their time in both workshops.

Major Themes that Emerged from Workshops

- Better informational and wayfinding signage
- Better street lighting
- More trees and vegetation along streets and sidewalks
- Need for bus stop shelters
- Traffic calming interventions
- Need for play spaces for children along sidewalks
- More benches with shade
- Need for trash and recycling receptacles

Play, Movement, Fun
Safety, Lighting

Materials

Concrete (bench + barrier)

Reclaimed wood + metal

Rubber surface

Design

Outdoor gym
walls for playing
Shade

Reclining benches

Signs w/ stretches + distances



BUS SHELTER INTERACTIVE BOARDS



**STREETS
FOR
PEOPLE**

**Louisiana Boulevard Interventions
for Safer Streets:**

Community-based solutions to calm traffic and
increase safety in the International District;
Short-term actions for long-term goals

For more information visit: bit.ly/louisianablvd



In September 2022, additional community feedback was sought for safety interventions along the Louisiana Blvd corridor through interactive boards installed at bus shelters. The signs also displayed a QR code allowing for public access to updates on the project. Through the help of translators from the City and Asian Family Services, the prompts “I want safer streets because...” and “I would like to seefor Louisiana Blvd” were translated into the 13 most commonly used languages in the International District. Large boards were fastened to bus stop shelters lining Louisiana Blvd from Gibson to Central. The boards contained space for pedestrians and transit users to write their responses in the small white squares at the sign's bottom. Sharpie markers were available in a cup fastened to the stop. Project staff checked boards every few days to refill the marker cup and to record public responses. The boards were maintained for two weeks and then taken down.

Responses to “I want safer streets because” included:

- we need assistance for the homeless
- we need rehabilitation for substance abuse
- we need more mental health assistance
- crime is a problem
- children need a safe space to live
- we walk to school and to the bus stop
- I want to feel safe when walking
- we need more lighting
- we need more frequent trash pick up
- we need more safety at bus stops and on transit

Responses to “I would like to see...for Louisiana Blvd” included:

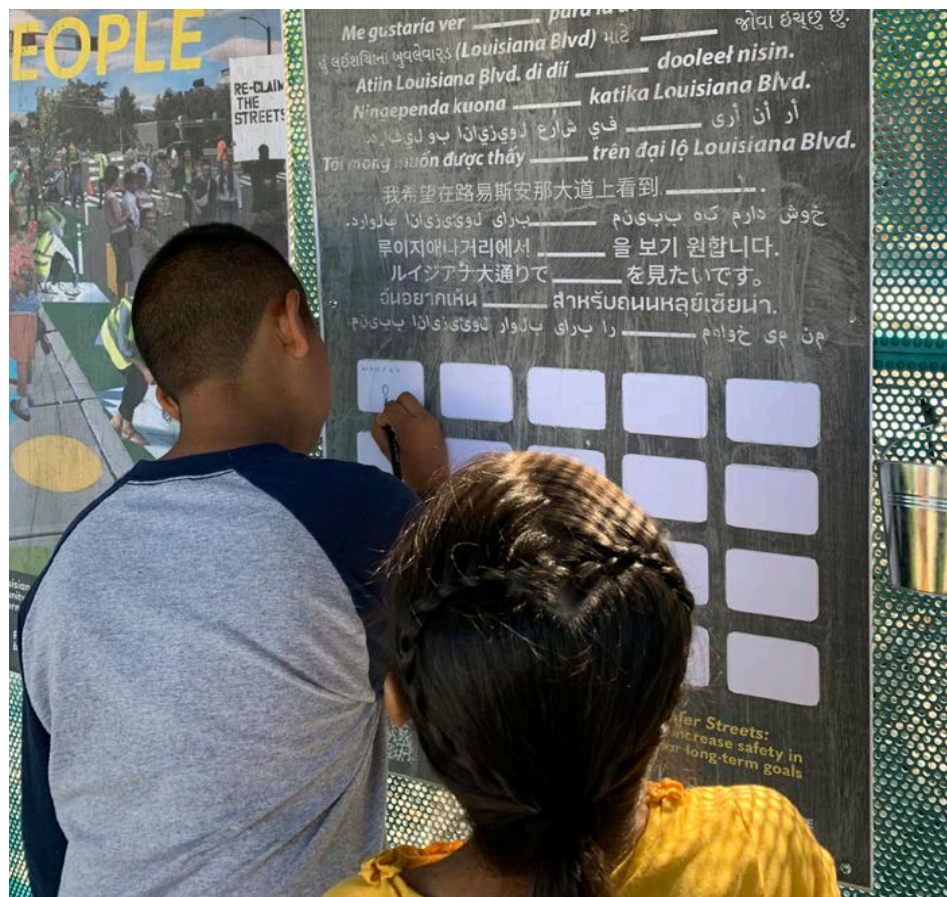
- people obeying traffic signals
- better traffic lights
- speeding tickets
- more cops
- a noise ordinance for cars
- more trees
- slower speeds
- a crosswalk at Louisiana and Continental
- more creative lighting for pedestrians
- more crosswalks for kids
- a free community bike shop



For the complete list of community responses collected, see Appendix E.



Installation of an interactive board at a bus shelter along Louisiana Blvd.



Young man writing commentary on an interactive bus shelter board along Louisiana Blvd.

COMMUNITY SAFETY STENCIL INSTALLATION

Feedback from the interactive bus stop signs, as well as themes from the community design workshops, informed five safety themed stencil designs created by local artist, Andrew Fearnside. Messages on the stencil templates emphasized the importance of pedestrian visibility through better street lights, safe routes to school for children, slowing vehicle speeds, safe pedestrian paths and safety in general. Stencil designs are included in Appendix G. The sidewalk stencils and spray paint were used as a teaching tool to engage community members during a family night at Van Buren Middle School in the International District. In April of 2023, Groundwork Studio, BHI and the City of Albuquerque Vision Zero Program hosted a table at the event and engaged youth and their caregivers in spray painting the stencils onto t-shirts. Over 30 youth created their own safety themed t-shirts. Some children also participated in using the stencils to paint pedestrian safety messages on the sidewalks surrounding the school. The activity also involved graphics and information on the updated Louisiana Blvd road diet design. Visitors to the booth viewed plans for the upcoming safety interventions along Louisiana Blvd.



Community safety stencils made by local children on sidewalk in front of Van Buren Middle School.



Young participant in the Van Buren Multicultural Event spray painting a sidewalk stencil.



Children painting sidewalk stencils at the Van Buren Multicultural Event.



A dad and his daughters at the Van Buren Multicultural Event spray painting a t-shirt with pedestrian safety messages.

APPENDICES

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APPENDIX A

CIQLOVIA PREFERENCE BOARDS

PEDESTRIAN REFUGES



Source: PPP Transportation Safety Innovations, Jacksonville, FL



Source: Streetblog Chicago



Source: NACTO

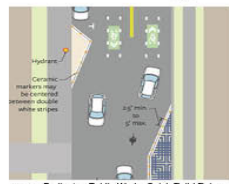
CHICANES



Source: National Association of City Transportation Officials



Source: Secondwave Media, Kalamazoo MI



Source: Burlington Public Works Quick Build Design and Materials Standards

BUFFER LANES



Salt Lake City, UT; source: Houston Bike Plan



Photo source: Houston Bicycle Toolbox



North Carolina bike lane; mtainnews.com

BUMP OUTS



Portland, OR Painted Curb Extension source: bikeportland.org



Source: HealthKids.org, Rochester, NY



Chicago Traffic Calming Intervention

LOUISIANA BLVD SAFETY INTERVENTIONS

NOVEMBER 2021

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BUS STOP SHELTER



Source: Brighton and Hove News, England



Photo source: Archtonic Bellalia



Source: City of Rockingham

PAVEMENT MURALS



Source: Traffic Calming Intervention in Baltimore, MD



Barelas Artful Life mural- Albuquerque, NM

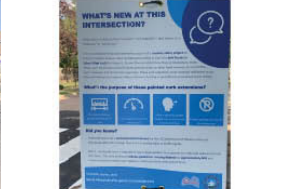


Road median pavement mural- NYC, brownstoner.com

SIGNAGE



Photo source: Secondwave Media, Kalamazoo MI

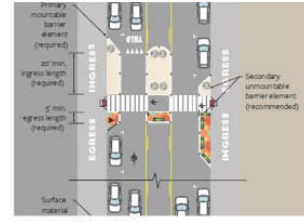


Source: Alexandria VA Complete Streets



Source: NYC Transformation Department

MIDBLOCK CROSSING



Burlington Public Works Quick Build Design and Materials Standards



Source: Streetblog USA, NACTO



Source: 3D Zebra Crossing, island.rumblum.com

LOUISIANA BLVD SAFETY INTERVENTIONS

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APPENDIX A

CIQLOVIA PREFERENCE BOARDS

PLACE MAKING AND ART



The art and benches in the International District



Maclovio Mural in Barelas, Nari Chacon

DELINEATORS



City of Boulder, CO

Wave delineators, San Jose, CA ~~11~~

source: reliance-foundry.com

TEMPORARY CURBS



Temporary rubber curb extension in British Columbia



City of Sarnia, Ontario, Canada



Washington, DC. source: ggwash.org

IMAGINE!



City of Oakland California: Buffers/delineators create protection

LOUISIANA BLVD SAFETY INTERVENTIONS

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APPENDIX B

TOGETHER FOR BROTHERS (T4B)

COMMUNITY BIKE & WALK AUDIT REPORT

Together for Brothers (T4B) & CiQlovía Community Bike & Walk Audit Report



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Overview	3
Audit Tool Responses	4
Evaluation & Feedback	8

T4B is grateful to Groundwork Studio for supporting the project financially and with other resources and capacity building for boys and young men of color as well as community partners. T4B's vision includes health equity and engagement of most impacted communities. This includes making the Louisiana Corridor in the International District more safe for boys and young men of color, their families and everyone. It's important to share funding and resources in transformational ways to ensure that the most impacted communities are at the center and table where decisions are being made.

OVERVIEW

Together for Brothers (T4B) hosted a community bike and walk audit in collaboration with CiQlovía to have fifteen participants complete the audit and tool with responses about comfort, biking, traffic, streetscaping and lighting. The brothers have completed similar and various audits in the International District and Westgate. The tool and responses include a scale as well as “open ended” question about why the participant chose that score.

The results include a general consensus that there was a need to slow down traffic and also reduce the size of lanes on Louisiana to make it safer for bikers, pedestrians, people in wheelchairs and others.

In addition to the tool and responses in this report, the participants also engaged with the boards (as seen in the photo) from Groundwork Studio to give their input into possible solutions to make the Louisiana Corridor safer. It was appreciated by participants that the solutions were presented as images to easily access and solicit feedback from community.

Finally at the end of the tool and responses there are additional responses for evaluation (do/learn/delta/plus) from the participants that provide other context for the audit.

AUDIT TOOL RESPONSES

How comfortable did you feel with the designated bike lane or route or walking lane or route?

1 - Very Uncomfortable 4- Comfortable

TOTAL: 46 AVERAGE: 3.06

Why?

It was pretty big so i liked it
Bike lanes were available but not well maintained. Glass and trash on bike lanes make it hard to navigate.
Not many bike lanes
n/a
Idk
Because some of the way there was good bike lanes.
I just know it very well
Because it isn't that bad
It does feel unsafe and at times uncomfortable, a bit ghetto in some places
There isn't really much of a bike lane at all and at sometimes the sidewalk is narrow
The cars go way too fast
Because most of the people I'm walking with I'm comfortable with
Road is wide and sidewalks need repair and upkeep (including trash)

How comfortable did you feel with the bicycle parking or storage?

1 - Very Uncomfortable 4- Comfortable

TOTAL: 30 AVERAGE: 2.0

Why?

There's not much besides the sidewalk
No bike storage in sight
Didn't see any
Because I can put stuff away
I ain't see one
We didn't see any
I just know it very well
It aint that bad
There isn't any bike racks

There was pretty much no bike rack or parking for any bikes
There's almost no space
We didn't bring bikes
NA
No where to safely park bikes

How comfortable did you feel with the driving lanes, posted speed, perceived speed and odors or sounds from vehicles?

1 - Very Uncomfortable 4- Comfortable

TOTAL: 40 AVERAGE: 2.66

Why?

They were driving fast
No issue
Cars were going somewhat fast
I don't know
You never know, gotta be alert
Some cars speed around us
I'm used to them
Because everyone drives how they want and all
I've driving down this street and at time the speed can get pretty high
Sometimes there are a lot of speeders that go 10-15 over
I only saw people speeding
We can use more
N/A
Cars seem to drive faster with so many lanes and how wide Louisiana and Southern are at this corner

How comfortable did you feel with "streetscaping" along the bike route including artwork, benches and shade trees?

1 - Very Uncomfortable 4- Comfortable

TOTAL: 42 AVERAGE: 2.8

Why?

It was good I liked that there was many trees
Some artwork and CiQlovia stations. Landscaping and shade was ok.
Nit a lot of art

I just like it
More art needed
They were nice
I thought some of it was really cool minus trash in the street
There in some good condition
Has a good amount of plants and trees could be improved also there is a good amount of trash
There are a number of shrubs and trees
There's no shade
There was a lot of shade trees and some art stuff
NA
There was none

How comfortable did you feel with street lighting?

1 - Very Uncomfortable 4- Comfortable

TOTAL: 32 AVERAGE: 2.6

Why?

There were many lines
Not enough
I think a little more light
No prblm
We were during the day
Sometimes it doesn't power on when it needs to
Because people sometimes don't follow it
Needs better lighting
There were not a lot of street lights along the residential streets as well as strips of Louisiana
There's no light
There was some light polls
N/a
I know from past experience that few work

How safe do you feel biking or walking along or crossing Louisiana?

1 - Very Uncomfortable 4- Comfortable

TOTAL: 33 AVERAGE: 2.2

Why?

Cars went fast
Slower speed limit. Better crossing light, bikers never got a green light to cross
Cars slow down
Yes I will
I don't know, there's a lot of cars
More protection
More stop signs for pedestrians
People speeding
Having a bit wider sidewalk
There were no posted bike lanes but there was a sidewalk
Speeding!!!!
Some people don't care about the signs and stops
Better sidewalks
Light seems to take long and no way to indicate I am here, and cars are moving fast and taking all the lanes

EVALUATION & FEEDBACK

What did you do on the ride/walk and audit?

Ride bike and look around at the art and trees
Bike
Bike
I didn't go
Just ride to Whittier
I drove my car behind the bikers
Ride my bike
We were looking what's wrong in the streets and what we could do better
Walked down a couple of blocks on Louisiana
We took a walk and surveyed the area
Walk and see what problems it had
I talked with some people and looked around the neighborhood
We went to the different booths
Felt how fast cars are going

What did you learn about the ride/walk and audit?

I learned that people drive fast around San Mateo
More about Ciqlovia
None
I learned that people will speed around bikers
I didn't
That it ain't safe
What we need to improve or talk about more and also how we felt during the walk
I learned that there needs to be more lighting
How many problems do Louisiana road have
I liked the walk it was fun
I learned about what is going on around the city
How unsafe the corridor is for biking, walking and people in wheelchairs

What did you like about the ride/walk and audit?

I liked being to ride my bike

Steady pace
Fun
The ride
I liked sitting in the car
The feeling of community
The walk and the talk
Able to see what need to be improved
I liked the walk itself
Walking
I liked how I talked with some people
I liked everything
Giving input and hearing from others

What would have made the ride/walk and audit better?

Nothing it was great
Seeing more people
Nothing
If people would stop speeding
More of our brothers showing up
Music snacks I don't know
A bit more shade the sun was hitting a bit too much
I would say that the weather was a little hot
More time
N/A
More time
More brothers and community partners

Why are "access to safe biking and walking" and "community bike and walk audits important" for young men of color and communities most impacted?

Because people need space too ride their bike and to get to work
Because everyone should have safe bike access including people most impacted
They help us make sure to see problems in our community
they can get exercises and be more active
I don't know, to ride around and get air

So that we can communicate what we need
Because we don't have access to transportation as cheaply as others do
For they could get to school with out worrying to much
Because having access gives them a chance to explore and enjoy the community but also being healthy and Building connections
It is important because safety should be the number one concern for YMOC when walking or biking
For exercise and safety
I think it's important because people can know how to respect the place
So they can have access to things easier
We need to have more impacted communities at the table to identify the problems and solutions for themselves, really like that BYMOC were able to give input on the images and possible solutions.

APPENDIX C
CIQLOVIA OUTREACH
SUMMARY

CiQlovía Outreach Summary

Most Popular Design Concepts

1. Bike Lane Delineators (33 responses)
2. Buffered Bike Lanes (27 responses)
3. Bump Outs (26 responses)
4. Pavement Murals (23 responses)
5. Bus Stop Shelter (22 responses)
6. Mid-block Crossing (22 responses)

Delineators



1) Wave delineators
(13 responses)



2) Parking stop
delineators
(12 responses)



3) Armadillos
(8 responses)

Bike Lanes



1) Bike Lane with K71 bollards
(15 responses)



2) Striped bike lane
(7 responses)



3) Bike lane with cement curbing and delineators
(5 responses)

Bump Outs



1) Parking stops,
bollards, painted
crosswalk
(11 responses)



2) Flex posts
(9 responses)



3) Polka dots with
flex posts
(6 responses)

Pavement Murals



1) Road median strip mural with geometric design (12 responses)



2) Intersection mural-rainbow with curb bump outs (6 responses)



3) Barelas artful life intersection mural (5 responses)

Bus Stop Shelter



1) Living roof bus stop shelter with solar
(13 responses)



2) Shelter with painted mural
(6 responses)

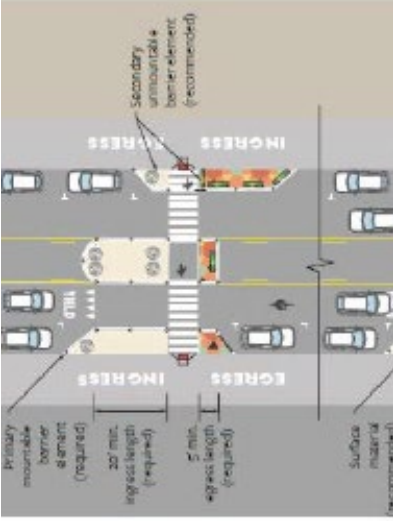


3) Simple bus shelter made from wood and metal
(3 responses)

Mid-Block Crossing



1) 3D Zebra Crosswalk
(11 responses)



2) Crosswalk with pedestrian median and painted bump outs with barriers
(9 responses)



3) Crossing with painted median and crosswalk signage
(3 responses)

Before

Street too wide

No place to wait

Six-foot-long sidewalk

No curb ramp for seniors, wheelchair users, etc.

After

Longer, more clearly marked crosswalk

Painted, protected median

Bike lanes

Curb ramp added

Before

Street too wide

No place to wait

Six-foot-long sidewalk

No curb ramp for seniors, wheelchair users, etc.

After

Longer, more clearly marked crosswalk

Painted, protected median

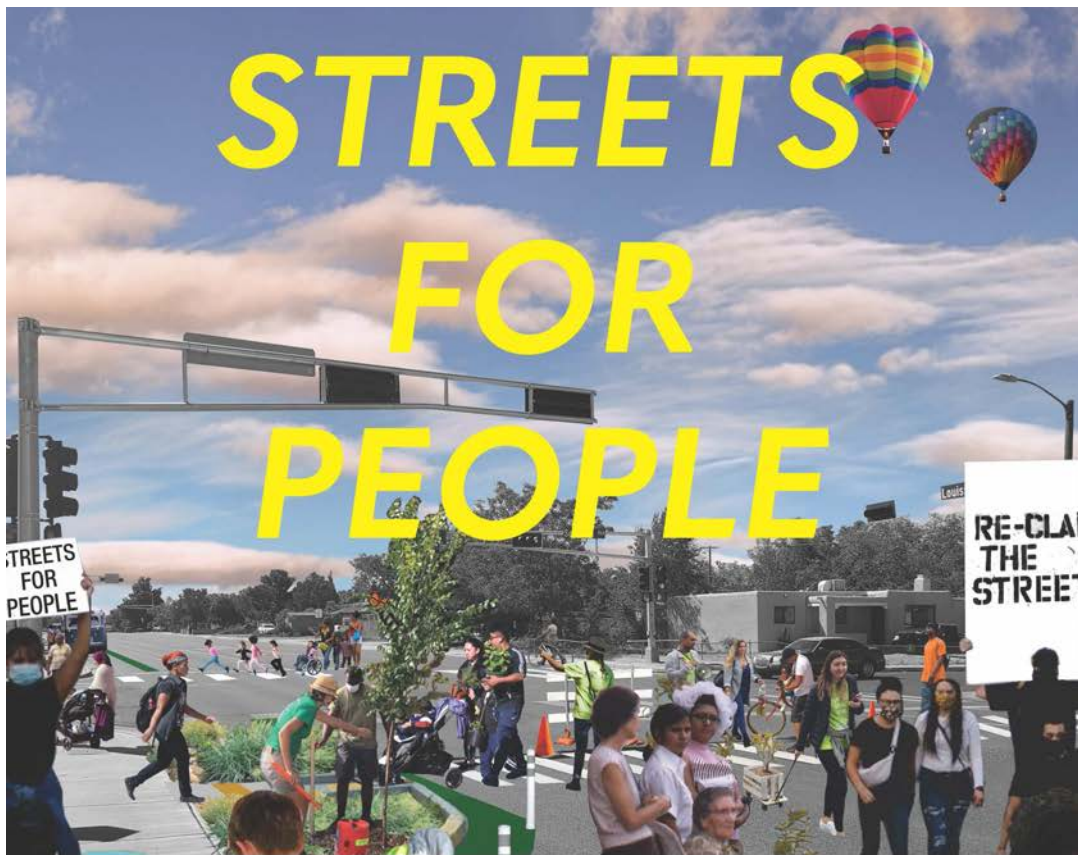
Bike lanes

Curb ramp added

Key Takeaways

- Protected and buffered bike lanes were the highest rated designs. However, this may be due to the high number of cyclists at CiQlovia, some of whom may not live in the International District.
- There were high levels of support for pavement murals, both at bump outs and in medians.
- Chicanes, signage, and temporary curb designs generated the least amount of engagement but still received some support.

BUS SHELTER INTERACTIVE INSTALLATIONS



Louisiana Boulevard Interventions for Safer Streets:

**Community-based solutions to calm traffic and
increase safety in the International District;
Short-term actions for long-term goals**

For more information visit: *bit.ly/louisianablvd*



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I WOULD LIKE TO SEE _____ FOR LOUISIANA BLVD.

Me gustaría ver _____ para la avenida Louisiana.

હું લુઈશયાના બુલેવાર્ડ (Louisiana Blvd) માટે _____ જોવા ઇચ્છુ છું.

Atiin Louisiana Blvd. di díí _____ dooleet nisin.

Ningependa kuona _____ katika Louisiana Blvd.

أر أن أرى _____ في شارع لويزيانا بوليوارد.

Tôi mong muốn được thấy _____ trên đại lộ Louisiana Blvd.

我希望在路易斯安那大道上看到 _____.

خوش دارم که ببینم _____ برای لوئیزیانا بولوارد.

루이지애나거리에서 _____ 을 보기 원합니다.

ルイジアナ大通りで _____ を見たいです。

ฉันอยากเห็น _____ สำหรับถนนหลุยเซียน่า.

من می خواهم _____ را برای بولوار لوئیزیانا ببینم.

Louisiana Boulevard Interventions for Safer Streets:
Community-based solutions to calm traffic and increase safety in
the International District; Short-term actions for long-term goals



For project information or to
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STREETS FOR PEOPLE

STREETS FOR PEOPLE

RE-CLAIM THE STREETS

I WALK BECAUSE...
I Love The Sunshine!!
I Love My Body (Gym)
I Love My Heart
I Love My Friends
I Love My Healthy Heart

Louisiana Boulevard Interventions for Safer Streets:
Community-based solutions to calm traffic and increase safety in the International District; Short-term actions for long-term goals

For project information please visit:
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APPENDIX E

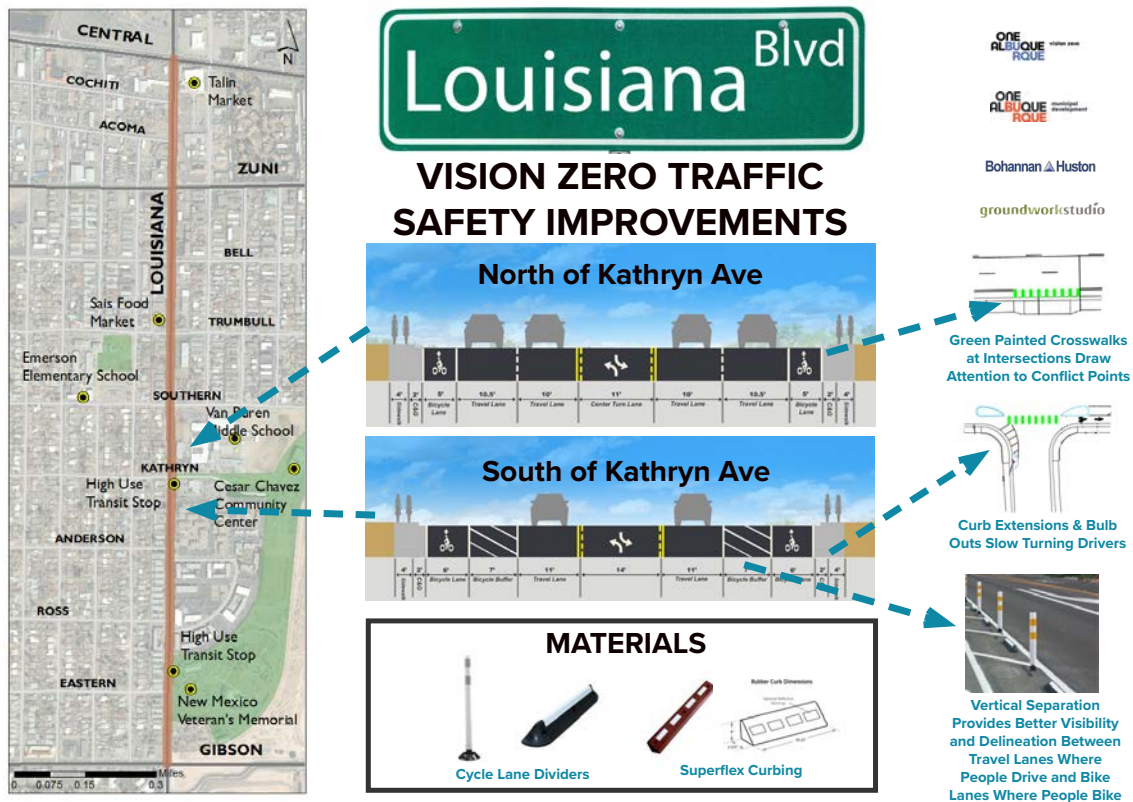
BUS SHELTER INTERACTIVE INSTALLATION RESULTS

Comment	Topic	Location	Prompt
In order to keep our streets safe, how about helping homeless, how about helping people to get rehabilitation from drugs or other mental illnesses	Homelessness/Drug	Us Veteran's Visitor Center and Louisiana	I want safer streets because
Because the kids are seeing everything that's going on a that is going to be a really bad outcome. In order for this stuff to stop people should treat the addicts like humans. Never know it could be the richest person out here, never know what life is going to bring		Veteran's Visitor Center and Louisiana	I want safer streets because
No more zombies!	Homelessness/Drug	Us Veteran's Visitor Center and Louisiana	I want safer streets because
Taking back safety from crime!	Safety	Veteran's Visitor Center and Louisiana	I want safer streets because
There shouldn't have to be security guards in every store and my kids should feel safe anywhere they are. But the way we live that's not our outcome	Safety	Veteran's Visitor Center and Louisiana	I want safer streets because
I walked to school and I walked to public bus stop		Continental and Louisiana	I want safer streets because
I walked the street to where I want to go. U r go for a walk		Continental and Louisiana	I want safer streets because
A lot more cops	Safety	Kathryn and Louisiana	I would like to see __ for Louisiana Blvd
People obeying traffic lights	Cars	Kathryn and Louisiana	I would like to see __ for Louisiana Blvd
Better traffic lights	Cars	Kathryn and Louisiana	I would like to see __ for Louisiana Blvd
Less needles and drugs	Homelessness/Drug	Us Kathryn and Louisiana	I would like to see __ for Louisiana Blvd
Slower speeds	Cars	Kathryn and Louisiana	I would like to see __ for Louisiana Blvd
A crosswalk at Louisiana and Continental	Pedestrian	Kathryn and Louisiana	I would like to see __ for Louisiana Blvd
Arboles		Gibson and Louisiana	I would like to see __ for Louisiana Blvd
Mufflers fixed. Noise ordinance enforced by cops	Cars	Gibson and Louisiana	I would like to see __ for Louisiana Blvd
Speeding tickets on Gibson also	Cars	Gibson and Louisiana	I would like to see __ for Louisiana Blvd
Caminar	Pedestrian	Veteran's Visitor Center and Louisiana	I want safer streets because
The only way to have safer streets to get rid of those in office who can't do their jobs, and bring in those who can. Tuffen up laws, bring back the death penalty, bring justice to the victims families, and have after school programs for childrens and summer programs		Veteran's Visitor Center and Louisiana	I want safer streets because
The only way to get street is to get rid of all the homeless people and people help for there drug problem	Homelessness/Drug	Us Veteran's Visitor Center and Louisiana	I want safer streets because
Never give up on yourself		Veteran's Visitor Center and Louisiana	I want safer streets because
Por los ninos		Continental and Louisiana	I want safer streets because
Por todas las personas		Meat Market and Louisiana	I want safer streets because
Luminas		Meat Market and Louisiana	I want safer streets because
Music on the buses	Buses	Veteran's Visitor Center and Louisiana	I want safer streets because
The buses stay clean	Buses	Veteran's Visitor Center and Louisiana	I want safer streets because
Safety at bus stops and on the buses	Buses	Veteran's Visitor Center and Louisiana	I want safer streets because
Noise ordinance - to loud	Cars	Gibson and Louisiana	I would like to see __ for Louisiana Blvd
Kids that listen!		Veteran's Visitor Center and Louisiana	I want safer streets because
Ask god to 1. Remove seeds of wild grape, 2. Remove evil structures, 3. Remove thrones of inequity		Continental and Louisiana	I want safer streets because
You have to believe in "god" first in order to do this!		Continental and Louisiana	I want safer streets because
We are a community, not some officials who don't care about nothing! To look out for one another. Keep heads up not facing down. La Guera		Continental and Louisiana	I want safer streets because
Can you please change the trash it stinks!		Continental and Louisiana	I want safer streets because
More "creative" lighting for pedestrians (ie neon or LED lighting)		Kathryn and Louisiana	I would like to see __ for Louisiana Blvd
Less speeding, more crosswalks for kids		Kathryn and Louisiana	I would like to see __ for Louisiana Blvd
More family activities, safe streets		Kathryn and Louisiana	I would like to see __ for Louisiana Blvd
Free community bike shop			
Safer pedestrian crosswalks			



APPENDIX F

UPDATED LOUISIANA BLVD STREET INTERVENTION DESIGN



APPENDIX G

SIDEWALK STENCIL DESIGNS

