CITY OF ALBUQUERQUE
DEPARTMENT OF MUNICIPAL DEVELOPMENT
PLANS FOR CONSTRUCTION

NEW MEXICO RAIL RUNNER EXPRESS
MONTANO STATION
PROJECT #559292

ZONE ATLAS MAP F-15

NOTES:

1. CONTRACTOR SHALL WORK CONTINUOUSLY, 24 HOURS PER DAY, ON ALL ARTERIAL ROADWAYS WHEN TRAFFIC LANES ARE CLOSED TO TRAFFIC UNLESS THE WORK VIOLATES THE CITY'S NOISE ORDINANCE.

2. IF THE CONTRACTOR IS NOT ALLOWED TO WORK AT NIGHT DUE TO THE CITY'S NOISE ORDINANCE, THE CONTRACTOR SHALL OPEN ALL TRAFFIC LANES TO TRAFFIC WITH THE PROPER USE OF TRENCH PLATES DURING NON-WORKING HOURS, AND MUST WORK MINIMUM HOURS FROM 9:00 A.M. TO 3:00 P.M. MONDAY THROUGH SATURDAY.

3. ARTERIAL STREETS ARE AS INDICATED IN THE "LONG RANGE ROADWAY SYSTEM" MAP PUBLISHED BY THE MID-REGION COUNCIL OF GOVERNMENTS (MRCOG).

4. CONSTRUCTION TYPE: VB NON-SPRINKLERED
OCCUPANCY TYPE: U-MISCELLANEOUS STRUCTURES
ALLOWABLE AREA: 1 STORY; 5500 S.F.
ACTUAL AREA: 1045 S.F. PER CANOPY

INDEX OF SHEETS

GENERAL NOTES

1. ALL WORK DETAILED ON THESE PLANS TO BE PERFORMED, EXCEPT AS OTHERWISE STATED OR PROVIDED HEREON, SHALL BE CONSTRUCTED IN ACCORDANCE WITH CITY OF ALBUQUERQUE STANDARD SPECIFICATIONS FOR PUBLIC WORKS CONSTRUCTION, 1986 EDITION THROUGH UPDATE #7, INCLUDING AMENDMENT NO. 1, AND WILL BE REFERRED TO HEREIN AS "STANDARD SPECIFICATIONS".
2. ALL CONSTRUCTION WITHIN CITY RIGHT-OF-WAY OR EASEMENTS MUST BE DONE FROM APPROVED WORK ORDER DOCUMENTS FROM THE CITY.
3. ALL WORK ON THIS PROJECT SHALL BE PERFORMED IN ACCORDANCE WITH APPLICABLE FEDERAL, STATE, AND LOCAL LAWS, ORDINANCES, RULES, AND REGULATIONS CONCERNING CONSTRUCTION SAFETY AND HEALTH.
4. CONTRACTOR AGREES THAT HE SHALL ASSUME THE SOLE AND COMPLETE RESPONSIBILITY FOR THE JOB SITE CONDITIONS DURING THE COURSE OF CONSTRUCTION OF THIS PROJECT, INCLUDING SAFETY OF ALL PERSONS AND PROPERTY. THIS REQUIREMENT SHALL APPLY CONTINUOUSLY AND NOT BE LIMITED TO NORMAL WORKING HOURS. CONTRACTOR SHALL DEFEND, INDEMNIFY, AND HOLD HARMLESS THE OWNER AND ENGINEER FROM ANY AND ALL LIABILITY REAL OR ALLEGED, IN CONNECTION WITH THE PERFORMANCE OF WORK ON THIS PROJECT, EXCEPT LIABILITY ARISING FROM THE SOLE NEGLIGENCE OF THE OWNER OR ENGINEER.
5. ALL EXCAVATION, TRENCHING, AND SHORING ACTIVITIES MUST BE ACCOMPLISHED IN ACCORDANCE WITH OSHA 29CFR 1926.650 SUBPART P.
6. AN EXCAVATION/CONSTRUCTION PERMIT WILL BE REQUIRED BEFORE BEGINNING ANY WORK WITHIN CITY RIGHT-OF-WAY.
7. CONTRACTOR SHALL NOTIFY THE ENGINEER NOT LESS THAN SEVEN (7) DAYS PRIOR TO STARTING WORK IN ORDER THAT THE CITY SURVEYOR MAY TAKE NECESSARY MEASURES TO INSURE THE PRESERVATION OF SURVEY MONUMENTS. CONTRACTOR SHALL NOT DISTURB PERMANENT SURVEY MONUMENTS WITHOUT THE CONSENT OF THE CITY SURVEYOR AND SHALL NOTIFY THE CITY SURVEYOR AND BEAR THE EXPENSE OF REPLACING ANY THAT MAY BE DISTURBED WITHOUT PERMISSION. ONLY THE CITY SURVEYOR SHALL REPLACE SURVEY MONUMENTS. WHEN A CHANGE IS MADE IN THE FINISHED ELEVATIONS OF THE PAVEMENT OF ANY ROADWAY IN WHICH A PERMANENT SURVEY MONUMENT IS LOCATED, CONTRACTOR SHALL, AT HIS OWN EXPENSE, ADJUST THE MONUMENT COVER TO THE NEW GRADE UNLESS OTHERWISE SPECIFIED. REFER TO STANDARD SPECIFICATIONS SECTION 4.4.
8. SEVEN (7) WORKING DAYS PRIOR TO BEGINNING CONSTRUCTION, CONTRACTOR SHALL SUBMIT TO DMD, CONSTRUCTION COORDINATION DIVISION A DETAILED CONSTRUCTION SCHEDULE. TWO (2) WORKING DAYS PRIOR TO CONSTRUCTION, CONTRACTOR SHALL OBTAIN A BARRICADING PERMIT FROM THE DMD, CONSTRUCTION COORDINATION DIVISION. CONTRACTOR SHALL NOTIFY BARRICADE ENGINEER (924-3400) PRIOR TO OCCUPYING AN INTERSECTION. REFER TO SECTION 19 OF THE STANDARD SPECIFICATIONS. PERMIT REQUESTS MAY BE DENIED OR DELAYED DUE TO CONFLICTS WITH OTHER PROJECTS IN THE AREA.
9. TWO (2) WORKING DAYS PRIOR TO ANY EXCAVATION, CONTRACTOR MUST CONTACT NEW MEXICO ONE CALL SYSTEM (260-1990) FOR LOCATION OF EXISTING UTILITIES.
10. CONTRACTOR SHALL ASSIST THE ENGINEER/INSPECTOR IN THE RECORDING OF DATA ON ALL UTILITY LINES AND ACCESSORIES AS REQUIRED BY THE CITY OF ALBUQUERQUE FOR THE PREPARATION OF "AS CONSTRUCTED" DRAWINGS. CONTRACTOR SHALL NOT COVER UTILITY LINES AND ACCESSORIES UNTIL ALL DATA HAS BEEN RECORDED.
11. AT HIS OWN EXPENSE, CONTRACTOR SHALL ASSUME RESPONSIBILITY FOR ANY DAMAGE TO EXISTING PAVEMENT, PAVEMENT MARKINGS, CURB AND GUTTER, WHEELCHAIR RAMPS, AND SIDEWALK DURING CONSTRUCTION APART FROM THOSE SECTIONS INDICATED FOR REMOVAL ON THE PLANS AND SHALL REPAIR OR REPLACE, PER STANDARD SPECIFICATIONS.
12. ALL STREET STRIPING, ALTERED OR DESTROYED, SHALL BE REPLACED WITH THERMOPLASTIC REFLECTORIZED PAVEMENT MARKINGS BY CONTRACTOR TO SAME LOCATION AS EXISTING, OR AS INDICATED BY THIS PLAN SET. CONTRACTOR SHALL COORDINATE WITH CITY TRAFFIC OPERATIONS.
13. CONTRACTOR SHALL MAINTAIN A GRAFFITI-FREE WORK SITE. CONTRACTOR SHALL PROMPTLY REMOVE ANY AND ALL GRAFFITI FROM EQUIPMENT, WHETHER PERMANENT OR TEMPORARY.
14. CONTRACTOR SHALL BE RESPONSIBLE TO PROVIDE AND MAINTAIN ALL CONSTRUCTION SIGNING UNTIL THE PROJECT HAS BEEN ACCEPTED BY THE CITY.
15. EXISTING UTILITY LINE LOCATIONS ARE SHOWN IN AN APPROXIMATE MANNER ONLY AND SUCH LINES MAY EXIST WHERE NONE ARE SHOWN. THE LOCATION OF ANY SUCH EXISTING LINES IS BASED UPON INFORMATION PROVIDED BY THE UTILITY COMPANY, THE OWNER, OR BY OTHERS, AND THE INFORMATION MAY BE INCOMPLETE OR MAY BE OBSOLETE BY THE TIME CONSTRUCTION COMMENCES.
16. THE ENGINEER HAS UNDERTAKEN NO FIELD VERIFICATION OF THE LOCATION, DEPTH, SIZE, OR TYPE OF EXISTING UNDERGROUND UTILITY LINES, MAKES NO REPRESENTATION PERTAINING THERETO, AND ASSUMES NO RESPONSIBILITY OR LIABILITY THEREFOR. CONTRACTOR SHALL INFORM ITSELF OF THE LOCATION OF ANY UTILITY LINE IN OR NEAR THE AREA OF THE WORK IN ADVANCE OF AND DURING EXCAVATION WORK. CONTRACTOR IS FULLY RESPONSIBLE FOR ANY AND ALL DAMAGE CAUSED BY ITS FAILURE TO LOCATE, IDENTIFY, AND PRESERVE ANY AND ALL EXISTING UTILITIES.
17. REMOVALS SHALL BE DISPOSED OF OFF-SITE AND SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
18. WHEN ABUTTING EXISTING PAVEMENT TO NEW, SAWCUT EXISTING PAVEMENT TO A STRAIGHT EDGE AND AT A RIGHT ANGLE, OR AS APPROVED BY THE FIELD ENGINEER. REMOVAL OF BROKEN OR CRACKED PAVEMENT WILL ALSO BE REQUIRED.
19. REMOVAL OF EXISTING CURB AND GUTTER OR SIDEWALK SHALL BE TO THE NEAREST JOINT OR SAW CUT.

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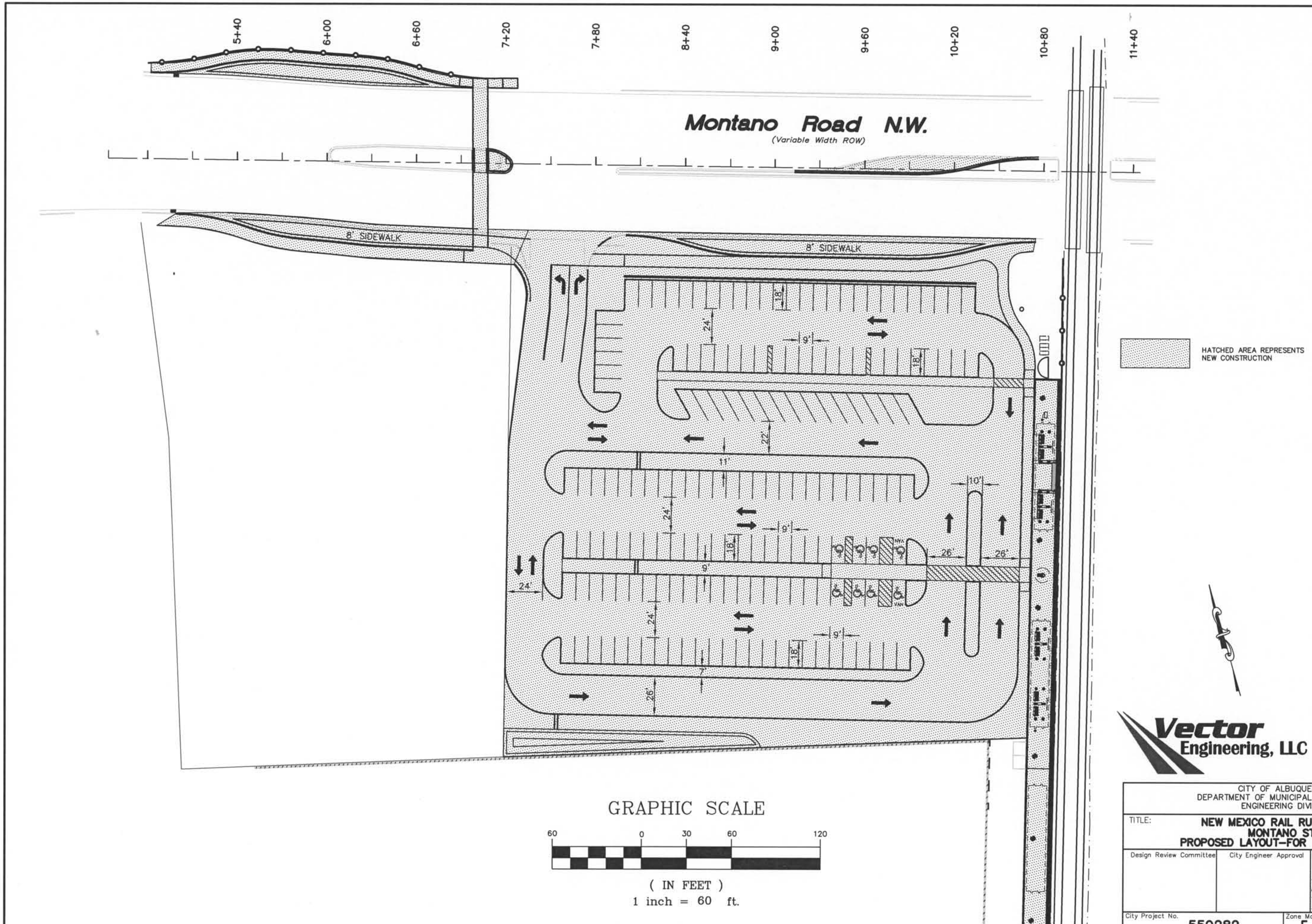
Vector Engineering, LLC

REV.	SHEETS	CITY ENGINEER	DATE	USER DEPARTMENT	DATE	USER DEPARTMENT	DATE

ENGINEER'S STAMP & SIGNATURE	APPROVED	ENGINEER	DATE	APPROVED FOR CONSTRUCTION
	DRC Chairman			
	Transportation			
	ABCWUA			
	Hydrology			
	C I P			
	AMAFCA			
	Constr. Coord.			CITY ENGINEER DATE

PROJECT NUMBER	ZONE ATLAS NO.	DRAWING NO. 1 OF 46
559292	F-15	



GRAPHIC SCALE

60 0 30 60 120

(IN FEET)
1 inch = 60 ft.

AS BUILT INFORMATION

CONTRACTOR
WORK STAKED BY — DATE
INSPECTOR'S ACCEPTANCE BY — DATE
FIELD VERIFICATION BY — DATE
DRAWINGS CORRECTED BY — DATE

MICRO-FILM INFORMATION

RECORDED BY — DATE
NO.

BENCH MARKS

NGS STAINLESS ROD SET BENEATH A 5 1/2" ACCESS COVER STAMPED "D-438, 1984", SE QUADRANT OF MONTANO RD. & THE BNSF RAILROAD TRACKS, 42.5 FT. EAST OF CENTERLINE OF THE TRACKS, 44 FT. SOUTH OF CENTERLINE OF MONTANO RD. NE, 1.1 FT. WEST OF CHAIN LINK FENCE.
DATUM NAVD 1988
ELEV. 4978.070

SURVEY INFORMATION

FIELD NOTES

NO.	BY	DATE

ENGINEER'S SEAL

REVISIONS

NO.	DATE	REMARKS	BY

DESIGN

DESIGNED BY SL	DATE	10/2010
DRAWN BY BN	DATE	10/2010
CHECKED BY KA	DATE	10/2010

Vector Engineering, LLC

CITY OF ALBUQUERQUE
DEPARTMENT OF MUNICIPAL DEVELOPMENT
ENGINEERING DIVISION

TITLE: **NEW MEXICO RAIL RUNNER EXPRESS**
MONTANO STATION
PROPOSED LAYOUT-FOR INFORMATION ONLY

Design Review Committee	City Engineer Approval	Mo. / Day / Yr.	Mo. / Day / Yr.
		Last Design Update	

City Project No.	Zone Map No.	Sheet	Of
559282	F-15	2	46

GENERAL:

1. PROJECT DOCUMENTS CONSIST OF THESE PLANS, PROJECT SPECIFICATIONS, PROJECT BIDDING INFORMATION, PROJECT CONTRACTS, AND ANY AND ALL SUBSEQUENT EXECUTED PROJECT DOCUMENTATION ISSUED AS, OR WITH, CHANGE ORDERS, AND RFI'S (REQUESTS FOR INFORMATION.) THE CONTRACTOR SHALL REVIEW ALL PROJECT DOCUMENTS AND VERIFY ALL DIMENSIONS, QUANTITIES, AND FIELD CONDITIONS. ANY CONFLICTS OR OMISSIONS WITH THE DOCUMENTS SHALL BE REPORTED TO THE ENGINEER/PROJECT MANAGER FOR CLARIFICATION PRIOR TO PERFORMANCE OF ANY WORK IN QUESTION. IN THE EVENT THE CONTRACTOR DOES NOT NOTIFY THE ENGINEER/PROJECT MANAGER, THE CONTRACTOR ASSUMES FULL RESPONSIBILITY AND ANY AND ALL EXPENSE FOR ANY REVISIONS NECESSARY OR CORRECTIONAL WORK REQUIRED.
2. ALL WORK SHALL BE PERFORMED IN ACCORDANCE WITH APPLICABLE FEDERAL, STATE, AND LOCAL LAWS, ORDINANCES AND REGULATIONS CONCERNING CONSTRUCTION SAFETY AND HEALTH.
3. THE CONTRACTOR SHALL NOT INSTALL ITEMS AS SHOWN ON THESE PLANS WHEN IT IS OBVIOUS THAT FIELD CONDITIONS ARE DIFFERENT THAN SHOWN IN THE PLANS. SUCH CONDITIONS SHALL BE BROUGHT TO THE ATTENTION OF THE ENGINEER IN A TIMELY MANNER. IN THE EVENT THE CONTRACTOR DOES NOT NOTIFY THE ENGINEER IN A TIMELY MANNER, THE CONTRACTOR ASSUMES FULL RESPONSIBILITY AND EXPENSE FOR ANY REVISIONS NECESSARY, INCLUDING ENGINEERING DESIGN FEES.
4. THE CONTRACTOR AGREES TO TAKE NECESSARY SAFETY PRECAUTIONS AS REQUIRED BY FEDERAL, STATE AND LOCAL AUTHORITIES TO PROTECT PEDESTRIAN AND VEHICULAR TRAFFIC IN THE CONSTRUCTION AREA, WHICH INCLUDE BUT ARE NOT LIMITED TO: MAINTAINING ADEQUATE WARNING SIGNS, BARRICADES, LIGHTS, GUARD FENCES, WALKS AND BRIDGES.
5. EXISTING SITE IMPROVEMENTS WHICH ARE DAMAGED OR DISPLACED BY THE CONTRACTOR SHALL BE REMOVED AND REPLACED BY THE CONTRACTOR AT THE CONTRACTOR'S EXPENSE. REPAIRS SHALL BE APPROVED BY THE OWNER PRIOR TO CONSTRUCTION OF THE REPAIRS. REPAIRS SHALL BE ACCEPTED BY THE OWNER PRIOR TO FINAL PAYMENT.
6. THE CONTRACTOR SHALL COORDINATE THE CONSTRUCTION ACTIVITIES WITH ALL OTHER CONTRACTORS AND UTILITY COMPANIES WORKING IN THE SAME AREA. THE CONTRACTOR MAY BE REQUIRED TO RESCHEDULE THEIR ACTIVITIES TO ALLOW UTILITY CREWS TO PERFORM THEIR REQUIRED WORK. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR DELAYS OR INCONVENIENCE CAUSED BY UTILITY COMPANY WORK CREWS. THE CONTRACTOR MAY BE ALLOWED AN EXTENSION OF THE CONTRACT TIME, DUE TO DELAYS, AS APPROVED BY OWNER.
7. ALL WORK WITHIN THE MONTANO BLVD RIGHT-OF-WAY REFERENCES THE CITY OF ALBUQUERQUE STANDARD SPECIFICATIONS FOR PUBLIC WORKS CONSTRUCTION, UPDATE NO. 7.
8. ONLY THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE SAFETY OF ALL WORK. ALL WORK, INCLUDING WORK WITHIN TRENCHES, SHALL BE IN ACCORDANCE WITH THE OCCUPATIONAL SAFETY AND HEALTH ACT (OSHA).
9. ALL MATERIALS AND WORKMANSHIP SHALL CONFORM TO PROJECT SPECIFICATIONS AND PLANS, AS AMENDED AND REVISED BY THE ENGINEER/PROJECT MANAGER. ALL INSTALLATION DETAILS ARE TYPICAL AND MAY BE CHANGED TO BETTER FIT EXISTING LOCAL CONDITIONS UPON APPROVAL BY THE ENGINEER/PROJECT MANAGER.
10. ALL EARTHWORK SHALL CONFORM TO RECOMMENDATIONS PROVIDED IN THE GEOTECHNICAL REPORT AS PROVIDED IN THE PROJECT DOCUMENTS FOR THIS PROJECT.
11. AT THE END OF EACH WORK DAY, THE CONTRACTOR SHALL CLEAN AND PICK UP THE WORK AREA TO THE SATISFACTION OF THE ENGINEER/PROJECT MANAGER. AT NO TIME SHALL THE WORK BE LEFT IN A MANNER THAT COULD ENDANGER THE WORKERS OR THE PUBLIC.
12. ALL EXISTING TRACK STATIONING IS BASED ON BNSF MAPS AND IS APPROXIMATE.
13. SIDEWALK, CURB RAMP, AND OTHER PEDESTRIAN DETAIL DRAWINGS PROVIDE GUIDANCE FOR COMPLIANCE WITH THE CURRENT AMERICANS WITH DISABILITIES ACT (ADA) AND STATE CODE. THESE DRAWINGS SHALL APPLY TO ALL NEW AND ALTERED SIDEWALKS.
14. ALL SUBGRADE COMPACTION WILL EXTEND 12" MINIMUM BEYOND LIMIT OF SIDEWALK OR CURB & GUTTER.
15. ALL PAVEMENT GRADES AND ELEVATIONS SHALL BE MET BY THE SURFACE COURSE.
16. CLEARING AND GRUBBING SHOULD BE PERFORMED IN ACCORDANCE WITH THE REQUIREMENTS OF SECTION 201 OF THE STANDARD SPECIFICATIONS.
17. STRIPPING OF EXISTING FILL PILES AND ZONES, LOOSE BACKFILL AND UNSTABLE SOILS, AND DEBRIS SHOULD BE PERFORMED IN ACCORDANCE WITH SECTION 202 OF THE STANDARD SPECIFICATIONS, AND THE STRIPPED SURFACE SHOULD BE OBSERVED FOR EVIDENCE OF DELETERIOUS MATERIAL, INCLUDING VEGETATION, DISTURBED SOILS, COBBLES AND BOULDERS OR BURIED DEBRIS. IF OBSERVED, THESE DELETERIOUS MATERIALS SHOULD BE REMOVED.
18. PREPARATION OF THE GROUND SURFACE IN AREAS TO RECEIVE FILL, AND IN EXCAVATED AREAS INCLUDING FOR STRUCTURES AND SLABS, SHOULD BE COMPLETED IN ACCORDANCE WITH SECTION 204 OF THE STANDARD SPECIFICATIONS, WITH THE EXCEPTION THAT SCARIFICATION, MOISTURE CONDITIONING AND COMPACTION SHALL BE PERFORMED TO A MINIMUM DEPTH OF 8 INCHES BELOW THE SURFACE, TO A MINIMUM OF 90 PERCENT OF THE MAXIMUM DRY DENSITY AS DETERMINED BY ASTM D1557 (STANDARD PROCTOR), WITHIN THE LIMITS OF 3 PERCENT BELOW TO 3 PERCENT ABOVE THE OPTIMUM MOISTURE CONTENT. EXCAVATED SLOPES AND AREAS STEEPER THAN 5H:1V (HORIZONTAL:VERTICAL) WHICH ARE TO RECEIVE FILL SHOULD BE BENCHED.
19. PAVEMENT MATERIALS QUALITY AND CONSTRUCTION REQUIREMENTS SHOULD CONFORM TO THE APPLICABLE SECTIONS OF THE STANDARD SPECIFICATIONS. TRANSVERSE AND LONGITUDINAL CONTRACTION JOINTS (SAWED JOINTS) ARE RECOMMENDED FOR ALL PCCP AREAS EXCEPT WHERE CONSTRUCTION JOINTS ARE NECESSARY DUE TO CONSTRUCTION DELAYS OR END OF SHIFT SHUTDOWNS.
20. ALL REFERENCES TO "NEW MEXICO STATE HIGHWAY & TRANSPORTATION DEPARTMENT" AND "NMSHTD" SHALL BE INTERCHANGEABLE WITH "NEW MEXICO DEPARTMENT OF TRANSPORTATION" AND "NMDOT".
21. ALL REFERENCES TO SERIAL DRAWINGS ARE TO NEW MEXICO DEPARTMENT OF TRANSPORTATION LATEST IMPERIAL EDITION OR THE CITY OF ALBUQUERQUE STANDARD DETAIL DRAWINGS FOR PUBLIC WORKS CONSTRUCTION, LATEST EDITION, UNLESS SPECIFIED OTHERWISE.

    THE CONTRACTOR SHALL OBTAIN AND PAY FOR THE LATEST EDITION OF THE SERIAL DRAWINGS FROM THE NEW MEXICO DEPARTMENT OF TRANSPORTATION GENERAL OFFICE AND THE STANDARD DETAIL DRAWINGS FROM CITY OF ALBUQUERQUE. THIS SHALL BE CONSIDERED INCIDENTAL AND NO ADDITIONAL PAYMENT SHALL BE MADE.
22. THE CONTRACTOR SHALL WARP SLOPES WHERE NECESSARY TO STAY WITHIN THE RIGHT OF WAY OR CONSTRUCTION EASEMENT LIMITS SUBJECT TO THE APPROVAL OF THE ENGINEER.
23. THE CONTRACTOR SHALL FIELD VERIFY ALL HORIZONTAL AND VERTICAL GEOMETRY AND RIGHT-OF WAY PRIOR TO THE BEGINNING OF CONSTRUCTION. THE HORIZONTAL AND VERTICAL GEOMETRY WERE BASED ON AS-BUILT AND FIELD SURVEY DATA. THE CONTRACTOR SHALL LIMIT ALL WORK ON THIS PROJECT TO WITHIN THE EXISTING RIGHTS-OF-WAY, ACQUIRED PARCELS, TEMPORARY CONSTRUCTION EASEMENTS, OR PUBLIC EASEMENTS. ANY ADJUSTMENT OR CHANGE TO THE GEOMETRY SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR. PAYMENT FOR THIS WILL BE CONSIDERED AS INCLUDED IN THE CONTRACT PRICE FOR ITEM 4.010 "CONSTRUCTION STAKING" AND NO SEPARATE MEASUREMENT OR PAYMENT WILL BE MADE.
24. CONCRETE DUMPING IN THE WORK ZONE IN THE PROCESS OF WASHING CONCRETE TRUCKS IS PROHIBITED.
25. THE CONTRACTOR WILL MAINTAIN A SET OF AS-BUILT PLANS FOR THIS PROJECT. THE CONTRACTOR'S SURVEYOR SHALL PROVIDE INFORMATION ON ANY REVISION TO THE HORIZONTAL AND VERTICAL GEOMETRY TO THE ENGINEER.

UTILITIES:

1. ANY UTILITY LINES, PIPELINES, OR UNDERGROUND UTILITY LINES SHOWN ON THE PLANS ARE SHOWN IN AN APPROXIMATE LOCATION ONLY BASED ON THE INFORMATION PROVIDED TO THE ENGINEER BY OTHERS THAT MAY BE INACCURATE OR INCOMPLETE. ADDITIONALLY, UNDERGROUND LINES MAY EXIST THAT ARE NOT SHOWN. THE CONTRACTOR SHALL VERIFY THE LOCATION OF ANY UTILITY LINE, PIPELINE, OR UNDERGROUND UTILITY LINE IN OR NEAR THE AREA OF THE WORK. ANY DAMAGE TO ANY OTHER UTILITIES OR COLLATERAL DAMAGE CAUSED BY THE CONTRACTOR SHALL BE THE FULL RESPONSIBILITY OF THE CONTRACTOR.
2. TWO (2) WORKING DAYS PRIOR TO ANY EXCAVATION, CONTRACTOR MUST CONTACT NEW MEXICO ONE CALL SYSTEM (260-1990) FOR LOCATION OF EXISTING UTILITIES. AFTER THE UTILITIES ARE SPOTTED, THE CONTRACTOR SHALL EXPOSE ALL PERTINENT UTILITIES TO VERIFY THEIR VERTICAL AND HORIZONTAL LOCATION. IF A CONFLICT EXISTS BETWEEN EXISTING UTILITIES AND PROPOSED CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE ENGINEER SO THAT THE CONFLICT CAN BE RESOLVED WITH MINIMAL DELAY.
3. THE CONTRACTOR SHALL EXERCISE DUE CARE TO AVOID DISTURBING ANY EXISTING UTILITIES, ABOVE OR BELOW GROUND. UTILITIES THAT ARE DAMAGED BY CARELESS CONSTRUCTION SHALL BE REPAIRED OR REPLACED AT THE CONTRACTOR'S EXPENSE. CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATION WITH UTILITY COMPANIES TO PREVENT DISRUPTION TO SERVICE.
4. ALL UTILITY LINES WHICH ARE NOT SPECIFICALLY DESIGNATED TO BE REMOVED AND REPLACED ON THE PLANS, SHALL BE MAINTAINED IN SERVICE. SHORING, SHEETING AND OTHER MEANS OF SUPPORT SHALL BE EMPLOYED BY THE CONTRACTOR TO PREVENT DAMAGE OR LOSS OF THESE EXISTING UTILITIES. BEAM AND CABLE OR OTHER ADEQUATE SUPPORTS SHALL BE USED FOR TEMPORARY SUPPORT OF ALL UTILITY LINES AS NECESSARY. ANY DAMAGE TO EXISTING UTILITIES SHALL PROMPTLY BE REPAIRED AT THE CONTRACTOR'S EXPENSE. THE CONTRACTOR SHALL NOTIFY THE ENGINEER IMMEDIATELY OF ANY SIGNIFICANT DEVIATION OF EXPOSED UTILITIES FROM THE LOCATIONS SHOWN ON THE PLANS SO THAT CONFLICTS CAN BE RESOLVED IN A TIMELY MANNER.
5. ALL INTERFERING PORTIONS OF ABANDONED UTILITY LINES WHICH ARE EXPOSED AS A RESULT OF CONSTRUCTION SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR.
6. THE CONTRACTOR SHALL COORDINATE ANY REQUIRED UTILITY INTERRUPTIONS WITH THE OWNER AND AFFECTED UTILITY COMPANY A MINIMUM OF THREE WORKING DAYS BEFORE THE INTERRUPTION.
7. EXISTING VALVES SHALL ONLY BE OPERATED BY THE UTILITY COMPANY. CONTRACTOR SHALL NOTIFY THE UTILITY A MINIMUM OF THREE (3) WORKING DAYS BEFORE ANY VALVE, NEW OR EXISTING, NEEDS TO BE OPERATED.
8. THE CONTRACTOR SHALL MAINTAIN A RECORD DRAWING SET OF PLANS AND PROMPTLY LOCATE ALL UTILITIES, EXISTING OR NEW, IN THEIR CORRECT LOCATION, HORIZONTAL AND VERTICAL. THIS RECORD SET OF DRAWINGS SHALL BE MAINTAINED ON THE PROJECT SITE AND SHALL BE AVAILABLE TO THE OWNER AND ENGINEER AT ANY TIME DURING CONSTRUCTION.
9. ALL GAS VALVES, GAS MANHOLES, ELECTRICAL MANHOLES, TELEPHONE MANHOLES, AND UTILITY POLES SHALL BE ADJUSTED TO GRADE BY EACH UTILITY COMPANY.
10. CONTRACTOR SHALL MAKE ALL WATER VALVES AND MANHOLES ACCESSIBLE TO THE OWNER AT ALL TIMES.
11. ALL WATER VALVES AND FIRE HYDRANTS REMOVED SHALL BE SALVAGED AND RETURNED TO THE OWNER UPON REQUEST.

SURVEY:

1. THE CONTRACTOR SHALL NOTIFY THE OWNER AT LEAST SEVEN (7) DAYS BEFORE BEGINNING ANY CONSTRUCTION ACTIVITY THAT COULD DAMAGE OR DISPLACE SURVEY MONUMENTS, PROPERTY CORNERS, OR PROJECT BENCHMARKS SO THESE ITEMS MAY BE PRESERVED OR RELOCATED.
2. THE CONTRACTOR SHALL NOTIFY THE ENGINEER IF A MONUMENT IS DISTURBED. REPLACEMENT SHALL BE DONE ONLY BY THE PUBLIC AGENCY SURVEY SECTION. WHEN A CHANGE IS MADE IN THE FINISHED ELEVATION OF THE PAVEMENT OF ANY ROADWAY IN WHICH A PERMANENT SURVEY MONUMENT IS LOCATED, CONTRACTOR SHALL, AT HIS OWN EXPENSE, ADJUST THE MONUMENT COVER TO THE NEW GRADE UNLESS OTHERWISE SPECIFIED.
3. ANY SURVEY MONUMENTS, PROPERTY CORNERS, OR BENCHMARKS THAT ARE NOT IDENTIFIED FOR RELOCATION ARE THE RESPONSIBILITY OF THE CONTRACTOR TO PRESERVE AND PROTECT. RELOCATION OR REPLACEMENT OF THESE ITEMS SHALL BE DONE BY THE OWNERS SURVEYOR AT THE EXPENSE OF THE CONTRACTOR.
4. THE CONTRACTOR SHALL SURVEY AND LOG EXISTING ELEVATIONS OF CURB AND GUTTER, SIDEWALK, AND PAVEMENT WHICH SHALL BE REMOVED FOR CONSTRUCTION OF IMPROVEMENTS. CONTRACTOR SHALL REPLACE REMOVED CURB AND GUTTER, SIDEWALK, DRIVEPADS, AND PAVEMENT TO ELEVATIONS PRIOR TO REMOVAL UNLESS OTHERWISE INDICATED ON THE PLANS.

Vector Engineering, LLC

AS BUILT INFORMATION
CONTRACTOR
WORK STAKED BY — DATE
INSPECTOR'S ACCEPTANCE BY — DATE
FIELD VERIFICATION BY — DATE
DRAWINGS CORRECTED BY — DATE

MICRO-FILM INFORMATION
RECORDED BY — DATE
NO.

BENCH MARKS
NGS STAINLESS ROD SET BENEATH A 5 1/2" ACCESS COVER STAMPED "D-438, 1984", SE QUADRANT OF MONTANO RD. & THE BNSF RAILROAD TRACKS, 42.5 FT. EAST OF CENTERLINE OF THE TRACKS, 44 FT. SOUTH OF CENTERLINE OF MONTANO RD. NE, 1.1 FT. WEST OF CHAIN LINK FENCE.
DATUM NAVD 1988
ELEV. 4978.070

SURVEY INFORMATION

FIELD NOTES		
NO.	BY	DATE

ENGINEER'S SEAL

REVISIONS

NO.	DATE	REMARKS	BY

DESIGN		
DESIGNED BY SL	DATE	10/2010
DRAWN BY BN	DATE	10/2010
CHECKED BY KA	DATE	10/2010

CITY OF ALBUQUERQUE
DEPARTMENT OF MUNICIPAL DEVELOPMENT
ENGINEERING DIVISION

TITLE: **NEW MEXICO RAIL RUNNER EXPRESS MONTANO STATION GENERAL NOTES**

Design Review Committee	City Engineer Approval	Last Design Update	Mo. / Day / Yr.	Mo. / Day / Yr.

City Project No.	Zone Map No.	Sheet	Of
559282	F-15	3	46

EXCAVATION:

1. A TEMPORARY SLOPE OF 1.5H:1V IS RECOMMENDED FOR EXCAVATION IN THE NEAR–SURFACE NATIVE SITE SOILS AND SHALLOW EXISTING FILL SOILS. IF COARSE–GRAINED SOIL STRATA, SUCH AS SILTY SANDS, SAND AND GRAVEL, AND CLAYEY TO SILTY SANDS WITH GRAVEL ARE ENCOUNTERED, THE RECOMMENDED SLOPE MAY BE SUBJECT TO LOCALIZED SLOUGHING OR RAVELING, AND MAY REQUIRE EITHER LAYING–BACK TO A FLATTER SLOPE OR OTHER STABILIZATION MEASURES.
2. EXPOSED CUT SLOPE SURFACES SHOULD BE CAREFULLY INSPECTED AND EVALUATED FOR PRESENCE OF ZONES OF SOFT, LOOSE OR RUNNING SOILS WHICH COULD REQUIRE LAYING–BACK, SHORING OR OTHER STABILIZING TREATMENT. PERIMETERS OF ALL EXCAVATIONS SHOULD BE PROTECTED AGAINST IMPACT FROM SURFACE WATER RUNOFF THROUGH THE USE OF BERMS OR OTHER DEVICES. CONSTRUCTION EQUIPMENT SHOULD NOT BE PERMITTED TO OPERATE CLOSER THAN 10 FEET TO THE EDGE OF ANY EXCAVATION.
3. TEMPORARY EXCAVATION SLOPES SHOULD BE MADE IN ACCORDANCE WITH THE REQUIREMENTS OF THE OSHA HEALTH AND SAFETY STANDARDS FOR EXCAVATIONS (29 CFR 1926, SUBPART P AND AREMA). BASED ON APPENDIX A OF SUBPART P, THE NEAR–SURFACE NATIVE SITE SOILS AT THE STATION SITES FALL PREDOMINANTLY WITHIN TYPE "C" SOILS. FOR EXCAVATION LESS THAN 20 FEET IN DEPTH, APPENDIX B OF SUBPART P INDICATES A MAXIMUM (STEEPEST) UNSHORED SLOPE OF 1.5H:1V.
4. THE RECOMMENDED TEMPORARY EXCAVATION SLOPES PRESENTED HEREIN ARE BASED ON SOIL MOISTURE CONTENTS BEING MAINTAINED AT OR NEAR THE IN–SITE MOISTURE CONTENTS. SIGNIFICANT INCREASES IN THE MOISTURE CONTENT OF THE SOILS IN CUT SLOPES COULD RESULT IN A REDUCTION IN SOIL SHEAR STRENGTH AND DECREASE IN SLOPE STABILITY.
5. BNSF REQUIREMENTS MAY REQUIRE USE OF FLATTER TEMPORARY EXCAVATION SLOPES IN THE IMMEDIATE VICINITY OF THE BNSF TRACKS. BNSF GUIDELINES FOR RAILROAD GRADE SEPARATION PROJECTS AND THE AREMA MANUAL OF RAILWAY ENGINEERING (MOST CURRENT EDITION) SHOULD BE CONSULTED FOR SPECIFIC REQUIREMENTS.
6. FOR TEMPORARY EXCAVATION IN THE IMMEDIATE VICINITY OF EXISTING CANALS, DRAINAGE CHANNELS OR RETENTION/DETENTION BASINS, WHERE LOCALIZED MOISTURE INCREASES IN SLOPE FACE SOILS COULD OCCUR IN RESPONSE TO LEAKAGE, IT IS RECOMMENDED THAT A TEMPORARY SLOPE OF 2H:1V BE UTILIZED. THIS TEMPORARY SLOPE SHOULD BE USED FOR EXCAVATIONS WITHIN A MINIMAL HORIZONTAL DISTANCE FROM THE NEAREST EDGE OF THE CANAL, CHANNEL OR BASIN OF TWO TIMES THE EXCAVATION DEPTH, MEASURED AT THE EXCAVATION CREST.
7. IF THE CONTRACTOR DESIRES TO USE TEMPORARY EXCAVATION SLOPES STEEPER THAT THOSE RECOMMENDED HEREIN, THE STABILITY OF THESE SLOPES SHOULD BE VERIFIED FOR THE CONTRACTOR THROUGH STABILITY ANALYSIS PERFORMED BY A GEOTECHNICAL ENGINEER REGISTERED IN THE STATE OF NEW MEXICO AND WHO HAS DEMONSTRATED KNOWLEDGE AND EXPERIENCE IN GEOTECHNICAL ANALYSIS.

FOUNDATIONS:

1. REFER TO FINAL GEOTECHNICAL ENGINEERING REPORT, NM RAIL RUNNER – MONTANO STATION, ALBUQUERQUE, NEW MEXICO, BY TERRACON, DATED MAY 5, 2010.
2. UNRESTRAINED FOUNDATION ELEMENTS:
   ACTIVE EARTH PRESSURE: 50 PSF/FT (LEVEL FILL)
   UNIT WEIGHT OF SOIL: 120 PCF
3. RETAINING WALL SPREAD FOOTINGS ARE DESIGNED FOR THE FOLLOWING ALLOWABLE SOIL PRESSURES:
   – SPREAD FOOTINGS BEARING ON MINIMUM THICKNESS OF 3 FEET ENGINEERED FILL: MEASURED FROM BOTTOM OF FOOTING = 2,500 PSF
4. RESISTANCE TO LATERAL LOADS:
   COEFFICIENT OF FRICTION = 0.35
   ULT. PASSIVE RESISTANCE = 300 PSF/FT (ALONE)
5. THE COEFFICIENT OF BASE FRICTION SHOULD BE REDUCED TO 0.30 WHEN USED IN CONJUNCTION WITH PASSIVE PRESSURE.

MATERIAL QUALITY:

1. STRUCTURAL FILL AND SUBBASE BENEATH FOOTINGS, SLABS AND PAVEMENTS, SHALL MEET THE REQUIREMENTS OF SECTION 210.2 OF THE NMDOT STANDARD SPECIFICATIONS, WITH THE FOLLOWING ADDITIONAL REQUIREMENTS:
   i. PARTICLE SIZE, MAXIMUM – 3 INCHES
   ii. 60 PERCENT FINES (MAX) (BY WEIGHT PASSING THE NO. 200 SIEVES)
   iii. LIQUID LIMIT = 30 (MAX), PLASTICITY INDEX = 15 (MAX)
2. STRUCTURAL BACKFILL AGAINST RETAINING WALLS AND STRUCTURES SHALL CONSIST OF FREE–DRAINING GRANULAR SOILS MEETING THE REQUIREMENTS OF SECTION 210.2 OF THE NMDOT STANDARD SPECIFICATIONS.
3. IMPORTED FILL SOILS SHOULD MEET THE REQUIREMENTS FOR STRUCTURAL FILL OR SUBBASE AS SPECIFIED ABOVE.
4. AGGREGATE BASE MATERIAL BENEATH CONCRETE SLABS AND FOR PAVEMENTS SHOULD MEET THE REQUIREMENTS FOR BASE COURSE, AS SPECIFIED IN SECTION 304 OF THE NMDOT STANDARD SPECIFICATIONS.
5. PLACEMENT AND COMPACTION OF FILL MATERIALS SHALL BE IN ACCORDANCE WITH SECTION 206, 210 OR 304 OF THE NMDOT STANDARD SPECIFICATIONS (AS APPLICABLE), WITH THE EXCEPTION THAT EACH LAYER OF FILL SHOULD BE COMPACTED AS SPECIFIED IN TABLE 1 BELOW (MAXIMUM DRY DENSITY & OPTIMUM MOISTURE CONTENT DETERMINED BY ASTM D1557 AS SPECIFIED):

TABLE 1: STRUCTURAL FILL, BACKFILL, BASE COURSE & SUBBASE COMPACTION REQUIREMENTS:

	MIN % COMPACTION	TEST METHOD	MOISTURE CONTENT
STRUCTURAL FILL AND STRUCTURE BACKFILL BENEATH FOOTINGS AND AGAINST BELOW–GRADE WALLS AND STRUCTURES	95	ASTM D1557	OPTIMUM +/– 3%
FILL BENEATH CONCRETE SLABS (ABOVE FOOTINGS)	95	ASTM D1557	OPTIMUM +/– 3%
BACKFILL OUTSIDE OF STRUCTURE AND SLAB AREA (NON STRUCTURAL AREAS)	90	ASTM D1557	OPTIMUM +/– 3%
GRAVEL BASE COURSE BENEATH SLABS AND PAVEMENTS	95	ASTM D1557	OPTIMUM +/– 3%
SUBBASE BENEATH PAVEMENTS	95	ASTM D1557	OPTIMUM +/– 3%

6. EXCAVATION AND BACKFILL FOR STRUCTURES SHOULD BE IN ACCORDANCE WITH SECTION 203 OR 210 OF THE NMDOT STANDARD SPECIFICATIONS, WITH THE EXCEPTION OF THE MINIMUM PERCENT COMPACTION AND MOISTURE CONTENT REQUIREMENTS SPECIFIED IN TABLE 1 ABOVE.
7. INTERIOR AND EXTERIOR CONCRETE SLABS SHOULD BE FOUNDED ON A MINIMUM 4 INCH THICKNESS OF GRAVEL BASE COURSE MEETING THE REQUIREMENTS OF SECTION 304 OF THE NMDOT STANDARD SPECIFICATIONS. IF DESIRED, EXTERIOR SLABS COULD BE FOUNDED ON PREPARED SUBGRADE CONSISTING OF EXISTING SITE SOILS OR IMPORTED FILL THAT MEET THE REQUIREMENTS OF THE GEOTECHNICAL ENGINEERING REPORT FOR THE SITE.
8. REQUIREMENTS FOR BACKFILL AGAINST RETAINING WALLS AND BELOW–GRADE STRUCTURES ARE PRESENTED ABOVE, AND SPECIFY FREE–DRAINING GRANULAR SOILS. ON–SITE OR IMPORTED CLAYEY SOILS SHOULD NOT BE USED AS WALL BACKFILL. ALL BACKFILL SHOULD BE COMPACTED IN ACCORDANCE WITH THE REQUIREMENTS OF TABLE 1 AND NOTE 5. WATER JETTING OR FLOODING OF BACKFILL SHALL NOT BE PERMITTED. COMPACTION WITHIN 3 FEET OF THE BACK FACE OF RETAINING WALLS SHOULD BE RESTRICTED TO HAND–OPERATED EQUIPMENT IN ORDER TO MINIMIZE COMPACTION–INDUCED LATERAL FORCES ON THE WALLS.

SHORING:

1. AS AN ALTERNATIVE TO STEEPER TEMPORARY EXCAVATION SLOPES DUE TO SPACE CONSTRAINTS, COST OR OTHER CONSIDERATIONS, TEMPOARY SHORING SYSTEMS SUCH AS SOIL NAIL WALLS, SOLDIER PILE WALLS WITH OR WITHOUT TIEBACKS, OR OTHER SYSTEM COULD BE UTILIZED. TEMPORARY SHORING SYSTEM DESIGN SHOULD BE PERFORMED BY AN EXPERIENCED ENGINEER REGISTERED IN THE STATE OF NEW MEXICO, FOR THE SPECIFIC EXCAVATION LOCATION AND GEOMETRY. BNSF DESIGN CRITERIA AND THE AREMA MANUAL OF RAILWAY ENGINEERING (MOST CURRENT EDITION) SHOULD BE CONSULTED FOR SPECIFIC SHORING REQUIREMENTS AND DESIGN LOADS/EARTH PRESSURES IN THE IMMEDIATE VICINITY OF THE TRACKS.
2. SOIL NAIL WALLS, IF PROPOSED, SHOULD BE DESIGNED FOLLOWING THE PROCEDURES PRESENTED IN THE FHWA MANUAL FOR DESIGN AND CONSTRUCTION MONITORING OF SOIL NAIL WALLS (1998). THE SOIL NAIL WALL DESIGN SHOULD INCORPORATE THE FOLLOWING ITEMS AND CRITERIA:
   - LIMIT EQUILIBRIUM ANALYSIS OF SOIL NAIL DESIGN
   - LONG TERM CREEP PERFORMANCE
   - SLIDING, OVERTURNING BEARING CAPACITY
   - NAIL BAR STEEL DESIGN
   - WALL FACING DESIGN
   - CORROSION PROTECTION
   - MAX NAIL SPACING IN ANY DIRECTION OF 5 FEET, SUBJECT TO LOCAL ADJUSTMENTS TO AVOID OBSTRUCTIONS OR UTILITIES, OR TO ENSURE THAT SUFFICIENT STRUCTURAL CONNECTIONS TO THE STRUCTURAL SHOTCRETE FACING THE FINAL ARCHITECTUAL FACING ARE MADE
   - TOP ROW OF NAILS MAX OF 3 FEET FROM TOP OF WALL
   - BOTTOM ROW OF NAILS MAX OF 4 FEET FROM BOTTOM OF WALL
   - MIN LENGTH OF ANY NAIL IN THE TOP ROW SHOULD BE THE GREATER OF 70 PERCENT OF THE WALL HEIGHT OR 10 FEET.
   - MIN STRUCTURAL SHOTCRETE THICKNESS OF 4 INCHES

   MIN FACTORS OF SAFETY UTILIZED IN THE SOIL NAIL WALL DESIGN SHOULD BE AS SPECIFIED IN THE REFERENCED FHWA MANUAL.

STRUCTURAL:

1. WORKMANSHIP AND MATERIAL SHALL CONFORM TO THE NMDOT STANDARD SPECIFICATIONS FOR HIGHWAY AND BRIDGE CONSTRUCTION, 2007 EDITION AND APPLICABLE SUPPLEMENTAL SPECIFICATIONS.
2. DESIGN SPECIFICATIONS–AASHTO STANDARD SPECIFICATIONS FOR HIGHWAY BRIDGES 17TH EDITION, 2002, CONCRETE =150 P.C.F.
3. DEAD LOAD – WEIGHT OF EARTH BACKFILL = 120 P.C.F.: WEIGHT OF REINFORCED CONCRETE = 150 P.C.F.
4. ALL CONCRETE SHALL BE CLASS A f'c = 3000 psi AT 28 DAYS.
5. REINFORCING STEEL SHALL CONFORM TO ASTM SPEC'S A615, GRADE 60.
6. ALL BEND DIMENSIONS FOR REINFORCING STEEL SHALL BE OUT–TO–OUT OF BARS. ALL PLACEMENT DIMENSIONS FOR REINFORCING STEEL SHALL BE TO CENTER OF BARS UNLESS NOTED OTHERWISE.
7. ALL REINFORCING STEEL SHALL HAVE 3 INCH CLEAR COVER WHEN CAST AGAINST EARTH. ALL OTHER SHALL HAVE 2 INCH CLEAR COVER UNLESS NOTED OTHERWISE.
8. UNCOATED REINFORCING SPLICES SHALL BE LAPPED MIN. 62 BAR DIAMETERS, UNLESS SHOWN OTHERWISE. WHEN TWO BARS OF SIZE ARE LAPPING, THE SMALLER OF THE TWO SHALL BE USED FOR DETERMINING THE LAP LENGTH.
9. CHAMFER ALL EXPOSED CORNERS 3/4" UNLESS OTHERWISE NOTED.
10. DIMENSIONS SHALL NOT BE SCALED FROM DRAWINGS.
11. ALL RETAINING WALLS SHALL HAVE CONSTRUCTION JOINTS SPACED AT NOT MORE THAN 30'–0" APART AS SHOWN. CONSTRUCTION JOINTS SHALL OCCUR AT FOOTING STEPS UNLESS OTHERWISE SHOWN AND REINFORCED STEEL SHALL PROJECT THROUGH THE JOINT.
12. EXPANSION JOINTS SHALL BE PROVIDED AT INTERVALS NOT EXCEEDING 90'–0" AND SHALL OCCUR AT FOOTING STEPS UNLESS OTHERWISE NOTED,
13. FOOTINGS MAY BE CONTINUOUS WITH NO JOINT.
14. ALL HOOKS ARE STANDARD UNLESS OTHERWISE NOTED.
15. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING TEMPOARY SHORING AS REQUIRED TO MAINTAIN TRAFFIC, TO PROTECT UTILITES, FOR PROTECTION OF WORKERS, OR AS OTHERWISE NEEDED TO ACCOMPLISH THE WORK. SHORING SHALL CONFORM TO THE AASHTO STANDARD SPECIFICATIONS FOR HIGHWAY BRIDGES, 17TH EDITION, 2002 AND THE AASHTO GUIDE DESIGN SPECIFICATIONS FOR BRIDGE TEMPORARY WORKS, AND NMDOT STANDARD SPECIFICATIONS SECTION 210.

AS BUILT INFORMATION
CONTRACTOR
WORK STAKED BY: DATE
INSPECTOR'S ACCEPTANCE BY: DATE
FIELD VERIFICATION BY: DATE
MARKED BY: DATE
CORRECTED BY: DATE

MICRO–FILM INFORMATION
RECORDED BY: DATE
NO.

BENCH MARKS
NGS STAINLESS ROD SET BENEATH A 5 1/2" ACCESS COVER STAMPED "D–438, 1984", SE QUADRANT OF MONTANO RD. & THE BNSF RAILROAD TRACKS, 42.5 FT. EAST OF CENTERLINE OF THE TRACKS, 44 FT. SOUTH OF CENTERLINE OF MONTANO RD. NE, 1.1 FT. WEST OF CHAIN LINK. FENCE.
DATUM NAVD 1988
ELEV. 4978.070

SURVEY INFORMATION
FIELD NOTES
NO. | BY | DATE

ENGINEER'S SEAL

REVISIONS
NO. | DATE | REMARKS | BY
DESIGN | DESIGNED BY SL | DATE 10/2010
DRAWN BY BN | DATE 10/2010
CHECKED BY KA | DATE 10/2010

Vector Engineering, LLC

CITY OF ALBUQUERQUE
DEPARTMENT OF MUNICIPAL DEVELOPMENT
ENGINEERING DIVISION

TITLE: NEW MEXICO RAIL RUNNER EXPRESS
MONTANO STATION
GENERAL NOTES CONT'D.

Design Review Committee | City Engineer Approval | Last Design Update | Mo. / Day / Yr. | Mo. / Day / Yr.

City Project No. 559282 | Zone Map No. F–15 | Sheet 4 Of 46

Montano Rail Runner Station

SUMMARY OF QUANTITIES - 65%

ITEM NO.	DESCRIPTION	UNIT	ESTIMATE
	ON-SITE (MONTANO STATION)		
	EARTHWORK		
201.01	SITE CLEAR AND GRUB	ACRE	3.0
205.01	FILL, BORROW, HAUL AND COMP	C.Y.	9,275
	PAVING		
301.02	SUBGRADE PREP, 12"	S.Y.	9,882
302.01	BASE COURSE, 6" (REGULAR & HEAVY DUTY PAVING)	S.Y.	9,127
336.022	HMA SPIII, 2" (REGULAR PAVING)	S.Y.	6,089
336.023	HMA SPIII, 2-1/2" (REGULAR PAVING)	S.Y.	6,089
336.024	HMA SPIII, 3" (HEAVY DUTY PAVING) 2 LIFTS	S.Y.	6,076
336.12	TACK COAT	S.Y.	9,127
337.02	CONCRETE PAVEMENT, 6" (BUS PARKING)	S.Y.	755
340.01	CONCRETE SIDEWALK, 4" (4' PED CONNECTIONS)	S.Y.	1,154
340.01	CONCRETE SIDEWALK, 4" (ADJACENT TO 300' PLATFORM)	S.Y.	175
340.11	HEADER CURB, PCC	L.F.	3,909
441.001	REFLECTORIZED PLASTIC MARKING, 4" WIDTH	L.F.	4,000
340.025	WHEELCHAIR RAMP	EACH	3
	WALLS		
XXXX	4' CMU SCREEN WALL	L.F.	235
XXXX	CONCRETE RETAINING WALL	S.F.	1,850
	LANDSCAPING AND IRRIGATION		
XXXX	LANDSCAPING AND IRRIGATION	L.S.	1
	SITE LIGHTING		
XXXX	SITE LIGHTING	EACH	23
	UTILITES		
901.XXX	4" SAS PIPE	L.F.	335
701.01	TRENCHING, BACKFILL, 4-15", <8' DEPTH	L.F.	335
XXXX	SAS CLEANOUT	EACH	3
XXXX	2" WATER LINE	L.F.	335
	STORM DRAIN		
910.XXX	8" RCP, III	L.F.	100
910.XXX	12" RCP, III	L.F.	773
910.005	18" RCP, III	L.F.	128
701.02	TRENCHING, BACKFILL, 4-15", 8-12' DEPTH	L.F.	773
701.11	TRENCHING, BACKFILL, 18-36", 8-12' DEPTH	L.F.	128
905.05	SINGLE D, DROP INLET	EACH	3
915.06	DOUBLE D, DROP INLET	EACH	1
920.07	MH, 4' DIA, C OR E, 6-10' DEPTH	EACH	2
340.21	SIDEWALK CULVERT, 24"	EACH	3
XXXX	CONNECTION TO EX. STORM DRAIN STUB	EACH	1
	MISCELLANEOUS		
XXXX	PLATFORM	L.F.	500
XXXX	SILT FENCE	L.F.	2,605
XXXX	DROP INLET PROJECTION TYPE I	EACH	4
XXXX	CONSTRUCTION TRAFFIC CONTROL	LS	1
XXXX	TRAFFIC CONTROL MANAGEMENT	LS	1
XXXX	MOBILIZATION (6%)	LS	1
XXXX	CONSTRUCTION STAKING BY THE CONTRACTOR	LS	1
XXXX	PNM UTILTY RELOCATION	LS	1
XXXX	SOLAR PANELS, LED LIGHTING AND RELATED EQUIPMENT	LS	1

ITEM NO.	DESCRIPTION	UNIT	ESTIMATE	
	OFF-SITE (MONTANO ROAD IMPROVEMENTS)			
	MONTANO ROAD			
301.02	SUBGRADE PREPARATION, 12"	S.Y.	562	
302.01	AGGREGATE BASE COURSE, 6" (2 LIFTS)	S.Y.	1,124	
336.023	ASPHALT CONCRETE, 2-1/2", M	S.Y.	298	
336.024	ASPHALT CONCRETE, 3", M	S.Y.	298	
336.12	TACK COAT	S.Y.	298	
337.02	CONCRETE PAVEMENT, 6"	S.Y.	264	
340.01	CONCRETE SIDEWALK, 4"	S.Y.	668	
340.023	WHEELCHAIR RAMP, 4" PCC	S.Y.	73	
340.13	DEPRESSED CURB AND GUTTER	L.F.	734	
340.06	MEDIAN CURB AND GUTTER	L.F.	251	
340.03	VALLEY GUTTER AND CURB	L.F	295	
XXXXX	STREET SIGNAGE	L.S.	1	
441.001	REFLECTORIZED PLASTIC MARKING, 4" WIDTH	L.F.	200	
441.005	REFLECTORIZED PLASTIC MARKING, 24" WIDTH	L.F.	215	
441.01	REFLECTORIZED PLASTIC ARROW, RIGHT	EACH	1	
441.02	ddedit	EACH	2	
XXXXX	NEW TRAFFIC SIGNAL	L.S.	1	
	UTILITIES			
801.002	6" WATER LINE, W/O FIT	LF	30	
801.113	FIRE HYDRANT, 4'	EACH	2	
801.119	FIRE HYDRANT, REMOVE AND SALVAGE	EACH	2	
802.51	1-1/2" - 2" WATER METER BOX	EACH	1	
802.51	2" WATER SERVICE, SINGLE	EACH	1	
XXXX	REMOVE EXISTING WATER METER BOXES	EACH	2	
XXXX	REMOVE AND RELOCATE EXISTING PULL BOX	EACH	3	

AS BUILT INFORMATION
CONTRACTOR
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INSPECTIONS ACCEPTANCE BY — DATE
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BENCH MARKS
NGS STAINLESS ROD SET BENEATH A 5 1/2" ACCESS COVER STAMPED "D-438, 1984", SE QUADRANT OF MONTANO RD. & THE BNSF RAILROAD TRACKS, 42.5 FT. EAST OF CENTERLINE OF THE TRACKS, 44 FT. SOUTH OF CENTERLINE OF MONTANO RD. NE, 1.1 FT. WEST OF CHAIN LINK FENCE.
DATUM NAVD 1988
ELEV. 4978.070

MICRO-FILM INFORMATION
RECORDED BY — DATE
NO.

SURVEY INFORMATION
FIELD NOTES
NO. | BY | DATE

ENGINEER'S SEAL

REVISIONS
NO. | DATE | REMARKS | BY

DESIGN — DATE
DESIGNED BY SL — 10/2010
DRAWN BY BN — 10/2010
CHECKED BY KA — 10/2010

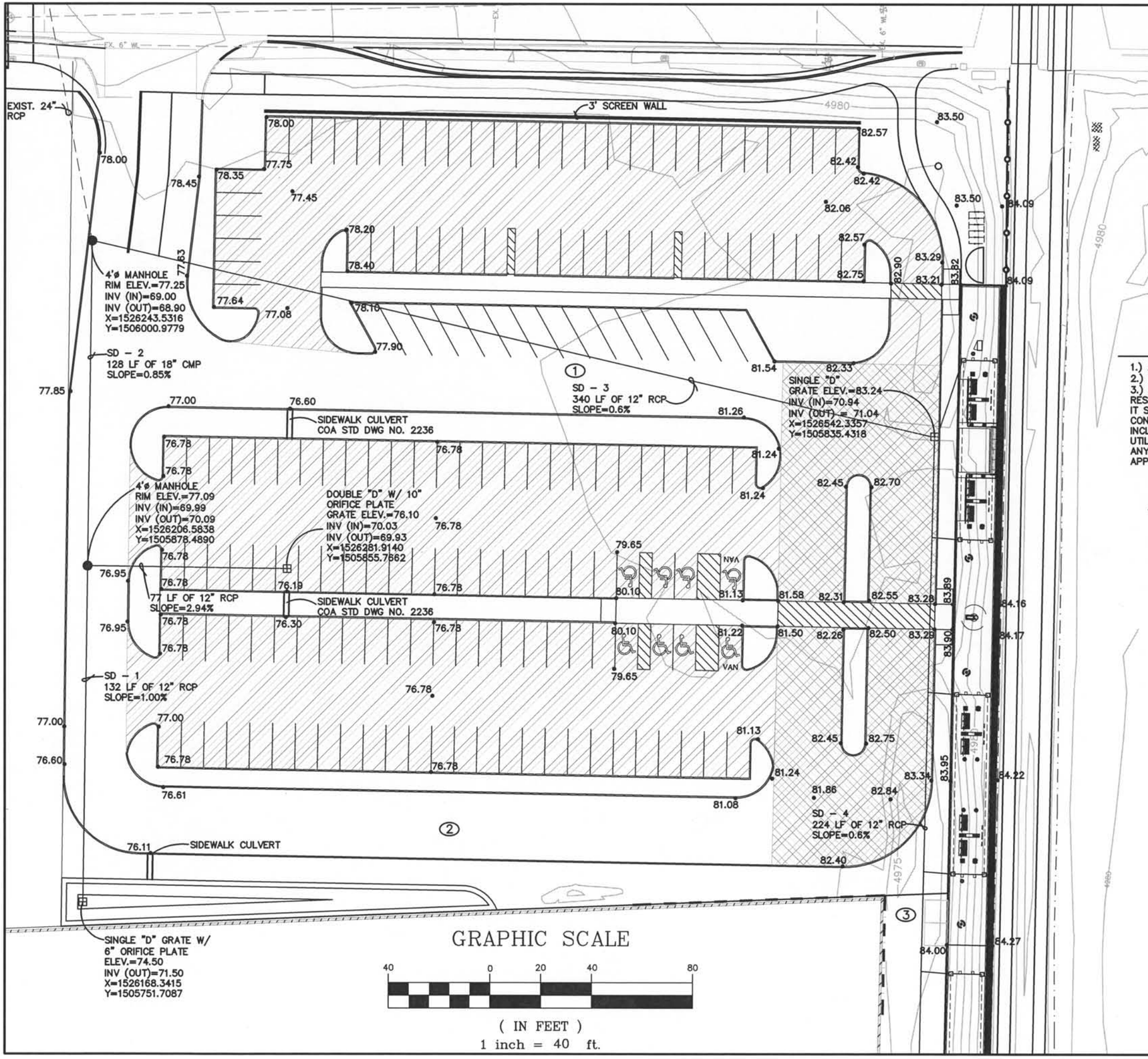
Vector Engineering, LLC

CITY OF ALBUQUERQUE
DEPARTMENT OF MUNICIPAL DEVELOPMENT
ENGINEERING DIVISION

TITLE: NEW MEXICO RAIL RUNNER EXPRESS
MONTANO STATION
SUMMARY OF QUANTITIES

Design Review Committee | City Engineer Approval | Last Design Update | No. / Day / Yr. | No. / Day / Yr.

City Project No. 559282 | Zone Map No. F-15 | Sheet 5 Of 46



GRAPHIC SCALE

40 0 20 40 80

(IN FEET)

1 inch = 40 ft.

EXISTING MINOR CONTOURS
EXISTING MAJOR CONTOURS
DRAINAGE BASINS
② DRAINAGE BASIN NUMBER
XX.XX PROPOSED SPOT ELEVATION
6" CONCRETE HEADER CURB SEE SHT. 9 FOR DETAIL
TYPE B PARALLEL CURB RAMP SEE SHT. 8 FOR DETAIL
PROPOSED ASPHALT PAVING (4 1/2")
PROPOSED CONCRETE PAVING (6")
PROPOSED ASPHALT PAVING (6")
6' CHAIN LINK FENCE SERIAL NO.: 607–04 1 OF 1
LOCATION OF 5' RETAINING WALL
STORM DRAIN
DROP INLET

1.) ALL ELEVATIONS ARE FLOWLINE UNLESS OTHERWISE NOTED
2.) ADD 4900 FEET TO ALL POINTS TO GET MEAN SEA ELEVATIONS.
3.) CAUTION–ALL EXISTING UTILITIES SHOWN WERE OBTAINED FROM RESEARCH, AS BUILTS, SURVEYS OR INFORMATION PROVIDED BY OTHERS. IT SHALL BE THE SOLE RESPONSIBILITY OF THE CONTRACTOR TO CONDUCT ALL NECESSARY FIELD INVESTIGATIONS PRIOR TO AND INCLUDING ANY EXCAVATION, TO DETERMINE THE ACTUAL LOCATION OF UTILITIES AND OTHER IMPROVEMENTS PRIOR TO STARTING THE WORK. ANY CHANGES FROM THIS PLAN SHALL BE COORDINATED WITH AND APPROVED BY THE ENGINEER.

DRAINAGE BASIN 1
AREA=2.4 AC
Q10=6.98 CFS
Q100=10.82 CFS

DRAINAGE BASIN 2
AREA=0.41 AC
Q10=1.04 CFS
Q100=1.68 CFS

DRAINAGE BASIN 3
AREA=0.30 AC
Q10=0.69 CFS
Q100=1.16 CFS

SINGLE D AND DOUBLE D GRATES
COA STD. DWG 2206

SIDEWALK CULVERTS
COA STD. DWG 2236

Vector Engineering, LLC

AS BUILT INFORMATION
CONTRACTOR
WORK STARTED BY DATE
INSPECTOR'S ACCEPTANCE BY DATE
VERIFICATION BY DATE
DRAWINGS CORRECTED BY DATE
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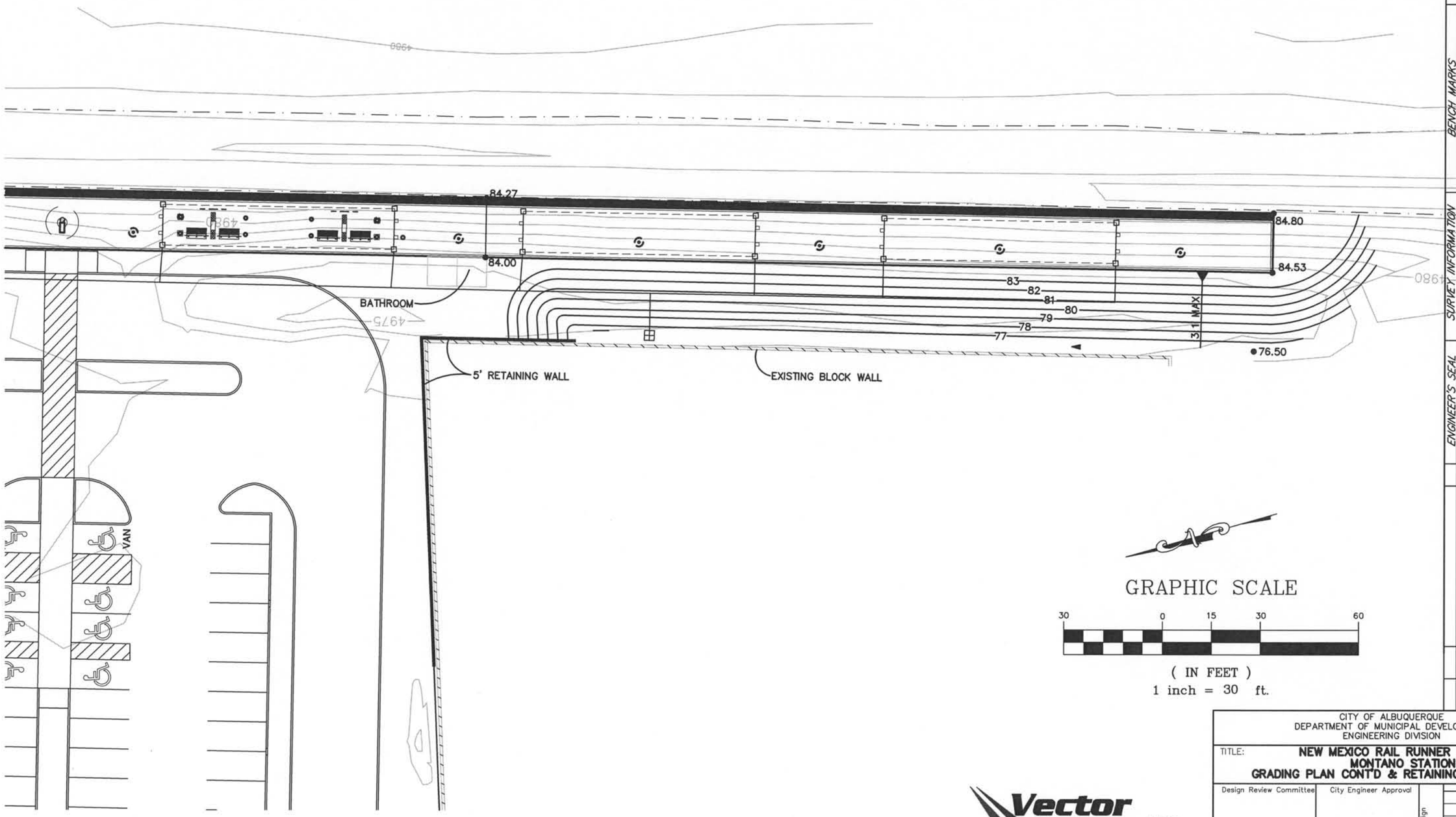
CITY OF ALBUQUERQUE
DEPARTMENT OF MUNICIPAL DEVELOPMENT
ENGINEERING DIVISION

TITLE: NEW MEXICO RAIL RUNNER EXPRESS
MONTANO STATION
PARKING LOT GRADING & DRAINAGE PLAN

Design Review Committee | City Engineer Approval | Last Design Update | No. / Dsg / Yr. | No. / Dsg / Yr.

City Project No. 559282 | Zone Map No. F–15 | Sheet 6 Of 46



GRAPHIC SCALE

30 0 15 30 60

(IN FEET)
1 inch = 30 ft.

Vector Engineering, LLC

BENCH MARKS
NGS STAINLESS ROD SET BENEATH A 5 1/2" ACCESS COVER STAMPED "D-438, 1984". SE QUADRANT OF MONTANO RD. & THE BNSF RAILROAD TRACKS, 42.5 FT. EAST OF CENTERLINE OF THE TRACKS, 44 FT. SOUTH OF CENTERLINE OF MONTANO RD. NE, 1.1 FT. WEST OF CHAIN LINK FENCE.
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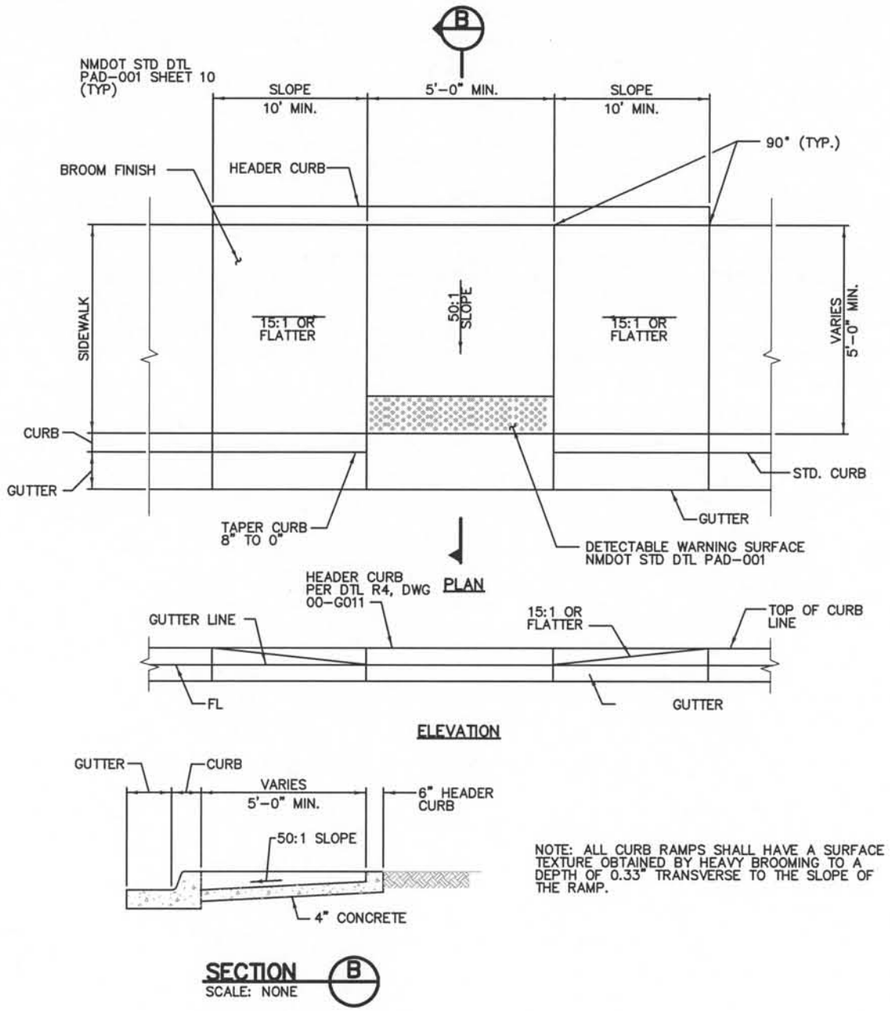
CITY OF ALBUQUERQUE
DEPARTMENT OF MUNICIPAL DEVELOPMENT
ENGINEERING DIVISION

TITLE: NEW MEXICO RAIL RUNNER EXPRESS
MONTANO STATION
GRADING PLAN CONT'D & RETAINING WALL LOCATION

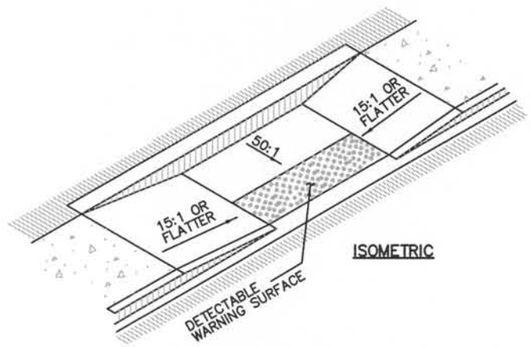
Design Review Committee | City Engineer Approval | Last Design Update | No. / Dsg / Yr. | No. / Dsg / Yr.

City Project No. 559282 | Zone Map No. F-15 | Sheet 7 | Of 46



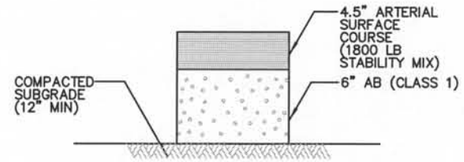
NOTE: ALL CURB RAMPS SHALL HAVE A SURFACE TEXTURE OBTAINED BY HEAVY BROOMING TO A DEPTH OF 0.33" TRANSVERSE TO THE SLOPE OF THE RAMP.

SECTION B
SCALE: NONE



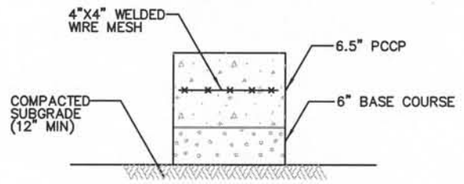
ISOMETRIC

TYPE B PARALLEL CURB RAMP
SCALE: NONE



PAVEMENT STRUCTURAL SECTION NO. 1

PARKING LOTS
AND AS NOTED ON PLANS

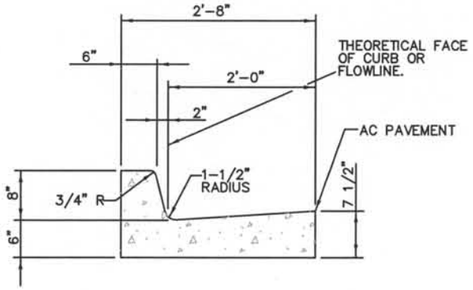


PAVEMENT STRUCTURAL SECTION NO. 2

BUS LANE & LOADING AREAS
AND AS NOTED ON PLANS

PAVEMENT STRUCTURAL SECTION NO. 3

PARKING LOTS
AND AS NOTED ON PLANS



STANDARD CURB & GUTTER
SCALE: NONE

SURVEY INFORMATION
FIELD NOTES
NO. | BY | DATE

BENCH MARKS:
NGS STAINLESS ROD SET BENEATH A 5 1/2" ACCESS COVER STAMPED "D-438, 1984", SE QUADRANT OF MONTANO RD. & THE BNSF RAILROAD TRACKS, 42.5 FT. EAST OF CENTERLINE OF THE TRACKS, 44 FT. SOUTH OF CENTERLINE OF MONTANO RD. NE, 1.1 FT. WEST OF CHAIN LINK FENCE.
DATUM NAVD 1988
ELEV. 4978.070

AS BUILT INFORMATION
CONTRACTOR
WORK STAKED BY — DATE
INSPECTED BY — DATE
ACCEPTANCE BY — DATE
FIELD VERIFICATION BY — DATE
DRAWINGS CORRECTED BY — DATE

MICRO-FILM INFORMATION
RECORDED BY — DATE
NO.

ENGINEER'S SEAL

NO.	DATE	REMARKS / REVISIONS	BY
		DESIGN — DATE 10/2010	SL
		DRAWN BY — DATE 10/2010	BN
		CHECKED BY — DATE 10/2010	KA

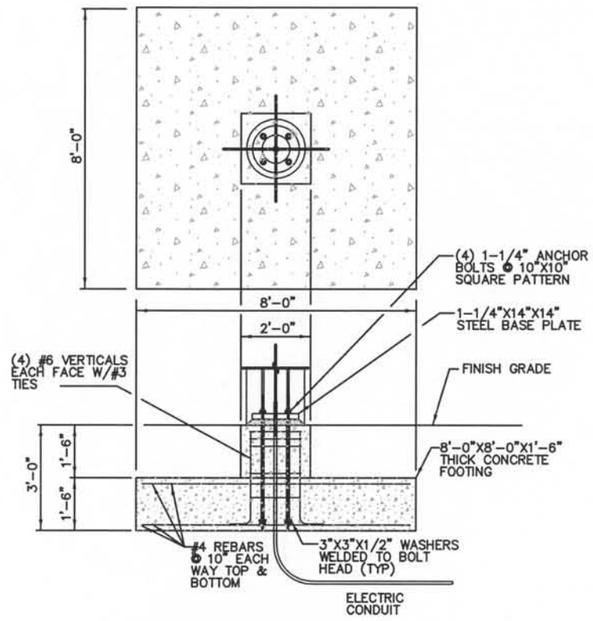
Vector Engineering, LLC

CITY OF ALBUQUERQUE
DEPARTMENT OF MUNICIPAL DEVELOPMENT
ENGINEERING DIVISION

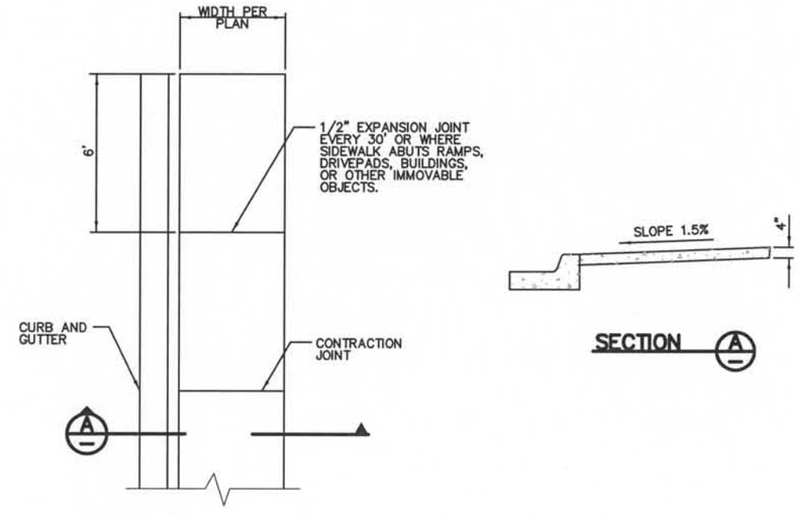
TITLE: **NEW MEXICO RAIL RUNNER EXPRESS**
MONTANO STATION
PARKING LOT DETAIL SHEET

Design Review Committee | City Engineer Approval | Last Design Update | No. / Dsg / Yr. | No. / Dsg / Yr.

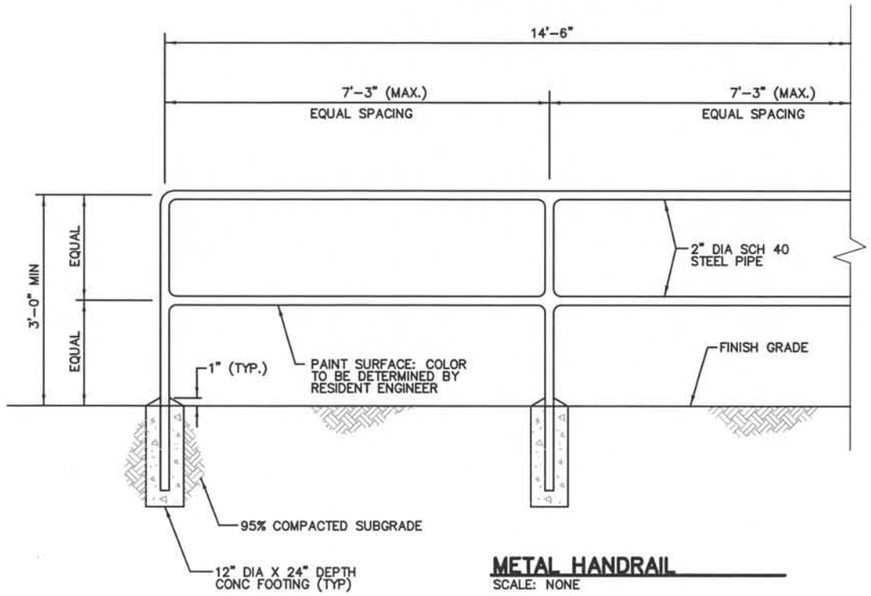
City Project No. **559282** | Zone Map No. **F-15** | Sheet **8** Of **46**



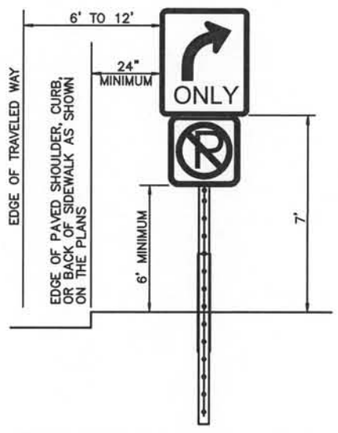
LIGHTING FOOTING DETAIL
SCALE: NONE



SIDEWALK
SCALE: NONE



METAL HANDRAIL
SCALE: NONE



SIGN INSTALLATION ON POST
SCALE: NONE

Vector Engineering, LLC

ENGINEER'S SEAL

SURVEY INFORMATION

FIELD NOTES

NO.	BY	DATE

BENCH MARKS

NGS STAINLESS ROD SET BENEATH A 5 1/2" ACCESS COVER STAMPED "D-438, 1984", SE QUADRANT OF MONTANO RD. & THE BNSF RAILROAD TRACKS, 42.5 FT. EAST OF CENTERLINE OF THE TRACKS, 44 FT. SOUTH OF CENTERLINE OF MONTANO RD. NE, 1.1 FT. WEST OF CHAIN LINK FENCE.
DATUM NAVD 1988
ELEV. 4978.070

AS BUILT INFORMATION

CONTRACTOR
WORK STAKED BY — DATE
INSPECTORS ACCEPTANCE BY — DATE
FIELD VERIFICATION BY — DATE
DRAWINGS CORRECTED BY — DATE

MICRO-FILM INFORMATION

RECORDED BY — DATE
NO.

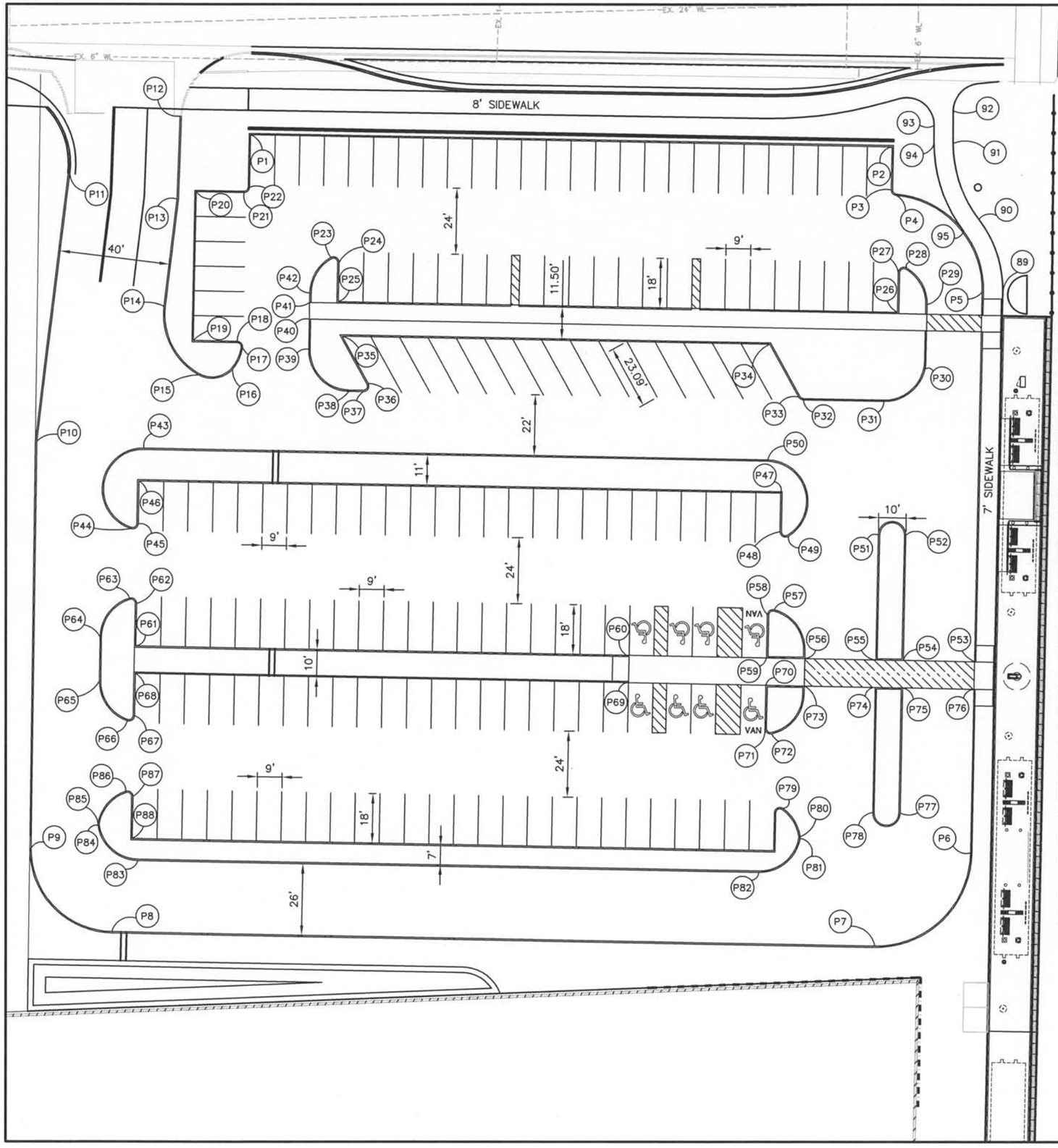
NO.	DATE	REMARKS	BY
		REVISIONS	
		DESIGN	
		DESIGNED BY SL	DATE 10/2010
		DRAWN BY BN	DATE 10/2010
		CHECKED BY KA	DATE 10/2010

CITY OF ALBUQUERQUE
DEPARTMENT OF MUNICIPAL DEVELOPMENT
ENGINEERING DIVISION

TITLE: **NEW MEXICO RAIL RUNNER EXPRESS MONTANO STATION PARKING LOT DETAIL SHEET**

Design Review Committee	City Engineer Approval	Last Design Update	No. / Dsg / Yr.	No. / Dsg / Yr.

City Project No. 559282 | Zone Map No. F-15 | Sheet 9 Of 46



GRAPHIC SCALE

40 0 20 40 80

(IN FEET)

1 inch = 40 ft.

POINT DATA

POINT NO.	X	Y
P1	1526322.9994	1506028.7975
P2	1526546.7268	1505960.2260
P3	1526542.1580	1505945.3194
P4	1526543.3506	1505942.8673
P5	1526563.6683	1505898.0311
P6	1526504.1499	1505705.5474
P7	1526461.0546	1505682.6719
P8	1526194.9787	1505764.2230
P9	1526175.0430	1505801.5524
P10	1526217.6497	1505943.1090
P11	1526255.8390	1506033.3843
P12	1526300.2710	1506042.3530
P13	1526290.9282	1506013.6631
P14	1526275.6367	1505971.1535
P15	1526280.9134	1505950.1686
P16	1526293.4375	1505948.8109
P17	1526299.0064	1505955.3179
P18	1526297.7650	1505958.0426
P19	1526282.5582	1505962.7035
P20	1526298.3824	1506014.3329
P21	1526314.9206	1506009.2640
P22	1526317.4189	1506010.5901
P23	1526338.9867	1505977.8933
P24	1526341.3474	1505975.3582
P25	1526337.1618	1505961.7019
P26	1526532.2062	1505901.9216
P27	1526536.4284	1505915.6972
P28	1526539.7773	1505916.5024
P29	1526543.3430	1505901.6721
P30	1526536.1664	1505879.2919
P31	1526518.5986	1505872.1262
P32	1526490.4682	1505880.7480
P33	1526531.9416	1505893.7521
P34	1526484.1850	1505903.5921
P35	1526335.1437	1505949.2726
P36	1526339.5461	1505930.1428
P37	1526337.0110	1526337.0110
P38	1526331.9478	1505929.3339
P39	1526322.0019	1505948.0710
P40	1526325.2180	1505958.5641
P41	1526326.9762	1505964.3007
P42	1526328.0092	1505967.6710
P43	1526254.5272	1505930.0528
P44	1526241.9902	1505903.1130
P45	1526244.9879	1505904.2067
P46	1526249.5321	1505919.0328
P47	1526473.2595	1505850.4613

POINT DATA

POINT NO.	X	Y
P48	1526469.1703	1505837.1195
P49	1526471.6183	1505834.6065
P50	1526471.9955	1505863.3997
P51	1526503.4300	1505826.0484
P52	1526512.9910	1505823.1180
P53	1526524.5163	1505771.9964
P54	1526499.6577	1505779.6154
P55	1526490.0967	1505782.5458
P56	1526465.2381	1526465.2381
P57	1526459.3622	1505810.3688
P58	1526456.4385	1505809.2296
P59	1526452.4772	1505794.5990
P60	1526403.9011	1505808.4414
P61	1526231.9497	1505861.6668
P62	1526236.4702	1505876.4160
P63	1526234.5565	1505879.0021
P64	1526220.2043	1505868.3253
P65	1526215.5928	1505853.0041
P66	1526221.4612	1505836.3183
P67	1526224.5060	1505837.3806
P68	1526229.0193	1505852.1058
P69	1526401.1173	1505799.3585
P70	1526448.9223	1505784.7065
P71	1526444.3364	1505769.7444
P72	1526446.1197	1505767.1624
P73	1526462.3077	1505780.6039
P74	1526487.1663	1505772.9848
P75	1526496.7273	1505770.0544
P76	1526521.5859	1505718.4303
P77	1526483.3939	1505726.5519
P78	1526473.8329	1505729.4823
P79	1526442.6542	1505739.9780
P80	1526445.8554	1505728.3477
P81	1526445.6698	1505727.7517
P82	1526427.8242	1505720.0507
P83	1526211.2128	1505786.4388
P84	1526200.6809	1505802.8167
P85	1526200.8617	1526200.8617
P86	1526213.9297	1505811.8886
P87	1526215.9005	1505809.3033
P88	1526211.4368	1505794.7398
P89	1526571.0936	1505898.3701
P90	1526569.9427	1505926.2530
P91	1526568.4774	1505955.3443
P92	1526571.7299	1505966.6873
P93	1526563.5775	1505963.6520
P94	1526561.7485	1505957.2738
P95	1526563.4419	1505923.6571

Vector Engineering, LLC

CITY OF ALBUQUERQUE
DEPARTMENT OF MUNICIPAL DEVELOPMENT
ENGINEERING DIVISION

TITLE: **NEW MEXICO RAIL RUNNER EXPRESS**
MONTANO STATION
PARKING LOT POINT DATA

Design Review Committee | City Engineer Approval | Last Design Update | No. / Sky / Yr. | No. / Sky / Yr.

City Project No. **559282** | Zone Map No. **F-15** | Sheet **10** Of **46**

AS BUILT INFORMATION
CONTRACTOR — DATE
WORK STARTED BY — DATE
INSPECTOR'S ACCEPTANCE BY — DATE
FIELD VERIFICATION BY — DATE
DRAWINGS CORRECTED BY — DATE
MICRO-FILM INFORMATION
RECORDED BY — DATE
NO.

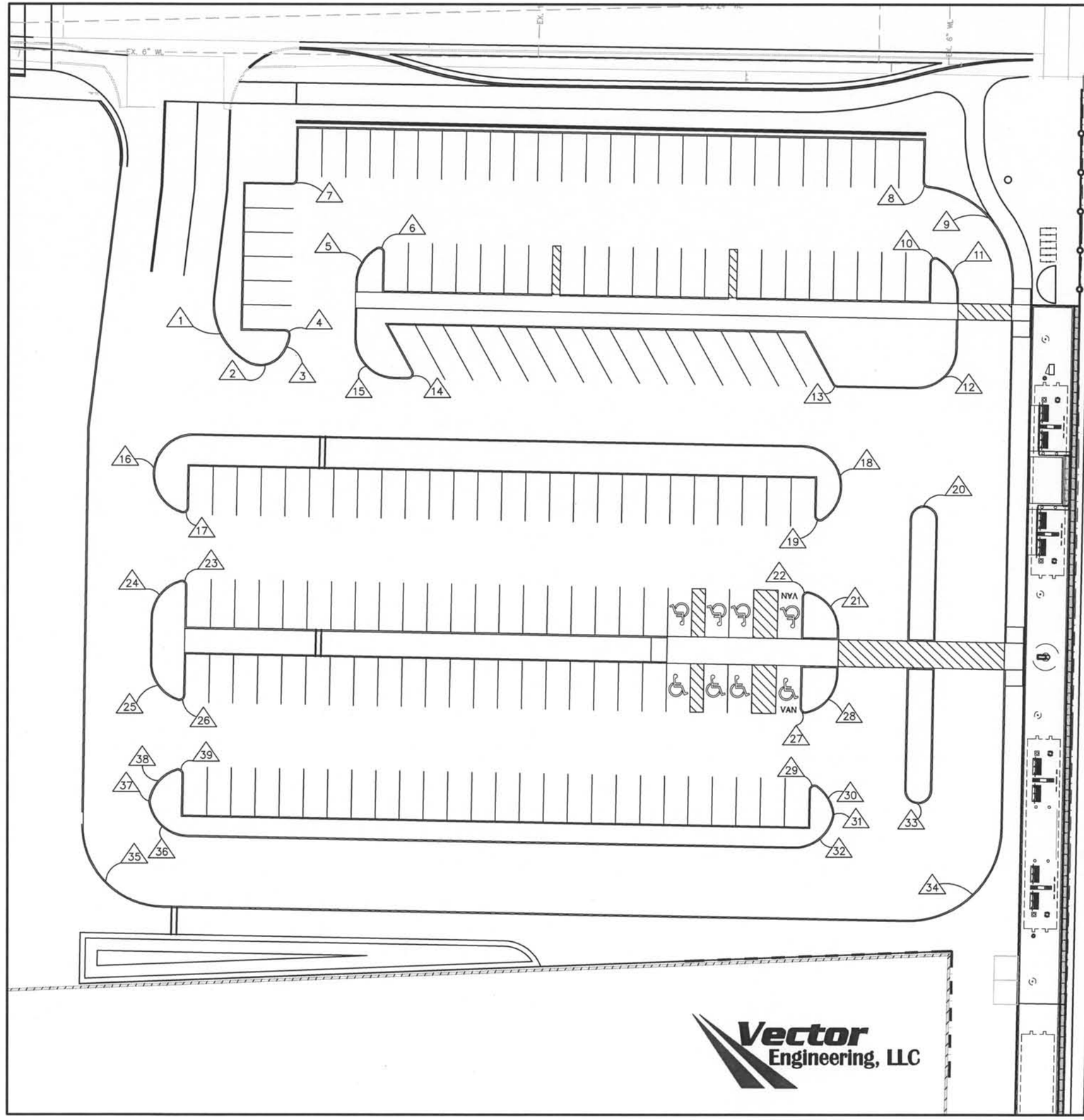
BENCH MARKS
NGS STAINLESS ROD SET BENEATH A 5 1/2" ACCESS COVER STAMPED "D-438, 1984", SE QUADRANT OF MONTANO RD. & THE BNSF RAILROAD TRACKS, 42.5 FT. EAST OF CENTERLINE OF THE TRACKS, 44 FT. SOUTH OF CENTERLINE OF MONTANO RD. NE, 1.1 FT. WEST OF CHAIN LINK FENCE.
DATUM NAVD 1988
ELEV. 4978.070

SURVEY INFORMATION
FIELD NOTES BY — DATE
NO.

ENGINEER'S SEAL

REVISIONS

NO.	DATE	REMARKS	BY
		DESIGN	SL
		DESIGNED BY	SL
		DRAWN BY	BN
		CHECKED BY	KA
		DATE	10/2010
		DATE	10/2010
		DATE	10/2010



GRAPHIC SCALE

40 0 20 40 80

(IN FEET)

1 inch = 40 ft.

△ CURVE DATA

CURVE NO.	RADIUS	TANGENT LENGTH	ARC LENGTH
C1	25.00'	16.77'	29.54'
C2	10.00'	8.11'	13.63'
C3	15.00'	4.47'	8.69'
C4	2.00'	2.26'	3.38'
C5	15.00'	8.66'	15.71'
C6	2.00'	3.46'	4.19'
C7	2.00'	2.00'	3.14'
C8	2.00'	1.86'	3.00'
C9	34.50'	32.15'	51.76'
C10	2.00'	3.39'	4.15'
C11	15.00'	8.86'	16.01'
C12	15.00'	15.00'	23.56'
C13	2.02'	1.24'	2.22'
C14	2.00'	3.46'	4.19'
C15	15.00'	15.00'	23.56'
C16	15.00'	107.88'	42.98'
C17	2.00'	2.65'	3.69'
C18	15.00'	51.33'	38.60'
C19	2.00'	3.65'	4.28'
C20	5.00'	--	15.71'
C21	15.00'	11.86'	20.07'
C22	2.00'	2.53'	3.61'
C23	2.00'	2.71'	3.74'
C24	15.00'	11.14'	19.17'
C25	15.00'	10.95'	18.92'
C26	2.00'	2.73'	3.75'
C27	2.00'	2.53'	3.61'
C28	15.00'	11.86'	20.07'
C29	2.00'	3.75'	4.32'
C30	15.00'	6.59'	12.41'
C31	2.00'	0.32'	0.63'
C32	15.00'	12.76'	21.14'
C33	5.00'	--	15.71'
C34	34.50'	34.50'	54.19'
C35	30.00'	29.85'	46.97'
C36	15.00'	12.80'	21.19'
C37	2.00'	0.32'	0.63'
C38	15.00'	9.11'	16.37'
C39	2.00'	2.79'	3.80'

AS BUILT INFORMATION

CONTRACTOR

WORK STAKED BY — DATE

INSPECTOR'S ACCEPTANCE BY — DATE

FIELD VERIFICATION BY — DATE

DRAWINGS CORRECTED BY — DATE

MICRO-FILM INFORMATION

RECORDED BY — DATE

NO.

BENCH MARKS

NGS STAINLESS ROD SET BENEATH A 5 1/2" ACCESS COVER STAMPED "D-438, 1984", SE QUADRANT OF MONTANO RD. & THE BNSF RAILROAD TRACKS, 42.5 FT. EAST OF CENTERLINE OF THE TRACKS, 44 FT. SOUTH OF CENTERLINE OF MONTANO RD. NE, 1.1 FT. WEST OF CHAIN LINK FENCE.

DATUM NAVD 1988

ELEV. 4978.070

SURVEY INFORMATION

FIELD NOTES

NO.	BY	DATE

ENGINEER'S SEAL

NO.	DATE	REMARKS	BY
		REVISIONS	
		DESIGN	

DESIGNED BY SL DATE 10/2010

DRAWN BY BN DATE 10/2010

CHECKED BY KA DATE 10/2010

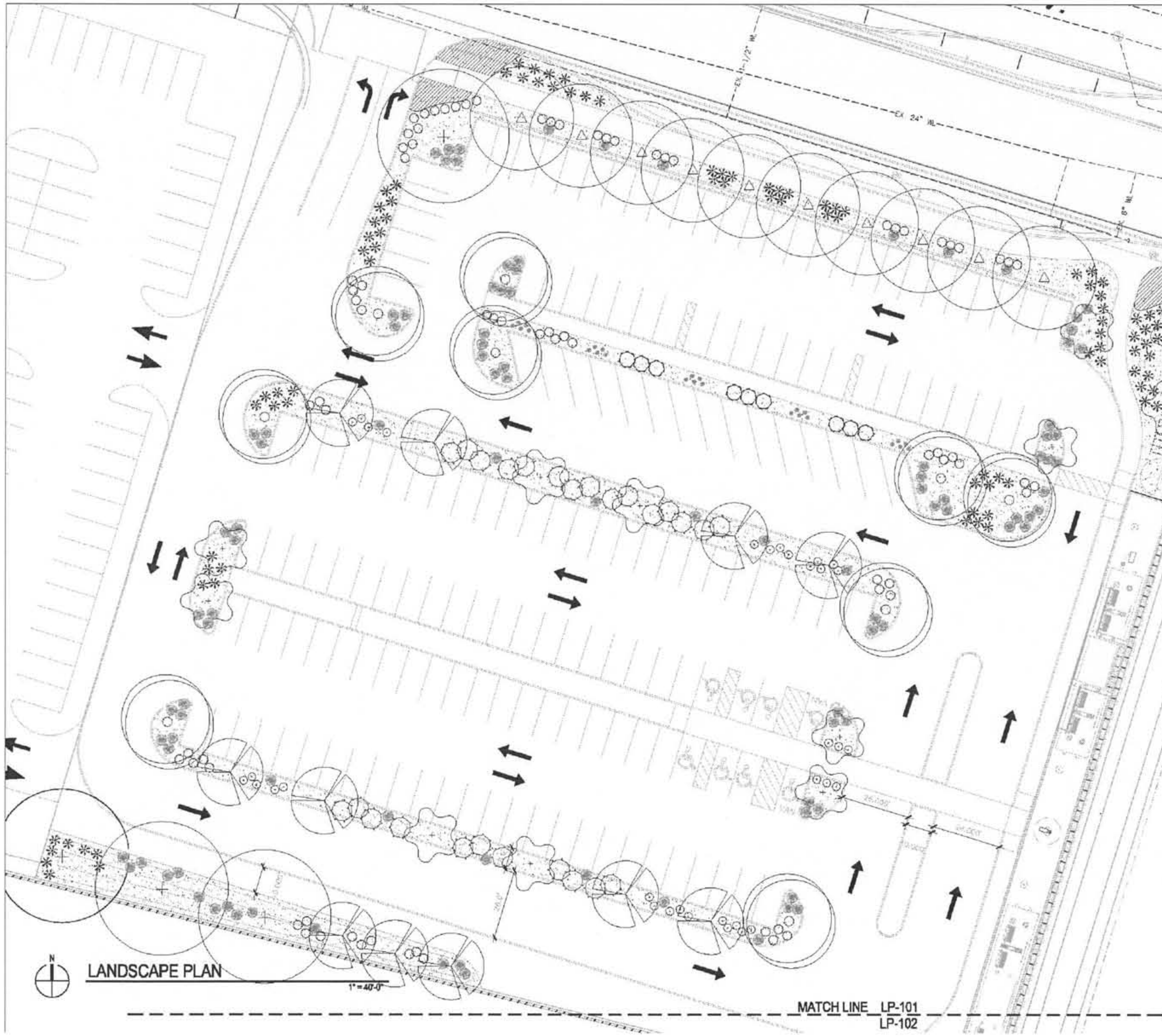
CITY OF ALBUQUERQUE
DEPARTMENT OF MUNICIPAL DEVELOPMENT
ENGINEERING DIVISION

TITLE: **NEW MEXICO RAIL RUNNER EXPRESS MONTANO STATION PARKING LOT CURVE DATA**

Design Review Committee | City Engineer Approval | Last Design Update | No. / Day / Yr. | No. / Day / Yr.

City Project No. 559282 | Zone Map No. F-15 | Sheet 11 Of 46

Vector Engineering, LLC



LEGEND

Symbol	Qty.	Description
░	26,245 SF	Rock Mulch

PLANTING LEGEND

Trees

Symbol	Qty.	Scientific Name	Common Name	Installed Size	Mature Height	Mature Width	Water Requirements
△	10	Pistacia chinensis	CHINESE PISTACHE	2.5" Cal. (B&B)	60.00	60.00	Medium Water Use
⊙	4	Populus fremontii wislizenii	RIO GRANDE COTTONWOOD	3" Cal.	50.00	60.00	Medium Water Use
◔	11	Prosopis glandulosa	HONEY MESQUITE	40" Box	25.00	30.00	Low Water Use
⊙	9	Sophora japonica	JAPANESE PAGODA TREE	2.5" Cal. (B&B)	35.00	35.00	Medium Water Use
✿	15	Vitax agnus-castus	CHASTE TREE	40" Box	20.00	20.00	Medium Water Use
✹	8	Pinus eldarica	AFGHAN PINE	12' Ht.	40.00	18.00	Medium Water Use

Shrubs and Accents

Symbol	Qty.	Scientific Name	Common Name	Installed Size	Mature Height	Mature Width	Water Requirements
○	26	Rhus aromatica 'Gro-Low'	GRO-LOW SUMAC	5 Gal.	3.00	8.00	Low Water Use
⊙	34	Ericameria laricifolia	TURPENTINE BUSH	5 Gal.	2.00	3.00	Low Water Use
○	77	Baccharis x 'Starn'	THOMPSON BROOM	5 Gal.	3.50	3.50	Low Water Use
○	18	Chamaebatiera millifolium	FERNBUSH	5 Gal.	6.00	6.00	Low Water Use
✳	104	Hesperaloe parviflora	RED YUCCA	5 Gal.	3.00	4.00	Low Water Use

Ornamental Grasses

Symbol	Qty.	Scientific Name	Common Name	Installed Size	Mature Height	Mature Width	Water Requirements
●	102	Muhlenbergia rigens	DEERGRASS	5 Gal.	4.00	4.00	Medium Water Use
✳	28	Elytrigia elongata 'Jose Select'	JOSE SELECT TALL WHEATGRASS	5 Gal.	5.00	1.50	Medium Water Use

Perennials

Symbol	Qty.	Scientific Name	Common Name	Installed Size	Mature Height	Mature Width	Water Requirements
▨	65	Baileya multiradiata	DESERT MARIGOLD	1 Gal.	1.50	1.50	Low Water Use

AS BUILT INFORMATION

CONTRACTOR

WORK STAKED BY — DATE

INSPECTOR'S ACCEPTANCE BY — DATE

FIELD VERIFICATION BY — DATE

DRAWINGS CORRECTED BY — DATE

MICRO-FILM INFORMATION

RECORDED BY — DATE

NO.

BENCH MARKS

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DATUM NAVD 1988

ELEV. 4978.070

SURVEY INFORMATION

FIELD NOTES

NO.	BY	DATE

ENGINEER'S SEAL

REVISIONS

NO.	DATE	REMARKS	BY

DESIGN

DESIGNED BY SL DATE 10/2010

DRAWN BY BN DATE 10/2010

CHECKED BY KA DATE 10/2010

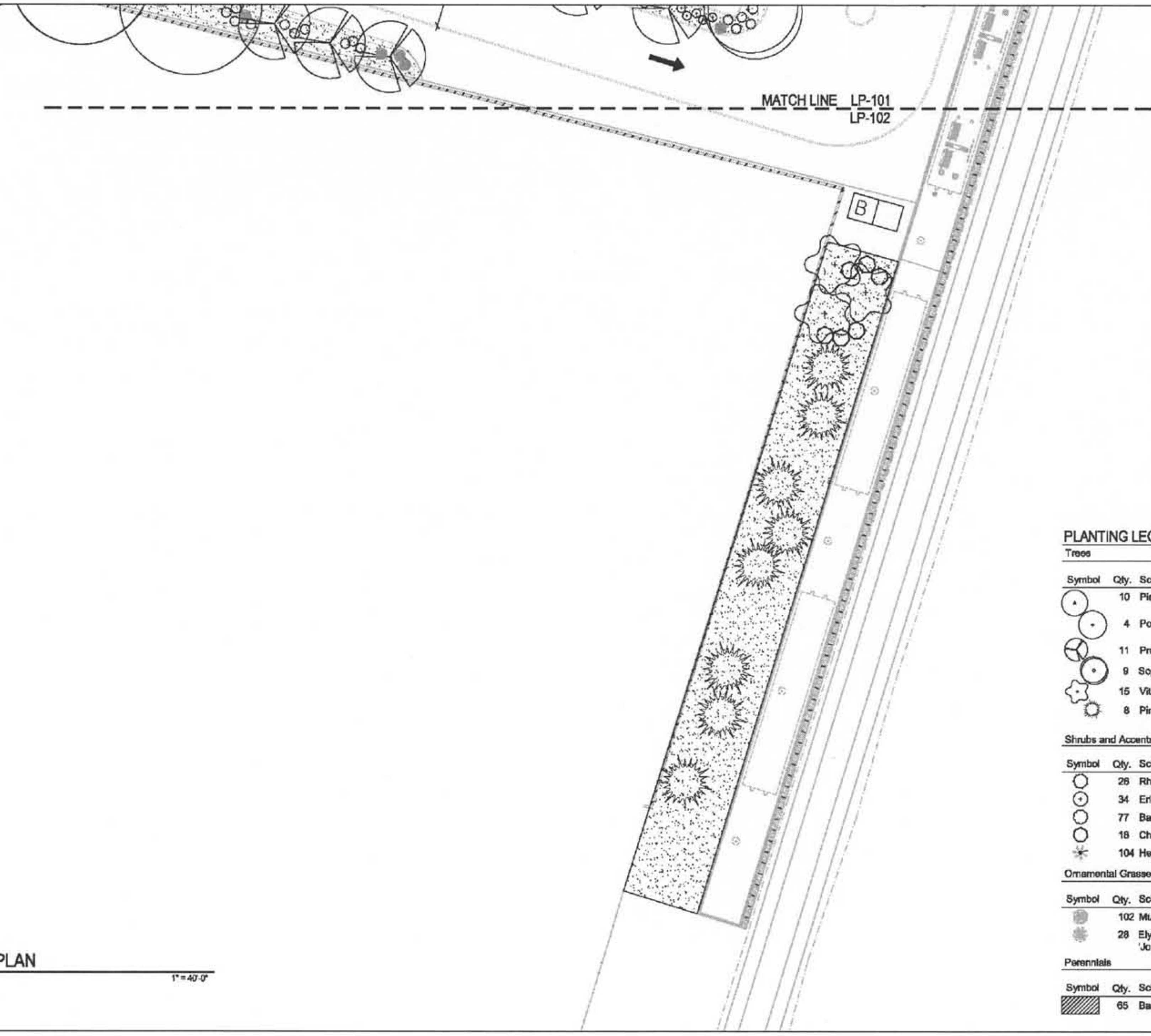
Dekker/Perich/Sabatini

CITY OF ALBUQUERQUE
DEPARTMENT OF MUNICIPAL DEVELOPMENT
ENGINEERING DIVISION

TITLE: **NEW MEXICO RAIL RUNNER EXPRESS MONTANO STATION LANDSCAPE PLAN - NORTH**

Design Review Committee | City Engineer Approval | Last Design Update | Mo. / Day / Yr. | Mo. / Day / Yr.

City Project No. 559282 | Zone Map No. F-15 | Sheet 12 Of 46



N

LANDSCAPE PLAN

1" = 40'-0"

LEGEND

Symbol	Qty.	Description
▨	26,245 SF	Rock Mulch

PLANTING LEGEND

Trees

Symbol	Qty.	Scientific Name	Common Name	Installed Size	Mature Height	Mature Width	Water Requirements
⊙	10	Pistacia chinensis	CHINESE PISTACHE	2.5" Cal. (B&B)	60.00	60.00	Medium Water Use
⊙	4	Populus fremontii wislizenii	RIO GRANDE COTTONWOOD	3" Cal.	50.00	60.00	Medium Water Use
⊗	11	Prosopis glandulosa	HONEY MESQUITE	40" Box	25.00	30.00	Low Water Use
⊙	9	Sophora japonica	JAPANESE PAGODA TREE	2.5" Cal. (B&B)	35.00	35.00	Medium Water Use
✿	15	Vitex agnus-castus	CHASTE TREE	40" Box	20.00	20.00	Medium Water Use
✹	8	Pinus eldarica	AFGHAN PINE	12' Ht.	40.00	18.00	Medium Water Use

Shrubs and Accents

Symbol	Qty.	Scientific Name	Common Name	Installed Size	Mature Height	Mature Width	Water Requirements
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○	77	Baccharis x 'Starn'	THOMPSON BROOM	5 Gal.	3.50	3.50	Low Water Use
○	18	Chamaebatiera millifolium	FERNBUSH	5 Gal.	6.00	6.00	Low Water Use
✳	104	Hesperaloe parviflora	RED YUCCA	5 Gal.	3.00	4.00	Low Water Use

Ornamental Grasses

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Perennials

Symbol	Qty.	Scientific Name	Common Name	Installed Size	Mature Height	Mature Width	Water Requirements
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CONTRACTOR
WORK STAKED BY DATE
INSPECTOR'S ACCEPTANCE BY DATE
FIELD VERIFICATION BY DATE
DRAWINGS CORRECTED BY DATE
MICRO-FILM INFORMATION
RECORDED BY DATE
NO.

BENCH MARKS
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DATUM NAVD 1988
ELEV. 4978.070

SURVEY INFORMATION
FIELD NOTES

NO.	BY	DATE

ENGINEER'S SEAL

REVISIONS

NO.	DATE	REMARKS	BY

DESIGN
DESIGNED BY DATE
DRAWN BY DATE
CHECKED BY DATE

Dekker/Perich/Sabatini

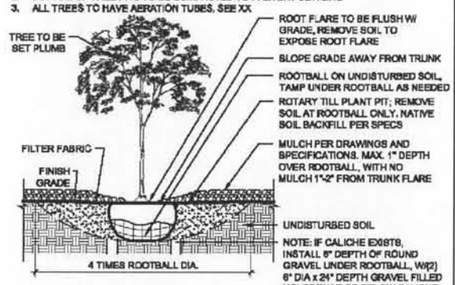
CITY OF ALBUQUERQUE
DEPARTMENT OF MUNICIPAL DEVELOPMENT
ENGINEERING DIVISION

TITLE: **NEW MEXICO RAIL RUNNER EXPRESS MONTANO STATION LANDSCAPE PLAN - SOUTH**

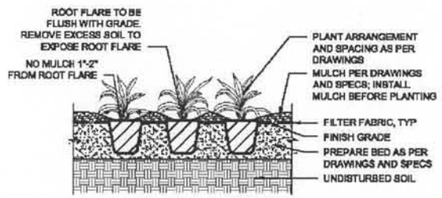
Design Review Committee | City Engineer Approval | Last Design Update | No. / Day / Yr. | No. / Day / Yr.

City Project No. 559282 | Zone Map No. F-15 | Sheet 13 Of 46

NOTES:
1. PRIOR TO BACKFILLING, ALL MATERIAL SUCH AS CONTAINERS, WIRE, BURLAP, AND ROPE SHALL BE REMOVED AS COMPLETELY AS POSSIBLE, WHILE STILL PROTECTING THE INTEGRITY OF THE ROOTBALL
2. BOTTOM OF TREE PITS TO BE COMPACTED TO PREVENT SETTLING
3. ALL TREES TO HAVE AERATION TUBES, SEE XX

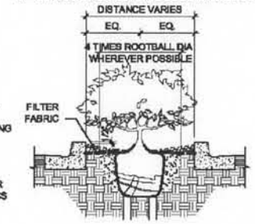


C5 TREE



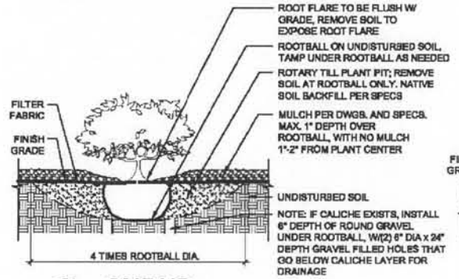
B2 LANDSCAPE BED

NOTES:
1. PRIOR TO BACKFILLING, ALL MATERIAL SUCH AS CONTAINERS, WIRE, BURLAP, AND ROPE SHALL BE REMOVED AS COMPLETELY AS POSSIBLE, WHILE STILL PROTECTING THE INTEGRITY OF THE ROOTBALL
2. SHRUB PLANTING TRENCH TO EXTEND 6'-0" TO EACH SIDE OF SHRUB, PARALLEL TO CURB
3. BOTTOM OF SHRUB PIT TO BE COMPACTED TO PREVENT SETTLING



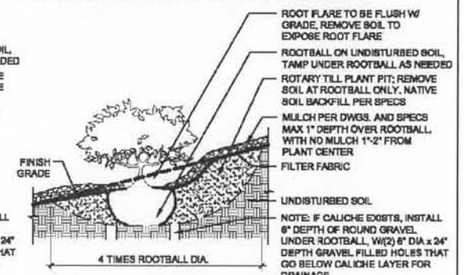
B3 SHRUB IN MEDIAN

NOTES:
1. PRIOR TO BACKFILLING, ALL MATERIAL SUCH AS CONTAINERS, WIRE, BURLAP, AND ROPE SHALL BE REMOVED AS COMPLETELY AS POSSIBLE, WHILE STILL PROTECTING THE INTEGRITY OF THE ROOTBALL
2. BOTTOM OF SHRUB PITS TO BE COMPACTED TO PREVENT SETTLING



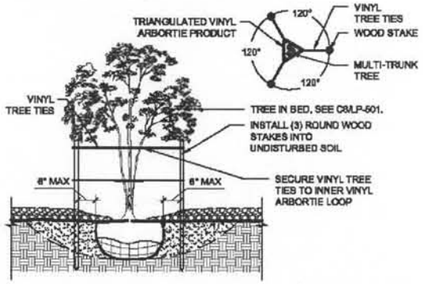
B4 SHRUB

NOTES:
1. PRIOR TO BACKFILLING, ALL MATERIAL SUCH AS CONTAINERS, WIRE, BURLAP, AND ROPE SHALL BE REMOVED AS COMPLETELY AS POSSIBLE, WHILE STILL PROTECTING THE INTEGRITY OF THE ROOTBALL
2. BOTTOM OF SHRUB PITS TO BE COMPACTED TO PREVENT SETTLING



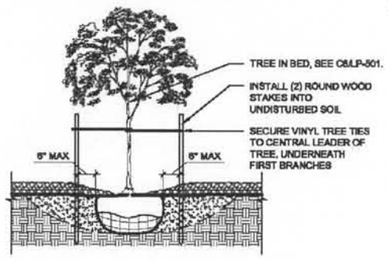
B5 SHRUB ON SLOPE

NOTES:
1. VINYL TREE TIES TO BE CINCH-TIE 32" OR APPROVED EQUAL, AVAILABLE FROM V.I.T. PRODUCTS - 800-729-1314
2. STAKING TO BE REMOVED AT THE END OF WARRANTY PERIOD



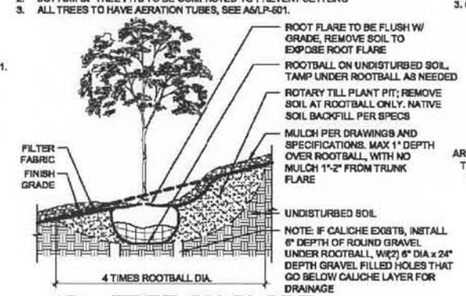
A1 MULTI-TRUNK TREE STAKING

NOTES:
1. VINYL TREE TIES TO BE CINCH-TIE 32" OR APPROVED EQUAL, AVAILABLE FROM V.I.T. PRODUCTS - 800-729-1314
2. STAKING TO BE REMOVED AT THE END OF WARRANTY PERIOD



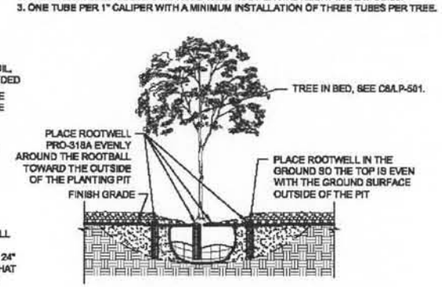
A2 TREE STAKING

NOTES:
1. PRIOR TO BACKFILLING, ALL MATERIAL SUCH AS CONTAINERS, WIRE, BURLAP, AND ROPE SHALL BE REMOVED AS COMPLETELY AS POSSIBLE, WHILE STILL PROTECTING THE INTEGRITY OF THE ROOTBALL
2. BOTTOM OF TREE PITS TO BE COMPACTED TO PREVENT SETTLING
3. ALL TREES TO HAVE AERATION TUBES, SEE A5/LP-501.



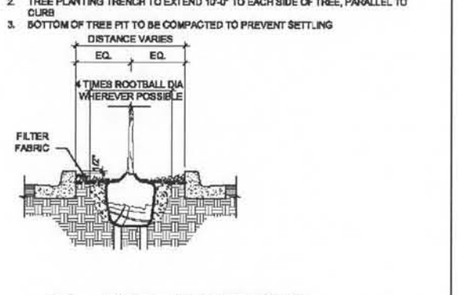
A3 TREE ON SLOPE

NOTE:
1. AERATION TUBES SHALL BE ROOTWELL PRO-318A OR APPROVED EQUAL WWW.ROOTWELL.COM
2. AERATION TUBES USED TO IMPROVE TREE ROOT ENVIRONMENT IN CLAY SOILS.
3. ONE TUBE PER 1" CALIPER WITH A MINIMUM INSTALLATION OF THREE TUBES PER TREE.



A4 TREE AERATION TUBES

NOTES:
1. PRIOR TO BACKFILLING, ALL MATERIAL SUCH AS CONTAINERS, WIRE, BURLAP, AND ROPE SHALL BE REMOVED AS COMPLETELY AS POSSIBLE, WHILE STILL PROTECTING THE INTEGRITY OF THE ROOTBALL
2. TREE PLANTING TRENCH TO EXTEND 10'-0" TO EACH SIDE OF TREE, PARALLEL TO CURB
3. BOTTOM OF TREE PIT TO BE COMPACTED TO PREVENT SETTLING



A5 TREE IN MEDIAN

AS BUILT INFORMATION
CONTRACTOR
WORK STARTED BY — DATE
INSPECTORS ACCEPTANCE BY — DATE
VERIFICATION BY — DATE
DRAWINGS PREPARED BY — DATE

BENCH MARKS
NGS STAINLESS ROD SET BENEATH A 5 1/2" ACCESS COVER STAMPED "D-438, 1984", SE QUADRANT OF MONTANO RD. & THE BNSF RAILROAD TRACKS, 42.5 FT. EAST OF CENTERLINE OF THE TRACKS, 44 FT. SOUTH OF CENTERLINE OF MONTANO RD. NE, 1.1 FT. WEST OF CHAIN LINK FENCE.
DATUM NAVD 1988
ELEV. 4978.070

MICRO-FILM INFORMATION
RECORDED BY — DATE
NO.

SURVEY INFORMATION
FIELD NOTES
NO. | BY | DATE

ENGINEER'S SEAL

REVISIONS
NO. | DATE | REMARKS | BY

DESIGNED BY — DATE
DRAWN BY — DATE
CHECKED BY — DATE

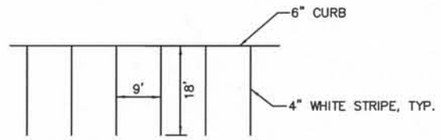
CITY OF ALBUQUERQUE
DEPARTMENT OF MUNICIPAL DEVELOPMENT
ENGINEERING DIVISION

TITLE: **NEW MEXICO RAIL RUNNER EXPRESS MONTANO STATION LANDSCAPE DETAILS**

Design Review Committee | City Engineer Approval | Last Design Update | No. / Dsg / Yr. | No. / Dsg / Yr.

Dekker/Perich/Sabatini

City Project No. 559282 | Zone Map No. F-15 | Sheet 14 Of 46

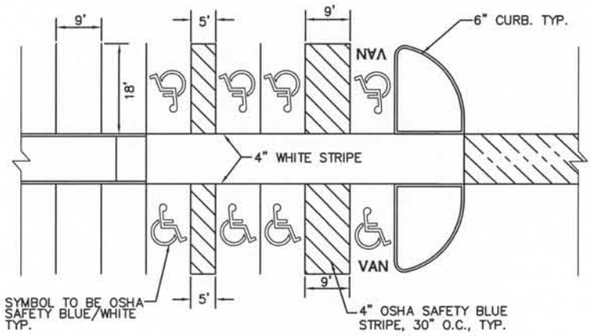


PARKING LOT STALL STRIPING
SCALE 1"=20'

R5-1

R1-1

R2-1-24-15



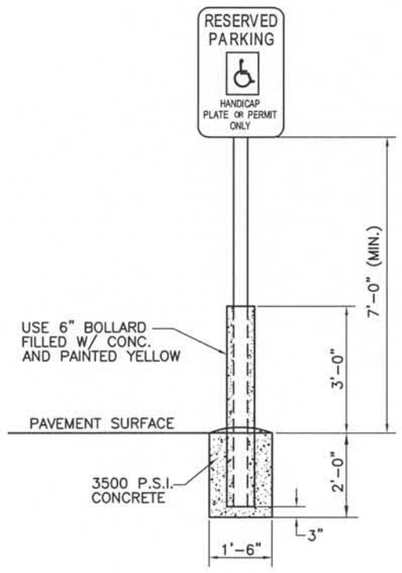
HANDICAP STRIPING AREA
SCALE 1"=20'

SPECIAL SIGN

R8-3a

INSTALL CUSTOM
SIGN AND POLE
PROVIDED BY OWNER

INSTALL CUSTOM SIGN AND POLE
PROVIDED BY OWNER



SPECIAL SIGN

W16-1

W11-1

SURVEY INFORMATION
FIELD NOTES

NO.	BY	DATE

BENCH MARKS

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DATUM NAVD 1988
ELEV. 4978.070

AS BUILT INFORMATION

CONTRACTOR — DATE
WORK STARTED BY — DATE
INSPECTOR'S ACCEPTANCE BY — DATE
AS-BUILTS VERIFICATION BY — DATE
DRAWINGS CORRECTED BY — DATE
MICRO-FILM INFORMATION
RECORDED BY — DATE
NO.

ENGINEER'S SEAL

REVISIONS

NO.	DATE	BY	REMARKS
			DESIGN
			DESIGNED BY SL — DATE 10/2010
			DRAWN BY BN — DATE 10/2010
			CHECKED BY KA — DATE 10/2010

CITY OF ALBUQUERQUE
DEPARTMENT OF MUNICIPAL DEVELOPMENT
ENGINEERING DIVISION

TITLE: **NEW MEXICO RAIL RUNNER EXPRESS**
MONTANO STATION
PERMANENT SIGNING SIGN FACE DETAILS

Design Review Committee | City Engineer Approval | Last Design Update | No. / Day / Yr. | No. / Day / Yr.

City Project No. 559282 | Zone Map No. F-15 | Sheet 15 Of 46

Vector Engineering, LLC

SIGNING AND STRIPING GENERAL NOTES

1. ALL SIGNS, UNLESS OTHERWISE SPECIFIED, SHALL HAVE REFLECTIVITY III SHEETING FOR THE LEGEND, BORDER, AND BACKGROUND. ONLY ALUMINUM PANEL SIGNS ARE PERMITTED.

2. QUANTITIES MAY VARY AS FIELD CONDITIONS DICTATE. THE CONTRACTOR WILL BE PAID FOR ACTUAL QUANTITIES USED.

3. ALL TRAFFIC CONTROL DEVICES SHALL COMPLY WITH THE NMDOT STANDARD SPECIFICATIONS FOR HIGHWAY AND BRIDGE CONSTRUCTION (2007 EDITION), ANY APPLICABLE SPECIAL PROVISIONS AND/OR SUPPLEMENTAL SPECIFICATIONS, AND ALSO THE CURRENT EDITION, WITH REVISIONS, OF THE M.U.T.C.D.

4. EACH SIGN FACE SHOWN ON PLANS SHALL MEET THE SPECIFICATIONS IN THE STANDARD HIGHWAY SIGNS MANUAL (CURRENT EDITION) FOR PROPER ARRANGEMENT, SPACING OF LETTERS, LETTER HEIGHT, LETTER SERIES, SYMBOLS, AND BORDERS FOR THE SPECIFIED SIZE AND MESSAGE AS SHOWN ON PLANS.

5. POST LENGTHS ARE BASED ON A MINIMUM OF 7 FT. FOR URBAN SECTIONS. SEE NMDOT STANDARD DRAWINGS 701-02 AND 701-03 AT THE END OF PLAN SET SECTION 7.

6. THE LATERAL CLEARANCE OF SIGNS SHALL BE NO CLOSER THAN 2 FT. FROM FACE OF CURB.

7. ALL SIGNING HARDWARE, INCLUDING BRACKETS, IS CONSIDERED INCIDENTAL TO SIGN INSTALLATION. NO PAYMENT WILL BE MADE THEREFOR.

ENGINEER'S SEAL

SURVEY INFORMATION

FIELD NOTES

NO.	BY	DATE

BENCH MARKS

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DATUM: NAVD 1988
ELEV. 4978.070

AS BUILT INFORMATION

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INSPECTOR'S ACCEPTANCE BY — DATE
FIELD VERIFICATION BY — DATE
DRAWINGS CORRECTED BY — DATE

MICRO-FILM INFORMATION

RECORDED BY — DATE
NO.

NO.	DATE	REMARKS

REVISIONS

DESIGN

DESIGNED BY SL 10/2010
DRAWN BY BN 10/2010
CHECKED BY KA 10/2010

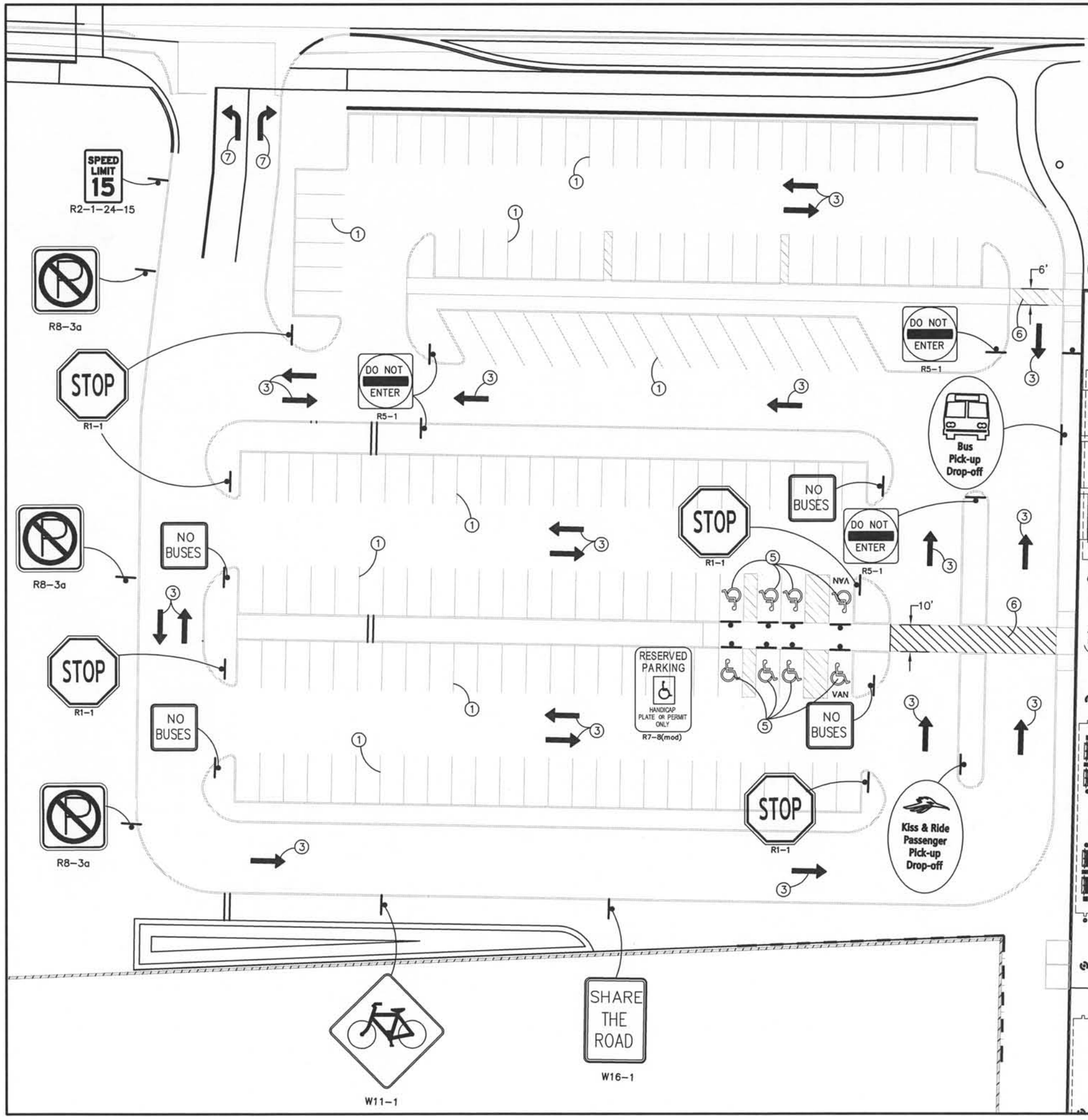
CITY OF ALBUQUERQUE
DEPARTMENT OF MUNICIPAL DEVELOPMENT
ENGINEERING DIVISION

TITLE: **NEW MEXICO RAIL RUNNER EXPRESS**
MONTANO STATION
PERMANENT SIGNING DETAILS & NOTES

Design Review Committee | City Engineer Approval | Last Design Update

No. / Dsg / Yr.	No. / Dsg / Yr.

City Project No. **559282** | Zone Map No. **F-15** | Sheet **16** Of **46**



GRAPHIC SCALE

40 0 20 40 80

(IN FEET)
1 inch = 40 ft.

LEGEND

1. 4" SOLID WHITE PAINTED STRIPE
2. HANDICAP STRIPING
3. WHITE PAINTED ARROW
4. 12" WHITE STOP BAR
5. PAVEMENT MARKING: INTERNATIONAL SYMBOL OF ACCESSIBILITY
6. CROSSWALK, 4" SOLID WHITE STRIPING, LADDER STRIPING AT 30" O.C.
7. 4" SOLID WHITE PAINTED LEFT/RIGHT ARROW

SIGN	MUTCD CODE	SIZE	QTY
STOP SIGN	R1-1	30" x 30"	5
SPEED LIMIT (15)	R2-1-24-15	24" x 30"	1
DO NOT ENTER	R5-1	30" x 30"	5
HANDICAP PARKING	R7-8 (MOD)	12" x 18"	8
NO PARKING	R8-3a	12" x 12"	2
BICYCLE TRAFFIC	W11-1	24" x 24"	1
SHARE THE ROAD	W16-1	24" x 18"	1
NO BUSES	SPECIAL	24" x 18"	4

AS BUILT INFORMATION

CONTRACTOR — DATE
WORK STAKED BY — DATE
INSPECTOR'S ACCEPTANCE BY — DATE
FIELD VERIFICATION BY — DATE
DRAWINGS CORRECTED BY — DATE

MICRO-FILM INFORMATION

RECORDED BY — DATE
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DATUM: NAVD 1988
ELEV. 4978.070

SURVEY INFORMATION

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NO.	BY	DATE

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REVISIONS

NO.	DATE	REMARKS	BY	DATE
		DESIGN	SL	10/2010
			BN	10/2010
			KA	10/2010

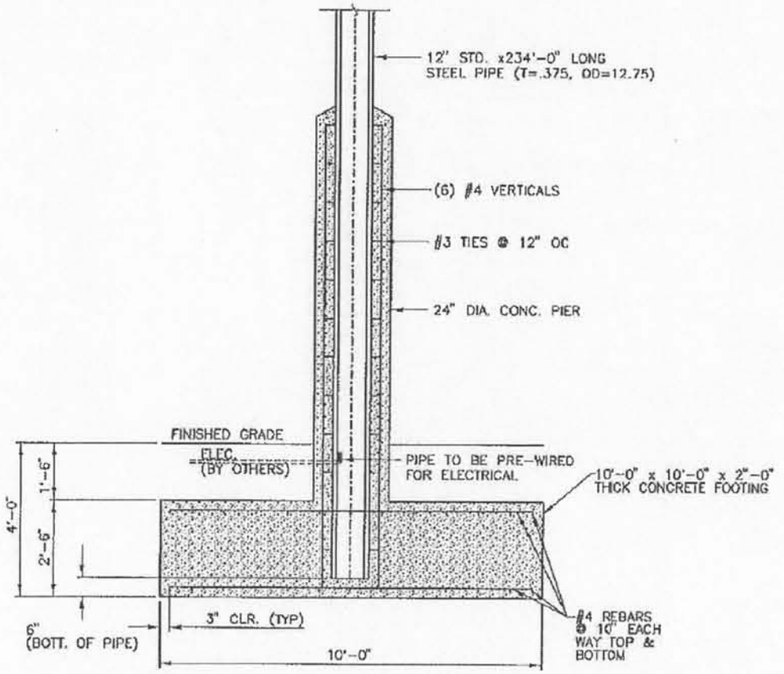
DESIGNED BY SL
DRAWN BY BN
CHECKED BY KA

CITY OF ALBUQUERQUE
DEPARTMENT OF MUNICIPAL DEVELOPMENT
ENGINEERING DIVISION

TITLE: **NEW MEXICO RAIL RUNNER EXPRESS MONTANO STATION PERMANENT SIGNING & STRIPING PLAN**

Design Review Committee | City Engineer Approval | Last Design Update | No. / Day / Yr. | No. / Day / Yr.

City Project No. **559282** | Zone Map No. **F-15** | Sheet **17** Of **46**



ALTERNATIVE FOOTING DETAIL
SCALE: NONE

x
—

ENGINEER'S SEAL

SURVEY INFORMATION
FIELD NOTES

NO.	BY	DATE

BENCH MARKS
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DATUM NAVD 1988
ELEV. 4978.070

AS BUILT INFORMATION
CONTRACTOR
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INSPECTORS ACCEPTANCE BY DATE
FIELD VERIFICATION BY DATE
DRAWINGS CORRECTED BY DATE
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NO.

NO.	DATE	REMARKS	BY
		REVISIONS	
		DESIGN	

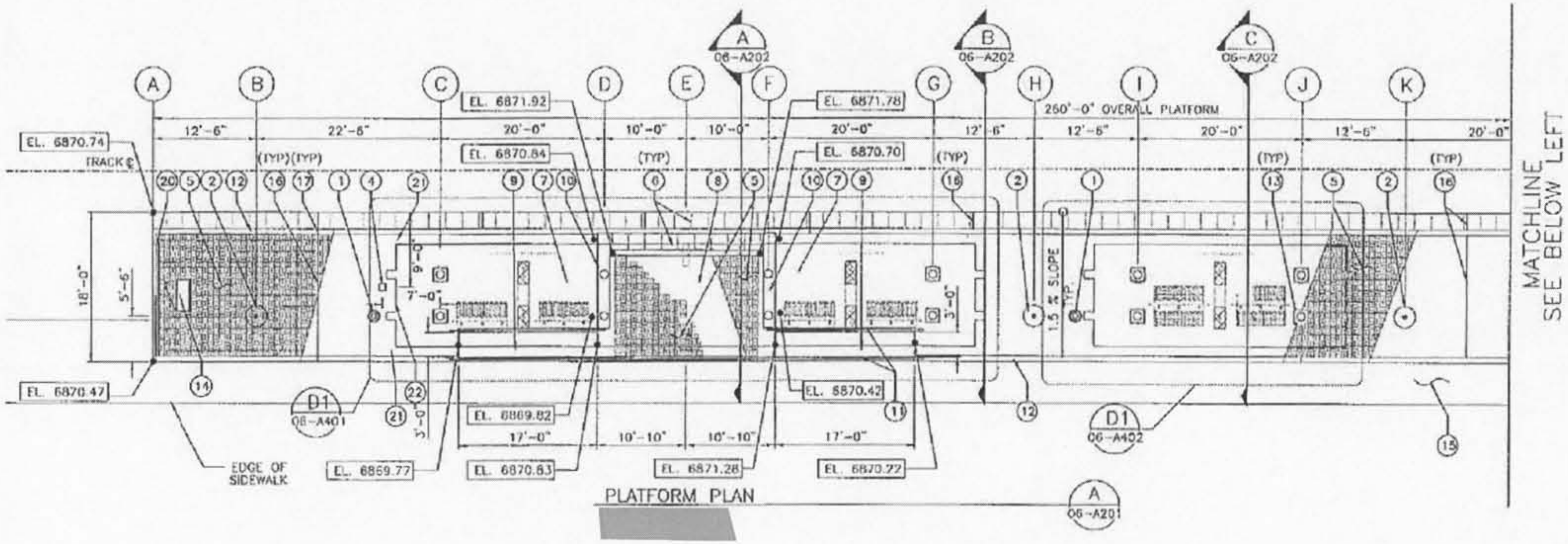
DESIGNED BY SL DATE 10/2010
DRAWN BY BN DATE 10/2010
CHECKED BY KA DATE 10/2010

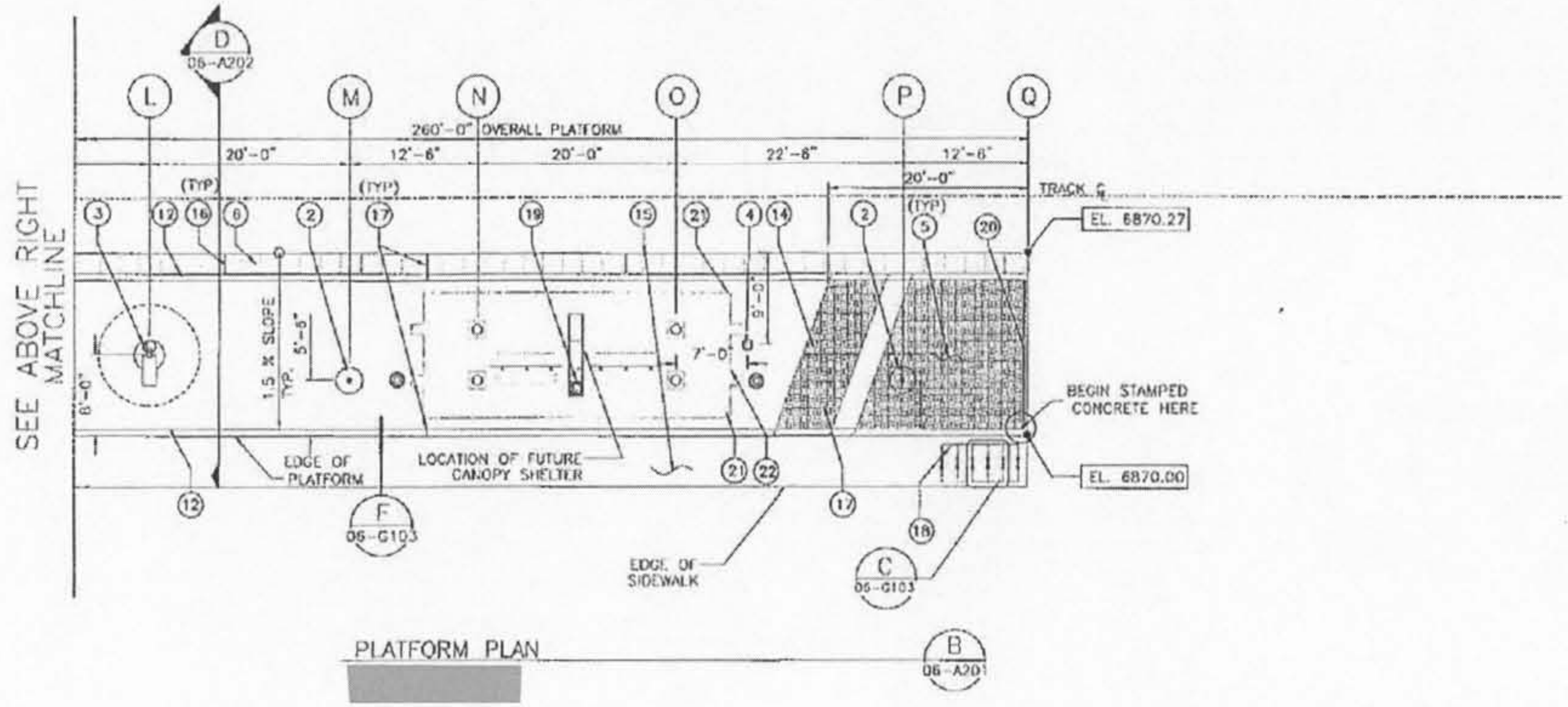
CITY OF ALBUQUERQUE
DEPARTMENT OF MUNICIPAL DEVELOPMENT
ENGINEERING DIVISION

TITLE: **NEW MEXICO RAIL RUNNER EXPRESS**
MONTANO STATION
ALTERNATIVE FOOTING DETAIL

Design Review Committee	City Engineer Approval	Last Design Update	No. / Day / Yr.	No. / Day / Yr.

City Project No.	Zone Map No.	Sheet	Of
559282	F-15	18	46



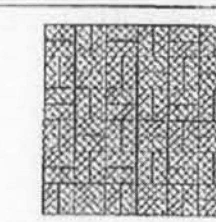


◯	PLATFORM KEYNOTES	DETAIL NO.
1	WASTE RECEPTACLE	C3/00-G039
2	LIGHT POLE	A3/00-G039
3	INFORMATION KIOSK	-
4	NON-FREEZE GROUND HYDRANT BOX FOR HOSE BIB	H/00-G103
5	COLORED, STAMPED CONCRETE	-
6	COMPOSITE SHELL TACTILE WARNING PANEL	A/00-G102
7	LOW PLATFORM	-
8	HIGH PLATFORM	-
9	CONCRETE RAMP	00-G101
10	CONCRETE STEPS	H/00-G103
11	1 1/2" ø SS PIPE HANDRAIL	B-C/00-G10 B/00-G103
12	CONCRETE TRIM	-
13	CONCRETE BASE	-
14	ELECTRICAL CABINET	1/00-G039
15	CONC. SIDEWALK (SEE CIVIL)	-
16	CONTROL JOINT @ 20' O.C.	00-G102
17	CONSTRUCTION JOINT @ 60' O.C. TYPICAL	00-G102
18	BIKE RACK	C/00-G103
19	JUNCTION BOX LOCATION	SEE ELECT.
20	1 1/2" ø SS PIPE GUARDRAIL	B & D/ 00-G103
21	DOWNSPOUT SCUPPER, R-3956, CRATE, R-4859-Q, REFERENCE NEENA	
22	6" CMP	

GENERAL NOTES

A	FOR CANOPY STRUCTURE CONST. NOTES & DIMENSIONS SEE DWG. 06-A401/06-A402
B	SEE TRACK ALIGNMENT FOR FINAL PLATFORM STATIONING.
C	SEE CIVIL DRAWINGS FOR PLATFORM AND RAMP GRADES AND SPOT ELEVATIONS.
D	SEE ELECTRICAL DRAWINGS FOR PLATFORM ELECTRICAL ELEMENTS.
E	SEE MECHANICAL DRAWINGS FOR PLATFORM MECHANICAL ELEMENTS.
F	SEE LANDSCAPE DRAWINGS FOR LANDSCAPE AND IRRIGATION PLANS.

STAMPED CONCRETE PATTERN SCHEDULE



▨ BRICK PATTERN

NOMINAL SIZE = 4" x 8"

EL. 4890.50 SPOT ELEVATION LEGEND

AS BUILT INFORMATION
CONTRACTOR
WORK STAKED BY DATE
INSPECTOR'S ACCEPTANCE BY DATE
FIELD VERIFICATION BY DATE
DRAWINGS CORRECTED BY DATE

MICRO-FILM INFORMATION
RECORDED BY DATE
NO.

BENCH MARKS
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DATUM NAVD 1988
ELEV. 4978.070

SURVEY INFORMATION
FIELD NOTES
NO. | BY | DATE

ENGINEER'S SEAL

REVISIONS
NO. | DATE | REMARKS | BY

DESIGN
DESIGNED BY SL DATE 10/2010
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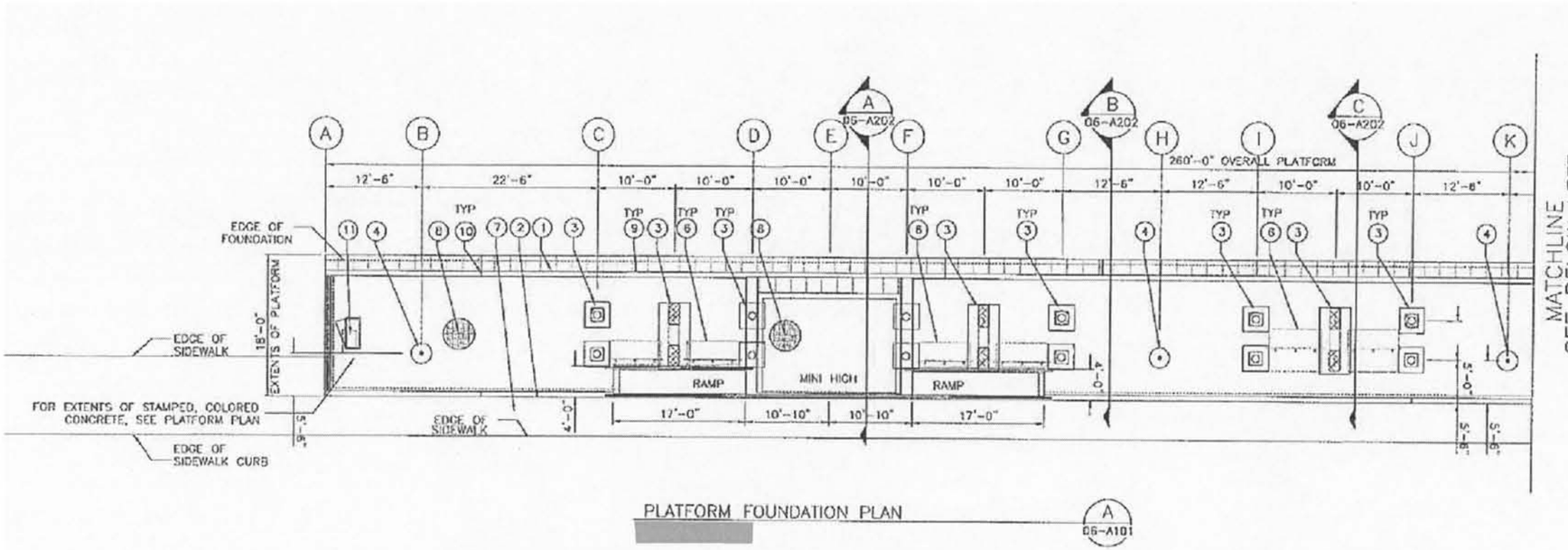
CHECKED BY KA DATE 10/2010

CITY OF ALBUQUERQUE
DEPARTMENT OF MUNICIPAL DEVELOPMENT
ENGINEERING DIVISION

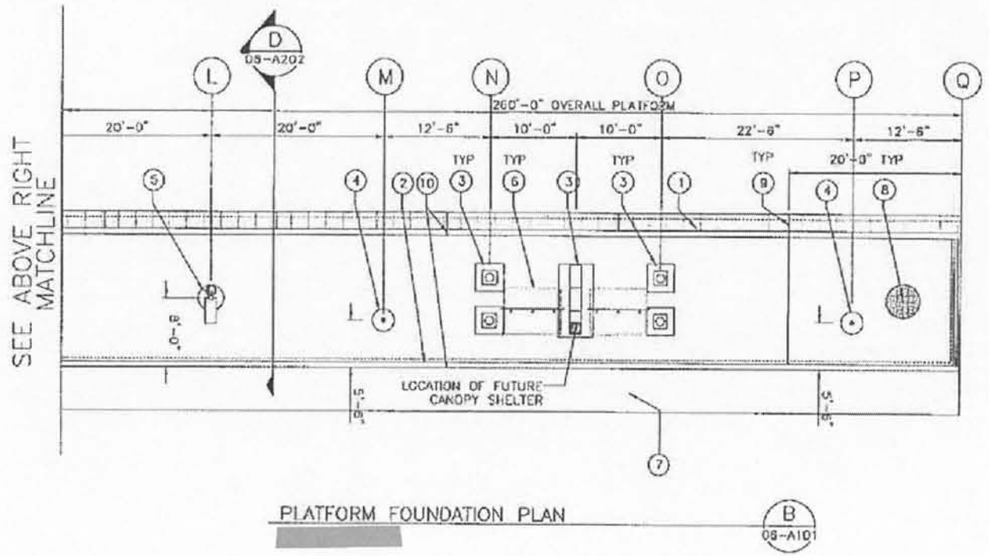
TITLE: **NEW MEXICO RAIL RUNNER EXPRESS**
MONTANO STATION
STATION PLATFORM PLAN

Design Review Committee	City Engineer Approval	Last Design Update	Mo. / Day / Yr.	Mo. / Day / Yr.

City Project No. 559282 | Zone Map No. F-15 | Sheet 19 Of 46



PLATFORM FOUNDATION PLAN — A 05-A101



PLATFORM FOUNDATION PLAN — B 05-A101

	STRUCTURAL KEYNOTES	DETAIL NO
1	FOUNDATION WALL A	00-G104
2	FOUNDATION WALL B	00-G104
3	CANOPY COLUMN FOOTING	02/S111
4	LIGHTING FOOTING	--
5	KIOSK FOUNDATION DESIGN	--
6	CONCRETE PAD	--
7	CONC. SIDEWALK (SEE CIVIL)	--
8	COLORED, STAMPED CONCRETE	00-G102
9	CONTROL JOINT @ 20' O.C. TYP	00-G102
10	CONSTRUCTION JOINT @ 60' O.C. TYPICAL	00-G102
11	ELECTRICAL CABINET BASE	3/00-G103

GENERAL NOTES

A. FOR CANOPY STRUCTURE FOOTINGS SEE DWG. 02-S111

B. FOR PLATFORM FOUNDATION DETAILS SEE DWG. 00-G104

C. FOR MISCELLANEOUS CONCRETE PAD DETAILS SEE DWG. 00-G033, 00-G035 AND 00-G039

SURVEY INFORMATION

FIELD NOTES

NO.	BY	DATE

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ELEV. 4978.070

AS BUILT INFORMATION

CONTRACTOR	DATE
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AS-BUILT VERIFICATION BY	DATE
DRAWINGS CORRECTED BY	DATE

MICRO-FILM INFORMATION

RECORDED BY — DATE

NO.

ENGINEER'S SEAL

NO.	DATE	REMARKS	BY
		REVISIONS	
		DESIGN	

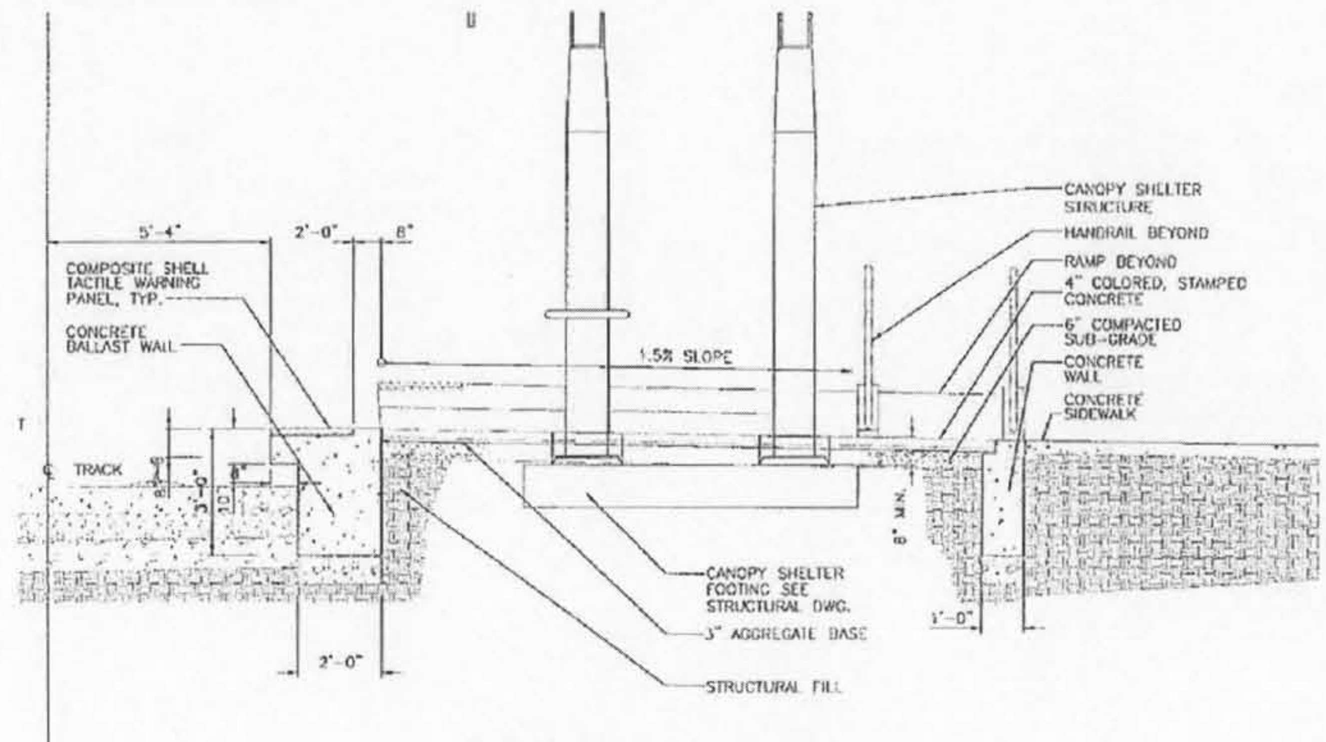
DESIGNED BY SL 10/2010
DRAWN BY BN 10/2010
CHECKED BY KA 10/2010

CITY OF ALBUQUERQUE
DEPARTMENT OF MUNICIPAL DEVELOPMENT
ENGINEERING DIVISION

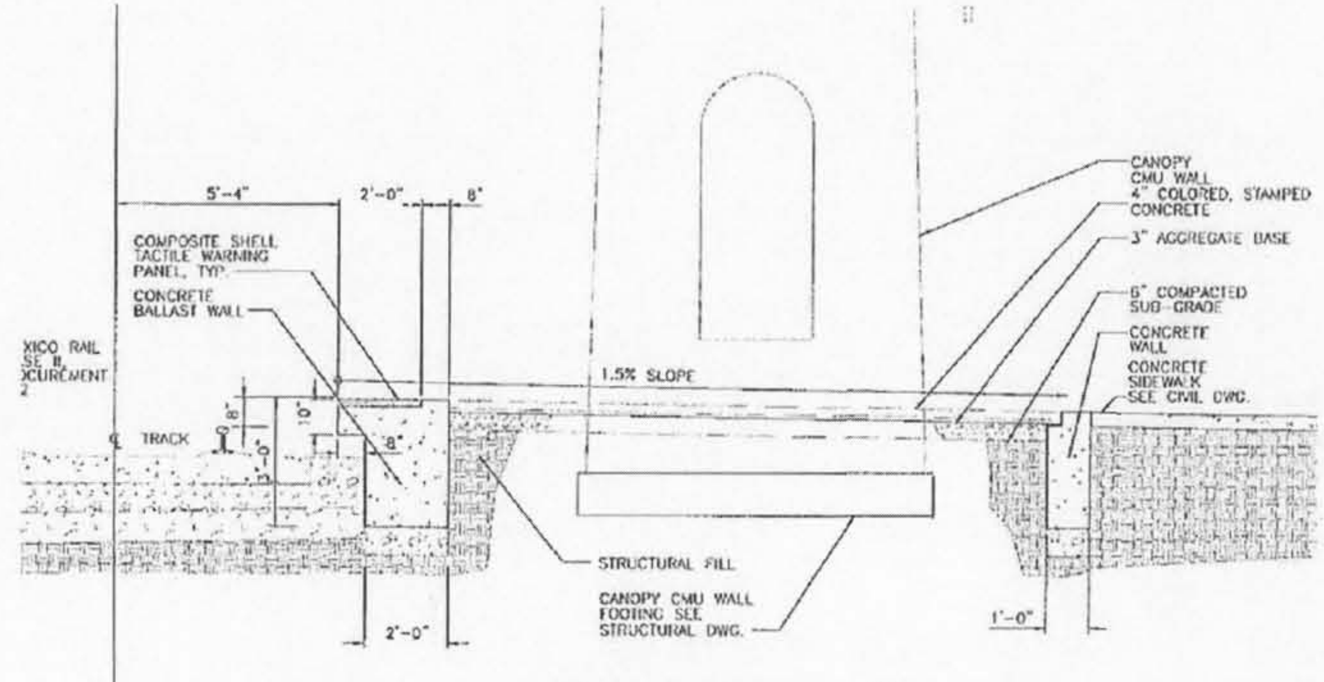
TITLE: **NEW MEXICO RAIL RUNNER EXPRESS**
MONTANO STATION
STATION PLATFORM FOUNDATION PLAN

Design Review Committee | City Engineer Approval | Last Design Update | No. / Dwg / Yr. | No. / Dwg / Yr.

City Project No. **559282** | Zone Map No. **F-15** | Sheet **20** Of **46**



PLATFORM SECTION @ MINI HIGH (A / 06-A201)



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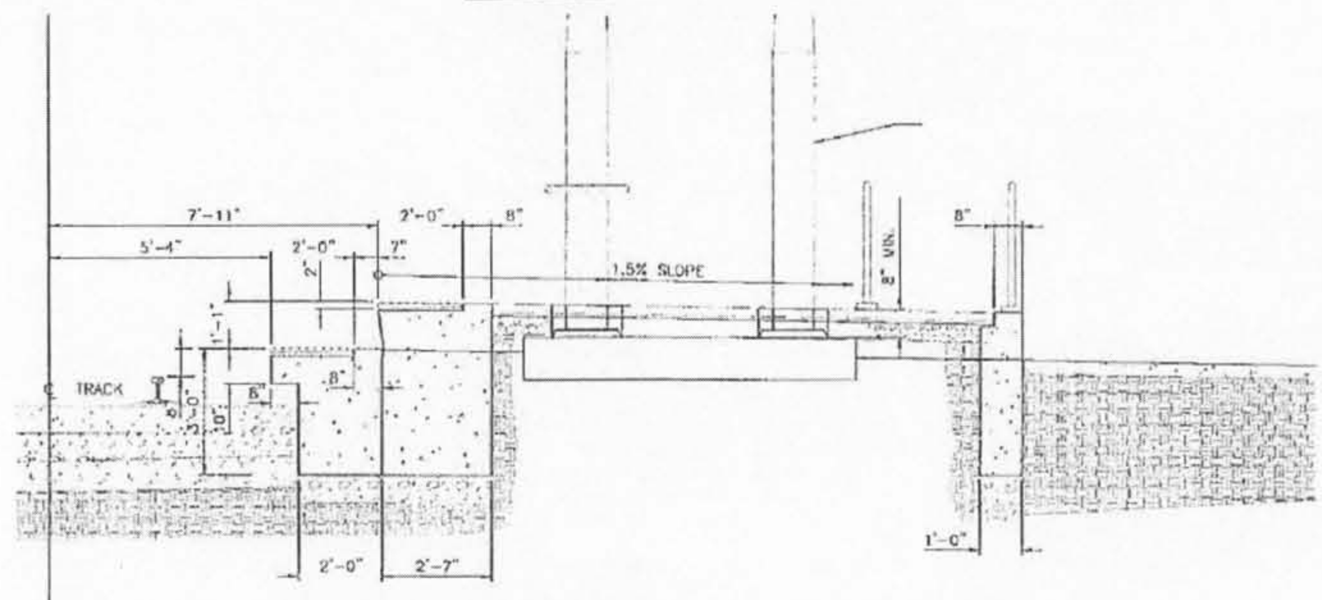

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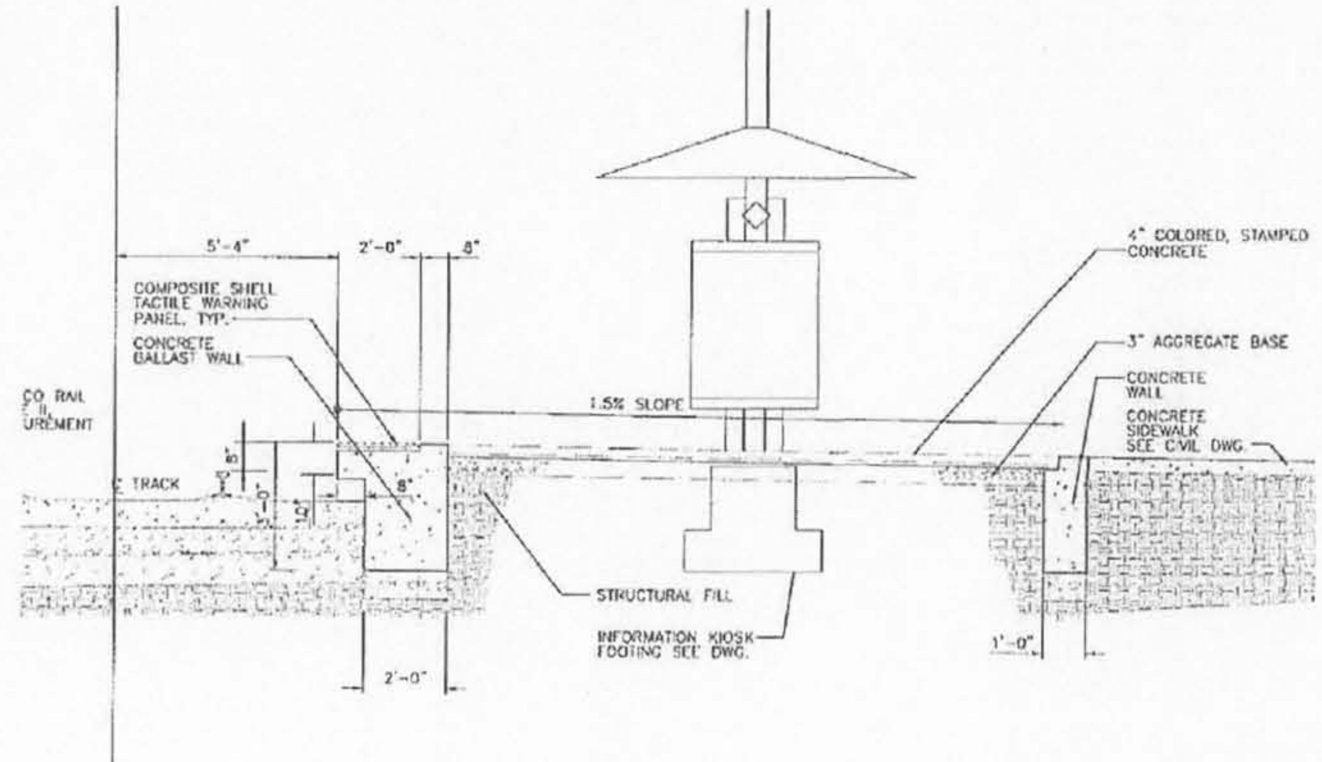


PLATFORM SECTION (D / 06-A201)

AS BUILT INFORMATION

CONTRACTOR
WORK STAKED BY — DATE
INSPECTOR'S ACCEPTANCE BY — DATE
FIELD VERIFICATION BY — DATE
DRAWINGS CORRECTED BY — DATE

MICRO-FILM INFORMATION

RECORDED BY — DATE
NO.

BENCH MARKS

NGS STAINLESS ROD SET BENEATH A 5 1/2" ACCESS COVER STAMPED "D-438, 1984", SE QUADRANT OF MONTANO RD. & THE BNSF RAILROAD TRACKS, 42.5 FT. EAST OF CENTERLINE OF THE TRACKS, 44 FT. SOUTH OF CENTERLINE OF MONTANO RD. NE, 1.1 FT. WEST OF CHAIN LINK FENCE.
DATUM NAVD 1988
ELEV. 4978.070

SURVEY INFORMATION

FIELD NOTES		
NO.	BY	DATE

ENGINEER'S SEAL

REVISIONS

NO.	DATE	REMARKS	BY

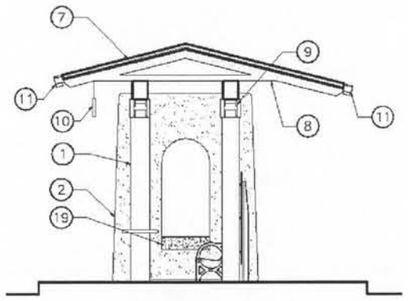
DESIGN
DESIGNED BY — DATE
DRAWN BY — DATE
CHECKED BY — DATE

CITY OF ALBUQUERQUE
DEPARTMENT OF MUNICIPAL DEVELOPMENT
ENGINEERING DIVISION

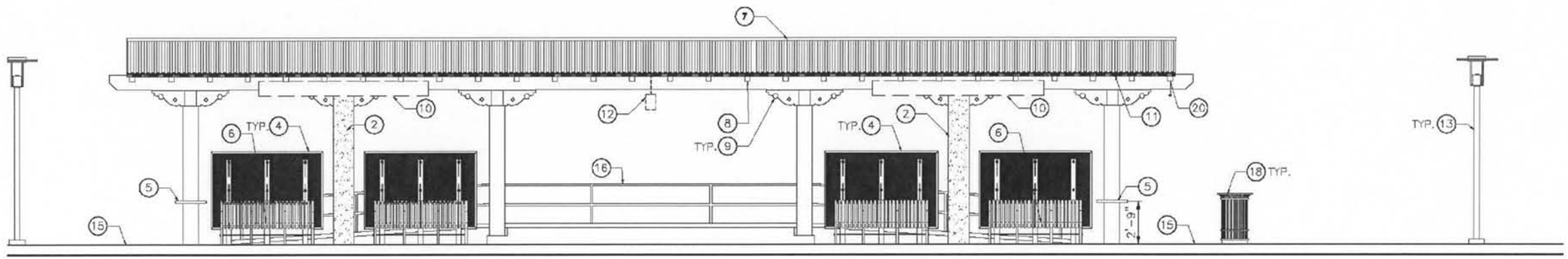
TITLE: **NEW MEXICO RAIL RUNNER EXPRESS MONTANO STATION STATION PLATFORM SECTIONS**

Design Review Committee	City Engineer Approval	Last Design Update	Mo. / Day / Yr.	Mo. / Day / Yr.

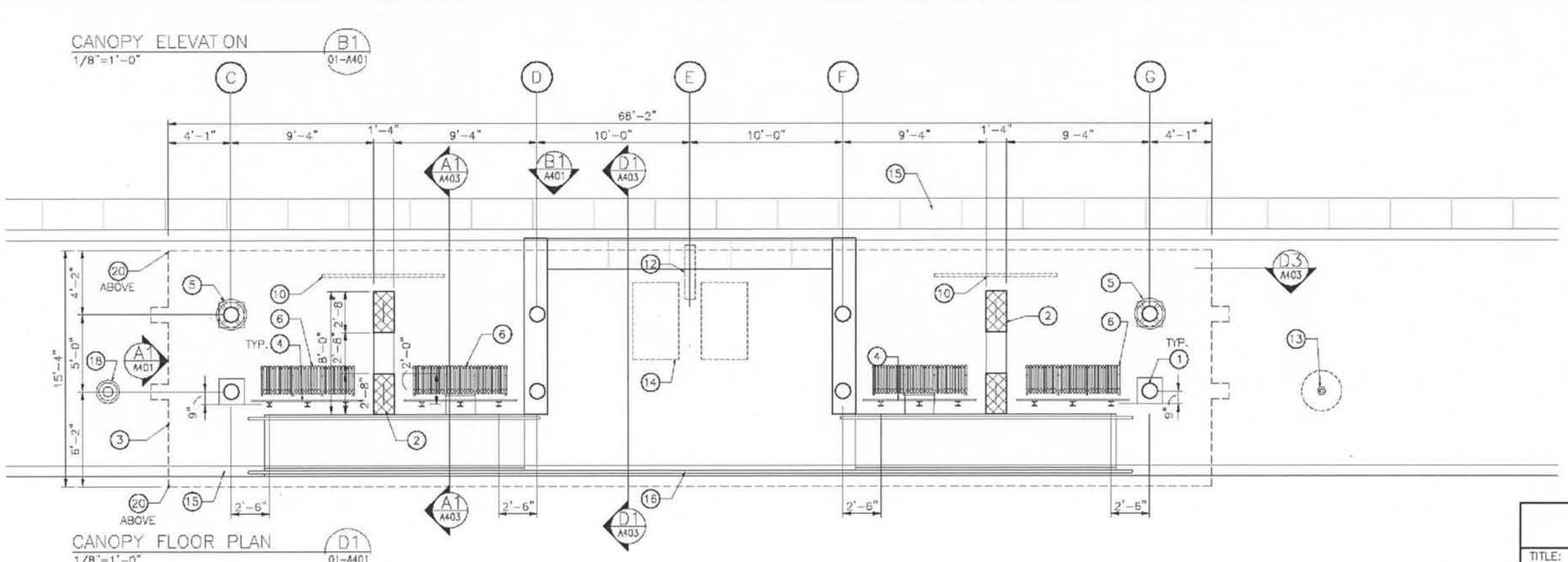
City Project No.	Zone Map No.	Sheet	Of
559282	F-15	21	46



CANOPY ELEVATION A1
1/8"=1'-0" 01-A401



CANOPY ELEVATION B1
1/8"=1'-0" 01-A401



CANOPY FLOOR PLAN D1
1/8"=1'-0" 01-A401

KEY NOTES

1. 10" DIAMETER STEEL COLUMN, PAINTED. RE: STRUCTURAL DWGS.
2. 8" CMU WALL WITH 3 COAT STUCCO FINISH.
3. LINE OF EDGE OF CANOPY ABOVE.
4. WIND SCREEN. SEE DETAIL A3/GN018.
5. STAINLESS STEEL LEAN RAIL ATTACHED TO STEEL COLUMN. SEE DETAIL D1/GN018.
6. SITE FURNISHINGS. BENCH. SEE DETAIL D1/GN018.
7. CORRUGATED METAL ROOF PANELS ON 5/8" EXPOSURE 1 PLYWOOD ON EPICORE DECK.
8. STEEL TUBE TRUSS, PAINTED.
9. PRECISION CUT STEEL PLATE CORBEL, PAINTED. RE: STRUCTURAL DWGS.
10. STATION ID SIGNAGE SUSPENDED FROM CANOPY STRUCTURE. RE: DETAIL D4/A403. FABRICATION AND INSTALL OF SIGNAGE UNDER SEPARATE CONTRACT, N.I.C.
11. CONTINUOUS RAIN GUTTER BOTH SIDES OF CANOPY, SLOPE TO END WITH OUTLET. RE: DETAIL A2/A403.
12. MESSAGE BOARD SIGNAGE. SUSPENDED FROM CANOPY STRUCTURE. RE: DETAIL D4/A403. FABRICATION AND INSTALL OF SIGNAGE UNDER SEPARATE CONTRACT, N.I.C.
13. LIGHT FIXTURE & POLE BASE AS SCHEDULED. RE: DETAIL A1/GN018.
14. WHEEL CHAIR SPACE
15. PLATFORM. REFER TO PLATFORM DRAWINGS.
16. STAINLESS STEEL HANDRAIL. RE: PLATFORM DWGS.
17. NOT USED.
18. SITE FURNISHINGS. TRASH RECEPTACLE. RE: DETAIL D3/GN018.
19. PRECAST CONCRETE SILL. RE: STRUCTURAL DWGS.
20. PROVIDE 4" DIA. BOTTOM OUTLET AT THIS END OF EACH GUTTER TO DIRECT WATER FLOW TO DRAIN INLET BELOW. EXTEND OUTLET 4" BELOW BOTTOM OF GUTTER AND TERMINATE.

SURVEY INFORMATION
FIELD NOTES

NO.	BY	DATE

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DATUM NAVD 1988
ELEV. 4978.070

AS BUILT INFORMATION

CONTRACTOR	DATE
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DRAWINGS CORRECTED BY	DATE

MICRO-FILM INFORMATION
RECORDED BY DATE
NO.

ENGINEER'S SEAL

NO.	DATE	REMARKS / REVISIONS	BY	DATE
		DESIGN	SL	10/2010
			BN	10/2010
			KA	10/2010

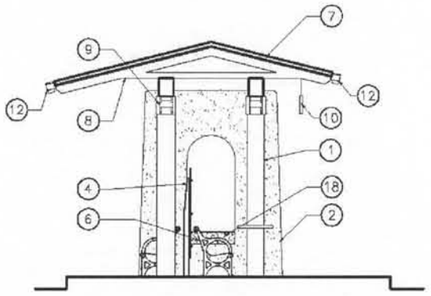
DESIGNED BY SL
DRAWN BY BN
CHECKED BY KA

CITY OF ALBUQUERQUE
DEPARTMENT OF MUNICIPAL DEVELOPMENT
ENGINEERING DIVISION

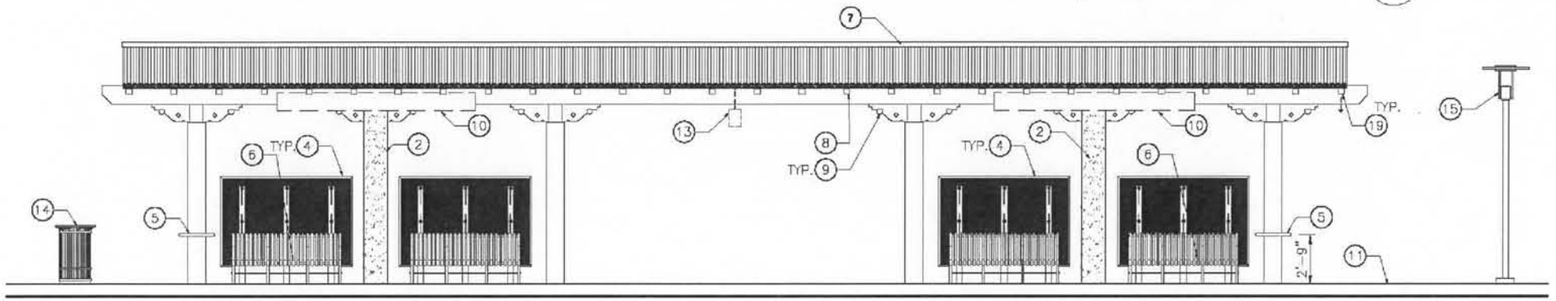
TITLE: **NEW MEXICO RAIL RUNNER EXPRESS**
MONTANO STATION
STATION CANOPY PLAN AND ELEVATION

Design Review Committee	City Engineer Approval	Last Design Update	No. / Day / Yr.	No. / Day / Yr.

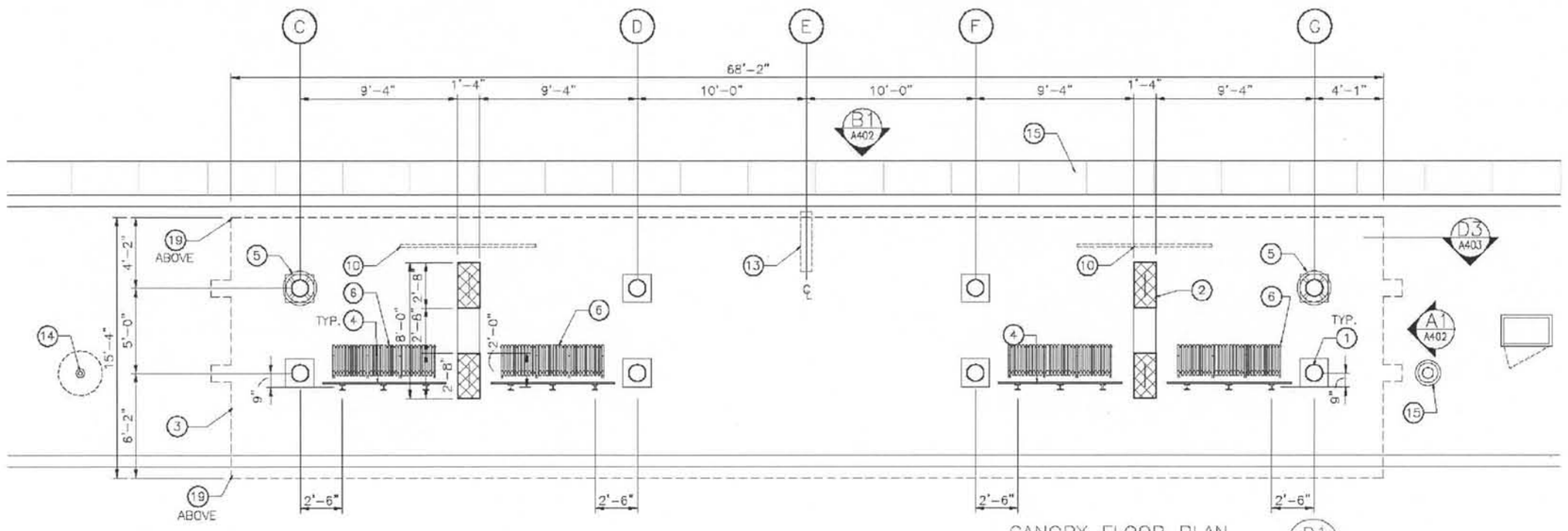
City Project No.	Zone Map No.	Sheet	Of
559282	F-15	22	46



CANOPY ELEVATION
1/8"=1'-0"
A1
01-A401



CANOPY ELEVATION
1/8"=1'-0"
B1
01-A401



CANOPY FLOOR PLAN
1/8"=1'-0"
D1
01-A401

KEY NOTES

1. 10" DIAMETER STEEL COLUMN, PAINTED. RE: STRUCTURAL DWGS.
2. 8" CMU WALL WITH 3 COAT STUCCO FINISH.
3. LINE OF EDGE OF CANOPY ABOVE.
4. WIND SCREEN. SEE DETAIL A3/GN018.
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14. SITE FURNISHINGS. TRASH RECEPTACLE. RE: DETAIL D3/GN018.
15. LIGHT POLE W/ FIXTURE AS SCHEDULED. RE: DETAIL A1/GN018.
16. NOT USED.
17. NOT USED.
18. PRECAST CONCRETE SILL. RE: STRUCTURAL DWGS.
19. PROVIDE 4" DIA. BOTTOM OUTLET AT THIS END OF EACH GUTTER TO DIRECT WATER FLOW TO DRAIN INLET BELOW. EXTEND OUTLET 4" BELOW BOTTOM OF GUTTER AND TERMINATE.

SURVEY INFORMATION

FIELD NOTES

NO.	BY	DATE

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NO.	DATE	REMARKS	BY
		REVISIONS	
		DESIGN	

DESIGNED BY SL — DATE 10/2010
DRAWN BY BN — DATE 10/2010
CHECKED BY KA — DATE 10/2010

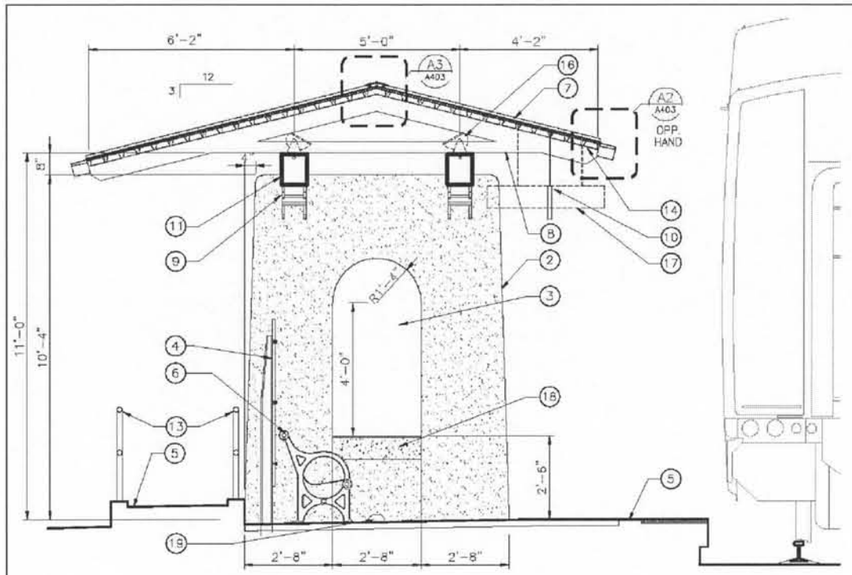
CITY OF ALBUQUERQUE
DEPARTMENT OF MUNICIPAL DEVELOPMENT
ENGINEERING DIVISION

TITLE: **NEW MEXICO RAIL RUNNER EXPRESS**
MONTANO STATION
STATION CANOPY PLAN AND ELEVATION

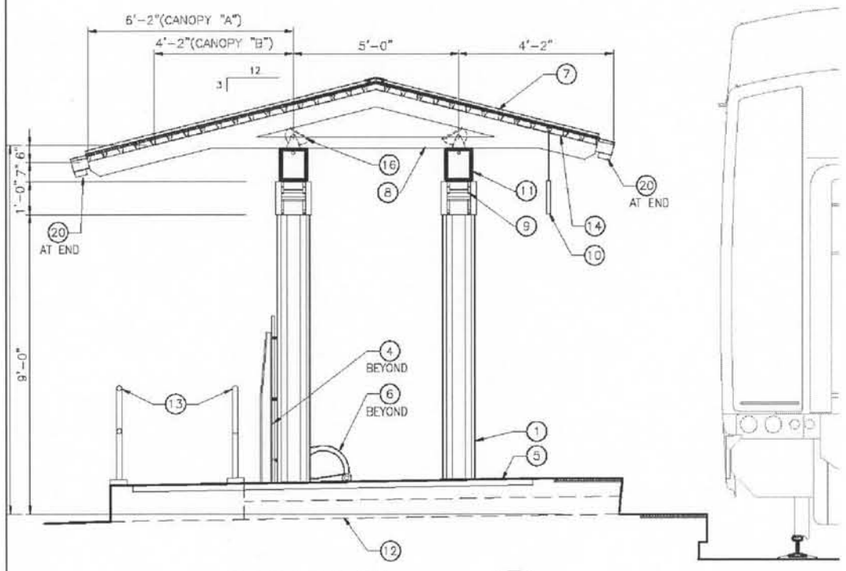
Design Review Committee | City Engineer Approval | Last Design Update

No. / Day / Yr.	No. / Day / Yr.

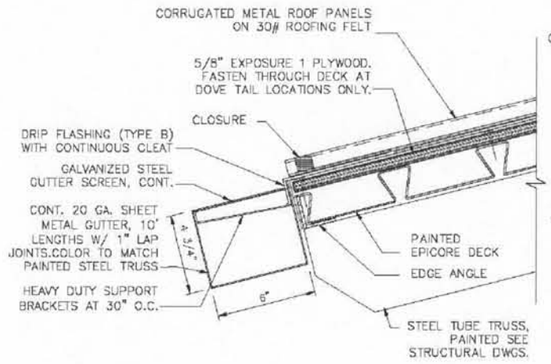
City Project No. **559282** | Zone Map No. **F-15** | Sheet **23** Of **46**



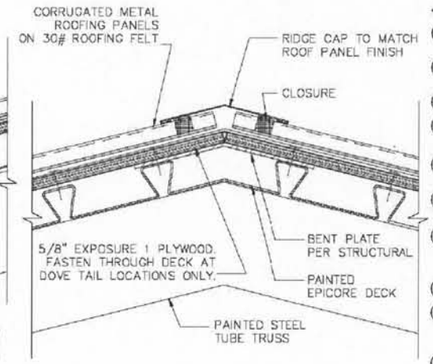
CANOPY SECTION A1
1/4"=1'-0" 01-A401



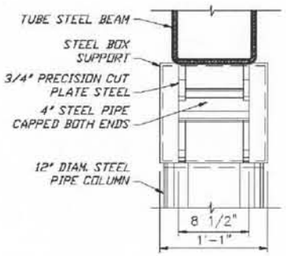
CANOPY SECTION D1
1/4"=1'-0" 01-A401



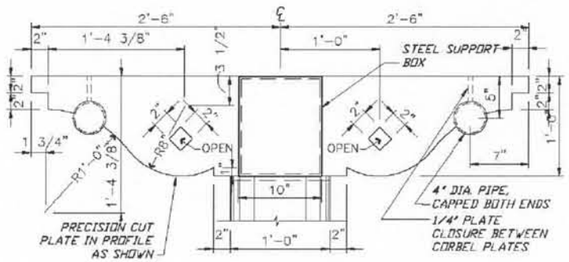
TYPICAL CANOPY EAVE A2
1 1/2"=1'-0" 01-?



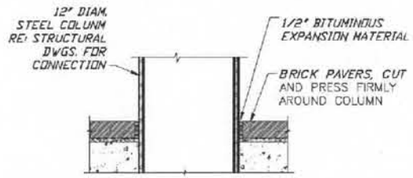
TYPICAL CANOPY RIDGE A3
1 1/2"=1'-0" 01-?



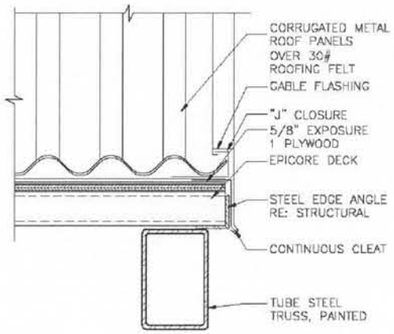
TYP. CORBEL ELEVATION C2
3/4"=1'-0" 01-?



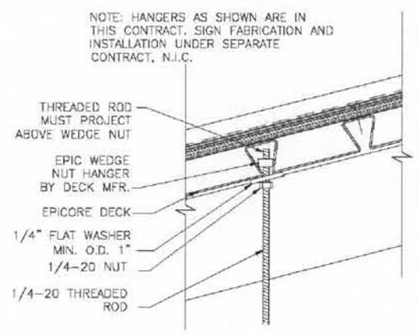
TYP. CORBEL ELEVATION C3
3/4"=1'-0" 01-?



COLUMN BASE D2
3/4"=1'-0" 01-?



CANOPY RAKE END D3
1 1/2"=1'-0" 01-?



SIGNAGE CONNECTION D4
1 1/2"=1'-0" 01-?

KEY NOTES

1. 10" DIAMETER STEEL COLUMN, PAINTED. RE: STRUCTURAL DWGS.
2. 8" CMU WALL WITH 3 COAT STUCCO FINISH.
3. OPEN.
4. WIND SCREEN. RE: DETAIL A3/GN018.
5. PLATFORM. RE: PLATFORM DRAWINGS.
6. SITE FURNISHINGS. BENCH. RE: DETAIL D1/GN018.
7. CORRUGATED METAL ROOF PANELS 5/8" EXPOSURE 1 PLYWOOD ON EPICORE DECK.
8. STEEL TUBE TRUSS, PAINTED.
9. PRECISION CUT STEEL PLATE CORBEL, PAINTED. RE: STRUCTURAL DWGS.
10. STATION ID SIGNAGE SUSPENDED FROM CANOPY STRUCTURE. RE: DETAIL D4/A403. FABRICATION AND INSTALL OF SIGNAGE UNDER SEPARATE CONTRACT, N.I.C.
11. TUBE STEEL BEAM, PAINTED.
12. LINE OF PLATFORM BEYOND.
13. STAINLESS STEEL HANDRAIL AT STAIR/RAMP. RE: PLATFORM DWGS.
14. EPICORE DECK, PAINTED.
15. NOT USED.
16. LIGHT FIXTURE AS SCHEDULED. CONCEAL CONDUIT INSIDE BEAM & DOWN CMU CELLS IN WALL.
17. MESSAGE BOARD SIGNAGE IN FOREGROUND. RE: D4/A403. FABRICATION AND INSTALL OF SIGNAGE UNDER SEPARATE CONTRACT, N.I.C.
18. PRE-CAST CONCRETE SILL RE: STRUCTURAL DWGS.
19. 1/2 ROUND 6"Ø PVC THROUGH WALL FOR POSITIVE DRAINAGE.
20. PROVIDE 4" DIA. BOTTOM OUTLET AT THIS END OF EACH GUTTER TO DIRECT WATER FLOW TO DRAIN INLET BELOW. EXTEND OUTLET 4" BELOW BOTTOM OF GUTTER AND TERMINATE.

ENGINEER'S SEAL

SURVEY INFORMATION
FIELD NOTES

NO.	BY	DATE

BENCH MARKS
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CITY OF ALBUQUERQUE
DEPARTMENT OF MUNICIPAL DEVELOPMENT
ENGINEERING DIVISION

TITLE: **NEW MEXICO RAIL RUNNER EXPRESS MONTANO STATION STATION CANOPY SECTIONS AND DETAILS**

Design Review Committee | City Engineer Approval | Last Design Update | No. / Day / Yr. | No. / Day / Yr.

City Project No. **559282** | Zone Map No. **F-15** | Sheet **24** Of **46**

GENERAL NOTES

GENERAL CRITERIA

1. COORDINATION WITH OTHER DRAWINGS:
   - A. SEE DRAWINGS OTHER THAN STRUCTURAL FOR KINDS OF FINISH AND THEIR LOCATION, OPENINGS REQUIRED BY ARCHITECTURAL FEATURES, WALKS, RAMPS, STAIRS, CURBS, ETC.
   - B. HOLES AND OPENINGS THROUGH WALLS, BEAMS, AND FLOORS FOR DUCTS, PIPING AND VENTILATION SHALL BE CHECKED BY THE CONTRACTOR, WHO SHALL VERIFY SIZES AND LOCATIONS OF SUCH HOLES OR OPENINGS WITH THE PLUMBING, HVAC AND ELECTRICAL DRAWINGS AND THESE SUB-CONTRACTORS.
   - C. DISCREPANCIES: COORDINATE STRUCTURAL DRAWINGS WITH OTHER DRAWINGS FOR INDIVIDUAL ITEMS. DISCREPANCIES UNCOVERED, IF ANY, SHALL BE REPORTED TO THE ARCHITECT BEFORE PROCEEDING WITH THE WORK.
   - D. TYPICAL EDGE OF STRUCTURE / SLAB IS SHOWN ON THE STRUCTURAL DRAWINGS. CONTRACTOR TO COORDINATE LOCATIONS, DIMENSIONS AND ELEVATIONS WITH ARCHITECTURAL SECTIONS.
2. INTENT: IF CERTAIN FEATURES ARE NOT FULLY SHOWN OR CALLED FOR ON THE DRAWINGS OR SPECIFICATIONS, THEIR CONSTRUCTION SHALL BE OF THE SAME CHARACTER AS FOR SIMILAR CONDITIONS THAT ARE CALLED FOR.
3. TYPICAL DETAILS ARE SHOWN ON SHEETS GN014 THROUGH GN017 AND APPLY TO ALL CONSTRUCTION EXCEPT WHERE SHOWN DIFFERENTLY ON THE PLANS AND DETAILS.
4. FOR DETAILS, LOCATIONS AND NUMBER OF INSERTS, EMBEDDED ITEMS, EQUIPMENT SUPPORT PADS, EQUIPMENT ANCHOR BOLTS AND SIMILAR ITEMS, REFER TO ARCHITECTURAL AND MECHANICAL DRAWINGS.
5. EXISTING CONDITIONS:
   - A. ALL NEW CONSTRUCTION MUST BE COORDINATED WITH EXISTING SITE CONDITIONS.
   - B. LOCATE AND PROTECT ALL EXISTING UNDERGROUND FACILITIES.
   - C. REMOVE ALL MATERIAL THAT WILL INTERFERE WITH NEW BUILDING FOUNDATIONS AS PER GEOTECHNICAL CONSULTANT'S RECOMMENDATIONS.
6. STRUCTURAL STABILITY:
   - A. THE STRUCTURE SHOWN ON THESE DRAWINGS HAS BEEN DESIGNED FOR STABILITY UNDER FINAL, FULLY CONSTRUCTED CONDITIONS.
   - B. PROVIDE SAFE AND ADEQUATE SHORING FOR ALL PARTS OF THE STRUCTURE DURING CONSTRUCTION.
   - C. WHERE BACKFILL IS PLACED AGAINST WALLS, THE WALLS SHALL BE BRACED OR OTHERWISE ADEQUATELY SHORED UNTIL PERMANENT BRACING ELEMENTS OR SLABS HAVE BEEN ERECTED AND HAVE ATTAINED DESIGN STRENGTH.

MATERIAL CRITERIA

1. STRUCTURAL STEEL:
   - A. ASTM A992 OR ASTM A572 GR. 50 AS MODIFIED BY AISC TECH. BULLETIN 3 (3/97) FOR ALL WIDE FLANGE STEEL SHAPES.
   - B. ASTM A36 FOR ALL STRUCTURAL AND MISCELLANEOUS STEEL CHANNELS, ANGLES, BARS, PLATES, AND CONNECTIONS UNLESS NOTED OTHERWISE.
   - C. ASTM A500 GRADE B (F_y = 46 KSI) FOR ALL STRUCTURAL TUBING.
   - D. ASTM A53, TYPE E OR S, GRADE B (F_y = 35 KSI) FOR ALL STRUCTURAL PIPE.
   - E. ASTM A325 TENSION CONTROL BOLTS, UNLESS SPECIFICALLY NOTED OTHERWISE WITH SIZES AS SHOWN ON DRAWINGS. ALL BOLTS SHALL BE TIGHTENED SO AS TO SHEAR THE SPLINE OFF THE BOLT. WHERE CLEARANCES DO NOT PERMIT, USE STANDARD A325 BOLTS, TIGHTENED AND INSPECTED IN ACCORDANCE WITH THE AISC "SPECIFICATION FOR STRUCTURAL JOINTS USING ASTM A325 OR A490 BOLTS."
   - F. ASTM F1554 GRADE 36 FOR ALL ANCHOR RODS EMBEDDED IN CONCRETE, UNLESS NOTED OTHERWISE IN DRAWINGS. PROVIDE FLAT WASHERS BETWEEN ALL NUTS AND BASEPLATES.
   - G. ALL WELDING SHALL COMPLY WITH THE LATEST EDITION OF THE AWS STRUCTURAL WELDING CODE.
   - H. ALL FIELD DRILLING SHALL BE DONE WITH A MAG DRILL. FLAME CUTTING OF HOLES OR ENLARGING OF UNFAIR HOLES WILL NOT BE PERMITTED.
   - I. HEADED ANCHOR STUDS AND SHEAR STUDS SHALL BE TYPE B, IN CONFORMANCE WITH AWS D1.1 STRUCTURAL WELDING CODE. STRUCTURAL STEEL TO RECEIVE SHEAR CONNECTORS SHALL BE CLEAN AND FREE OF PAINT. WELDING PREQUALIFICATION REQUIRED.
   - J. STRUCTURAL STEEL TO BE FABRICATED & ERECTED IN ACCORDANCE WITH LATEST OSHA REGULATIONS.
2. STEEL DECK:
   - A. ALL STEEL DECK SHALL BE FABRICATED AND ERECTED IN ACCORDANCE WITH THE LATEST STEEL DECK INSTITUTE SPECIFICATIONS.
   - B. SEE PLANS FOR STEEL DECK TYPE, GAGE, FINISH AND CONNECTIONS.
   - C. PROVIDE 1 1/2 INCH MINIMUM BEARING AT ALL STEEL DECK SUPPORT CONDITIONS.
   - D. ALL SPLICES AND LAPS SHALL BE A MINIMUM OF 2 INCHES AND SHALL BE CENTERED OVER SUPPORTS.
   - E. ALL STEEL DECK SHALL BE CONTINUOUS OVER THREE SPANS WHEREVER FRAMING ALLOWS.
   - F. OPENINGS THROUGH STEEL DECK ROOFS OR FRAMING PLANS ARE NOT COMPLETE AS TO NUMBER, SIZE AND LOCATION. FOR COMPLETE INFORMATION, REFER TO DRAWINGS OTHER THAN STRUCTURAL.
   - G. PROVIDE STANDARD ACCESSORY MATERIALS, INCLUDING BUT NOT LIMITED TO: CLOSURE STRIPS, POUR STOPS, GIRDER FILLERS, ETC. ACCORDING TO SDI RECOMMENDATIONS, TO PROVIDE TIGHT FITTING CLOSURE AT OPEN ENDS AND SIDES OF DECKING.
3. CONCRETE
   - A. ALL CONCRETE WORK SHALL CONFORM TO THE LATEST EDITION OF "SPECIFICATIONS FOR STRUCTURAL CONCRETE," ACI 301.
   - B. BASIS FOR DESIGN, STRENGTH AT 28 DAYS:
     1. F'_c = 4000 PSI (NORMAL WEIGHT, AIR ENTRAINED) ALL EXPOSED CONCRETE FLATWORK AND RETAINING WALLS
     2. F'_c = 3000 PSI (NORMAL WEIGHT, AIR ENTRAINED) ALL FOUNDATION CONCRETE (FOOTINGS, TIE BEAMS, STEM WALLS, GRADE BEAMS, DRILLED PIERS)
     3. F'_c = 3000 PSI (NORMAL WEIGHT) ALL INTERIOR SLABS ON GRADE
     4. F'_c = 3000 PSI (NORMAL WEIGHT) ALL OTHER CONCRETE
   - C. ALL CONCRETE SHALL BE REINFORCED UNLESS SPECIFICALLY NOTED "NOT REINFORCED."
   - D. FOUNDATIONS, STEM WALLS AND RETAINING WALLS SHALL NOT BE CAST AGAINST EXCAVATED VERTICAL SIDE SURFACES.
4. REINFORCED CONCRETE MASONRY:
   - A. BASIS FOR DESIGN: ALL MASONRY UNITS SHALL BE TYPE I WITH A MINIMUM COMPRESSIVE STRENGTH OF 1900 PSI (NET AREA). F'_m = 1500 PSI.
   - B. MORTAR SHALL BE TYPE S.
   - C. GROUT = F'_c = 2000 PSI.
   - D. CELLS CONTAINING REBAR SHALL BE GROUTED SOLID FROM THE BOTTOM TO THE TOP OF THE WALL IN ACCORDANCE WITH THE INTERNATIONAL BUILDING CODE.
   - E. ALL VERTICAL REBAR SHALL BE IN PLACE AND SECURED WITH REBAR POSITIONERS PRIOR TO GROUTING.
   - F. UNLESS OTHERWISE NOTED, MASONRY CELLS SHALL BE GROUTED IN ACCORDANCE WITH THE LOW-LIFT METHOD AS DESCRIBED IN THE INTERNATIONAL BUILDING CODE. (MAX. 4 FT. LIFTS)
   - G. ALL CELLS BELOW GRADE SHALL BE GROUTED SOLID UP TO GRADE.
   - H. LAP ALL REBAR 48 BAR DIAMETERS OR 24" MINIMUM UNLESS NOTED OTHERWISE. LAP ALL JOINT REINFORCEMENT 7S WIRE DIAMETERS.
   - I. ALL HORIZONTAL REINFORCING IN BOND BEAMS SHALL BE CONTINUOUS AROUND CORNERS OR HAVE CORNER BARS OF THE SAME SIZE AND A LAP OF 48 BAR DIAMETERS OR 24 INCHES MINIMUM. VERTICAL REBAR SHALL CONTINUE THROUGH BOND BEAMS.
   - J. PROVIDE STANDARD TRUSS TYPE 'JOINT' REINFORCING AT 16" O.C. (ALTERNATE COURSES) UNLESS NOTED OTHERWISE. USE PREFABRICATED CORNERS AND TEES AT ALL WALL CORNERS AND INTERSECTIONS RESPECTIVELY.
   - K. ALL CMU SHALL BE REINFORCED UNLESS SPECIFICALLY NOTED "NOT REINFORCED."
   - L. TYPICAL REINFORCEMENT UNLESS OTHERWISE SHOWN:
     1. 6" CMU WALLS: #4 AT 8' O.C. VERTICAL
     2. 8" CMU WALLS: #4 AT 16" O.C. VERTICAL
     3. 12" CMU WALLS: #5 AT 16" O.C. VERTICAL
5. REINFORCING STEEL:
   - A. ALL REINFORCING STEEL SHALL BE FABRICATED AND PLACED IN ACCORDANCE WITH THE BUILDING CODE REQUIREMENTS FOR REINFORCED CONCRETE (ACI 318) AND THE STANDARD MANUAL (ACI 315).
   - B. USE ASTM A615 GRADE 60 FOR ALL REINFORCING STEEL #4 AND LARGER, GRADE 40 FOR #3 AND SMALLER.
   - C. USE ASTM A185 FOR ALL WELDED WIRE FABRIC. PROVIDE IN FLAT SHEETS ONLY.
   - D. ALL REINFORCEMENT SHALL BE CONTINUOUS. STAGGER SPLICES WHERE POSSIBLE. LAPS FOR SPLICES SHALL BE AS INDICATED ON SHEET GN014, UNLESS OTHERWISE SHOWN OR NOTED.
   - E. BAR SUPPORTS AND SPACERS FOR REINFORCING SHALL BE PROVIDED IN ACCORDANCE WITH ACI 315. CHAIRS WITH 22 GA. SAND PLATES OR PRECAST BLOCKS SHALL BE PROVIDED FOR ALL REINFORCING OF CONCRETE IN CONTACT WITH GRADE. REINFORCING SHALL BE SECURELY TIED TO SUPPORTS.
   - F. REINFORCING SHALL NOT BE TACK WELDED OR WELDED IN ANY MANNER UNLESS SPECIFICALLY DETAILED ON THE STRUCTURAL PLANS.
   - G. MINIMUM CONCRETE PROTECTION FOR REINFORCEMENT (CLEAR DISTANCE):
     1. CONCRETE CAST AGAINST AND PERMANENTLY EXPOSED TO EARTH: 3"
     2. CONCRETE EXPOSED TO EARTH OR WEATHER: 2" (#6 & LARGER); 1 1/2" (#5 & SMALLER)
     3. COLUMNS: 1 1/2"
     4. STRUCTURAL WALLS: 3/4"
   - H. TYPICAL REINFORCEMENT UNLESS OTHERWISE SHOWN:
     1. UP TO 8" CONCRETE WALLS: #4 AT 8' O.C. EA. WAY AT CENTER OF WALL
     2. OVER 8" TO 12" CONCRETE WALLS: #4 AT 12" O.C. EA. WAY, EA. FACE
     3. OVER 12" THICK: #5 AT 12" O.C. EA. WAY, EA. FACE
   - I. ALL HORIZONTAL REINFORCING IN FOOTINGS AND WALLS SHALL BE CONTINUOUS AROUND CORNERS OR HAVE CORNER BARS OF THE SAME SIZE AND SPACING AS THE HORIZONTAL BARS AND LAP SPLICES PER SCHEDULE.
6. QUALITY ASSURANCE PROGRAM:
   - A. SEISMIC FORCE RESISTING SYSTEM (DUAL SYSTEM) = SPECIAL REINFORCED MASONRY WALLS & INTERMEDIATE STEEL MOMENT FRAMES.
   - B. SPECIAL INSPECTIONS/TESTING –
     "SPECIAL STRUCTURAL INSPECTION" SHALL NOT RELIEVE THE OWNER OR THEIR AGENT FROM REQUESTING THE JURISDICTION BUILDING DEPARTMENT INSPECTIONS REQUIRED BY SECTION 109 OF THE IBC.
     IN ACCORDANCE WITH IBC CHAPTER 17, THE FOLLOWING TYPES OF WORK REQUIRE SPECIAL INSPECTIONS AND TESTING:

INSPECTION/TESTING		CONTINUOUS	PERIODIC	IBC REFERENCE
STEEL				
–MATERIAL VERIFICATION FOR HIGH-STRENGTH BOLTS, NUTS, AND WASHERS (MANUFACTURER'S CERTIFICATION REQUIRED)			X	TABLE 1704.3
–INSPECTION OF HIGH STRENGTH BOLTS	BEARING		X	1704.3.3
	SLIP CRITICAL		X	
–INSPECTION OF WELDING	COMPLETE AND PARTIAL PEN.	X		1704.3.1
	MULTI PASS FILLET WELDS	X		
	SINGLE PASS FILLET WELDS > 5/16	X		
	SINGLE PASS FILLET WELDS ≤ 5/16		X	
	FLOOR AND ROOF DECK WELDS		X	
	REINFORCING STEEL		X	1933.5.2
–INSPECTIONS OF STEEL FRAME JOINT DETAILS W/ APPROVED CONSTRUCTION DOCUMENTS			X	1704.3.2
CONCRETE				
–REINFORCING STEEL PLACEMENT			X	1933.5
–BOLTS INSTALLED IN CONCRETE	EPOXY ANCHORS		X	1912.5
	EXPANSION ANCHORS WHERE CALLED FOR IN DRAWINGS		X	
	ANCHOR BOLTS	X		
–VERIFYING USE OF REQUIRED DESIGN MIX			X	1905.2
–SLUMP, AIR CONTENT AND TEMP TESTS		X		1908.8
–INSPECTION OF CONCRETE AND SHOTCRETE PLACEMENT TECH.		X		1908.9
–INSPECTION OF SPECIFIED CURING TECH.			X	1908.11
–VERIFICATION OF IN-SITU STRENGTH OF STRUCTURAL SLABS			X	1908.2
MASONRY				
–MORTAR PREP., MORTAR JOINTS, LOCATION OF REINFORCEMENT			X	TABLE 1704.5.1 2108.9.2.11
–SIZE; LOCATION OF STRUCTURAL ELEMENTS, INCLUDING ANCHORS, REINFORCING BARS			X	
–HOT OR COLD WEATHER PROTECTION			X	2104.3
–GROUT PREPARATION			X	TABLE 1704.5.1
–GROUT INSTALLATION		X		2103.3
–PREPARATION OF GROUT, MORTAR AND/OR PRISMS		X		
MISC.				
–ERECTION AND FASTENING OF EXTERIOR CLADDING AND VENEER			X	1707.6
TESTING	UNIT STRENGTH METHOD		X	1706.1.3
–VERIFICATION OF MASONRY f'm BY:	PRISM TEST (3 PRISMS PER 5000 SQ. FT OF WALL)	X		
–REINFORCING AND PRESTRESSING STEEL CERTIFIED MILL CERTS.			X	1706.3
–STRUCTURAL STEEL WELDING ULTRASONIC TESTING FOR THROUGH THICKNESS WELDS		X		1706.4

SPECIAL INSPECTION AND TESTING REPORTS SHALL BE COMPLETED AND DISTRIBUTED AT THE COMPLETION OF EACH TASK. IF A TASK IS TO TAKE LONGER THAN (3) DAYS, PROVIDE REPORTS FOR EACH DAY. PROVIDE COPIES OF REPORTS TO: CONTRACTOR, OWNER, ARCHITECT/ENGINEER, AND BUILDING OFFICIAL.
SPECIAL INSPECTOR TO KEEP A NON-COMPLIANCE LIST DOCUMENTING ITEMS INSPECTED NOT MEETING APPROVED CONSTRUCTION DOCUMENTS AND WHEN/HOW RESOLVED.

FOUNDATION NOTES

DESIGN CRITERIA:

THE FOLLOWING CRITERIA COVERS THE STRUCTURAL DESIGN OF THIS BUILDING STRUCTURE.

1. CODES AND MANUALS – MOST STRINGENT OF:
   - A. INTERNATIONAL BUILDING CODE, 2006 EDITION.
   - B. AISC MANUAL OF STEEL CONSTRUCTION, LATEST EDITION.
   - C. ACI BUILDING CODE REQUIREMENTS FOR REINFORCED CONCRETE, ACI 318 – LATEST EDITION.
2. DESIGN LOADS:

   VERTICAL:
   - A. DEAD LOADS – BUILDING IS DESIGNED FOR THE ACTUAL IN-PLACE WEIGHTS OF ALL MATERIALS SHOWN ON THE CONSTRUCTION DOCUMENTS.
   - B. LIVE LOADS:
     ROOF (SNOW) ..... 20 PSF (NON-REDUCIBLE)
   - C. CONCENTRATED LOAD (PER IBC 1607.4): 2000 LBS.

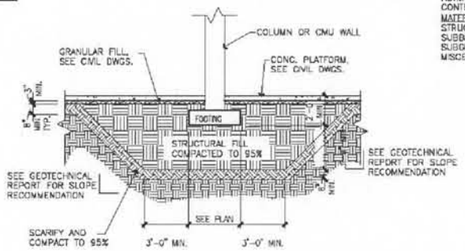
   HORIZONTAL:
   - A. WIND: BASIC WIND SPEED (3 SEC. GUST) = 90 MPH
     ANALYSIS PROCEDURE: SIMPLIFIED METHOD
     1. EXPOSURE C
        IMPORTANCE FACTOR = 1.00 (TABLE 1604.5)
        STRUCTURE CATEGORY = II
   - B. SEISMIC:
     OCCUPANCY CATEGORY = II
     IMPORTANCE FACTOR, I_E = 1.00
     SITE CLASS = D
     SPECTRAL RESPONSE ACCELERATIONS: S_S = 0.433 (FIGURE 1615(1)); S_1 = 0.137 (FIGURE 1615(2))
     SITE COEFFICIENTS: F_A = 0.625 (TABLE 1615.1.2(1)); F_V = 0.309 (TABLE 1615.1.2. (2))
     SPECTRAL RESPONSE ACCELERATION PARAMETERS: S_{MS} = 0.407 (16-18); S_{M1} = 0.205 (16-19)
     SEISMIC DESIGN CATEGORY: D
     RESPONSE MODIFICATION COEFFICIENT, R: 1 1/2

1. GENERAL CRITERIA:
   - A. A SUBSURFACE GEOTECHNICAL INVESTIGATION HAS BEEN PERFORMED BY TERRACON. A REPORT OF THIS INVESTIGATION DATED MAY 5, 2010 AND NUMBERED 66105019 IS AVAILABLE. IMPORTANT ADDITIONAL INFORMATION CONCERNING SPECIFIC SOIL
   - B. CONDITIONS IS CONTAINED IN THIS REPORT AND SHALL BE REVIEWED PRIOR TO START OF CONSTRUCTION. THE GEOTECHNICAL INVESTIGATION REPORT CONTAINS SPECIFIC
   - C. REQUIREMENTS CONCERNING CLEARING AND GRUBBING, SITE, SUBFLOOR AND BEARING SURFACE PREPARATION, STRUCTURAL FILL REQUIREMENTS, AND COMPACTION REQUIREMENTS NOT NECESSARILY SHOWN ON THESE DRAWINGS. REFER ANY CONFLICTS BETWEEN THESE DRAWINGS AND THE REPORT TO THE ARCHITECT FOR DIRECTION PRIOR TO BEGINNING ANY WORK.
2. BASIS FOR DESIGN:
   - A. ALLOWABLE SOIL BEARING PRESSURE = 2500 PSF.
3. FIELD OBSERVATION AND TESTING REQUIREMENTS:
   - A. EMPLOY THE SERVICES OF A REGISTERED, LICENSED GEOTECHNICAL ENGINEER TO OBSERVE ALL CONTROLLED EARTHWORK. THE GEOTECHNICAL ENGINEER SHALL PROVIDE CONTINUOUS ON-SITE OBSERVATION BY EXPERIENCED PERSONNEL DURING CONSTRUCTION OF CONTROLLED EARTHWORK. NOTIFY THE GEOTECHNICAL ENGINEER AT LEAST TWO WORKING DAYS IN ADVANCE OF ANY FIELD OPERATIONS OF THE CONTROLLED EARTHWORK. A REPRESENTATIVE OF THE GEOTECHNICAL ENGINEER SHALL BE PRESENT TO CONFIRM THE COMPLETE EXCAVATION OF ANY UNCONTROLLED FILL.
   - B. TESTS OF MATERIALS SHALL BE MADE AT THE FOLLOWING RATES:
     1. ONE FIELD DENSITY TEST PER EACH 500 SQUARE YARDS OF COMPACTED SUBGRADE PRIOR TO PLACING STRUCTURAL FILL OR FLOOR SLAB CONSTRUCTION WITH A MINIMUM OF 3 TESTS.
     2. ONE FIELD DENSITY TEST PER EACH 300 CUBIC YARDS OF STRUCTURAL FILL PLACED OR EACH HORIZONTAL LAYER OF STRUCTURAL FILL, WHICHEVER IS GREATER.
     3. ONE MOISTURE-DENSITY CURVE FOR EACH TYPE OF MATERIAL USED, AS INDICATED BY SIEVE ANALYSIS AND PLASTICITY INDEX.
     4. THE GEOTECHNICAL ENGINEER SHALL SUBMIT THE RESULTS OF ALL REQUIRED TESTS TO THE ARCHITECT WITHIN 2 WORKING DAYS AFTER THE TEST.
4. SPECIFIC SOIL PREPARATION REQUIREMENTS:
   - A. CLEARING AND GRUBBING:
     1. REMOVE ALL BRUSH, RUBBISH, GRASS, AND GRASS ROOTS FROM THE CONSTRUCTION AREA.
     2. REMOVE STUMPS, MATTED ROOTS AND ROOTS LARGER THAN 2 INCHES IN DIAMETER WITHIN 6 INCHES OF THE SURFACE OF AREAS ON WHICH FILL AND/OR FOOTINGS ARE TO BE CONSTRUCTED.
     3. REMOVE ALL TOPSOIL FROM THE CONSTRUCTION AREA. THIS MATERIAL SHALL NOT BE USED AS FILL MATERIAL, BUT MAY BE STOCKPILED AND LATER USED IN THE TOP 6 INCHES OF FILL OUTSIDE THE BUILDING PAD.
   - B. SITE AND SUBSURFACE PREPARATION:
     1. OVEREXCAVATE ALL SOIL UNDERLYING FOOTINGS AND SLABS AND ALL UNCONTROLLED FILL TO A MINIMUM DEPTH OF 3 FEET.
     2. SCARIFY ALL EXPOSED SUBGRADE SOILS TO A DEPTH OF 12 INCHES. MOISTEN TO OPTIMUM MOISTURE CONTENT ±2% AND COMPACT TO DENSITY SPECIFIED IN THESE REQUIREMENTS.
     3. ALL EARTHWORK FOR FOOTINGS AND SLABS SHALL EXTEND A MINIMUM OF 2 FEET BEYOND THE FOOTINGS AND SLAB EDGES.
     4. PLACE ALL STRUCTURAL FILL IN APPROXIMATELY HORIZONTAL LAYERS NOT GREATER THAN 8 INCHES IN THICKNESS, MOISTEN TO OPTIMUM MOISTURE CONTENT ±2% AND COMPACT TO DENSITY SPECIFIED IN THESE REQUIREMENTS.
   - C. STRUCTURAL FILL REQUIREMENTS:
     1. GRADATION (PER ASTM D422):

SIEVE SIZE	PERCENT PASSING BY WEIGHT
6"	100
3"	70–100
NO. 4	50–100
NO. 200	60 MAX.

     2. PLASTICITY INDEX (ASTM D4318): 15 MAXIMUM
     3. MATERIAL LARGER THAN 6 INCHES SHALL NOT BE PLACED IN THE STRUCTURAL FILL, AND MATERIAL LARGER THAN 4 INCHES SHALL NOT BE PLACED WITHIN TWELVE INCHES OF THE BEARING SURFACES OF SLABS OR FOUNDATIONS.
     4. NO BRUSH, SOD, FROZEN MATERIAL OR OTHER UNSUITABLE MATERIAL SHALL BE PLACED IN THE STRUCTURAL FILL. MATERIAL SHALL BE PLACED IN SUCH A MANNER AS TO RESULT IN UNIFORMLY COMPACTED FILL.
   - D. COMPACTION REQUIREMENTS
     1. SUBGRADE SOILS AND STRUCTURAL FILL MATERIALS SHALL BE COMPACTED TO THE FOLLOWING MINIMUM PERCENTAGES OF THE ASTM D1557 MAXIMUM DRY DENSITY AT ±2% OPTIMUM MOISTURE CONTENT.

MATERIAL	PERCENT COMPACTION
STRUCTURAL FILL	95
SUBBASE FOR SLAB SUPPORT	95
SUBGRADE BELOW STRUCTURAL FILL	95
MISCELLANEOUS BACKFILL	90



TYPICAL OVEREXCAVATION DETAIL — D5 (GN011)
N.T.S. TYP. @ MASONRY WALLS & COLUMNS

ENGINEER'S SEAL

SURVEY INFORMATION — FIELD NOTES

NO.	BY	DATE

AS BUILT INFORMATION

BENCH MARKS
NGS STAINLESS ROD SET BENEATH A 5 1/2" ACCESS COVER STAMPED "D-438, 1984", SE QUADRANT OF MONTANO RD. & THE BNSF RAILROAD TRACKS, 42.5 FT. EAST OF CENTERLINE OF THE TRACKS, 44 FT. SOUTH OF CENTERLINE OF MONTANO RD. NE, 1.1 FT. WEST OF CHAIN LINK FENCE.
DATUM NAVD 1988
ELEV. 4978.070

CONTRACTOR	DATE
WORK PERFORMED BY	
INSPECTOR'S ACCEPTANCE BY	DATE
FIELD VERIFICATION BY	DATE
DRAWINGS RECORDED BY	DATE
MICRO-FILM INFORMATION	
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NO.	

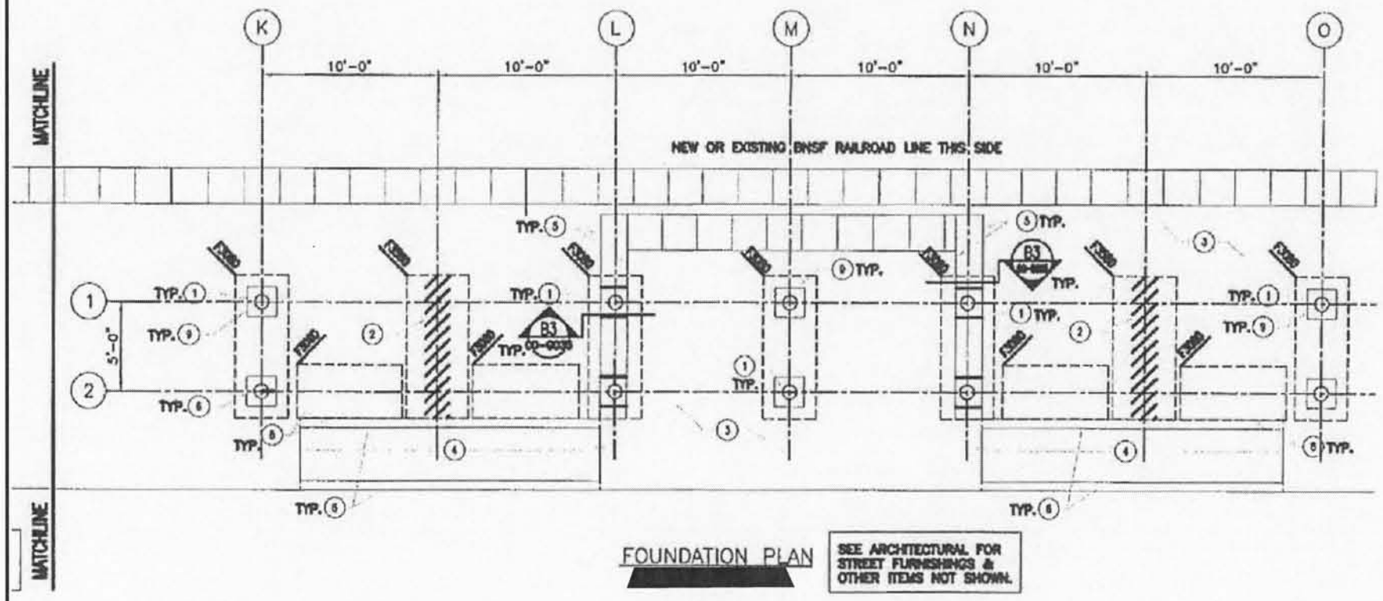
NO.	DATE	REMARKS	BY
		REVISIONS	
		DESIGN	
		DESIGNED BY SL	DATE 10/2010
		DRAWN BY BN	DATE 10/2010
		CHECKED BY KA	DATE 10/2010

CITY OF ALBUQUERQUE
DEPARTMENT OF MUNICIPAL DEVELOPMENT
ENGINEERING DIVISION

TITLE: **NEW MEXICO RAIL RUNNER EXPRESS MONTANO STATION STATION CANOPY STRUCTURAL NOTES**

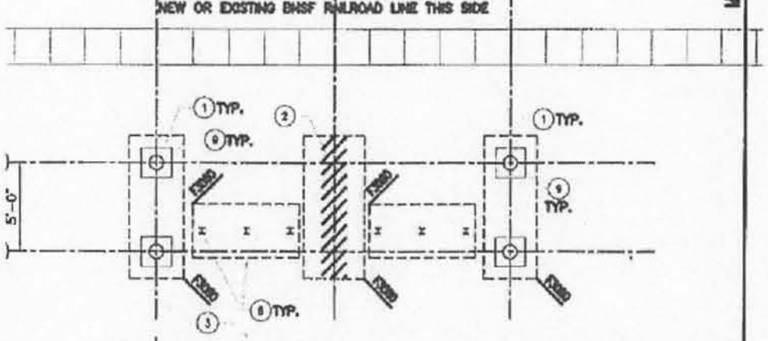
Design Review Committee	City Engineer Approval	Last Design Update	No. / Day / Yr.	No. / Day / Yr.

City Project No.	Zone Map No.	Sheet	Of
559282	F-15	25	46



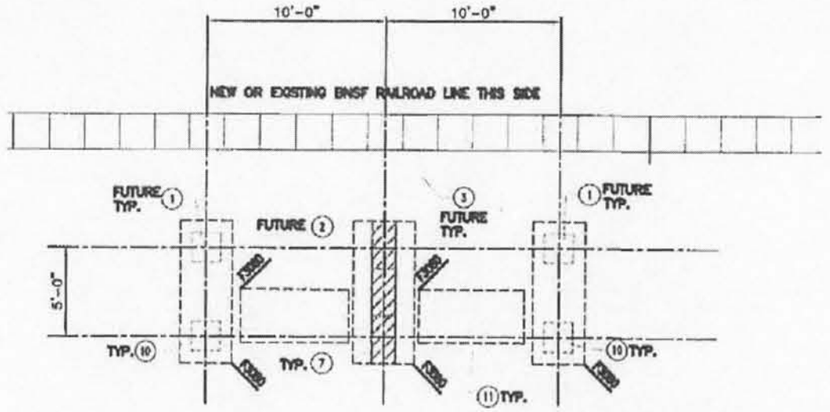
FOUNDATION PLAN

SEE ARCHITECTURAL FOR STREET FURNISHINGS & OTHER ITEMS NOT SHOWN.



FOUNDATION PLAN (FUTURE SHORT CANOPY)

SEE ARCHITECTURAL FOR STREET FURNISHINGS & OTHER ITEMS NOT SHOWN.



FOUNDATION PLAN (FUTURE SHORT CANOPY)

NOTES:
SEE ARCHITECTURAL FOR STREET FURNISHINGS & OTHER ITEMS NOT SHOWN.
FRAMING AT FUTURE SHORT CANOPY TO MATCH SHORT CANOPY FRAMING SHOWN ON SHEET CO-0034.

General Notes

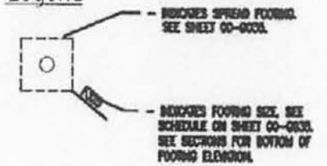
SEE CIVIL FOR M.S.L.E. AT EACH SHE.

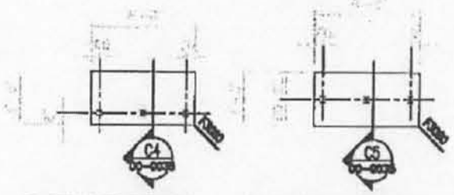
REFERENCE FINISHED FLOOR ELEVATION = 100'-0" AT EDGE OF PLATFORM AT RAIL SIDE, TO BE 8" ABOVE TOP OF NEAREST RAIL PERPENDICULAR TO EACH POINT U.N.O.

Keyed Notes

1. 10"Ø STANDARD PIPE COLUMN. SEE A5/CO-0037 FOR BASE PLATE.
2. 8" DOUBLE BYPASS CMU ARCH. SEE SHEETS CO-0038 & CO-0039.
3. COLORED STAMPED CONCRETE ON AGGREGATE BASE BY OTHERS. SEE ARCHITECTURAL & CIVIL SHEETS FOR ELEVATIONS & SLOPES.
4. CONCRETE RAMP & CURBS, SEE PLATFORM DRAWINGS.
5. CONCRETE STEPS, SEE PLATFORM DRAWINGS.
6. CONCRETE CURB, SEE PLATFORM DRAWINGS.
7. THREADED DOWELS FOR FUTURE REINFORCING, SEE DETAIL B1/0032.
8. TS6x13 POSTS & FOOTING AT WINDSCREEN, SEE DETAILS C4 & C5/CO-0035. SEE ARCHITECTURAL FOR WINDSCREEN LOCATIONS & POST SPACING.
9. 20"x20" CONCRETE PEDESTAL @ COLUMN, SEE B5/CO-0035.
10. ANCHOR BOLTS FOR FUTURE COLUMN, SEE DETAIL B4/CO-0035.
11. FOOTING @ FUTURE WINDSCREEN, SEE DETAIL C5/CO-0035. SEE ARCHITECTURAL FOR WINDSCREEN LOCATION & POST SPACING.

Legend





SINGLE BENCH DOUBLE BENCH

FOOTING @ WINDSCREEN C5 CO-0035

ENGINEER'S SEAL

SURVEY INFORMATION

FIELD NOTES

NO.	BY	DATE

BENCH MARKS

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DRAWINGS CORRECTED BY DATE

MICRO-FILM INFORMATION

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NO.

NO.	DATE	REMARKS	BY
		REVISIONS	
		DESIGN	
		DESIGNED BY	SL
		DRAWN BY	BN
		CHECKED BY	KA

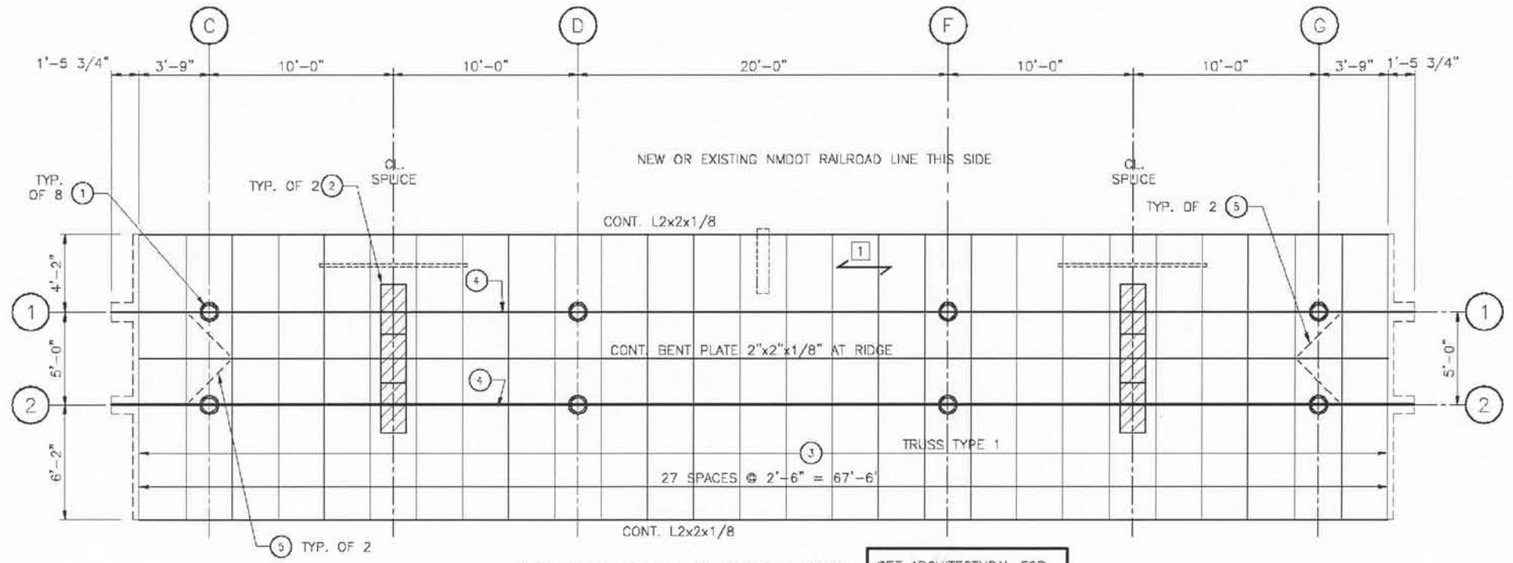
DATE 10/2010
DATE 10/2010
DATE 10/2010

CITY OF ALBUQUERQUE
DEPARTMENT OF MUNICIPAL DEVELOPMENT
ENGINEERING DIVISION

TITLE: NEW MEXICO RAIL RUNNER EXPRESS
MONTANO STATION
STATION CANOPY FOUNDATION NOTES

Design Review Committee | City Engineer Approval | Last Design Update | No. / Dsy / Yr. | No. / Dsy / Yr.

City Project No. 559282 | Zone Map No. F-15 | Sheet 26 Of 46



FRAMING PLAN (CANOPY "A")
SCALE: 1/8" = 1'-0"

SEE ARCHITECTURAL FOR SIGNAGE & OTHER ITEMS NOT SHOWN.

MATCHLINE

MATCHLINE

General Notes

SEE ARCHITECTURAL FOR ITEMS NOT NOTED OR SHOWN.

Keyed Notes

1. 10"Ø STANDARD PIPE COLUMN. SEE A5/GN016 FOR BASE PLATE.
2. 8" DOUBLE WYTHE CMU ARCH. SEE SHEETS GN015 & GN017.
3. STEEL TRUSS. SEE SHEET GN017.
4. HSS12x12x1/4 BEAM. TOP OF BEAM ELEVATION = 11'-0" ABOVE TOP OF REFERENCE ELEVATION AT ALL POINTS AS SHOWN ON GN012.
5. SKEWED & SLOPED HSS2x2x1/8 BRACE FROM BOTTOM OF TRUSS TO TOP OF ADJACENT TRUSS. SEE C5/GN016.

Legend

[1] —— INDICATE STEEL DECK TYPE & DIRECTION. SEE TYPICAL STEEL DECK DETAILS ON SHEET GN016.

AS BUILT INFORMATION

CONTRACTOR — DATE
MARKED BY
INSPECTOR'S ACCEPTANCE BY — DATE
FIELD VERIFICATION BY — DATE
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SURVEY INFORMATION

FIELD NOTES BY — NO. — DATE

ENGINEER'S SEAL

REVISIONS

NO.	DATE	REMARKS	BY

DESIGN

DESIGNED BY SL — DATE 10/2010
DRAWN BY BN — DATE 10/2010
CHECKED BY KA — DATE 10/2010

CITY OF ALBUQUERQUE
DEPARTMENT OF MUNICIPAL DEVELOPMENT
ENGINEERING DIVISION

TITLE: **NEW MEXICO RAIL RUNNER EXPRESS**
MONTANO STATION
STATION CANOPY FRAMING PLANS

Design Review Committee | City Engineer Approval | Last Design Update

No. / Day / Yr. | No. / Day / Yr.

City Project No. **559282** | Zone Map No. **F-15** | Sheet **27** Of **46**

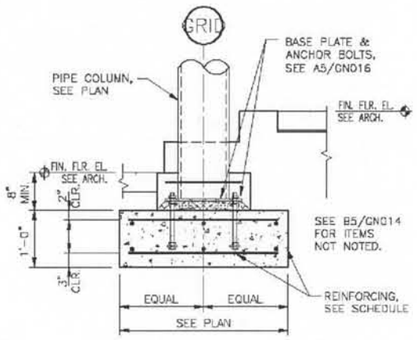
MARK	SIZE			REINFORCING				REMARKS
	LENGTH	WIDTH	DEPTH	BOTTOM		TOP		
				TRANS.	LONGIT.	TRANS.	LONGIT.	
F3050	6'-0"	3'-0"	12"	6 - #5	3 - #5	---	---	POST LOCATIONS VARY
F3080	8'-0"	3'-0"	12"	8 - #5	3 - #5	8 - #5	3 - #5	
F3580	8'-0"	3'-6	12"	8 - #5	4 - #5	8 - #5	4 - #5	

BAR SIZE	SPLICE LENGTH	EMBEDMENT
#3	1'-7"	1'-3"
#4	2'-1"	1'-7"
#5	2'-7"	2'-0"
#6	3'-1"	2'-5"
#7	4'-6"	3'-6"
#8	5'-2"	3'-11"
#9	5'-10"	4'-6"
#10	6'-7"	5'-1"
#11	7'-3"	5'-9"

NOTES:
1. SPLICE LENGTHS ARE TYPICAL UNLESS OTHERWISE NOTED ON PLANS AND DETAILS.
2. SPLICES SHALL BE STAGGERED AT LEAST 24" AND BARS SHALL BE LAPPED ONLY WHERE INDICATED ON DRAWINGS OR AS SPECIFICALLY PERMITTED BY THE STRUCTURAL ENGINEER.
3. VERTICAL WALL REINFORCING SHALL LAP A MINIMUM OF 48 DIAMETER AT CONSTRUCTION JOINTS.
4. WELDED WIRE FABRIC SHALL LAP A MINIMUM OF 8" AT SPLICES.
5. FOR HORIZONTAL BARS WITH MORE THAN 12" OF FRESH CONCRETE CAST BELOW THE SPLICE OR EMBEDMENT, LENGTHS SHALL BE 130% OF THAT SHOWN.

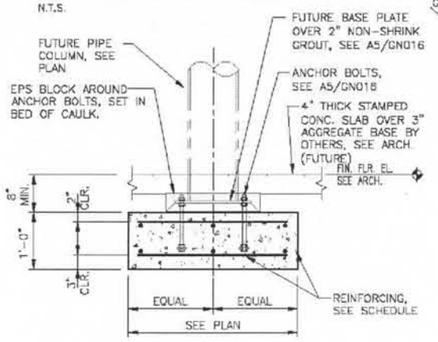
SPREAD FOOTING SCHEDULE (A3 / GN014)

N.T.S.



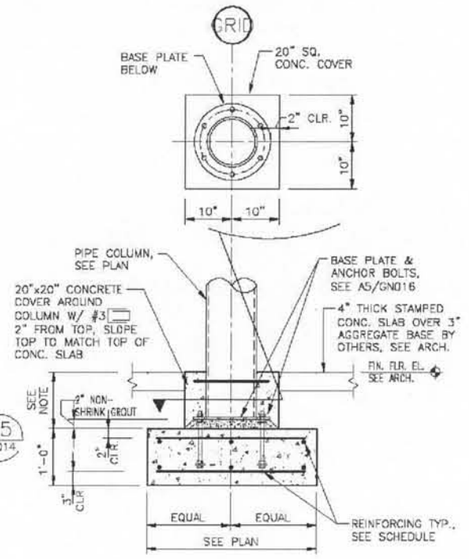
LAP SPLICE & EMBED SCHEDULE (A5 / GN014)

N.T.S.



SECTION @ COLUMN (B5 / GN012)

3/8" = 1'-0"



SECTION @ COLUMN (B3 / GN012)

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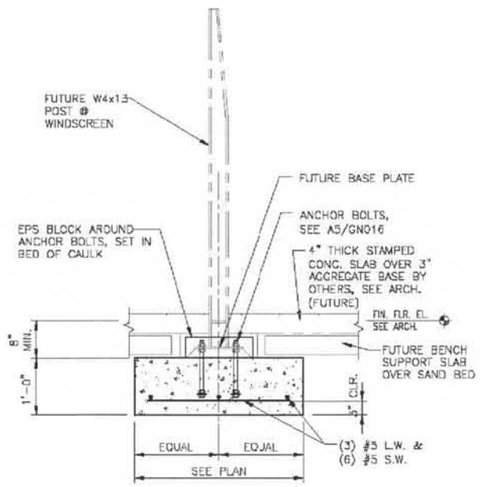
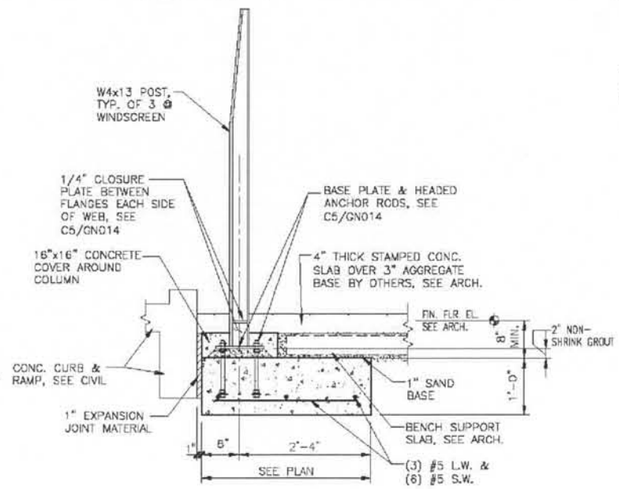


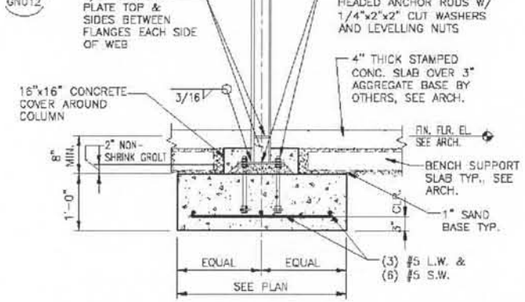
SECTION @ FUTURE COLUMN (B4 / GN012)

3/8" = 1'-0"



SECTION @ WINDSCREEN POST (C5 / GN012)

3/8" = 1'-0"



SECTION @ FUTURE WINDSCREEN POST (C3 / GN012)

3/8" = 1'-0"

SECTION @ WINDSCREEN POST (C4 / GN012)

3/8" = 1'-0"

ENGINEER'S SEAL

SURVEY INFORMATION

FIELD NOTES

NO.	DATE	BY

BENCH MARKS

NGS STAINLESS ROD SET BENEATH A 5 1/2"
ACCESS COVER STAMPED "D-438, 1984".
SE QUADRANT OF MONTANO RD. & THE
BNSF RAILROAD TRACKS, 42.5 FT. EAST OF
CENTERLINE OF THE TRACKS, 44 FT. SOUTH
OF CENTERLINE OF MONTANO RD. NE, 1.1 FT.
WEST OF CHAIN LINK FENCE.
DATUM NAVD 1988
ELEV. 4978.070

AS BUILT INFORMATION

CONTRACTOR
WORK STARTED BY — DATE
INSPECTOR'S ACCEPTANCE BY — DATE
AS-BUILT VERIFICATION BY — DATE
DRAWINGS CORRECTED BY — DATE

MICRO-FILM INFORMATION

RECORDED BY — DATE
NO.

REVISIONS

NO.	DATE	REMARKS	BY
		DESIGN	
	10/2010		SL
	10/2010		BN
	10/2010		KA

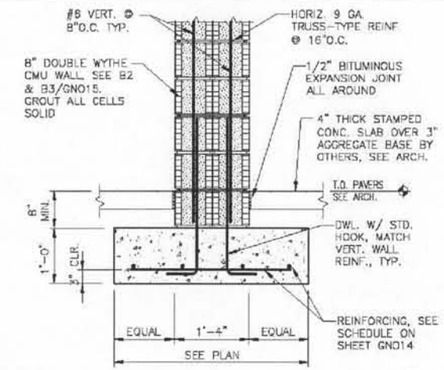
DESIGNED BY SL
DRAWN BY BN
CHECKED BY KA

CITY OF ALBUQUERQUE
DEPARTMENT OF MUNICIPAL DEVELOPMENT
ENGINEERING DIVISION

TITLE: **NEW MEXICO RAIL RUNNER EXPRESS MONTANO STATION STATION CANOPY CONCRETE SCHEDULES**

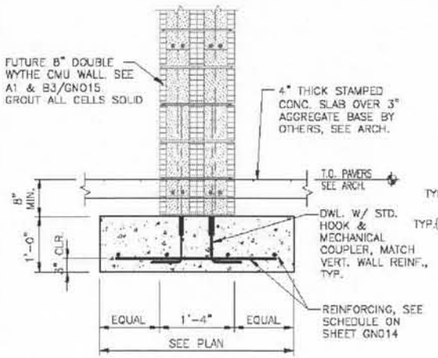
Design Review Committee | City Engineer Approval | Last Design Update | No. / Day / Yr. | No. / Day / Yr.

City Project No. **559282** | Zone Map No. **F-15** | Sheet **28** Of **46**



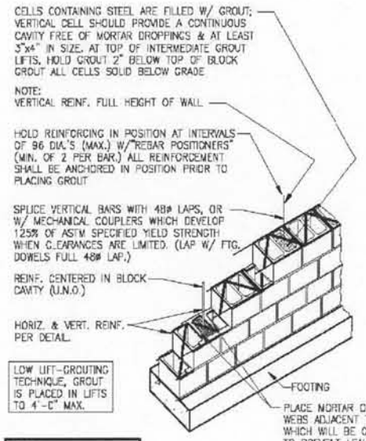
SECTION @ ARCH (A1 / GN0112)

3/8" = 1'-0"



SECTION @ FUTURE ARCH (B1 / GN0112)

3/8" = 1'-0"

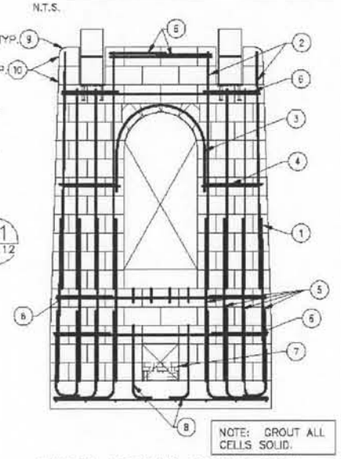


LOW LIFT-GROUTING TECHNIQUE, GROUT IS PLACED IN LIFTS TO 4'-0" MAX.

NOTE: SPECIAL INSPECTION IS REQUIRED.

TYP. REINFORCED CMU CONSTRUCTION (B2 / GN015)

N.T.S.

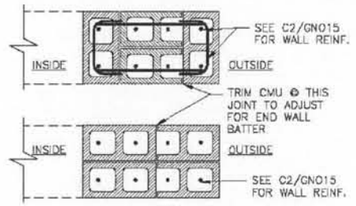


TYPICAL OPENING @ CMU WALL (C2 / GN015)

N.T.S.

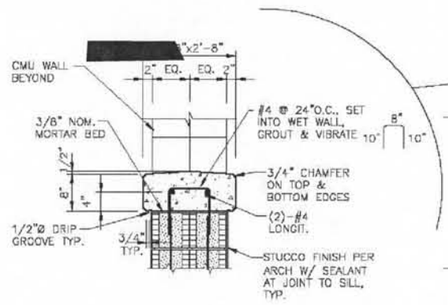
KEYED NOTES

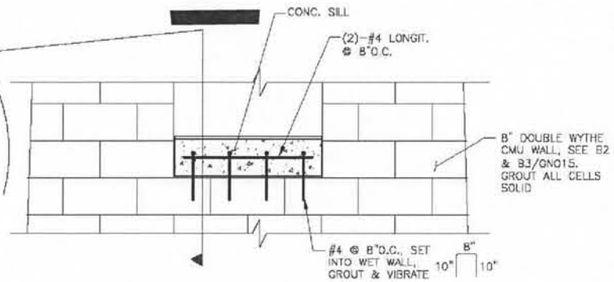
1. SEE CMU WALL REINF. DIAGRAM FOR SPLICES IN VERT. REINF.
2. EXTEND (2)–#6 VERT. UP EACH SIDE OF CORBEL, EACH WYTHE.
3. #4 HALF-ROUND EACH WYTHE, 38"Ø W/ 24" TAILS.
4. (2)–#4 8"□ 24"/24" IN BOND BEAM TYING BOTH WYTHES TOGETHER.
5. (8)–#6 VERT. @ 8"O.C. EACH SIDE OF OPENING (4 IN EACH WYTHE).
6. #4 BOND BEAM IN EACH WYTHE W/ #4 8"□ 24"/24" EACH END TO TIE BOTH WYTHES TOGETHER.
7. PAVE OPENING FOR DRAINAGE.
8. (2)–#5 DOWEL (32" LONG W/ 8" HOOK) EACH SIDE OF OPENING. EACH WYTHE.
9. COPE TO MATCH ARCHITECTURAL.
10. PLACE TOP 2 COURSES AFTER SETTING BEAMS & CORBELS.



DETAIL @ WALL BATTER (B4 / GN015)

3/8" = 1'-0"





DETAIL @ CONC. SILL (ALL) (C5 / GN015)

3/8" = 1'-0"

SURVEY INFORMATION

FIELD NOTES

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DATUM NAVD 1988

ELEV. 4978.070

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DRAWINGS CORRECTED BY — DATE

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ENGINEER'S SEAL

REVISIONS

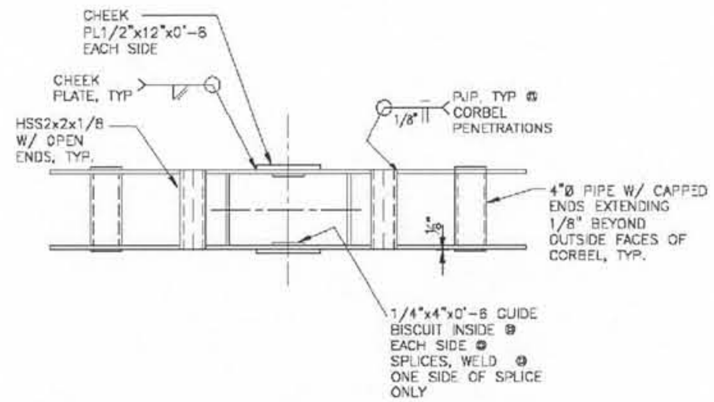
NO.	DATE	REMARKS	BY
		DESIGN	
			DESIGNED BY — DATE 10/2010
			DRAWN BY — DATE 10/2010
			CHECKED BY — DATE 10/2010

CITY OF ALBUQUERQUE
DEPARTMENT OF MUNICIPAL DEVELOPMENT
ENGINEERING DIVISION

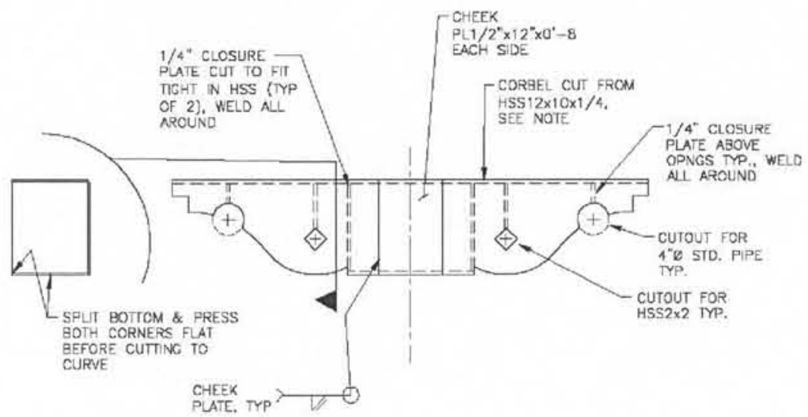
TITLE: **NEW MEXICO RAIL RUNNER EXPRESS**
MONTANO STATION
STATION CANOPY MASONRY SCHEDULE

Design Review Committee | City Engineer Approval | Last Design Update | No. / Dwg / Yr. | No. / Dwg / Yr.

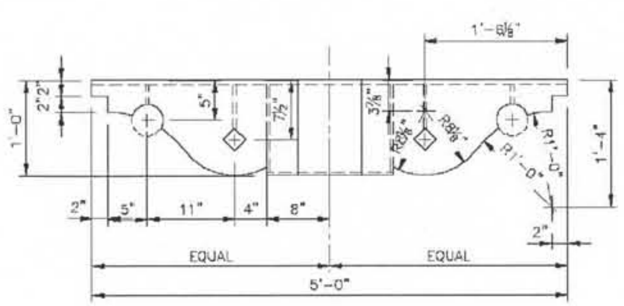
City Project No. **559282** | Zone Map No. **F-15** | Sheet **29** Of **46**



CORBEL – PLAN (A1 / GN016)
1/2" = 1'-0"



CORBEL – ANNOTATED DETAIL (B1 / GN016)
1/2" = 1'-0"

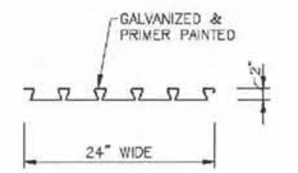


CORBEL – DIMENSION DETAIL (C1 / GN016)
1/2" = 1'-0"

STEEL DECK SCHEDULE						
DECK MARK	MINIMUM DECK PROPERTIES			ATTACHMENT (SEE NOTES BELOW)		
	I_S (in⁴/ft)	S_{BOTTOM} (in³/ft)	S_{TOP} (in³/ft)	PERP. SUPPORTS	PARALLEL SUPPORTS	SIDELAPS
1	0.398	0.269	0.247	PE-2	PA-1	SL-2

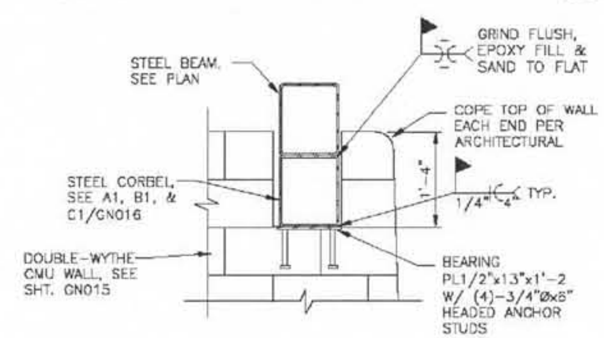
ATTACHMENT NOTES

PE-1 3/4"Ø PUDDLE WELDS @ 12" O.C.
PA-1 3/4"Ø PUDDLE WELDS @ 12" O.C.
SL-1 BUTTON PUNCH @ 36" O.C.
SL-2 #10 TEK SCREWS @ 36" O.C.

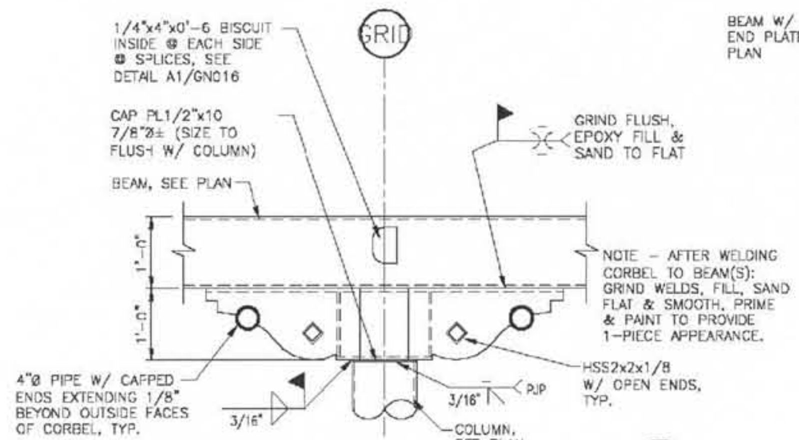


SIMILAR TO:
EPICORE ER2, THICKNESS 0.0295", 2" DEEP
MARK 1

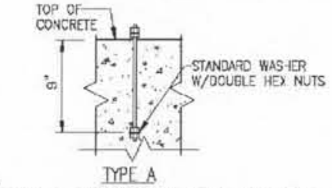
DECK SECTIONS & SCHEDULE (A4 / GN016)
3/8" = 1'-0"



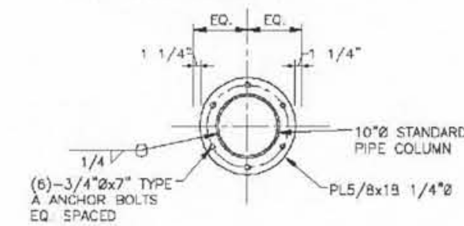
CORBEL @ MASONRY (B4 / GN013)
3/8" = 1'-0"



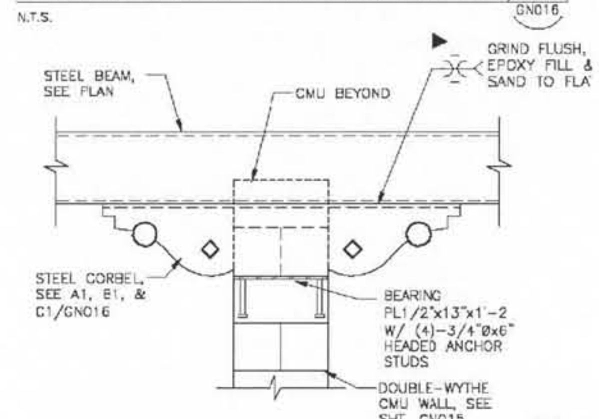
CORBEL @ COLUMN (C4 / GN013)
3/8" = 1'-0"



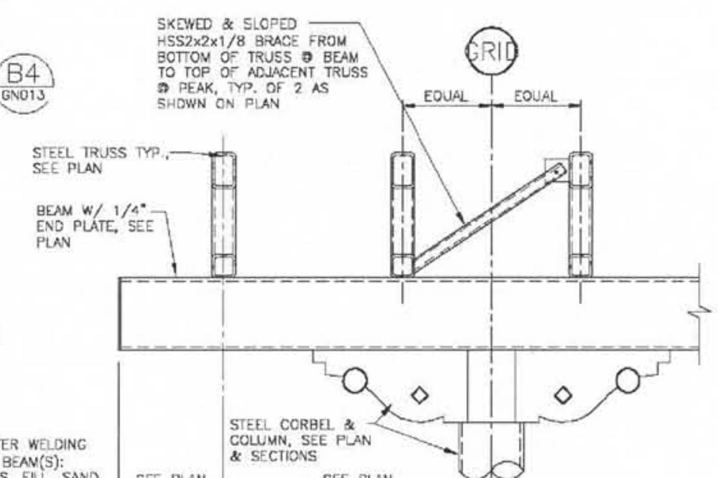
NOTE:
PROVIDE NUTS AND WASHERS ABOVE AND BELOW BASE PLATE AND ADEQUATE BOLT LENGTH TO FULLY ENGAGE TIGHTENING NUT



BASE PLATE SCHEDULE & ANCHORAGE TYPES (A5 / GN016)
N.T.S.



CORBEL @ MASONRY (B5 / GN013)
3/8" = 1'-0"



SKEWED & SLOPED BRACE (C5 / GN013)
3/8" = 1'-0"

AS BUILT INFORMATION
CONTRACTOR
WORK STAKED BY — DATE
INSPECTOR'S ACCEPTANCE BY — DATE
FIELD VERIFICATION BY — DATE
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MICRO-FILM INFORMATION
RECORDED BY — DATE
NO.

BENCH MARKS
NGS STAINLESS ROD SET BENEATH A 5 1/2" ACCESS COVER STAMPED "D-438, 1984", SE QUADRANT OF MONTANO RD. & THE BNSF RAILROAD TRACKS, 42.5 FT. EAST OF CENTERLINE OF THE TRACKS, 44 FT. SOUTH OF CENTERLINE OF MONTANO RD. NE, 1.1 FT. WEST OF CHAIN LINK FENCE.
DATUM NAVD 1988
ELEV. 4978.070

SURVEY INFORMATION
FIELD NOTES
NO. | BY | DATE

ENGINEER'S SEAL

REVISIONS
NO. | DATE | REMARKS | BY

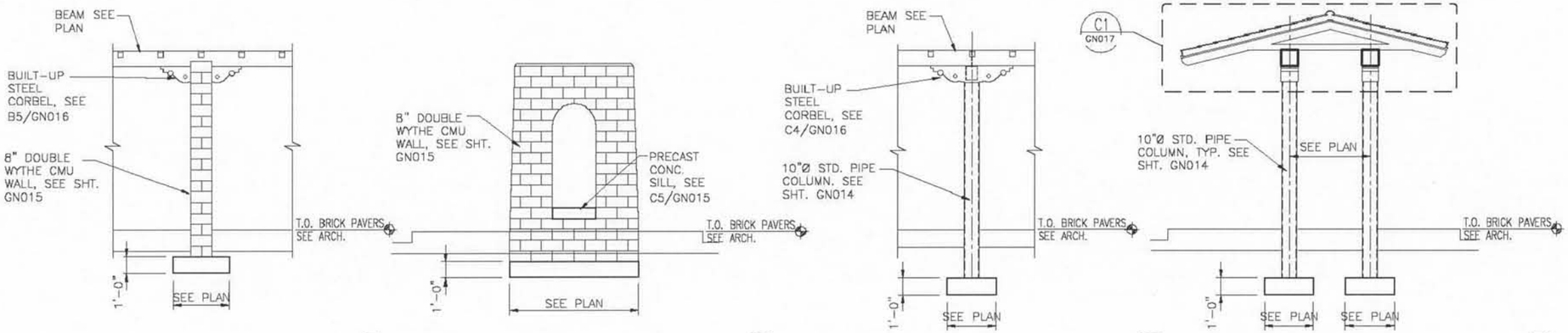
DESIGN
DESIGNED BY SL DATE 10/2010
DRAWN BY BN DATE 10/2010
CHECKED BY KA DATE 10/2010

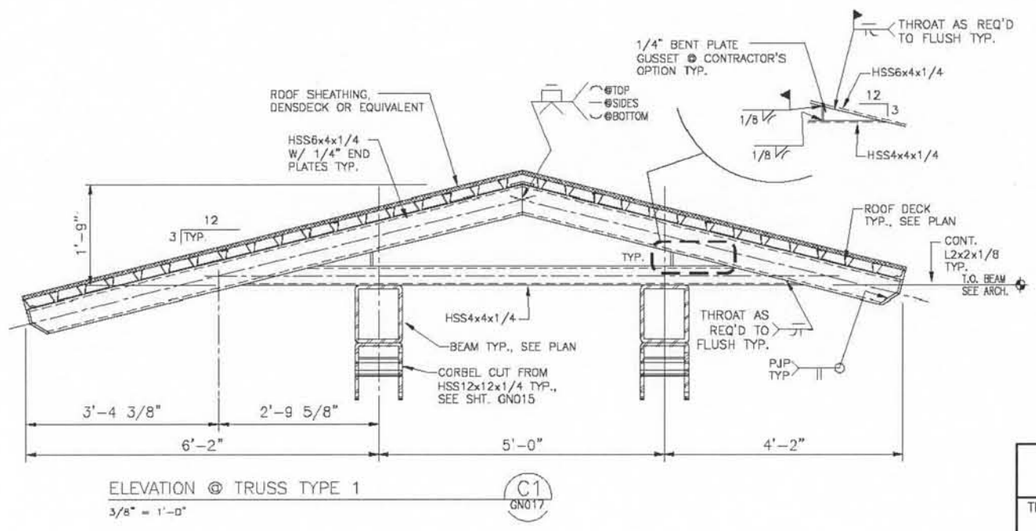
CITY OF ALBUQUERQUE
DEPARTMENT OF MUNICIPAL DEVELOPMENT
ENGINEERING DIVISION

TITLE: **NEW MEXICO RAIL RUNNER EXPRESS**
MONTANO STATION
STATION CANOPY STEEL AND DECK DETAILS

Design Review Committee | City Engineer Approval | Last Design Update | Mo. / Day / Yr. | Mo. / Day / Yr.

City Project No. **559282** | Zone Map No. **F-15** | Sheet **30** Of **46**





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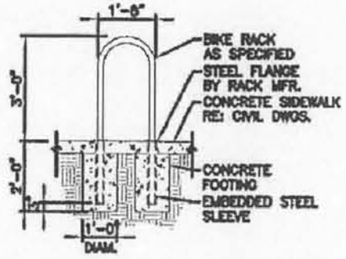
CITY OF ALBUQUERQUE
DEPARTMENT OF MUNICIPAL DEVELOPMENT
ENGINEERING DIVISION

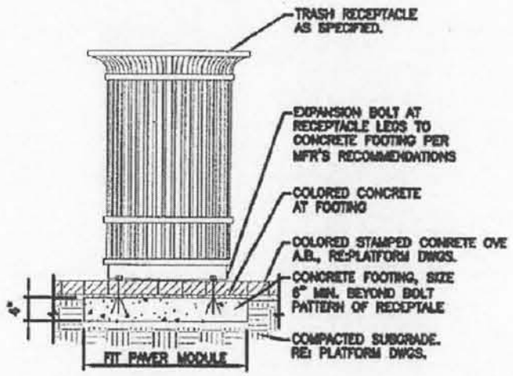
TITLE: **NEW MEXICO RAIL RUNNER EXPRESS**
MONTANO STATION
STATION CANOPY FRAMING ELEVATIONS & SECTIONS

Design Review Committee | City Engineer Approval | Last Design Update | No. / Day / Yr. | No. / Day / Yr.

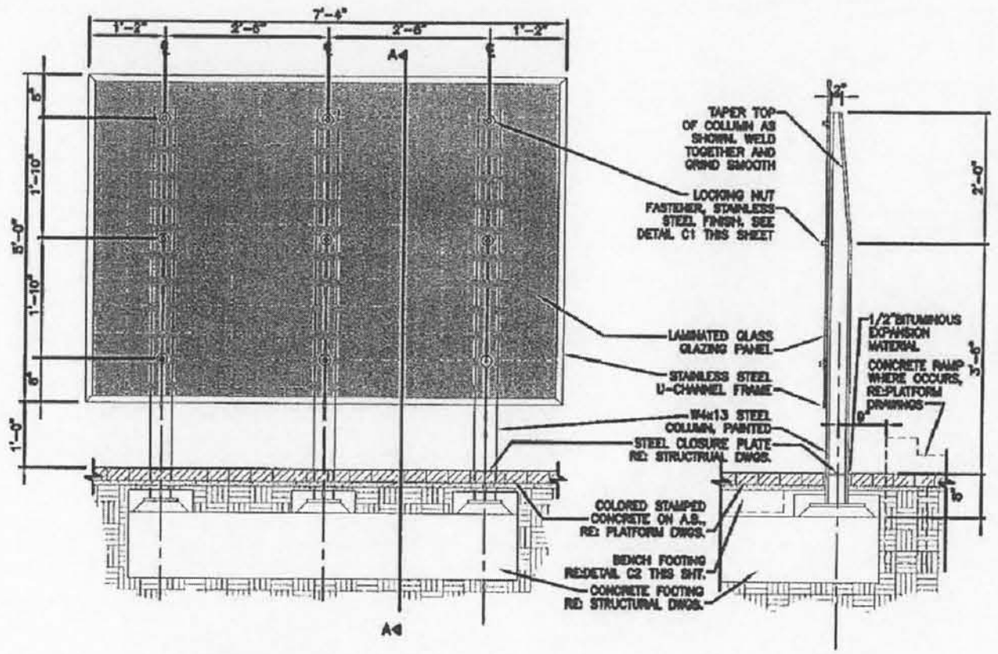
City Project No. **559282** | Zone Map No. **F-15** | Sheet **31** Of **46**



BIKE RACK
1/2"=1'-0"
A2 (00-6103)



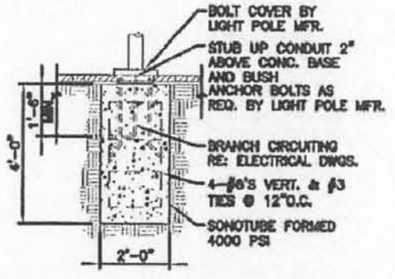
TRASH RECPT. MOUNTING
1"=1'-0"
C3 (M01)



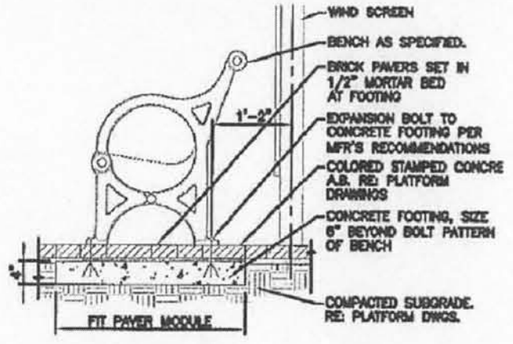
ELEVATION—RAIL SIDE

SECTION "A"

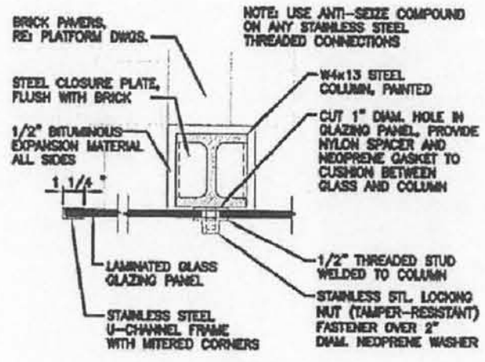
WIND SCREEN ELEV./SECT.
1"=1'-0"
A1 (M01)



POLE BASE DETAIL
1/2"=1'-0"
A3 (M01)

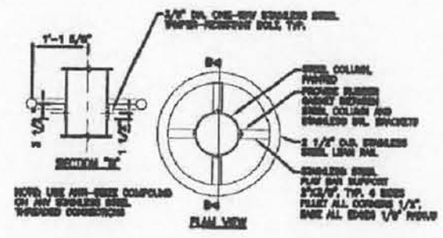


BENCH MOUNTING
1"=1'-0"
C2 (M01)



PLAN VIEW

WIND SCREEN FASTENER
3"=1'-0"
C1 (M01)



LEAN RAIL
1"=1'-0"
D1 (M01)

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FIELD NOTES

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ENGINEER'S SEAL

NO.	DATE	REMARKS	BY
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		DRAWN BY BN	DATE 10/2010
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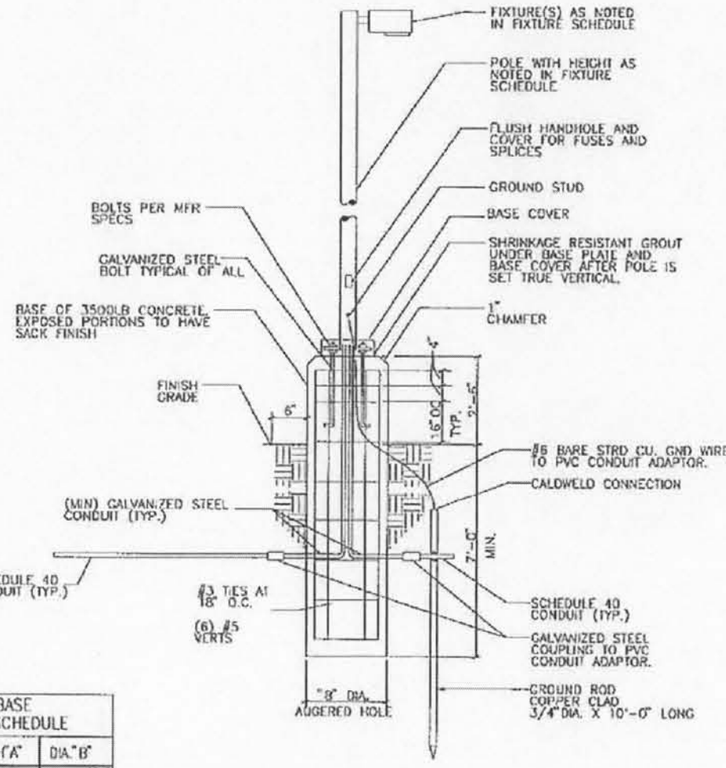
CITY OF ALBUQUERQUE
DEPARTMENT OF MUNICIPAL DEVELOPMENT
ENGINEERING DIVISION

TITLE: NEW MEXICO RAIL RUNNER EXPRESS
MONTANO STATION
STATION CANOPY MISCELLANEOUS DETAILS

Design Review Committee | City Engineer Approval | Last Design Update | No. / Day / Yr. | No. / Day / Yr.

City Project No. 559282 | Zone Map No. F-15 | Sheet 32 Of 46



POLE BASE DIMENSION SCHEDULE		
POLE LENGTH	DEPTH "A"	DIA. "B"
10'-20'	5'-0"	18"
20'-30'	6'-0"	24"

POLE BASE DETAIL
PARKING FIXTURE (E1)
SCALE: NONE

LIGHTING FIXTURE SCHEDULE

SYMBOL	LOCATION	DESCRIPTION	VOLTAGE	LAMP	MANUFACTURER	REMARKS
A	POLE MOUNTED PLATFORM AREA	FLUSH MOUNTED, FULLY SEALED, FIXTURE TYPE V SQUARE, MOUNTED ON PRA POLE	240V	1-175W MH	KIM FM BNS1H5 175PMH CBA PRA OR APPROVED EQUAL	SEE SITE PLAN FOR POLE HEIGHT
B	PLATFORM CANOPY	OUTDOOR, FLUORESCENT FIXTURE, 2' WITH 2 T5H0 LAMPS	120V	2-T5H0	INSIGHT EX5 SMS TA T5HO 24W 2' 120V CBA PC OR APPROVED EQUAL	UL LISTED FOR WET LOCATIONS
C	WALL MOUNTED BRIDGE OVERPASS	SURFACE WALL MOUNTED, 175 MH FIXTURE.	240V	1-175W MH	KENALL TSQD C 0 1 175MH QT REI AH OR APPROVED EQUAL	UL LISTED FOR WET LOCATIONS
SA	POLE MOUNTED PARKING AREA	(2) FULLY SHIELDED FIXTURES, TYPE IV WIDE, FORWARD THROW, MOUNTED ON SSS POLE	240V	1-400W MH	LITHONIA AS2 400M SR4W 240 PSWA DCAS2 SSS 17.5 OR APPROVED EQUAL	
SB	POLE MOUNTED PARKING AREA	FULLY SHIELDED FIXTURE, TYPE IV WIDE, FORWARD THROW, MOUNTED ON SSS POLE	240V	1-400W MH	LITHONIA AS2 400M SR4W 240 PSWA DCAS2 SSS 17.5 OR APPROVED EQUAL	
SC	POLE MOUNTED PARKING AREA	FULLY SHIELDED FIXTURE, TYPE III ASSYMETRIC, MOUNTED ON SSS POLE	240V	1-400W MH	LITHONIA AS2 400M SR3 240 PSWA DCAS2 SSS 17.5 OR APPROVED EQUAL	
SD	POLE MOUNTED PARKING AREA	FULLY SHIELDED FIXTURE, TYPE V SQUARE, MOUNTED ON SSS POLE	240V	1-400W MH	LITHONIA AS2 400M SR5S 240 PSWA DCAS2 SSS 17.5 OR APPROVED EQUAL	
SA1	POLE MOUNTED PARKING AREA	FULLY SHIELDED FIXTURE, TYPE V SQUARE, MOUNTED ON SSS 12.5' POLE	240V	1-250W MH	LITHONIA AS2 250M SR5S 240 PSWA DCAS2 SSS 12.5 OR APPROVED EQUAL	
SB1	POLE MOUNTED PARKING AREA	FULLY SHIELDED FIXTURE, TYPE IV WIDE, FORWARD THROW, MOUNTED ON SSS 12.5' POLE	240V	1-250W MH	LITHONIA AS2 250M SR4W 240 PSWA DCAS2 SSS 12.5 OR APPROVED EQUAL	
SC1	POLE MOUNTED PARKING AREA	FULLY SHIELDED FIXTURE, TYPE III ASSYMETRIC, MOUNTED ON SSS 12.5' POLE	240V	1-250W MH	LITHONIA AS2 250M SR3 240 PSWA DCAS2 SSS 12.5 OR APPROVED EQUAL	

AS BUILT INFORMATION
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FIELD VERIFICATION BY — DATE
DRAWINGS CORRECTED BY — DATE
MICRO-FILM INFORMATION
RECORDED BY — DATE
NO.

BENCH MARKS
NGS STAINLESS ROD SET BENEATH A 5 1/2" ACCESS COVER STAMPED "D-438, 1984". SE QUADRANT OF MONTANO RD. & THE BNSF RAILROAD TRACKS, 42.5 FT. EAST OF CENTERLINE OF THE TRACKS, 44 FT. SOUTH OF CENTERLINE OF MONTANO RD. NE, 1.1 FT. WEST OF CHAIN LINK FENCE.
DATUM NAVD 1988
ELEV. 4978.070

SURVEY INFORMATION
FIELD NOTES
NO. | BY | DATE

ENGINEER'S SEAL

REVISIONS
NO. | DATE | REMARKS | BY

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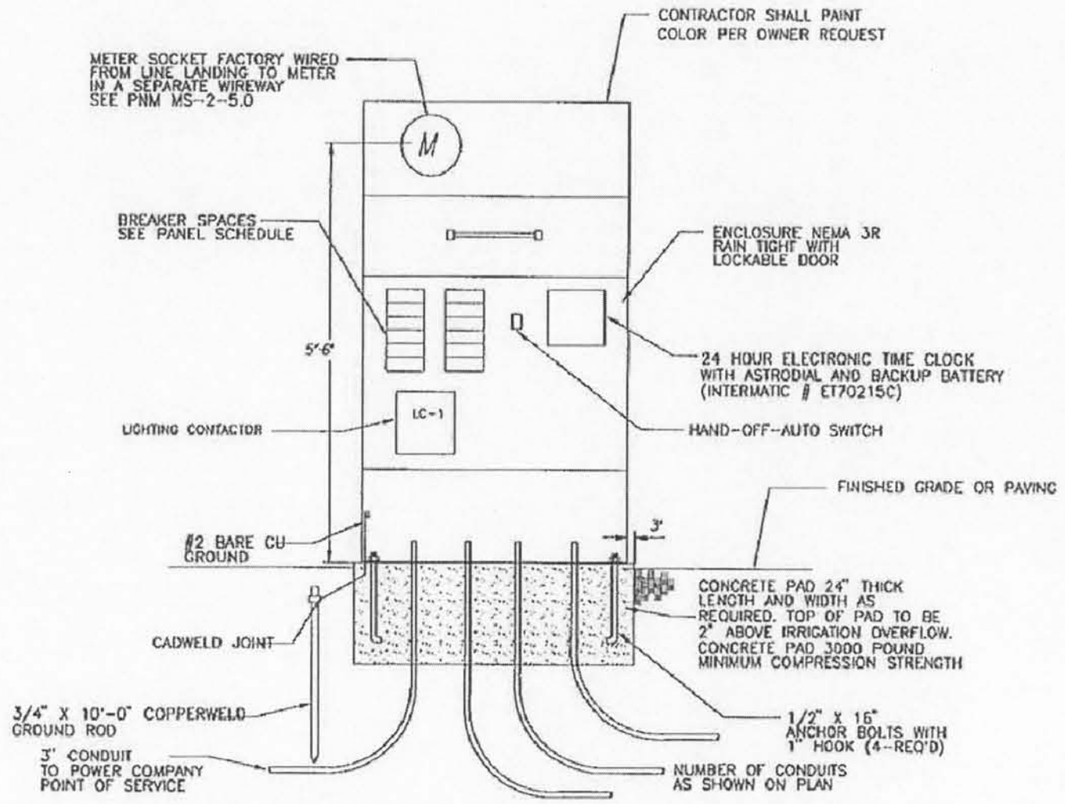
DRAWN BY — DATE
CHECKED BY — DATE

CITY OF ALBUQUERQUE
DEPARTMENT OF MUNICIPAL DEVELOPMENT
ENGINEERING DIVISION

TITLE: **NEW MEXICO RAIL RUNNER EXPRESS MONTANO STATION ELECTRICAL DETAILS**

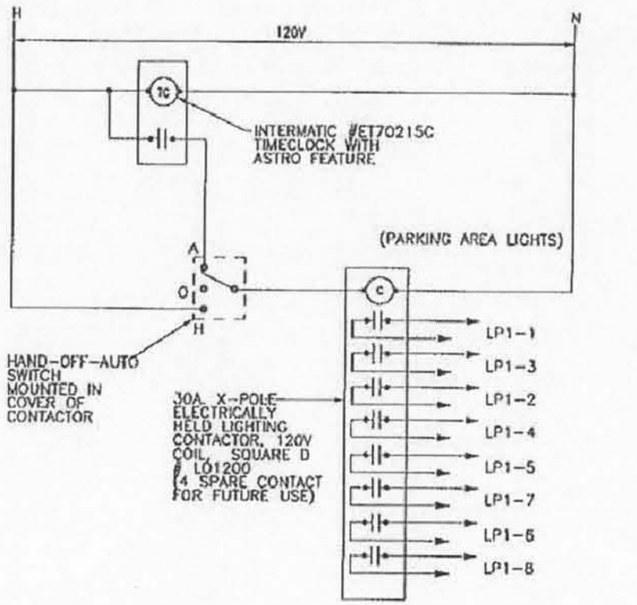
Design Review Committee | City Engineer Approval | Last Design Update | No. / Day / Yr. | No. / Day / Yr.

City Project No. **559282** | Zone Map No. **F-15** | Sheet **33** Of **46**



SES/LIGHTING CONTROL CABINET ELEVATION — E2

SCALE: NONE



EXTERIOR LIGHTING CONTACTOR — E4

SCALE: NONE

SURVEY INFORMATION

NO.	BY	FIELD NOTES DATE

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CITY OF ALBUQUERQUE
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MONTANO STATION
ELECTRICAL DETAILS

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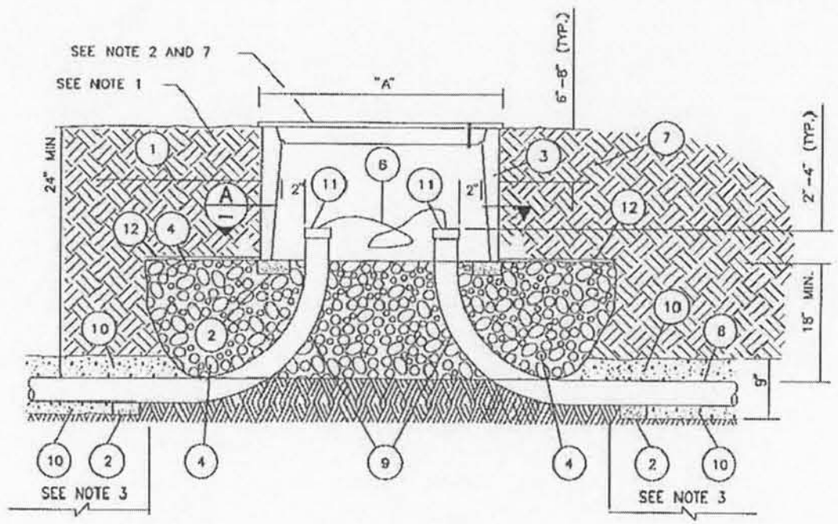
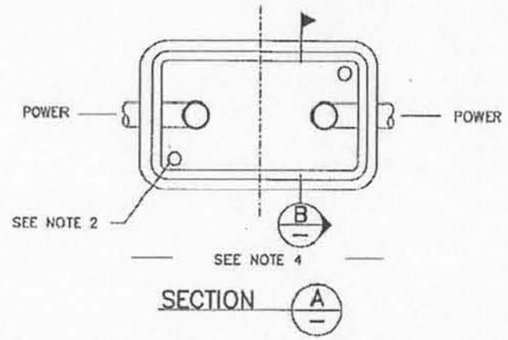
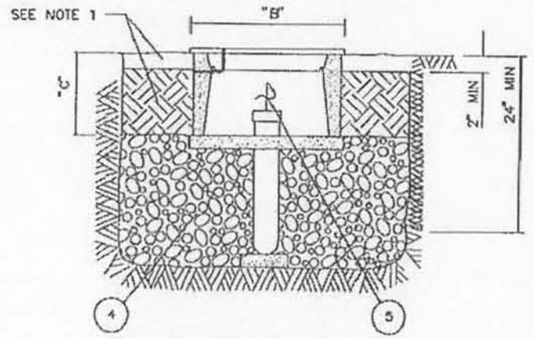




SECTION A



SECTION B

NOTES:

1. BACKFILL SHALL MEET THE REQUIREMENTS OF NMDOT STANDARD SPECIFICATIONS SECTION 660 – EXCAVATION, TRENCHING, AND BACKFILLING FOR UTILITIES TO BOTTOM OF THE PULLBOX. BACKFILL AROUND THE SIDES OF THE PULLBOX WITH SELECT EXCAVATED MATERIAL AND THOROUGHLY COMPACT. SET PULLBOX 2 INCHES ABOVE FINISHED GRADE TO ALLOW FOR FUTURE LANDSCAPING.
2. THIS BOX IS DESIGNED FOR NON-TRAFFIC AREAS. CONCRETE COVERS SHALL BE USED. COVERS SHALL BE SECURED WITH ¾" "L" BOLTS, NUTS, AND WASHERS.
3. CONDUIT FROM THE TYPICAL TRENCH SECTION SHALL NOT DEFLECT BY MORE THAN 1 INCH PER 1 FOOT FROM THE ALIGNMENT PRECEDING OR FOLLOWING THE PULLBOX.
4. THE CONTRACTOR SHALL ENSURE THAT THE FUTURE CONDUIT IS INSTALLED NEAREST TO THE EDGE OF ROADWAY.
5. REFER TO PULLBOX DATA TABLE AND T.S. 1-4 FOR ALL PULLBOX DIMENSIONS.
6. NUMBERS IN CIRCLES REFER TO ITEMS IN MATERIAL LIST.
7. COVER LETTERING SHALL BE 1". LETTERS CAST IN STANDARD MARKINGS: "STREET LIGHTING" OR "ELECTRIC HIGH VOLTAGE" AS REQUIRED.
8. USE PVC TO EXTEND INTO PULLBOX, IF NECESSARY.
9. INSTALL FLEXIBLE MARKERS 12 INCHES IN FRONT OF EACH PULLBOX WITHOUT TOUCHING CONDUIT.
10. POUR CONTROLLED LOW STRENGTH MATERIAL UP TO WITHIN 24 INCHES OF PULLBOX.
11. THE DIMENSION OF THE PULL BOXES SHOWN ARE NOMINAL DIMENSIONS AND MAY VARY AS TO THE MAUFACTURER'S RECOMMENDATIONS. ALL DIMENSIONS SHALL BE VERIFIED BY THE PROJECT MANAGER.
12. ALL PULL BOX COVERS SHALL BE HEAVY DUTY REINFORCED POLYMER MORTAR, HAVING A SERVICE LOAD OF 22,568 LBS OVER 10" SQUARE (225 PSI).

MATERIAL LIST	
ITEM	DESCRIPTION
1	WARNING TAPE
2	CONCRETE BUILDING BLOCK 2" x 4" x 8"
3	NO. 5 PULLBOX W/ EXCEPTIONS AS DRAWN
4	CLASS "D" CONCRETE AGGREGATE
5	CONDUIT WITH DETECTABLE PULLBOX TAPE FOR FUTURE USE
6	DETECTABLE PULLBOX TAPE COILED AND CONNECTED
7	PULLBOX DELINEATOR (FLEXIBLE MARKER) - SEE NOTE 9
8	1" DIA SCHEDULE 40 PVC CONDUIT
9	90 DEGREE PVC ELBOW, 18" RADIUS
10	CONTROLLED LOW STRENGTH MATERIAL (ROD IN)
11	APPROVED BUSHING SEE NOTE 8
12	30 lb. FELT PAPER

PULLBOX DATA TABLE						
PULLBOX TYPE	PULLBOX LENGTH "A"	PULLBOX WIDTH "B"	PULLBOX HEIGHT "C"	LID LENGTH "D"	LID WIDTH "E"	LID HEIGHT "F"
STANDARD # 5	25-0"	15-1/2"	12"	23-1/4"	13-3/4"	2"
LARGE # 7	32-1/4"	19-1/4"	12"	30-1/2"	17-1/2"	2"

PULLBOX DETAIL E9
SCALE: NONE

AS BUILT INFORMATION
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DRAWINGS CORRECTED BY DATE
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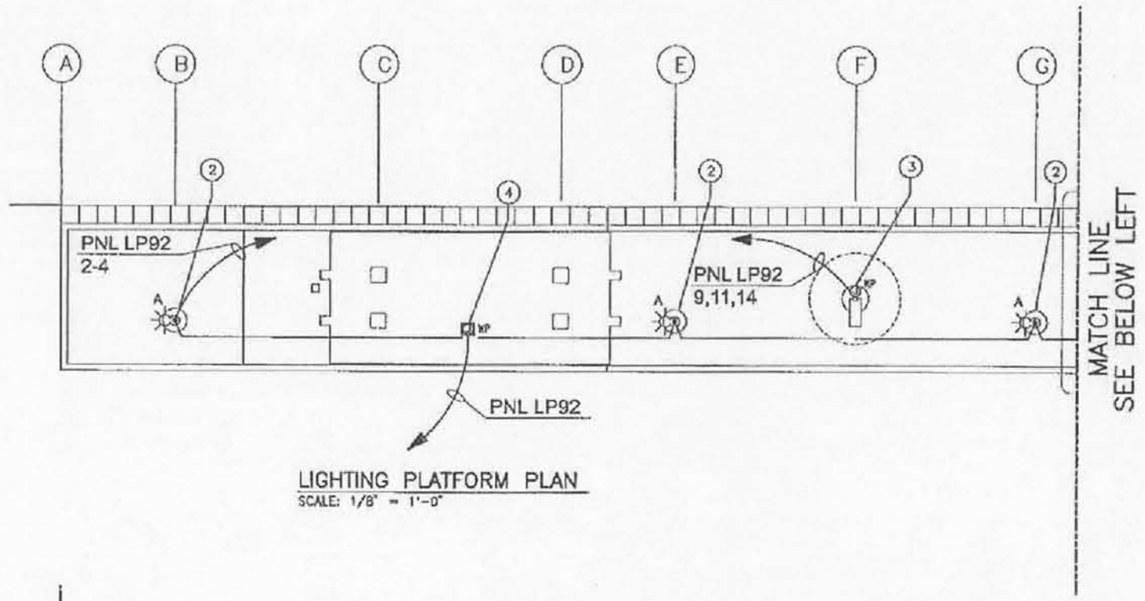
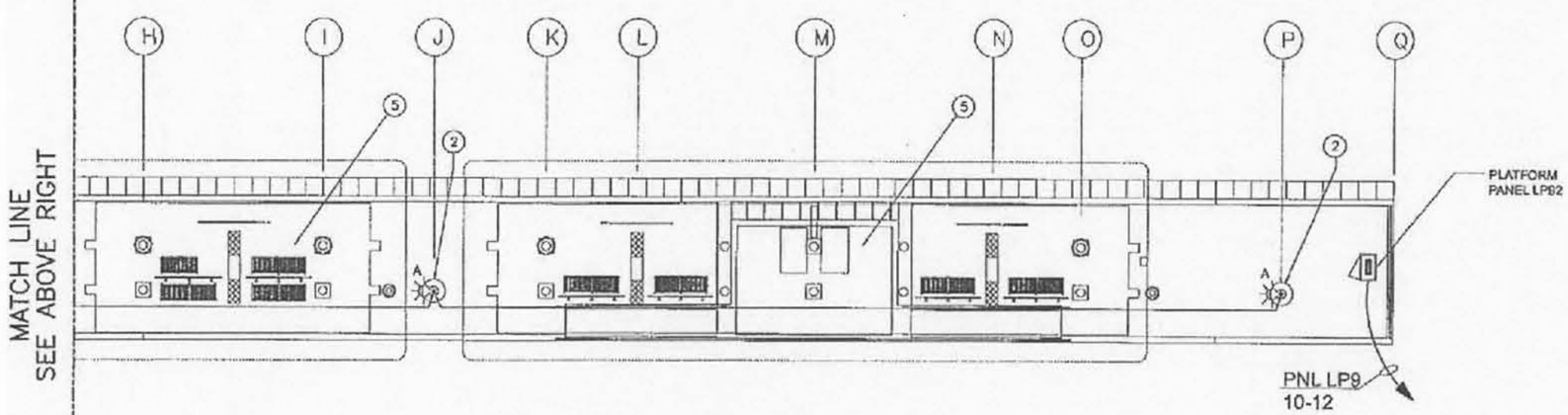


LIGHTING PLATFORM PLAN
SCALE: 1/8" = 1'-0"



LIGHTING PLATFORM PLAN
SCALE: 1/8" = 1'-0"

CONSTRUCTION NOTES

1. NOT USED
2. FURNISH AND INSTALL NEW 175 W MH FIXTURE TYPE "A" MOUNTED AT 15' OVERALL. SEE FIXTURE SCHEDULE ON GO-G050.
3. INFORMATION KIOSK. PROVIDE WP J-BOX FOR POWER CONNECTION.
4. RUN 1" EMPTY CONDUIT WITH PULLSTRING STUB OUT IN J-BOX FOR FUTURE CANOPY. VERIFY EXACT LOCATION.
5. FOR CANOPY POWER AND LIGHTING SEE DWG GO-G055.

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			CHECKED BY	KA

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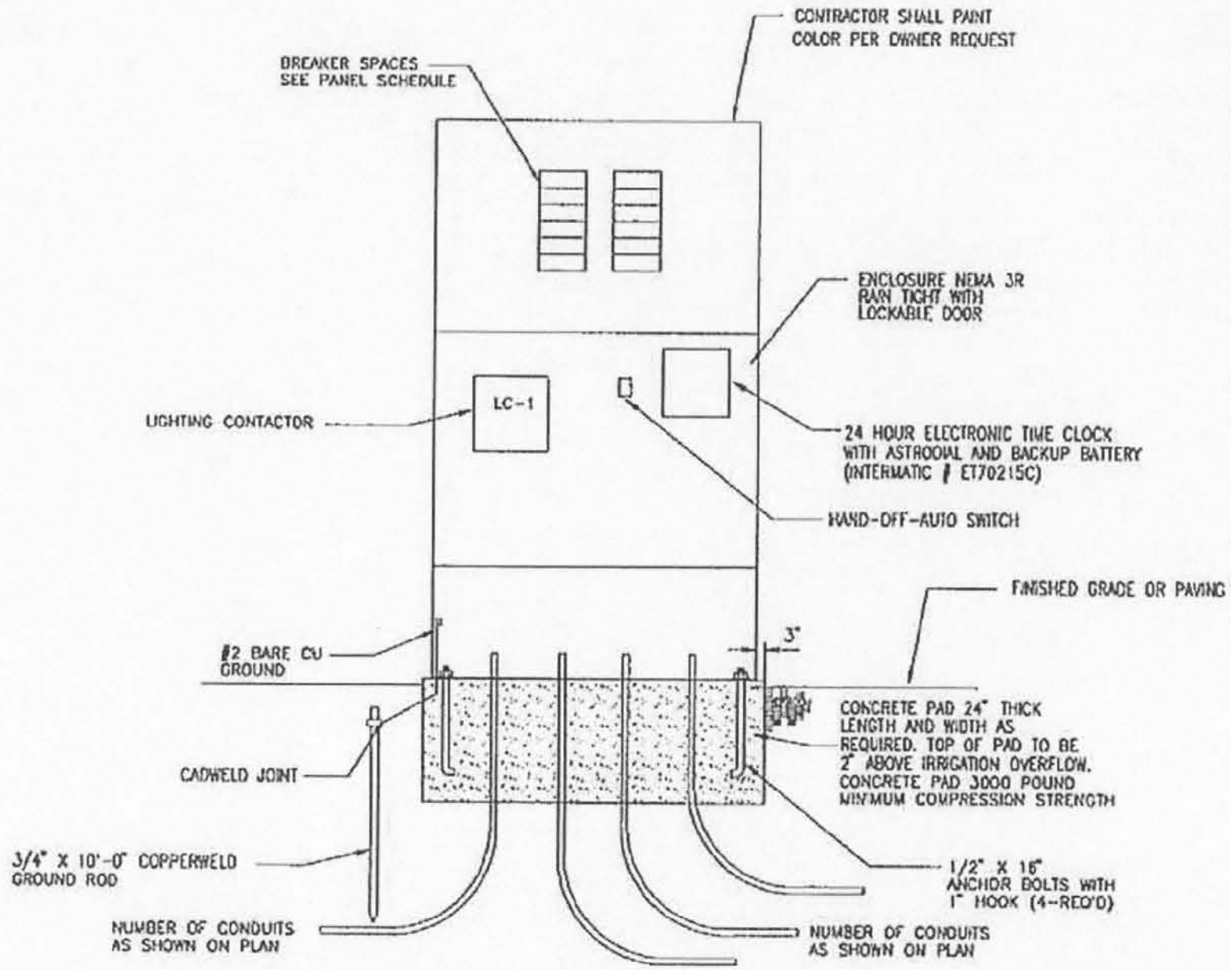
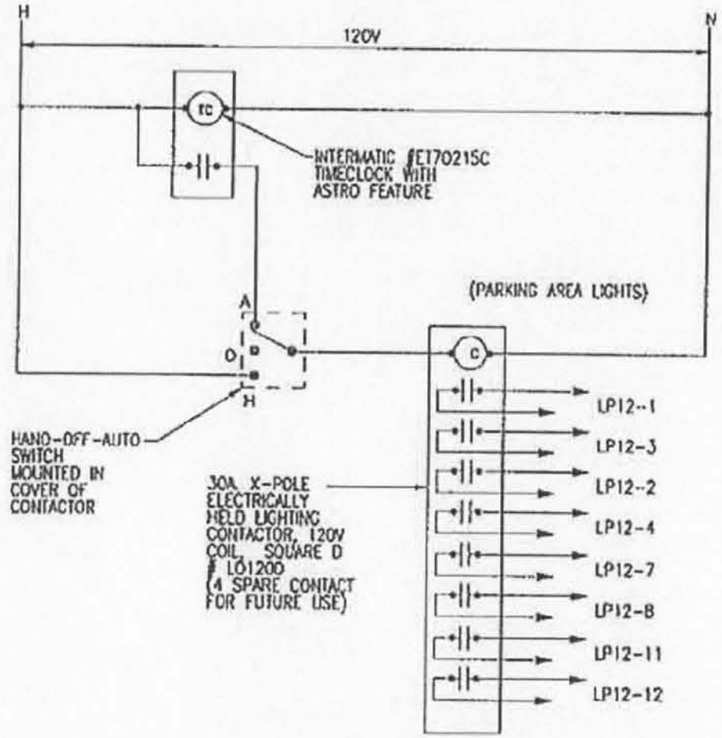


PLATFORM CONTROL CABINET ELEVATION

SCALE: NONE



EXTERIOR LIGHTING CONTACTOR

SCALE: NONE

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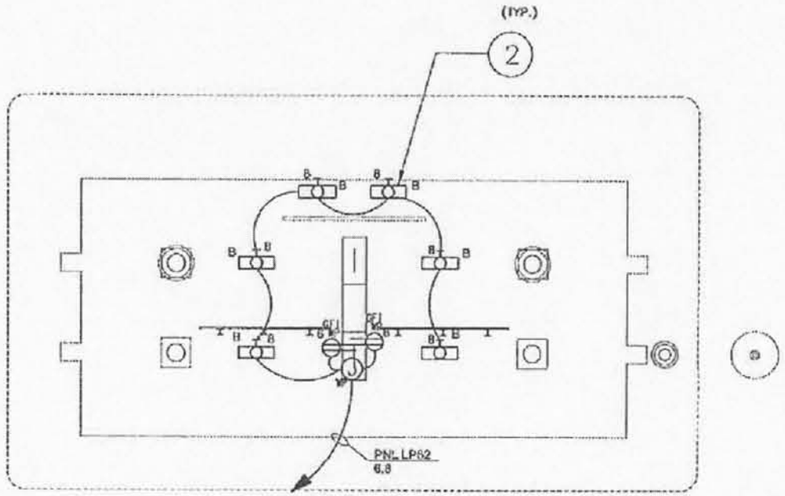
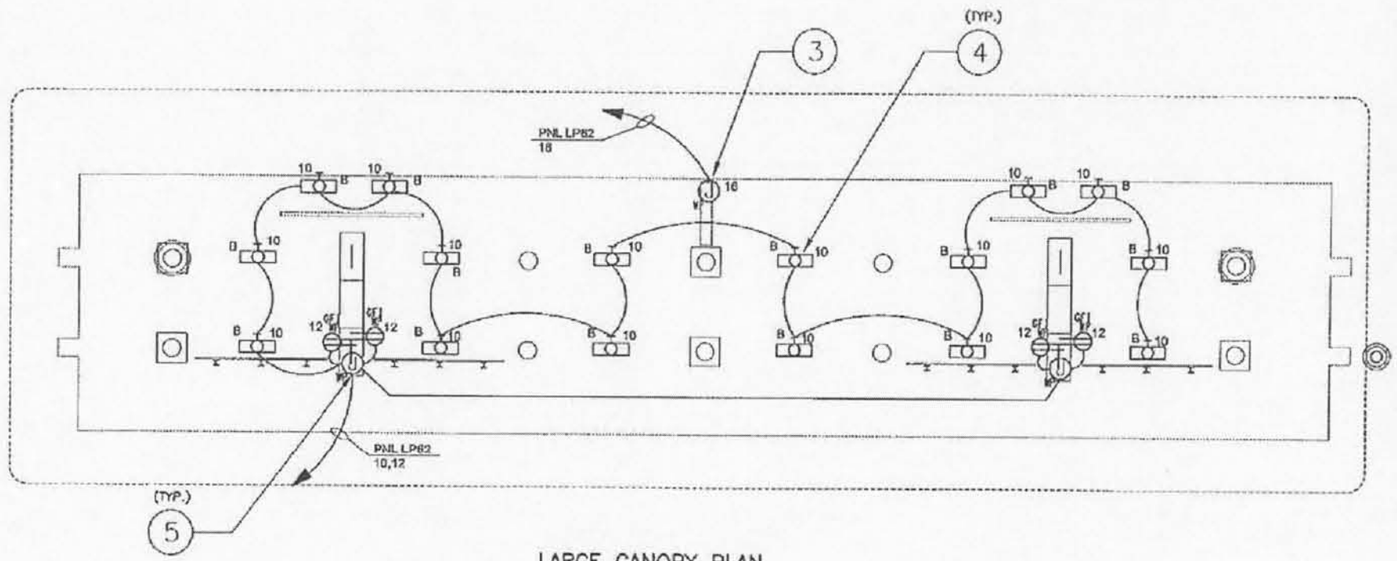
CONSTRUCTION NOTES

1. NOT USED
2. DIRECTIONAL LIGHTING FOR SIGNAGE.
3. POWER TO MESSAGE BOARD SUSPENDED FROM ROOF DECK.
4. INDIRECT FIXTURE MOUNTED AT TOP OF STEEL BEAM. SEE LIGHT FIXTURE SCHEDULE ON DWG 00-G0050.
5. RUN CONDUIT UP-THROUGH CMU CELLS INTO TUBE STEEL BEAM ABOVE. EXTEND CONDUIT AND ADD J-BOX FOR LIGHTING CIRCUIT.



FUTURE SMALL CANOPY PLAN
SCALE: 1/4" = 1'-0"



LARGE CANOPY PLAN
SCALE: 1/4" = 1'-0"

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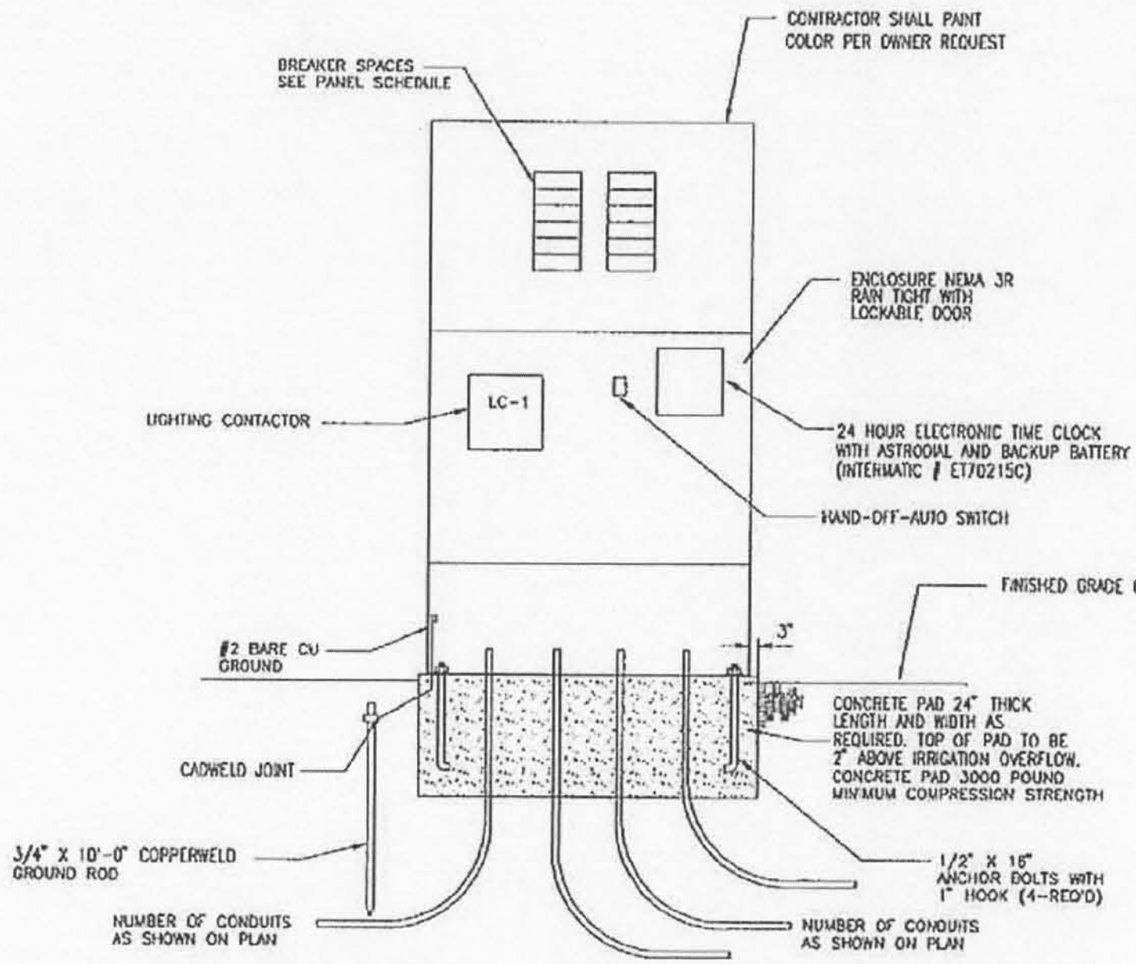
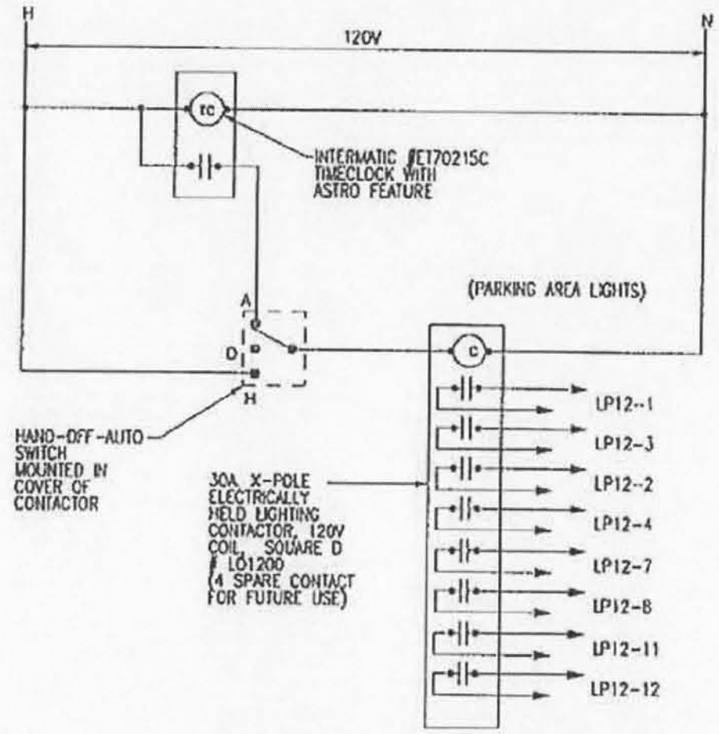


PLATFORM CONTROL CABINET ELEVATION

SCALE: NONE



EXTERIOR LIGHTING CONTACTOR

SCALE: NONE

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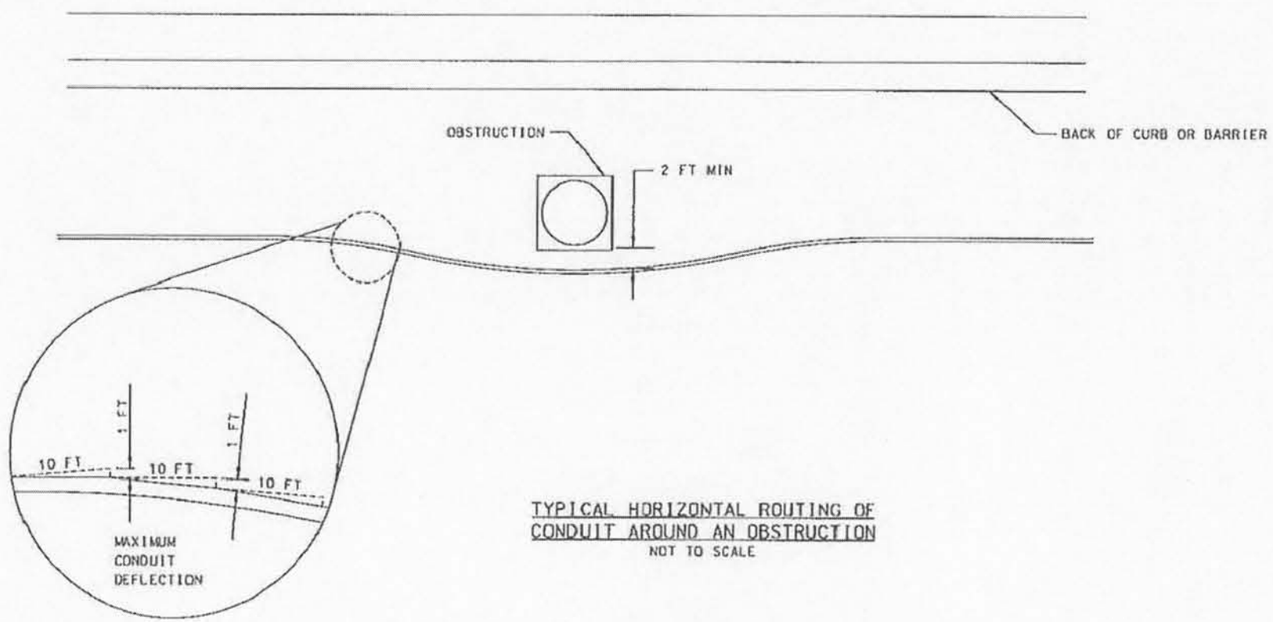
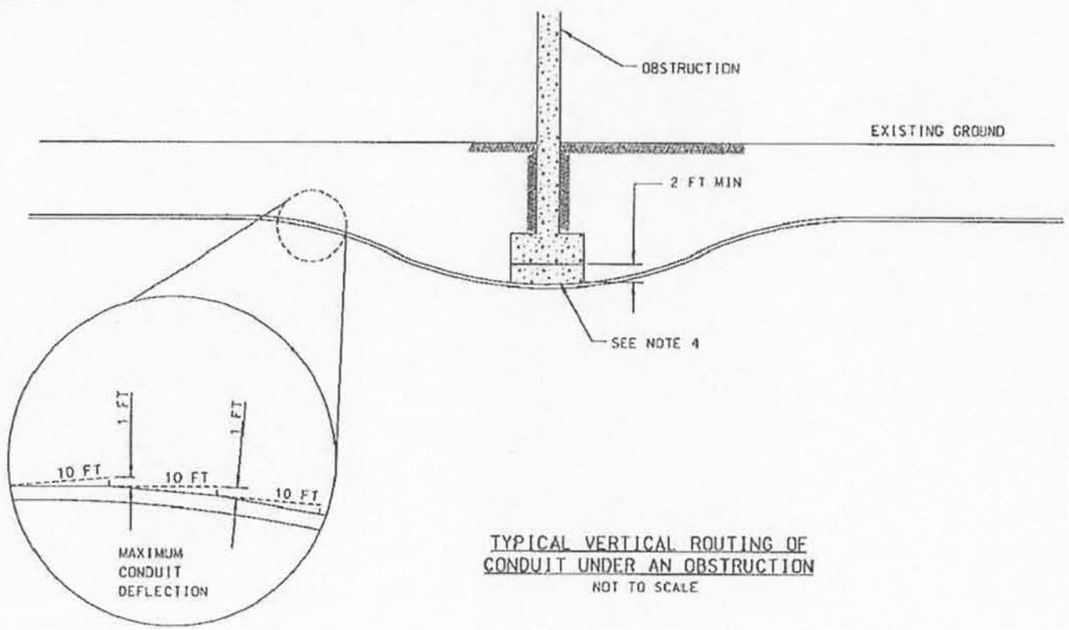


TYPICAL HORIZONTAL ROUTING OF
CONDUIT AROUND AN OBSTRUCTION
NOT TO SCALE

NOTES:

1. CONDUIT DEFLECTION SHALL NOT EXCEED ONE FOOT IN THE HORIZONTAL OR VERTICAL DIRECTION PER 10 FEET IN LONGITUDINAL DIRECTION (TYP).
2. CONDUIT SHALL BE ROUTED NO CLOSER THAN 2 FT TO ANY OBSTRUCTION.
3. CORE DRILLING THROUGH AN OBSTRUCTION MAY BE USED AS AN ALTERNATIVE METHOD, SUBJECT TO PROJECT MANAGER'S APPROVAL.
4. BACKFILL UNDER FOOTING SHALL BE CEMENT SLURRY PER SECTION 306 OF THE NMDOT SPECIFICATIONS.



TYPICAL VERTICAL ROUTING OF
CONDUIT UNDER AN OBSTRUCTION
NOT TO SCALE

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MONTANO STATION
BURIED CONDUIT TYPICAL SECTION

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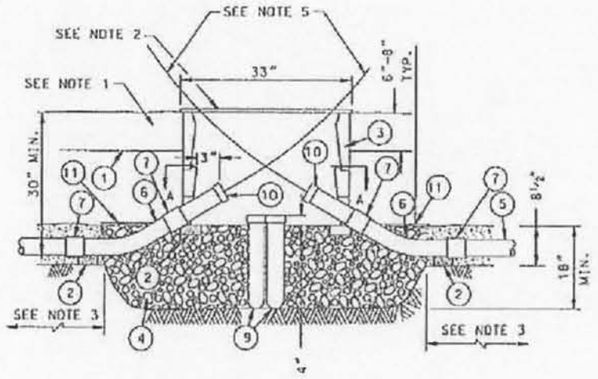
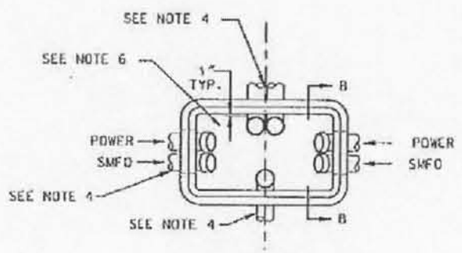
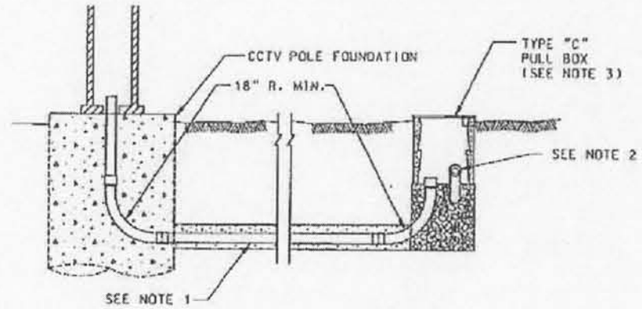




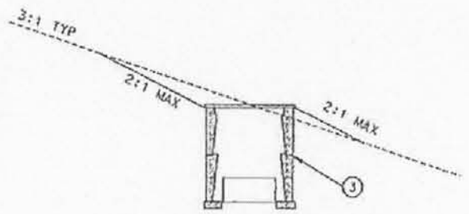
SECTION A-A



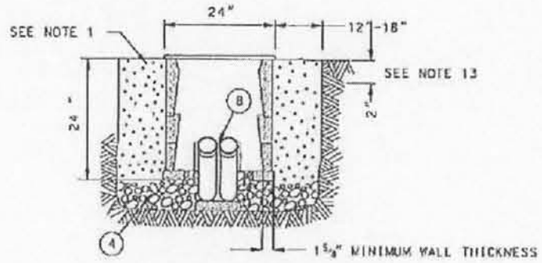
CONDUIT ARRANGEMENT FOR CCTV POLE FOUNDATION

NOTES:

1. 2-3" SCH 40 PVC CONDUIT FROM TYPE "C" PULL BOX TO CCTV FOUNDATION FOR COMMUNICATIONS AND POWER.
2. SEE PLANS FOR CONDUIT LAYOUT AND CONTENTS AT EACH CCTV LOCATION.
3. TYPE "C" PULL BOX NOT RELEVANT TO EVERY INSTALLATION. PER PLAN SHEETS.



INSTALLATION IN SLOPED AREAS



SECTION B-B

MATERIAL LIST	
ITEM	DESCRIPTION
1	MARKER TAPE
2	CONCRETE BUILDING BLOCK 2" x 4" x 8"
3	TYPE "C" PULL BOX WITH EXTENSION W/ EXCEPTIONS AS DRAWN
4	CLASS "D" CONCRETE AGGREGATE
5	SCHEDULE 40 P.V.C. CONDUIT
6	90 DEGREE G.R.C. ELBOW, 15" RADIUS
7	G.R.C. TO P.V.C. COUPLING
8	KNOCK OUT 6" X 12" - SEE NOTE 10
9	90 DEGREE ELBOW, 15" RADIUS
10	BELL END FOR PVC - SEE NOTE 9
11	30 lb. FELT PAPER
12	PULL BOX DELINEATOR (FLEXIBLE MARKER) - SEE NOTE 11

NOTES:

1. BACKFILL SHALL MEET THE REQUIREMENTS OF NMDOT STANDARD SPECIFICATIONS SECTION 660 - EXCAVATION, TRENCHING, AND BACKFILLING FOR UTILITIES, TO BOTTOM OF THE PULL BOX. BACKFILL AROUND SIDES OF THE PULL BOX WITH SELECT EXCAVATED MATERIAL AND THOROUGHLY COMPACT. 2" OF DECORATIVE ROCK MATCHING EXISTING SHALL BE USED TO MATCH SLOPES.
2. PULL BOXES SHALL BE DESIGNED FOR LIGHT VEHICULAR TRAFFIC. AASHTO H10 LOADING. NON-CONCRETE PULL BOXES SHALL BE DESIGNED IN ACCORDANCE WITH STRUCTURAL REQUIREMENTS OF WESTERN UNDERGROUND COMMITTEE GUIDE NO. 3.6, INCIDENTAL TRAFFIC LOADING (PARTS 4.1.3, 4.2.1 AND 4.2.3).
3. CONDUIT FROM THE TYPICAL TRENCH SECTION SHALL NOT DEFLECT BY MORE THAN 1"/FT. FROM THE ALIGNMENT PRECEDING OR FOLLOWING THE PULL BOX.
4. SIZE AND TYPE OF CONDUITS AS INDICATED ON PLANS.
5. CONDUIT C/L SHALL BE ALIGNED TO TOP EDGE OF PULL BOX TO FACILIATE CABLE PULLING.
6. ALL POWER AND COMMUNICATIONS CABLE SHALL BE TAGGED WITH CABLE IDENTIFICATION.
7. NUMBERS IN CIRCLES REFER TO ITEMS IN MATERIAL LIST.
8. "NMDOT COMMUNICATIONS" SHALL BE THE TITLE EMBOSSED ON THE LID.
9. USE PVC TO EXTEND INTO PULL BOX.
10. USE FELT PAPER TO BLOCK OPENING BETWEEN CONDUITS.
11. INSTALL FLEXIBLE MARKERS 12" IN FRONT OF EACH PULL BOX WITHOUT TOUCHING CONDUIT.
12. POUR CONTROLLED LOW STRENGTH MATERIAL UP TO WITHIN 12" OF PULL BOX.
13. IF A PULL BOX IS INSTALLED IN PAVEMENT OR SIDEWALK, THE TOP OF THE PULL BOX SHALL BE FLUSH WITH THE TOP OF THE PAVEMENT OR SIDEWALK.

SURVEY INFORMATION

FIELD NOTES

NO.	BY	DATE

BENCH MARKS

NGS STAINLESS ROD SET BENEATH A 5 1/2" ACCESS COVER STAMPED "D-438, 1984". SE QUADRANT OF MONTANO RD. & THE BNSF RAILROAD TRACKS, 42.5 FT. EAST OF CENTERLINE OF THE TRACKS, 44 FT. SOUTH OF CENTERLINE OF MONTANO RD. NE, 1.1 FT. WEST OF CHAIN LINK FENCE.

DATUM NAVD 1988

ELEV. 4978.070

AS BUILT INFORMATION

CONTRACTOR

WORK STARTED BY DATE

INSPECTOR'S ACCEPTANCE BY DATE

FIELD VERIFICATION BY DATE

DRAWINGS CORRECTED BY DATE

MICRO-FILM INFORMATION

RECORDED BY DATE

NO.

ENGINEER'S SEAL

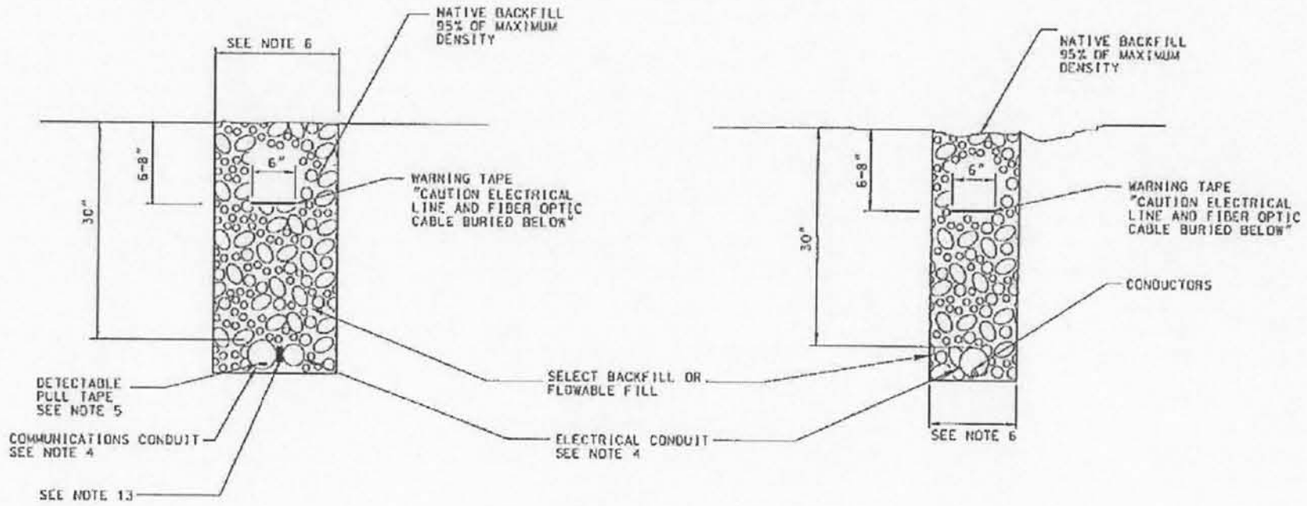
NO.	DATE	REMARKS	BY
		REVISIONS	
		DESIGN	
		DESIGNED BY SL DATE 10/2010	
		DRAWN BY BN DATE 10/2010	
		CHECKED BY KA DATE 10/2010	

CITY OF ALBUQUERQUE
DEPARTMENT OF MUNICIPAL DEVELOPMENT
ENGINEERING DIVISION

TITLE: **NEW MEXICO RAIL RUNNER EXPRESS**
MONTANO STATION
PULL BOX TYP C INSTALLATION DETAILS

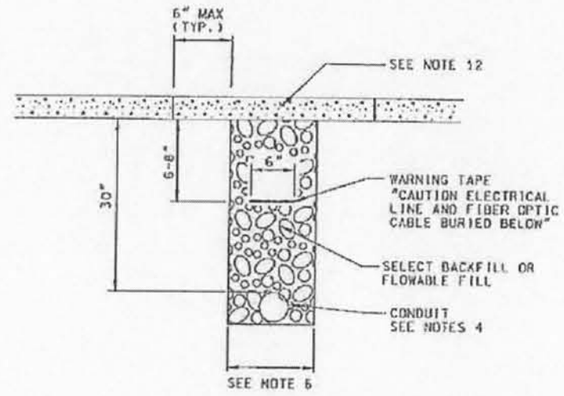
Design Review Committee | City Engineer Approval | Last Design Update | No. / Day / Yr. | No. / Day / Yr.

City Project No. 559282 | Zone Map No. F-15 | Sheet 41 Of 46



TYPICAL UNPAVED TRENCHES

NOT TO SCALE



TYPICAL PAVED TRENCHES

NOT TO SCALE

NOTES:

1. THE TRENCH BOTTOM SHALL BE SMOOTH, FLAT AND WITHOUT ROCKS OR OTHER IMPEDIMENTS. FLOWABLE FILL SHALL MEET THE REQUIREMENTS OF SECTION 516 OF THE STANDARD SPECIFICATIONS.
2. ALL TRENCHES BETWEEN POWER SERVICES AND DEVICES SHALL MEET THE TRENCH REQUIREMENTS OF THE UTILITY COMPANY AND MUST BE APPROVED BY A UTILITY COMPANY CUSTOMER REPRESENTATIVE PRIOR TO TRENCHING.
3. CONDUIT COUPLINGS SHALL BE STAGGERED.
4. CONDUIT SIZE AND NUMBER MAY VARY. SEE PLANS.
5. DETECTABLE PULL TAPE SHALL BE INSTALLED INSIDE THE CONDUIT OR ONE OF THE GEOTEXTILE INNERDUCTS IF CONDUIT OR INNERDUCT CONTAINS NO COPPER CONDUCTORS.
6. TOTAL TRENCH WIDTH SHALL BE 3" NOMINAL WIDER THAN THE SUM OF OUTSIDE DIAMETERS OF CONDUIT(S) INSTALLED. CONDUIT(S) SHALL BE CENTERED IN TRENCH. SEE PLANS FOR NUMBER AND SIZE. SAWCUT SHALL BE NO MORE THAN 12" WIDER THAN TRENCH WIDTH.
7. COORDINATE WITH "NEW MEXICO ONE CALL" AT 505-260-1990 TO LOCATE ALL EXISTING UTILITIES PRIOR TO DIGGING.
8. TRENCH DEPTHS AND CONDUIT COVER ARE TO BE MEASURED FROM FINAL GRADE.
9. NATIVE BACKFILL SHALL MEET THE REQUIREMENTS OF SECTION 705.22 OF THE STANDARD SPECIFICATIONS, AND SHALL NOT CONTAIN LARGER THAN 4", LARGE PIECES OF CONCRETE, VEGETATION, AND OTHER EXTRANEOUS SUBSTANCES.
10. ALL SPOIL MATERIALS SHALL BE REMOVED OFFSITE BY THE CONTRACTOR.
11. RETURN DISTURBED AREA TO MATCH EXISTING GRADE.
12. FOR PAVED TRENCHES, REMOVE AND REPLACE EXISTING SURFACE. NEW SURFACE MATERIAL SHALL BE FROM AN APPROVED COMMERCIAL SOURCE. PAVEMENT REMOVAL SHALL BE BY SAWCUT METHOD.
13. USE CONDUIT SPACERS TO SEPARATE MULTIPLE CONDUITS IN TRENCH BY AT LEAST 1". PLACE SPACERS AT INTERVALS OF A MAXIMUM 5 FEET.
14. INSTALL ALL CONDUIT PER UNIFORM STANDARD DRAWINGS AND SPECIFICATIONS FOR CONSTRUCTION.

ENGINEER'S SEAL

SURVEY INFORMATION

FIELD NOTES

NO.	BY	DATE

BENCH MARKS

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DATUM NAVD 1988

ELEV. 4978.070

AS BUILT INFORMATION

CONTRACTOR — DATE

WORK STAKED BY — DATE

INSPECTOR'S ACCEPTANCE BY — DATE

VERIFICATION BY — DATE

DRAWINGS CORRECTED BY — DATE

MICRO-FILM INFORMATION

RECORDED BY — DATE

NO.

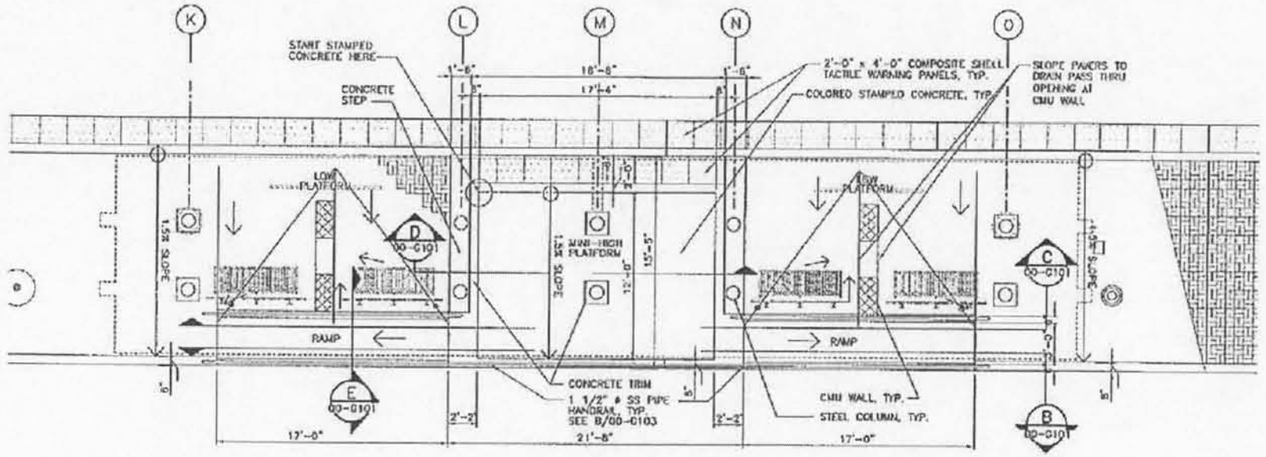
NO.	DATE	REVISIONS / REMARKS	BY
		DESIGNED	SL — 10/2010
		DRAWN	BN — 10/2010
		CHECKED	KA — 10/2010

CITY OF ALBUQUERQUE
DEPARTMENT OF MUNICIPAL DEVELOPMENT
ENGINEERING DIVISION

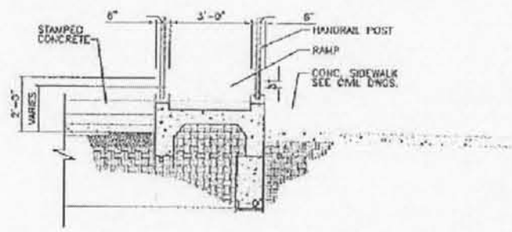
TITLE: **NEW MEXICO RAIL RUNNER EXPRESS**
MONTANO STATION
TYPICAL TRENCH DETAIL

Design Review Committee	City Engineer Approval	Last Design Update	No. / Day / Yr.	No. / Day / Yr.

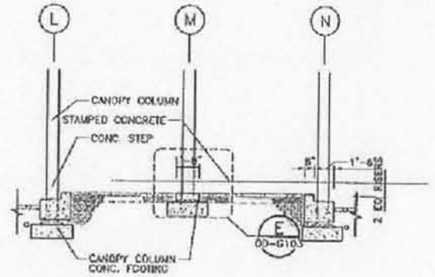
City Project No.	Zone Map No.	Sheet	Of
559282	F-15	42	46



TYPICAL MINI-HIGH ENLARGED PLAN
SCALE 1/4" = 1'-0"
A 00-G101

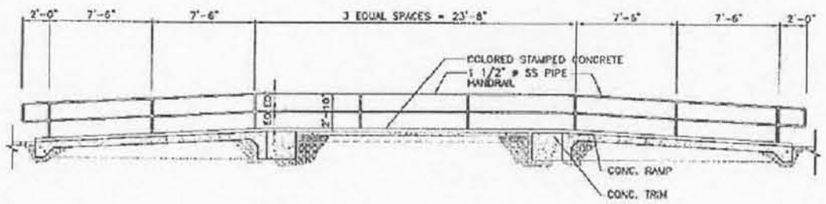


RAMP CROSS SECTION
SCALE 1/2" = 1'-0"
E 00-G101

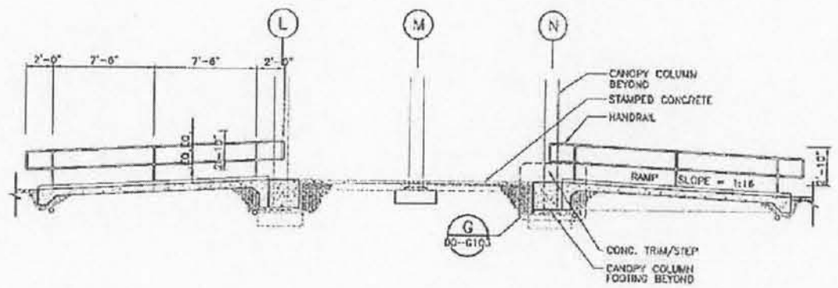


MINI-HIGH PLATFORM SECTION
SCALE 1/4" = 1'-0"
D 00-G101

GENERAL NOTES	
A	FOR CANOPY STRUCTURE CONST. NOTES & DIMENSIONS SEE DWG. 01-A401/01-A402, 02-A401/02-A402, 03-A401/03-A402, 05-A401/05-A402, 07-A401/07-A402, 08-A401/08-A402
B	FOR TYPICAL MINI HIGH DETAILS SEE DWG. 00-G103 AND 00-G104
C	FOR SPOT ELEVATIONS SEE STATION PLATFORM PLANS
D	FOR REINFORCING DETAILS SEE DWG 00-G104



SECTION THROUGH RAMP
SCALE 1/4" = 1'-0"
B 00-G101



SECTION THROUGH RAMP
SCALE 1/4" = 1'-0"
C 00-G101

2' 0' 2' 4'

AS BUILT INFORMATION
CONTRACTOR — DATE
WORK STAKED BY — DATE
INSPECTORS ACCEPTANCE BY — DATE
FIELD VERIFICATION BY — DATE
DRAWINGS REVISED BY — DATE
MICRO-FILM INFORMATION
RECORDED BY — DATE
NO.

BENCH MARKS
NGS STAINLESS ROD SET BENEATH A 5 1/2" ACCESS COVER STAMPED "D-438, 1984", SE QUADRANT OF MONTANO RD. & THE BNSF RAILROAD TRACKS, 42.5 FT. EAST OF CENTERLINE OF THE TRACKS, 44 FT. SOUTH OF CENTERLINE OF MONTANO RD. NE, 1.1 FT. WEST OF CHAIN LINK FENCE.
DATUM NAVD 1988
ELEV. 4978.070

SURVEY INFORMATION
FIELD NOTES

NO.	BY	DATE

ENGINEER'S SEAL

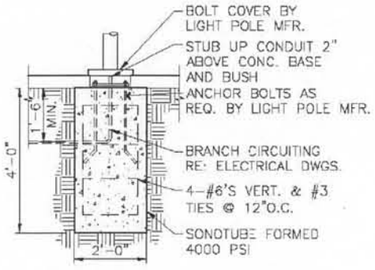
NO.	DATE	REMARKS	BY
		REVISIONS	
		DESIGN	
		DESIGNED BY	DATE
		DRAWN BY	DATE
		CHECKED BY	DATE

CITY OF ALBUQUERQUE
DEPARTMENT OF MUNICIPAL DEVELOPMENT
ENGINEERING DIVISION

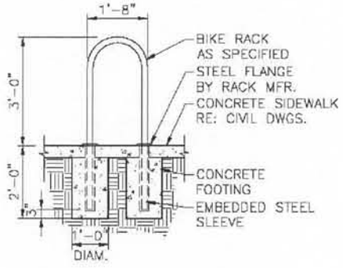
TITLE: NEW MEXICO RAIL RUNNER EXPRESS
MONTANO STATION
TYPICAL MINI HIGH PLATFORM SECTIONS

Design Review Committee	City Engineer Approval	Last Design Update	No. / Sky / Yr.	No. / Sky / Yr.

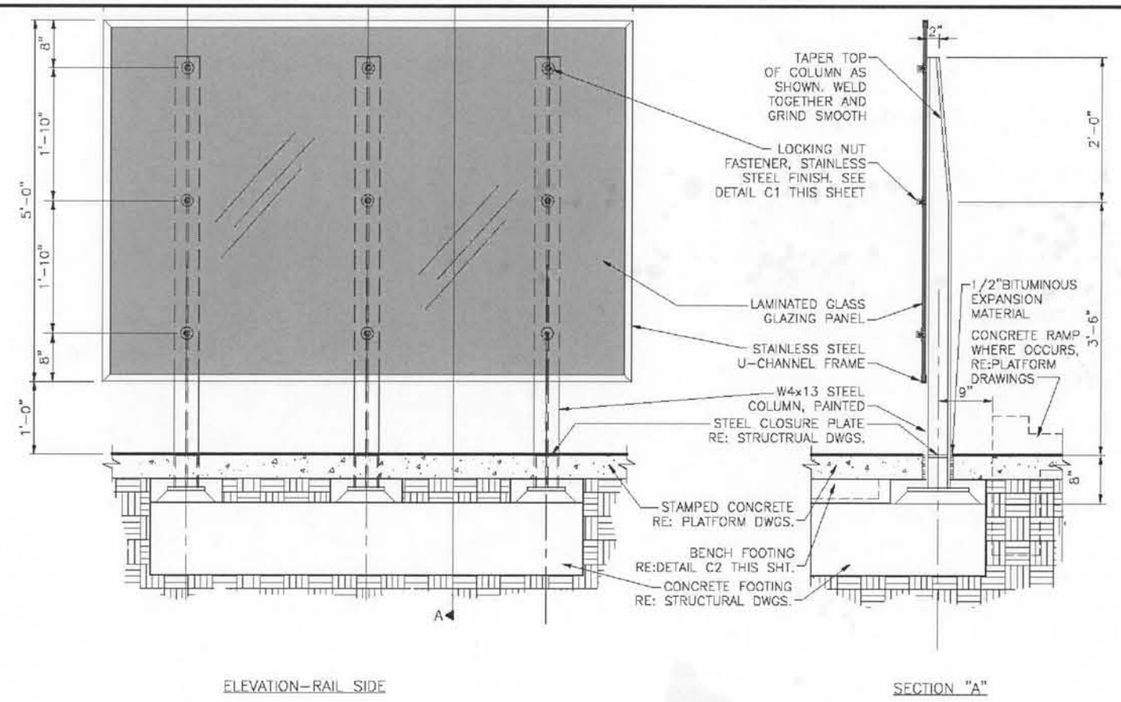
City Project No.	Zone Map No.	Sheet	Of
559282	F-15	43	46



PCLE BASE DETAIL — A1 — 1/4"=1'-0" — 01-XX



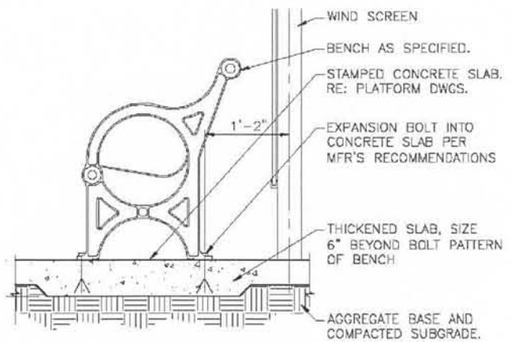
BIKE RACK — A2 — 1/4"=1'-0" — 01-XX



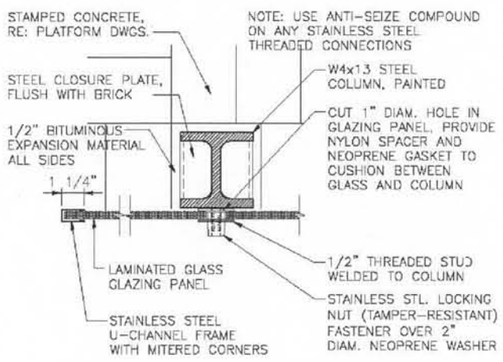
ELEVATION–RAIL SIDE

SECTION "A"

WIND SCREEN ELEV./SECT. — A3 — 1/2"=1'-0" — 01-XX

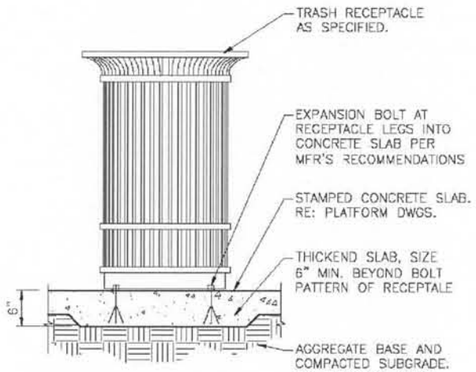


BENCH MOUNTING — D1 — 1/2"=1'-0" — 01-XXX

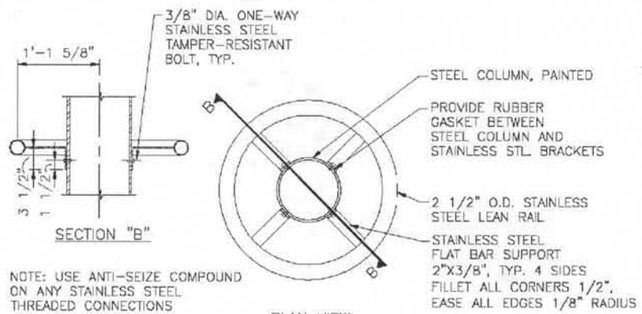


PLAN VIEW

WIND SCREEN FASTENER — D2 — 1 1/2"=1'-0" — 01-A403



TRASH RECEPT. MOUNTING — D3 — 1/2"=1'-0" — 01-XX



PLAN VIEW

LEAN RAIL — D4 — 1/2"=1'-0" — 01-XX

SURVEY INFORMATION

FIELD NOTES

NO.	BY	DATE

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DATUM: NAVD 1988

ELEV. 4978.070

AS BUILT INFORMATION

CONTRACTOR

WORK COMPLETED BY — DATE

INSPECTOR'S ACCEPTANCE BY — DATE

FIELD VERIFICATION BY — DATE

DRAWINGS CORRECTED BY — DATE

MICRO-FILM INFORMATION

RECORDED BY — DATE

NO.

ENGINEER'S SEAL

NO.	DATE	REMARKS	BY
		REVISIONS	
		DESIGN	

DESIGNED BY SL — DATE 10/2010

DRAWN BY BN — DATE 10/2010

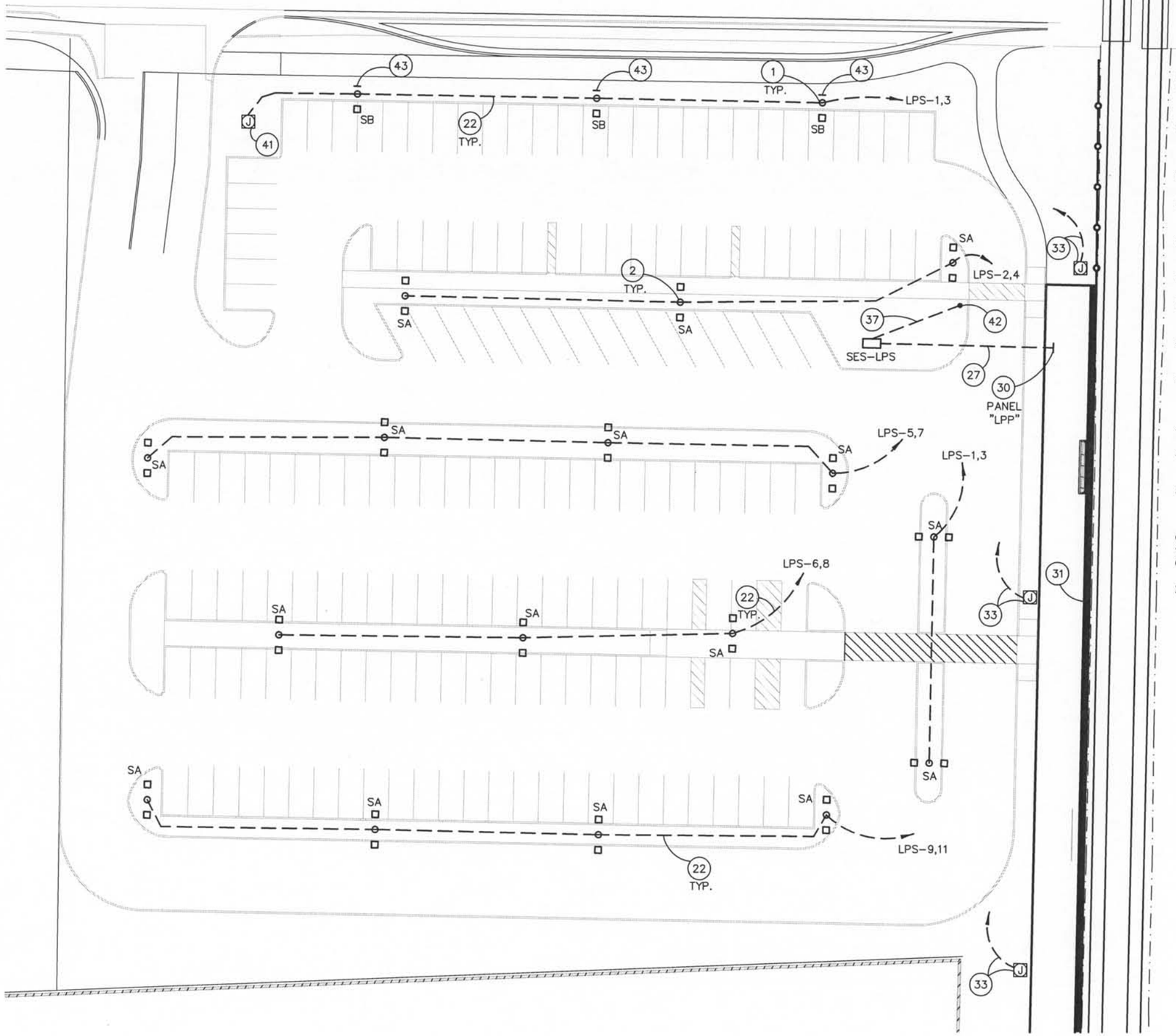
CHECKED BY KA — DATE 10/2010

CITY OF ALBUQUERQUE
DEPARTMENT OF MUNICIPAL DEVELOPMENT
ENGINEERING DIVISION

TITLE: **NEW MEXICO RAIL RUNNER EXPRESS**
MONTANO STATION
MISCELLANEOUS PLATFORM DETAILS

Design Review Committee	City Engineer Approval	Last Design Update	No. / Dsg / Yr.	No. / Dsg / Yr.

City Project No. **559282** — Zone Map No. **F-15** — Sheet **44** Of **46**



KEYED NOTES:

1.) FURNISH AND INSTALL NEW 400 W MH FIXTURE MOUNTED AT 20' OVERALL. FIXTURE SCHEDULES AND FOUNDATIONS PER DETAIL ON DWG 00-G050.

2.) FURNISH AND INSTALL TWO NEW 400 W MH FIXTURES MOUNTED AT 20' OVERALL. FIXTURE SCHEDULES AND FOUNDATIONS PER DETAIL ON DWG 00-G050.

9.) NEW SES/LIGHTING CONTROL CABINET LPS.

22.) INSTALL 1" SCHEDULE 40 PVC CONDUIT WITH 2 #8 CONDUCTORS AND 1 #8 GROUND.

27.) INSTALL 2" SCHEDULE 40 PVC CONDUIT WITH 3 #1 CONDUCTORS AND 1 #6 GROUND.

30.) PLATFORM PANEL. VERIFY PANEL LOCATION.

31.) FOR PLATFORM POWER AND LIGHTING, SEE DETAILS ON DWG 00-G053 AND DWG 00-G055.

33.) INSTALL 1" SCHEDULE 40 PVC EMPTY CONDUIT WITH PULL STRING. STUB-OUT IN NEW WB J-BOX.

37.) INSTALL 3" SCHEDULE 40 PVC CONDUIT WITH 3 #40 CONDUCTORS AND POLE RISER PER PNM DETAILS DS-4-10.0. LOOP CONDUCTORS AND HANG ON POLE FOR CONNECTION BY PNM.

41.) CONDUIT AND CONDUCTORS TO TERMINATE AT STATION ENTRY SIGN.

42.) EXISTING POWER/COMMUNICATIONS POLE. COORDINATE WITH PNM AND QWEST COMMUNICATIONS.

43.) FURNISH AND INSTALL HOUSE-SIDE GLARE SHIELD ON STREET SIDE OF FIXTURE.

GENERAL NOTES:

1.) FOR SES/LIGHTING CONTROL CABINET ELEVATION AND EXTERIOR LIGHTING CONTRACTOR DETAILS SEE DWG 00-G050.

2.) FOR PLATFORM CONTROL CABINET ELEVATION AND EXTERIOR LIGHTING CONTRACTOR DETAILS SEE DWG 00-G054.

GRAPHIC SCALE

40 0 20 40 80

(IN FEET)

1 inch = 40 ft.

Vector Engineering, LLC

AS BUILT INFORMATION
CONTRACTOR
WORK STAKED BY — DATE
INSPECTED BY — DATE
ACCEPTANCE BY — DATE
FIELD VERIFICATION BY — DATE
DRAWINGS CORRECTED BY — DATE

BENCH MARKS
NGS STAINLESS ROD SET BENEATH A 5 1/2" ACCESS COVER STAMPED "D-438, 1984", SE QUADRANT OF MONTANO RD. & THE BNSF RAILROAD TRACKS, 42.5 FT. EAST OF CENTERLINE OF THE TRACKS, 44 FT. SOUTH OF CENTERLINE OF MONTANO RD. NE, 1.1 FT. WEST OF CHAIN LINK FENCE.
DATUM NAVD 1988
ELEV. 4978.070

MICRO-FILM INFORMATION
RECORDED BY — DATE
NO.

SURVEY INFORMATION
FIELD NOTES
NO. | BY | DATE

ENGINEER'S SEAL

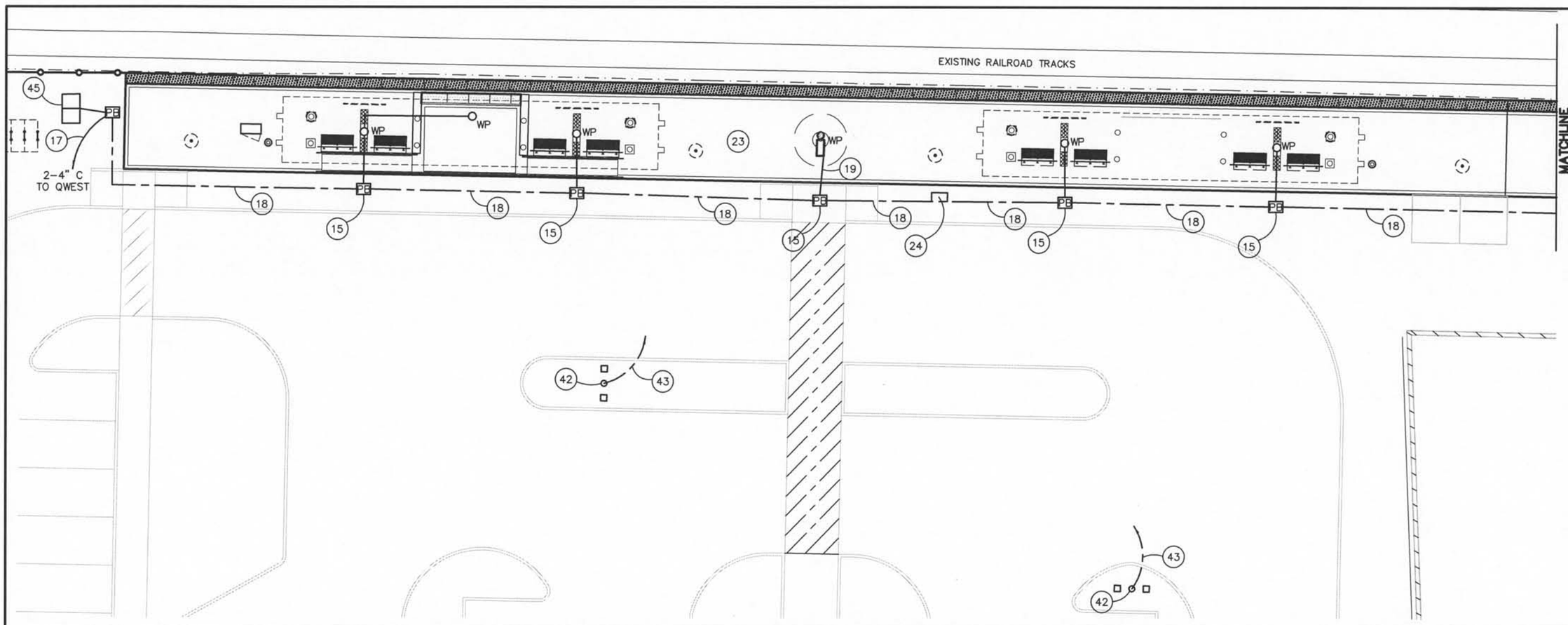
NO.	DATE	REMARKS	BY
		REVISIONS	
		DESIGN	
		DESIGNED BY NA	DATE 10/2010
		DRAWN BY BN	DATE 10/2010
		CHECKED BY KA	DATE 10/2010

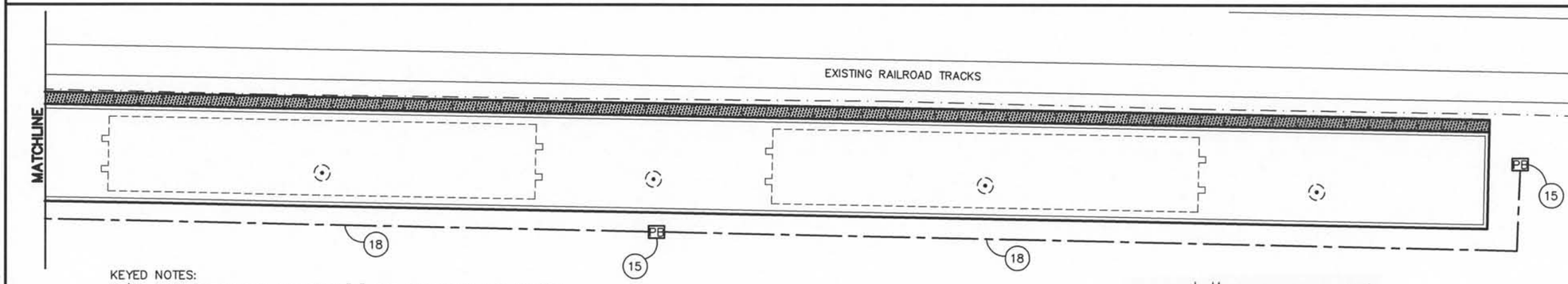
CITY OF ALBUQUERQUE
DEPARTMENT OF MUNICIPAL DEVELOPMENT
ENGINEERING DIVISION

TITLE: NEW MEXICO RAIL RUNNER EXPRESS
MONTANO STATION
LIGHTING PLAN

Design Review Committee | City Engineer Approval | Last Design Update | No. / Day / Yr. | No. / Day / Yr.

City Project No. 559282 | Zone Map No. F-15 | Sheet 45 Of 46





KEYED NOTES:

15.) COMMUNICATION PULL BOX TYPE "C". SEE DETAILS ON DWG 00-G061.

17.) (2) 4" CONDUITS W/PULLSTRING TO COMMUNICATION POINT OF SERVICE LOCATION. COORDINATE LOCATION W/QWEST.

18.) INSTALL (2) 4" SCHEDULE 40 PVC CONDUITS. SEE DETAILS ON DWG 00-G060 AND 00-G062.

19.) INSTALL (2) 2" SCHEDULE 40 PVC CONDUITS. VERIFY SIZE BEFORE INSTALLATION. SEE DETAILS ON DWG 00-G060 AND 00-G062.

23.) FOR PLATFORM INSTALLATION, SEE DETAILS ON DWG 00-G059.

24.) FUTURE TICKET VENDING MACHINE. (TVM)

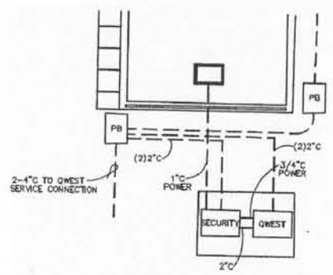
42.) MOUNT FUTURE CAMERA ON PARKING LIGHT POLE.

43.) CONDUIT FOR FUTURE CAMERA TO RUN TO COMMUNICATION BOX LOCATED ON PLATFORM. VERIFY WITH CONTRACTOR AND MRCOG.

GENERAL NOTES:

1.) CONTRACTOR IS ALERTED TO THE PRESENCE OF EXISTING UNDERGROUND AND OVERHEAD UTIITIES SITE. CONTRACTOR IS RESPONSIBLE FOR THE VERIFICATION AND AVOIDANCE OF SAID LINES.

2.) ADDITIONAL CAMERA(S) TO BE MOUNTED IN PLATFORM AREA. CONTRACTOR SHALL COORDINATE LOCATION(S) WITH MRCOG.



COMMUNICATION DETAIL

ENGINEER'S SEAL

SURVEY INFORMATION

FIELD NOTES

NO.	BY	DATE

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NGS STAINLESS ROD SET BENEATH A 5 1/2" ACCESS COVER STAMPED "D-438, 1984", SE QUADRANT OF MONTANO RD. & THE BNSF RAILROAD TRACKS, 42.5 FT. EAST OF CENTERLINE OF THE TRACKS, 44 FT. SOUTH OF CENTERLINE OF MONTANO RD. NE, 1.1 FT. WEST OF CHAIN LINK FENCE.

DATUM NAVD 1988

ELEV. 4978.070

AS BUILT INFORMATION

CONTRACTOR — DATE

WORK STAKED BY — DATE

INSPECTOR'S ACCEPTANCE BY — DATE

FIELD VERIFICATION BY — DATE

DRAWINGS CORRECTED BY — DATE

MICRO-FILM INFORMATION

RECORDED BY — DATE

NO.

NO.	DATE	REMARKS	BY
REVISIONS			
		DESIGN	
		DESIGNED BY SL	DATE 10/2010
		DRAWN BY BN	DATE 10/2010
		CHECKED BY KA	DATE 10/2010

CITY OF ALBUQUERQUE
DEPARTMENT OF MUNICIPAL DEVELOPMENT
ENGINEERING DIVISION

TITLE: **NEW MEXICO RAIL RUNNER EXPRESS MONTANO STATION COMMUNICATION PLAN**

Design Review Committee	City Engineer Approval	Last Design Update	No. / Day / Yr.	No. / Day / Yr.

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