

## Chapter 4 Zoning and Development Regulations

## Zoning and Development Regulations

This chapter establishes the regulatory framework to realize City and Community vision for the plan area. The zoning is prescriptive so that development outcomes are more predictable for property owners, the City Review authorities, and community stakeholders, while ensuring flexibility to encourage development.

This chapter contains the following sections:

- 1.0 Zone Descriptions
- 2.0 Development Compliance
- 3.0 Development Approval Process
- 4.0 Zone Map
- 5.0 Useful Definitions
- 6.0 Zones
- 7.0 General Development Standards

### 1.0 Zone Descriptions

The zoning developed for the Plan consists of both conventional zones and form based zones. For the purpose of this Plan, a conventional zone is defined as “use based” zone that is typical of Comprehensive City Zoning Code prior to the adoption of the Form based zones. Whereas, in this Plan, form based zones prescribe what the development of a zone shall look like and may or may not specify what uses are permitted. Form based zones require that a use be accommodated within specific building types. All zones included in this Plan are subject to the General Development Standards of this Plan.

Modified conventional zones: These zones consist of modifications to existing Comprehensive Zoning Code zones. The SU- 2 zone within the Plan includes new regulations and references the Comprehensive Zoning Code for all other requirements. The conventional zones included in this Plan are as follows and are articulated beginning on p. X.

**SU-2/W66 R-2 - W66 Moderate Density Residential Zone.** The W66 Moderate Density Residential zone corresponds to the R-2 zone of the Comprehensive zoning code with the addition of the General Development Standards of this Plan.

**SU-2/W66 C-2 – W66 Community Commercial Zone.** The W66 Community Commercial zone creates a mixed commercial and residential zone, based on City C-2 zoning, with added residential opportunities to increase development flexibility while providing community commercial goods and services. The residential component only is form based. The W66 Community Commercial zone is subject to the General Development Standards of this Plan.

**SU-2/W66 O-1 – W66 Office Zone.** The W66 Office zone corresponds to the O-1 zone in the Comprehensive Zoning Code with the addition of the General Development Standards of this Plan.

**SU-2/W66 IP - W66 Industrial Park Zone.** The W66 Industrial Park zone corresponds to the IP zone in the Comprehensive Zoning Code with the addition of the General Development Standards of this Plan.

**SU-2/W66 SU-1 - W66 Special Use Zone.** The W66 Special Use zone corresponds to the SU-1 zone in the Comprehensive Zone Code. Sites within SU-1 zones without approved, current site development plans are also regulated by the General Development Standards of this Plan and applicable regulations of the Zoning Code.

## Zoning and Development Regulations

Form based zones: These zones have been developed for the Plan by tailoring the form based zones in the Comprehensive Zoning Code. The SU-2 form based zones within the plan include new regulations and reference the Comprehensive Zoning Code for all other requirements including references to Building and Frontage Types and the General Standards sections. The form based zones developed for this Plan are as follows and are articulated beginning on p. X

**SU-2/W66 CAC – W66 Community Activity Center Zone.** The W66 Community Activity Center zone is based on the Zoning Code’s Community Activity Center Zone (FBZ TOD-COM) in order to develop a mixed use center in a Comprehensive Plan designated Community Activity Center. A Community Activity Center is envisioned as a mixed use center with attractive, high quality development that meets the commercial and service needs of a community wide area in an environment that is comfortable and inviting for pedestrians and bicyclists.

**SU-2/W66 EPR – W66 Employment Park Residential Zone.** The W66 Employment Park Residential zone creates a new zone which allows for employment and residential opportunities. This zone creates the opportunity to mix employment and residential in a campus environment where residents have the option to live close enough to their place of employment to be able to walk to work or to have the option of living within a live/work development.

**SU-2/W66 MAC – W66 Major Activity Center Zone.** The W66 Major Activity Center zone is based on the Zoning Code’s Major Activity Center zone (FBZ TOD-MAC) in order to develop a commercial, service and employment center in designated Major Activity Centers. A Major Activity Center is envisioned as a mixture of uses at a higher intensity of development that serves a regional population. The W66 MAC zoning is intended to create a center with commercial and employment development to serve all of southwest Albuquerque.

**SU-2/W66 MX – W66 Mixed Use Zone.** The W66 Mixed Use zone is a form based zone based on the Zoning Code’s Mixed Use zone (FBZ MX) and is sensitive to the existing built environment of the West Central corridor. The W66 Mixed Use zone is located in a part of the corridor that is envisioned as a vibrant pedestrian zone that includes a mix of commercial and residential activity and is easily accessed by pedestrians, bicyclists and automobiles alike. The zone is intended to enhance a fine grained, existing development character of minimal setbacks and orientation toward the street.

**SU-2/W66 RA – W66 River Activity Zone.** The W66 River Activity zone is a form based zone developed to support and facilitate limited residential, commercial development and public facilities that capitalize on proximity to the Rio Grande and serve cultural, retail and recreational needs of residents and visitors. The proposed zone provides for multi-modal access and lower density commercial uses such as restaurants, recreational goods and services, community garden, museum, educational facilities and low impact outdoor recreational uses that are appropriate in locations adjacent to the river, the acequia system and the Bosque trails.

**SU-2/W66 SAC – W66 Special Activity Center Zone.** The W66 Special Activity Center zone is a mixed use, form based zone developed to support and facilitate development which serves neighborhood and tourism needs while remaining sensitive to the character of the Old Town and BioPark areas. The Special Activity Center is envisioned as a distinctive district of land uses that support the existing amenities in the area. Amenities such as the BioPark and Tingley Beach are currently location specific attractions. People visit these sites then leave the area. The intent of SAC zone is to create a special activity district where, for example, people can visit the BioPark and then walk down the street to have lunch at a café with an outdoor patio before crossing Central Ave. to take in some afternoon fishing at Tingley Lakes.

## Development Compliance

### 2.0 Development Compliance

Development shall comply with the goals, intent and design regulations of the West Route 66 Sector Development Plan as follows:

- A. For undeveloped sites: all new development.
- B. For sites with existing structures: when there is an increase of 25% or more of a building's existing square footage.
- C. Exemptions to compliance with the zoning regulations of this Plan:
  1. Repairs, facade improvements and maintenance of existing structures and/or buildings.
  2. Construction of a replacement building after involuntary damage to or destruction of existing building (such as fire damage).
  3. Change in ownership of a parcel.

### 2.1 Existing Entitlements

All properties with existing approved site development plans, including SU-1 sites that have been rezoned by the Sector Development Plan, will retain their existing zoning entitlements. Site with approved, current site development plans are regulated by the General Development Standards of this Plan and applicable regulations of the Zoning Code for additions of 25% or more of building area.

	W66 C2	W66 CAC	W66 EPR	W66 MAC	W66 MX	W66 RA	W66 SAC
<b>Establish compliance and process *</b>	Pre-Application Review	Pre-Application Review	Pre-Application Review	Pre-Application Review	Pre-Application Review	Pre-Application Review	Pre-Application Review
<b>Compliant on Use and Form</b>	< 5 acres: Building Permit 5 acres or more: SDP by EPC	Building Permit or SDP by DRB**					
<b>Compliant on Use and Minor Modifications to Form</b>	Form modifications: Planning Director then as above	Form modifications: Planning Director then as above	Form modifications: Planning Director then as above	Form modifications: Planning Director then as above	Form modifications: Planning Director then as above	Form modifications: Planning Director then as above	Form modifications: Planning Director then as above
<b>Compliant on Use and Major Modifications to Form</b>	SDP by EPC	SDP by EPC	SDP by EPC	SDP by EPC	SDP by EPC	SDP by EPC	SDP by EPC

Table 1: Development Approval Matrix.

\* Developments with a single land use and on sites of under 1 acre are exempt from Pre-Application Review.

\*\* DRB approval is required if development includes phasing or platting, or requires infrastructure. DRB provides sketch plat review prior to submittal.

#### Notes:

*Conditional uses:* Conditional use approval by the Zoning Hearing Examiner (ZHE) is required prior to building permit or site development plan approval.

*Conventional zones:* Building permit or approval of site development plan by DRB.

*Modifications to DPM and subdivision standards that are not described by the W66 form based zones:* EPC approval is required.

*Public notification:* All cases heard by the EPC or ZHE, and all site development plans heard by the DRB are publicly notified.

*SDP:* Site Development Plan. The amount of detail on the SDP shall be sufficient to demonstrate compliance with applicable requirements of the Plan.

## 3.0 Development Approval Process

All development occurring in the following SU-2 zones requires review and approval as outlined in the matrix below.

### 3.1 Modifications to Sector Plan Zoning Regulations

The zoning regulations in this Plan attain a level of detail in order to provide certainty for applicants, neighborhoods and zoning staff. However, it is not the intent of the regulations to limit design creativity or ignore unique site conditions. As such, two levels of modifications to the zoning regulations are permitted as follows:

A. Minor: The Planning Director or his/her designee may approve deviations from any dimensional standards by no more than 10%.

B. Major: Any modification of dimensional standard that is greater than 10% shall be reviewed by EPC.

In order for the EPC to grant the deviation(s) and approve the site development plan, the applicant must demonstrate that 1) the original standard(s) cannot be reasonably met without substantial hardship, due to the uniqueness of the site, and 2) applicable goals, policies and intents of the Plan are still met, even with the proposed deviation(s).

### 3.2 Non-conforming uses

Any existing, legal use that becomes a non-conforming use upon the adoption of the Plan shall not require a public hearing to become a conditional use. An application for Conditional Use shall be filed with the Planning Department within 6 months of the adoption of this plan.

Existing uses that are non-conforming upon adoption of the plan are APPROVED CONDITIONAL USES per the requirements in §14-16-4-2(D) of the Zoning Code. An approved conditional use shall be void/expired if the use ceases for a continuous period of one year or more. Upon expiration of the approved conditional use, the property owner is required to comply with the regulations of the adopted zone.

### 3.3 Relationship with other City Codes

Where a conflict arises between zoning regulations and general development standards in this Plan and the Zoning Code, this Plan shall prevail. When the Plan is silent on an issue that would otherwise be governed by the Zoning Code,

or other applicable City codes and Rank III plans, including the Rio Grande Boulevard Corridor Plan and the H-1 Historic Old Town zone, those codes and plans shall prevail.

*Modifications/flexibility from DPM standards.* The regulations set forth by the form based zones allow variation from standard DPM practices. Deviation from the DPM standards not described by the form based zones, but which support the greater intent of the form based zones, may be granted by the Environmental Planning Commission (EPC) upon reasonable justification by the applicant and only in conjunction with a site development plan for form based zones. Deviations granted by the EPC shall be acknowledged and implemented by the Development Review Board.

### 3.4 Subdivision and Public Right-of-Way Regulations

Subdivision and Public Right-of-Way Regulations were added to the 1987 plan in 2009 for the purposes of creating more pedestrian oriented subdivision and roadway development standards and improving connectivity between residential areas and commercial and community services. Applicable standards related to land use have been incorporated in the zoning regulations and general development standards of this Plan.

*Modifications/flexibility from subdivision standards.* In order to create a more compact urban form and allow for more efficient infrastructure design, the Environmental Planning Commission may grant deviations from the city's subdivision standards for form based zones upon reasonable justification by the applicant. Modifications granted by the EPC shall be acknowledged and implemented by the Development Review Board.

## 4.0 Zone Map

The following maps establish the SU-2 zoning for the plan area including new zones indicated by the prefix W66.

# Zone Map

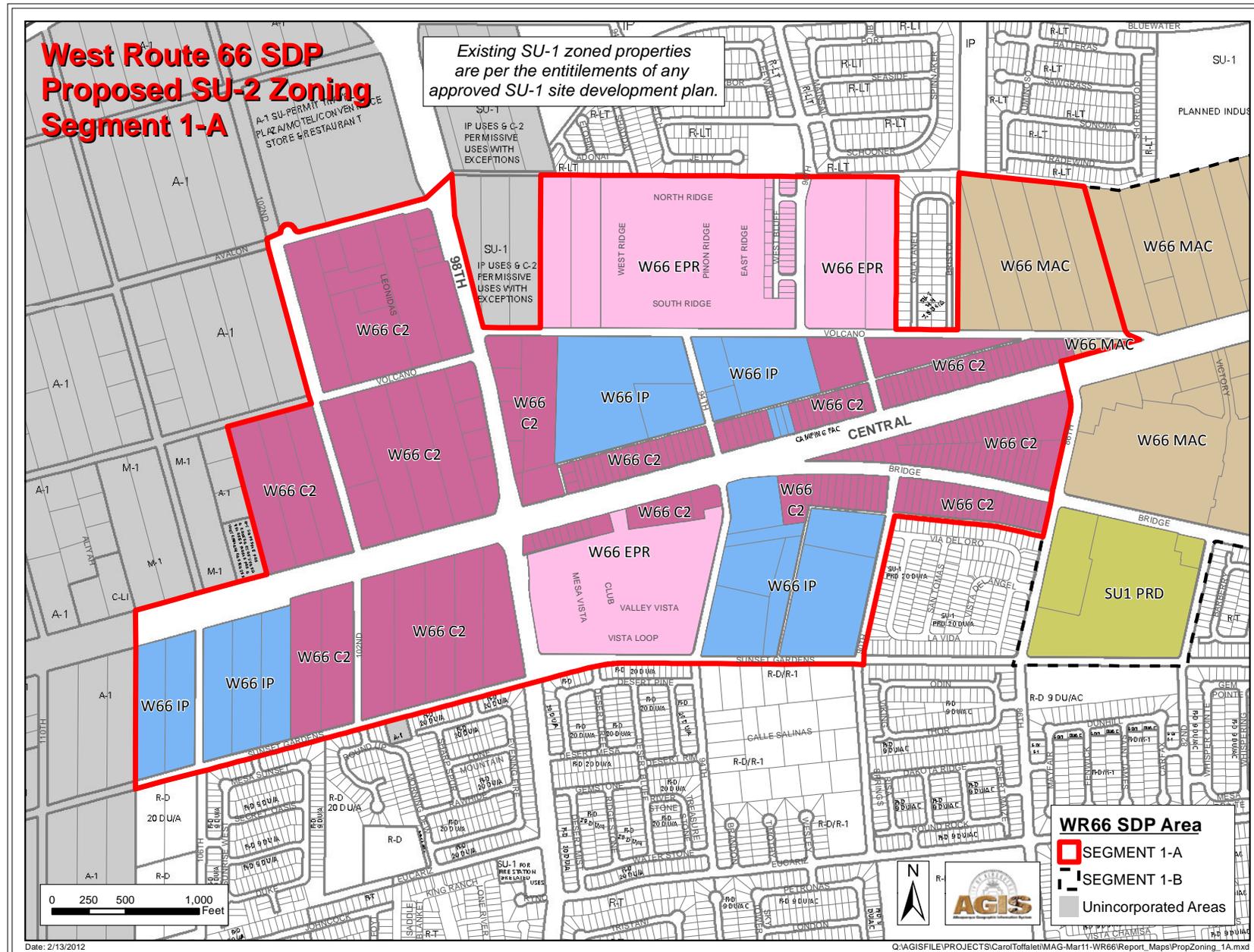


Figure: 38 Zoning Segment 1-A

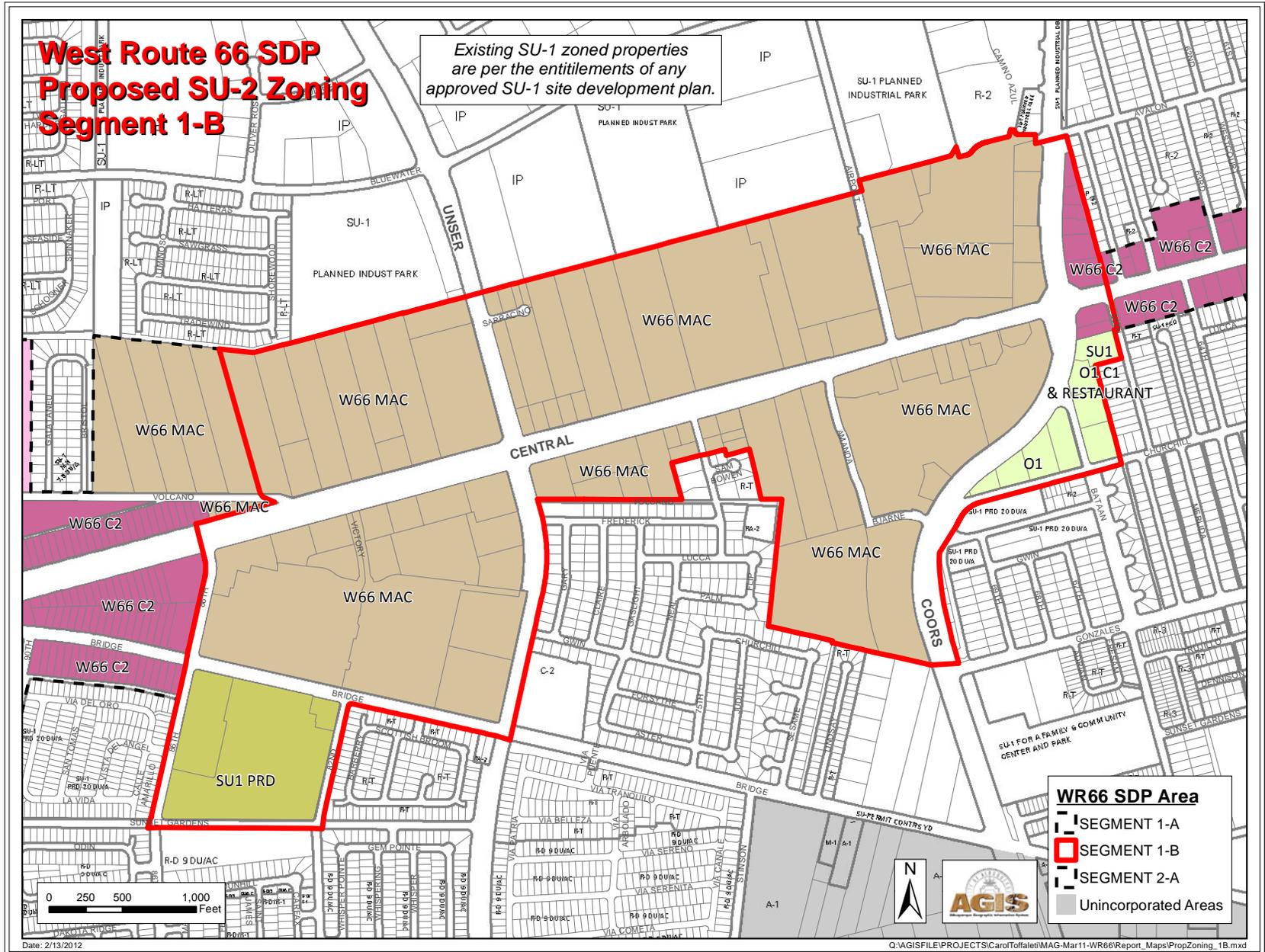


Figure: 39 Zoning Segment 1-B

# Zone Map

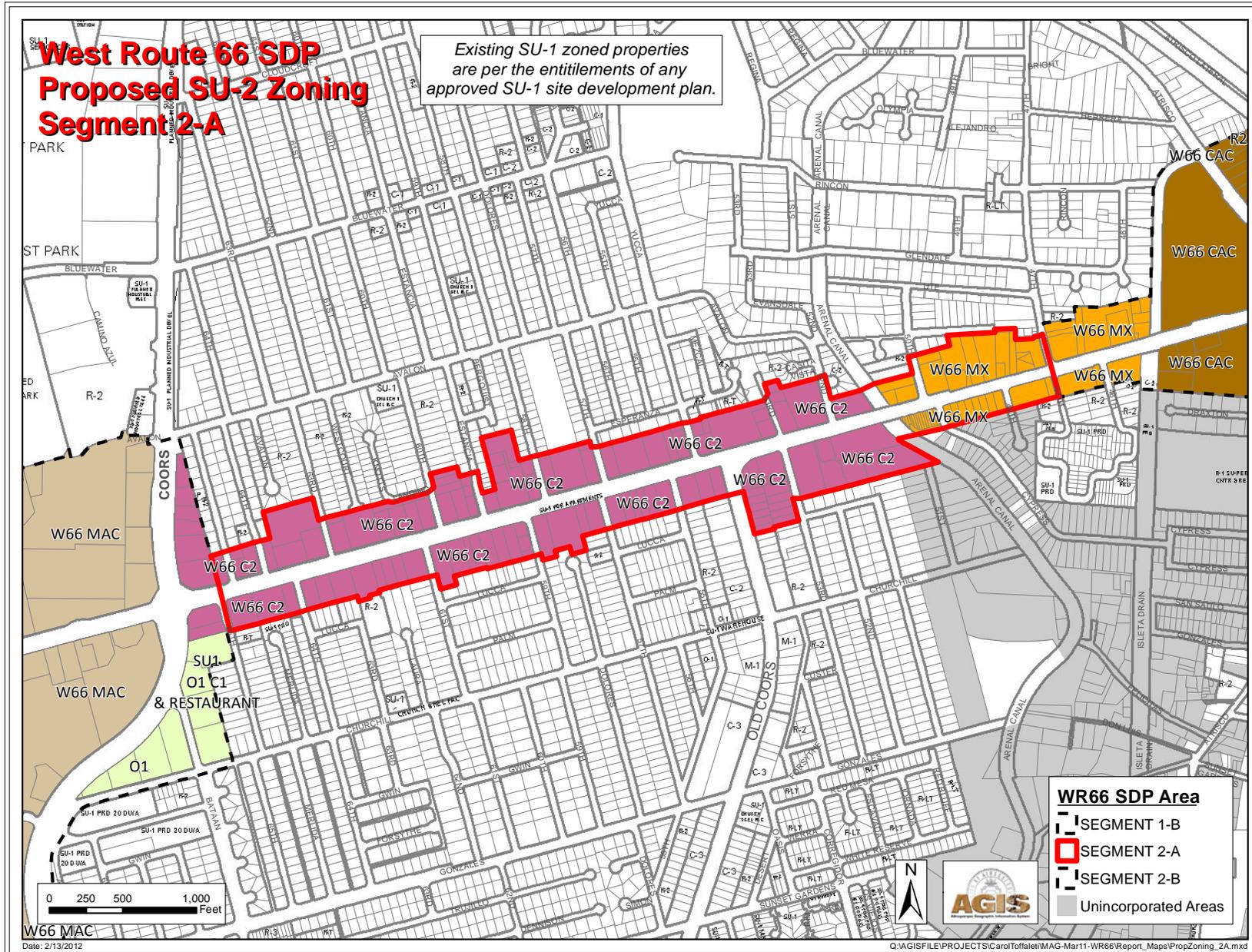


Figure 40: Zoning Segment 2-A

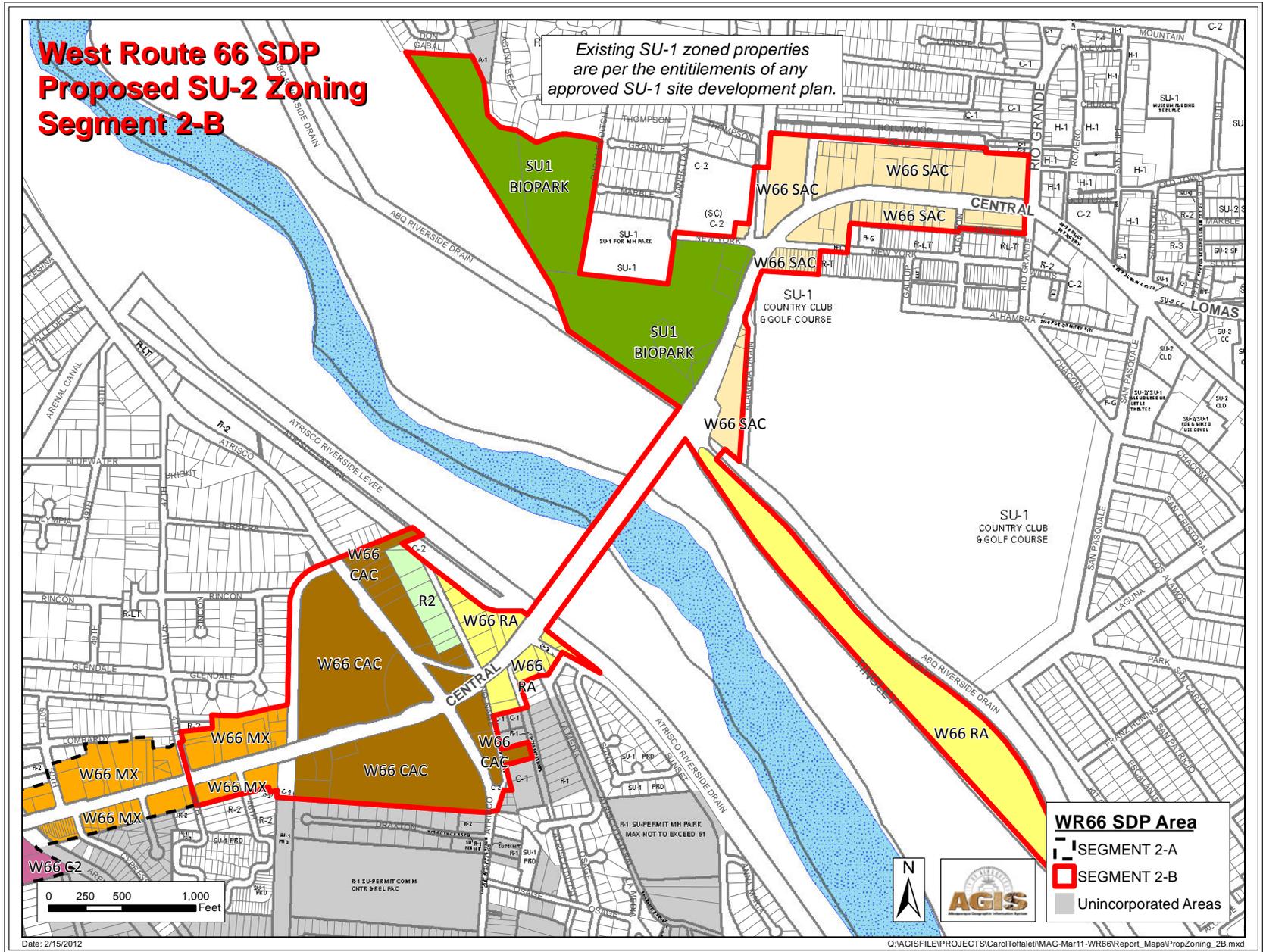


Figure: 41 Zoning Segment 2 - B

# Zones

## 5.0 Useful Definitions.

The following terms are used throughout this section.

**Allee.** A walkway lined with trees.

**Articulation.** Off-sets, projections, recessed walls, windows, doors, etc. that provide variation to a building façade.

**Building mass.** The aggregate size of a building, or the total height, width, and depth of all its parts.

**Building orientation to the street.** Primary facade of building fronting/facing street.

**Building Type.** The category of a building, as described in the General Development standards.

**Clear Height.** Unobstructed vertical distance between two objects.

**DMD.** Department of Municipal Development.

**DRB.** Design Review Board

**Drive aisle/Driveway.** An improved (e.g., paved) driving surface for one line of vehicles.

**EPC.** Environmental Planning Commission.

**Façade.** The face or front of a building.

**Fenestration.** The openings which form a part of a building façade.

**Frontage Types.** The category of building façades, as described on pages X.

**Form Based Zones.** Zones which use physical form, rather than separation of land uses, as their organizing principle.

**Grove.** A group of trees.

**LID.** Low Impact Development.

**Mixed use development.** Development in which multiple land uses are permitted such as retail and residential.

**Pad Site.** An individual freestanding site for a retailer, often adjacent to a larger shopping center .

**Paseo.** A street or path for pedestrians only. Usually wider than a sidewalk and landscaped.

**Pedestrian-friendly/pedestrian-oriented.** Development which is designed with an emphasis primarily on the street sidewalk and on pedestrian access to the site and building, rather than on auto access and parking areas. The building is generally placed close to the street and the main entrance is oriented to the street sidewalk. There are generally windows or display cases along building façades which face the street. Typically, buildings cover a large portion of the site. When parking areas are provided, they are generally limited in size and they are not emphasized by the design of the site.

**Plaza.** A public square or extra-wide sidewalk (e.g., as on a street corner) that allows for special events, outdoor seating, sidewalk sales and similar pedestrian activity.

**Primary entrance.** The entrance to a building that most pedestrians are expected to use. Generally, each building has one primary entrance. It is the widest entrance of those provided for use by pedestrians. In multi-tenant buildings, primary entrances open directly into the building's lobby or principal interior ground level circulation space. When a multi-tenant building does not have a lobby or common interior circulation space, each tenant's outside entrance is a primary entrance. In single-tenant buildings, primary entrances open directly into lobby, reception, or sales areas.

**Right-of-way (ROW).** Land that is owned in fee simple by the public, usually for transportation facilities.

**(ZHE).** Zoning Hearing Examiner

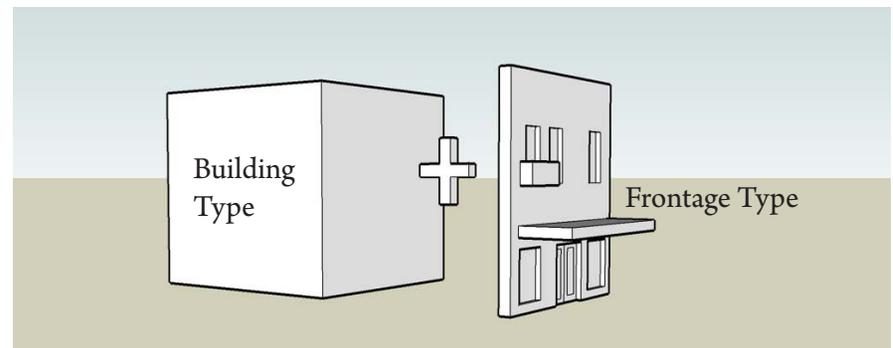
Definition Sources:

CABQ Form Based Code, Form Based Codes Institute, Model Development Code and User's Guide for Small Cities, 1999 (Oregon TGM Program), Lexicon of the New Urbanism, Duany Plater-Zyberk & Company, Version 2.1 (1999), Webster's New Collage Dictionary (1995)

## 6.0 Zones

This section contains the zones for the West Route 66 Sector Development Plan. Each zone contains specific requirements for development including height, building placement, parking, open space and additional development requirements specific to the zone. In addition, the General Development Standards are applicable to all zones.

The Form base zones are included in the Plan rely on using the allowable building form to control the density, intensity and use of development. The building form, known as Building Type, prescribes the building envelope, building access and orientation, access to light and air and general site requirements. All activity in a Form based zone shall be conducted within the prescribed form or Building Type. In addition to the prescribed form. In addition, each Building Type dictates allowable Frontage Types. Frontage Types prescribe building articulation along the street frontage and include requirements pertaining to building penetrations, shading elements and street access. These referenced Building and Frontage Types are located within the General Development Standards.



Building Type and Frontage Type Diagram

**Intent:** The W66 Community Commercial (W66 C-2) zone creates a mixed commercial and residential zone to increase development flexibility while continuing to provide opportunities for the sale of goods and services that serve the community. Residential uses are regulated by form based standards in order to ensure quality design.

**Justification:** Current platting and land uses are consistent with C-2 uses and site requirements. The majority of the parcels are currently zoned C-2. The addition of R-2 uses on these parcels increases development entitlements, promotes housing choice, and increases transit and retail demand by promoting population growth along the corridor.

**Permitted Uses**

1. Permissive and Conditional uses per the C-2 zone with the addition of R-2 permitted and Conditional uses. R-2 uses shall be limited to 20 du/acre.
2. Nursing and Rest homes.

**Prohibited Uses**

1. House, one per lot.

**Permitted Residential Building Types**

1. Residential uses shall comply with the following Building Types, p. 112: Stacked flats, Terrace apartments, Courtyard apartments, Townhouse, Rowhouse, Loft, Duplex/Triplex/Fourplex

**Usable Open Space (for residential uses only)**

1. Per R-2 zone.

**Landscape Standards**

1. Per §14-16-3-10 in the Zoning Code, with the exception of 5. in Additional Development Requirements.

**Off Street Parking Requirements**

1. Per §14-16-3-1 in the Zoning Code with the exception of 3. and 4. in Additional Development Requirements.

**Building Placement**

1. Setbacks, per C-2 zone.

**Height**

1. Per C-2 zone but not to exceed 52 feet.
2. Within 50 feet of Central Avenue, heights shall be limited to 36 feet.

**Additional Development Requirements**

1. General Development Standards, p. 111, shall apply to this zone.

**Applicable to development fronting Central Avenue**

2. In order to facilitate pedestrian circulation, no additional curb cuts shall be permitted. On parcels with no existing curb cut, one curb cut shall be permitted per 150 feet of street frontage.
3. Parking shall not be permitted between building and public R.O.W.
4. Parking shall not be located within 10 feet of the front property line. Area between front property line and parking shall be landscaped with a minimum of 50% vegetative cover.
5. At least 75% of building frontage shall be at the minimum setback. A deviation up to 20 additional feet is permitted to accommodate building articulation, patios and courtyards. Where more than one building is located per site, at least one building shall meet the 75% requirement. Other buildings shall meet this requirement where possible.
6. For lots greater than 75 feet in width, a maximum of 50% of the street frontage shall consist of parking at the minimum setback.
7. Signage shall be regulated by the General Development Standards.

## W66 - CAC Zone

**Intent:** The W66 - Community Activity Center zone (W66 CAC) is a form based zone developed to provide a mixture of moderate density residential, commercial, entertainment, civic, and employment uses within a designated Community Activity Center which supports transit and pedestrian accessibility, while accommodating automobiles.

**Justification:** The Comprehensive Plan designates the Atrisco Community Activity Center in order to provide a greater variety of commercial and entertainment uses in conjunction with community-wide services, civic land uses, employment, and the most intense land uses within the community sub-area.



### CAC Development Characteristics

1. Roadways and private drives with sidewalk, landscaping and on-street parking.
2. Limited curb cuts.
3. No new parking between a building and a public right of way.
4. Limited street frontage consisting of parking areas. Parking areas buffered with landscaping.
5. Aggregated public space.
6. Aggregated shared parking area.
7. Pedestrian linkages between parking areas and buildings.

CAC zone potential development diagram.

**Permitted Uses**

1. Any use not listed as a limited, conditional or prohibited use shall be permitted.

**Limited Uses**

1. New Drive up service windows shall be permitted in association with a bank or pharmacy.
2. Drive up service windows for all other uses are based on a cap and replace system limited to 3 within in the CAC. A new drive up service window shall only replace an existing use. Use is not limited to a specific site.

**Conditional Uses**

1. Alcohol sales for off-premise consumption shall be a conditional use.

**Prohibited Uses**

1. The following uses are prohibited: Vehicle sales, rental service, repair or storage; taxidermy services; single family house, one per lot; cold storage plants; drive-in theatres; kennels; mobile home development; tire capping or retreading; transfer or storage of household goods; uses permitted and conditional in the P or PR zone; truck plaza; drive-in facilities; gasoline, oil, liquefied petroleum gas or other vehicle fuel sales; the following SU-1 uses: adult amusement establishment or adult store, automobile dismantling yard, campground, cemetery, drilling, production or refining of petroleum gas or hydrocarbons, gravel, sand or dirt removal activity, stockpiling, processing or distribution, hatching plant, ore reduction, smelting, planned development area, planned residential development.

**Permitted Building Types**

1. Development shall occur in one or more of the following Building Types: Stacked flats, Terrace apartments, Courtyard apartments, Townhouse, Rowhouse, Duplex,/Triplex,/Fourplex Live/work Loft, Podium building, Flex building, Courtyard building, Liner building, Civic institutional building, Structured parking - no ground floor uses shall be located behind another structure which screens a minimum of 50% of the parking structure on a public façade, Structured parking - ground floor uses, Drive through/Service Station (shall meet limited use requirements above).

**Usable Open Space**

1. A minimum 10% of the site area shall be designated as usable open space in the form of patios, plazas, balconies, roof decks, courtyards, or exterior walkways.

**Public Space**

1. One public space area, a minimum of 200 square feet, shall be provided for every 10,000 square feet of building space.
2. The public space area shall be privately owned and maintained and should typically contain seating and shade.
3. An aggregate of buildings 125,000 square feet or greater shall provide pedestrian plaza space in the amount of 400 square feet for every 20,000 square feet of building space.
4. A minimum of 50% of the required public space shall be provided in the form of aggregate space that encourages its use and that serves as the focal point for the development.
5. See General Development Standards for additional public space requirements.

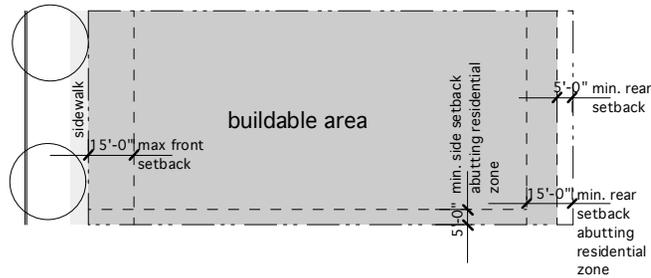
**Landscape Standards**

1. Landscape standards shall be per the City of Albuquerque Zoning Code (§ 14-16-3-10) except that the total landscape area required for each development shall be a minimum 5% of the net lot area.

**Off Street Parking Requirements**

1. Per §14-16-3-22(C) in the Zoning Code, with the following additions: Parking shall be located to the side or rear of buildings where possible. Parking area shall be setback a minimum of 10 feet from the public R.O.W. Parking area shall be buffered from public R.O.W. by a minimum 10 foot wide landscaped area and a site wall or continuous evergreen plant material a minimum of 3 feet high. See Additional Development requirements for maximum allowable parking frontage.
2. Off-street parking shall be 1 space per 1000 sq ft minimum; for all uses, except that for all lots the maximum number of allowed off-street parking spaces shall be no more than 110% of the required minimum.

# W66 - CAC Development Requirements



Preliminary building placement diagram.

## Building Placement

### Front Setback

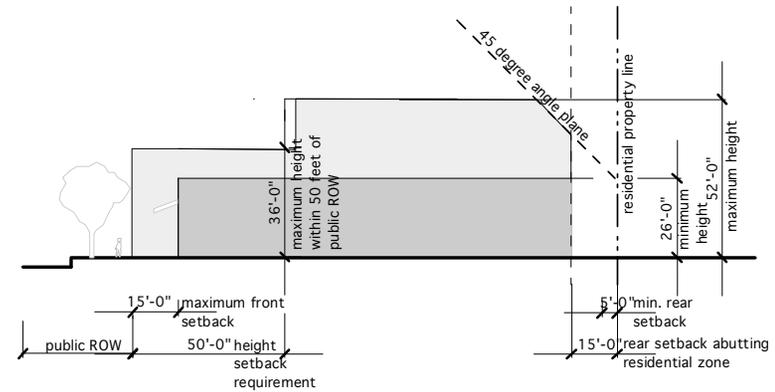
1. Maximum setback: 15 feet  
 \*\*upper floors, balconies, portals, shading devices and signage may protrude into the front setback within the property line a minimum of 8 feet above sidewalk.

### Side Setback

1. Internal: none
2. Minimum side setback: 0 feet; except 5 feet minimum from abutting residential zone.

### Rear Setback

1. Minimum rear setback: 5 feet; except 15 feet where site abuts the rear of a lot in a residential zone.



Building height diagram.

## Building Height

1. Minimum height shall be 26 feet.
2. Heights shall be limited to 52 feet.
3. Lots abutting R-1 through R-T zone properties building heights shall not exceed a 45 degree angle plane that begins at a height of 26 feet, measured from the residential property line.
4. Within 50 feet of a public right-of-way, heights shall be limited to 36 feet.

## W66 - CAC Development Requirements

### *Additional Development Requirements*

#### All properties

1. The Plan's General Development Standards, p. 111, shall apply to this zone.
2. All other requirement not included in this zone shall conform to TOD-COM in the Comprehensive Zoning Code as well as the standards of this Plan.
3. In order to facilitate pedestrian circulation, no additional curb cuts shall be permitted. On parcels with no existing curb cut, one curb cut shall be permitted per 150 feet of street frontage.
4. No new parking shall be permitted between a building and a public right of way.
5. For lots greater than 75 feet in width, a maximum of 30% of the street frontage shall consist of parking areas. All other parking areas shall be located behind a building.
6. A 10 foot wide minimum landscaped setback is required between the sidewalk and parking area. Remaining street frontage shall consist of buildings, courtyards, patios and seating areas, site walls up to 4 feet in height and landscaping.
7. Stand alone ATM stations shall be walk up only. See General Development Standards for Stand alone ATM requirements.

#### For properties greater than 4 acres

1. In order to provide connectivity between public sidewalks, parking areas and development, no more than 400 linear feet of development shall front Central or Atrisco without a pedestrian passage to parking areas or a vehicular drive with sidewalks on both sides.
2. Platted roadways, which may remain private, shall separate blocks. Roadways or private drives shall not have more than two travel lanes a maximum of 10 feet in width. All on site roadways and private drives shall have on-street parking. On-street parking may be parallel, angled or perpendicular.
3. A minimum 6 foot wide sidewalk and 5 foot wide landscaped area, and stand up curb shall align both sides of all on site roadways and private drives. Cuts shall be permitted into curb to allow water to flow into planting areas, see Low Impact Development standards.

## W66 -EPR Zone

**Intent:** The W66 Employment Park Residential (W66 EPR) zone creates opportunities for employment, commercial and multifamily residential uses to increase development flexibility while creating more consistent land use in the overall area. The EPR zone incorporates form based design standards to ensure quality mixed use development and provides opportunities for clustered multi-story office buildings, multi-family residential communities, and compact mixed use development.

**Justification:** Current demand in the area for larger parcels is for multi-family development and employment centers. There is minimal opportunity within the plan area for residential uses adjacent to employment facilities. The addition of multi-family uses on these parcels increases development entitlements, provides opportunities for people to live and work in close proximity and increases potential for transit ridership along the Central corridor.



### General EPR Development Characteristics

1. Clustered parking areas separated by buildings or landscaped areas.
2. Buildings setback from public R.O.W.
3. Pedestrian and bicycle linkages throughout site
4. Opportunities for residential, commercial and employment
5. Campus style layout

EPR zone potential development diagram.

**Permitted Uses**

1. Permissive Uses within the R-2, C-1 and O-1 zone.
2. Hotel, auditorium, health gymnasium, clinic, small animal clinic, outdoor restaurant seating.
3. Nursing and Rest homes.

**Prohibited Uses**

1. House, one per lot.

**Conditional Uses**

1. Schools, other than public.

**Usable Open Space Requirements**

1. Over the entire site, at least 1 square foot of usable open space shall be provided for every 1 square foot of surface parking.
2. Balconies, patios, courtyards, plazas, parks or portals shall count toward required open space. At least 50% of the open space shall be landscaped and contain pedestrian network facilities. Courtyards and plazas shall be formally landscaped or hardscaped; and surrounded on least two sides by building(s).

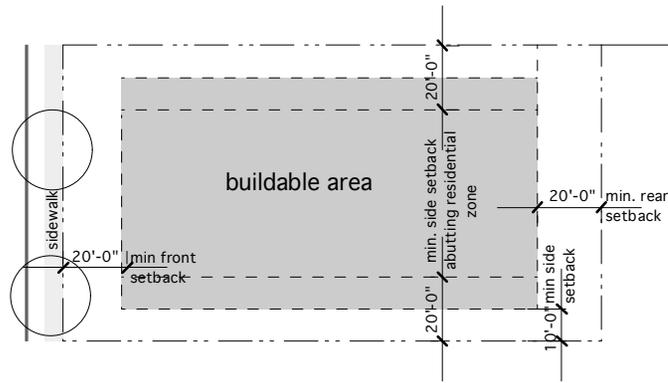
**Landscape Standards**

1. Landscape standards shall be per the City of Albuquerque Zoning Code § 14-16-3-10.

**Off Street Parking Standards**

1. Residential uses: 1.5 spaces per unit
2. Other: off-street parking regulations in the Comprehensive City Zoning Code.
3. Parking shall not be permitted in front setback areas.
4. Parking shall be located to the side or rear of building.
5. Each parking area shall not exceed 50 parking spaces.
6. Parking areas shall be spaced a minimum of 50 feet apart separated by buildings or landscaped areas.
7. Parking shall be buffered from the public R.O.W. by landscaping and a minimum 3 foot high wall or continuous landscaping a minimum of 3 feet in mature height.
8. Parking areas shall be buffered from residential areas by a minimum 10 foot landscaped buffer with trees spaced a minimum of 30 feet on center.

## W66 - EPR Development Requirements



Site Layout Diagram

### Building Placement

#### Front Setback

Minimum: 20 feet

Maximum: none

All others: 10 feet

#### Side setback

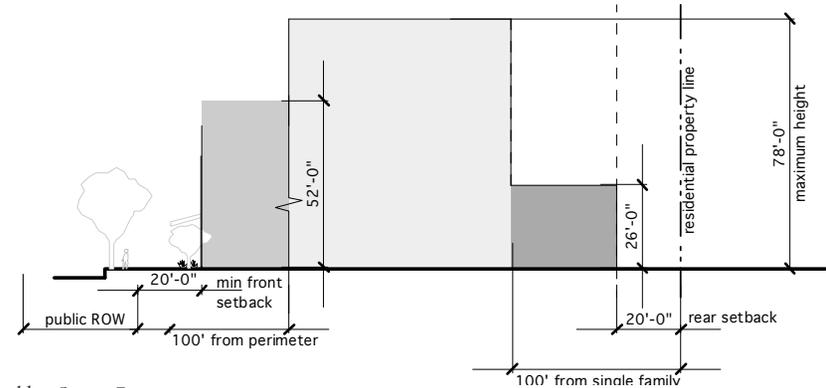
Internal: none

Abutting a zone with residential uses: 20 feet minimum

All others: 10 feet minimum

#### Rear setback

Setback from rear property line: 20 feet minimum



Building Section Diagram

### Building Height

1. Within 100 feet of the property line of a single family residential zone, buildings shall not exceed 26 feet in height.
2. On other perimeter locations, maximum height is 52 feet.
3. Beyond 100 feet from the perimeter, height is limited to 78 feet in height.

## W66 - EPR Development Requirements

### *Additional Development Requirements*

#### General

1. General Development Standards, p. 111, shall apply to this zone.
2. Lot Size. No future subdivision of land shall create lots of less than one acre.
3. Pedestrian connections shall include a network of interior paths that links buildings, parking areas and open space with pedestrian walkways, bikeways, plazas, and trails.
4. Additional curb cuts are discouraged. On corner lots, additional curb cuts shall be on side streets only. For properties over 4 acres private or platted roads shall provide access to site from public R.O.W.
5. All setback areas shall be landscaped.

#### Campus - properties which are 4 or more acres

1. Site shall be divided into blocks of a maximum of 4 acres. Blocks shall be separated by platted or private drives, pedestrian parkways or plazas a minimum of 20 feet in width.
2. A minimum 6 foot wide sidewalk and 5 foot wide landscaped area, and stand up curb shall align both sides of all on site roadways and private drives. Cuts shall be permitted into curb to allow water to flow into planting areas, see Low Impact Development standards.

## W66 - MAC Zone

**Intent:** The W66 Major Activity Center (W66 MAC) zone is a form based zone developed to provide a highly concentrated location of commercial, service, residential and employment uses on the Southwest Mesa in conjunction with area-wide needs in a designated Major Activity Center.

**Justification:** The W66 MAC area consists of over 300 acres and serves the metropolitan population and beyond, providing commercial, office, and technology centers, including medium to high density residential in sensitive relationship to employment. The W66 MAC is accessible by all modes of travel including a major transit transfer point.



### General MAC Development Characteristics

1. Frontage road trail.
2. Parking visible from street is limited and buffered by landscaping.
3. Linkages to transit facility.
4. Shared parking areas.
5. Aggregated open space for parks, trails and plazas.
6. Higher density development along Central, buffered by frontage road or landscaping setback.
7. Pedestrian connectivity through site using a fine grained network of streets and trails.
8. North/South trail to improve connectivity.

MAC zone potential use

**Permitted Uses**

1. Any use not listed as a limited or prohibited use shall be permitted.

**Limited Uses**

1. Within 200 feet of the Central ROW, residential uses shall be prohibited in first floor buildings, except that motels built between 1926 and 1956 may be rehabilitated for residential use.
2. Drive through/up service windows and queuing lanes are prohibited within 150 feet of a major intersection and within 50 feet of Central Avenue. Drive through/up service windows location and queuing lanes are encouraged to be internal to the site. Queuing lanes shall not be permitted adjacent to streets.

**Prohibited Uses**

1. The following uses shall be prohibited: Adult amusement establishment and adult store, except in areas previously zoned C-3, IP, M-1, M-2 as of the adoption of this zone; Uses first permitted and conditional in the R-1 and M-2 zone; the following SU-1 uses: automobile dismantling yard, campground, cemetery, drilling, production or refining of petroleum gas or hydrocarbons, gravel, sand or dirt removal activity, stockpiling, processing or distribution and hatching plant, ore reduction, smelting, Planned Development Area, Planned Residential Development, truck plaza, uses permitted and conditional in the P zone.

**Permitted Building Types**

1. Development shall occur in one or more of the following: Stacked flats, Courtyard apartments, Podium building, Courtyard building, Loft unit, Flex building, Liner building, Civic institutional building, Structured parking - no ground floor uses, Structured parking - ground floor uses, Drive Through/Service Station.

**Usable Open Space**

1. A minimum 10% of the site area shall be designated as usable open space in the form of patios, plazas, balconies, roof decks, courtyards or exterior walkways.
2. Usable open space is not required if building is located within 1,500 feet of a park, plaza or other designated usable open space that is at least one acre in size and is accessible to the public.

**Public Space**

1. One public space area, a minimum of 200 square feet, shall be provided for every 10,000 square feet of building space.
2. The public space area shall be privately owned and maintained and should typically contain seating and shade.
3. An aggregate of buildings 125,000 square feet or greater shall provide pedestrian plaza space in the amount of 400 square feet for every 20,000 square feet of building space.
4. A minimum of 50% of the required public space shall be provided in the form of aggregate space that encourages its use and that serves as the focal point for the development.
5. See p. X for additional public space requirements.

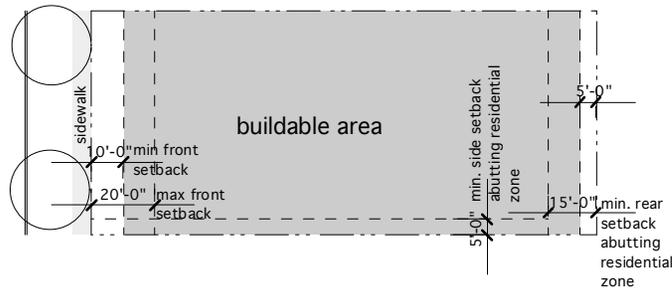
**Landscape Standards**

1. Landscape standards shall be per the City of Albuquerque Zoning Code (§ 14-16-3-10) except that the total landscape area required for each development shall be a minimum 10% of the net lot area.

**Off-street parking Requirements**

1. All uses: No minimum parking requirement.
2. All other parking requirements shall conform to TOD-MAC in the Comprehensive Zoning Code as well as the standards of this Plan.

# W66 - MAC Development Requirements



Site Layout Diagram

## Building Placement

### Front Setback

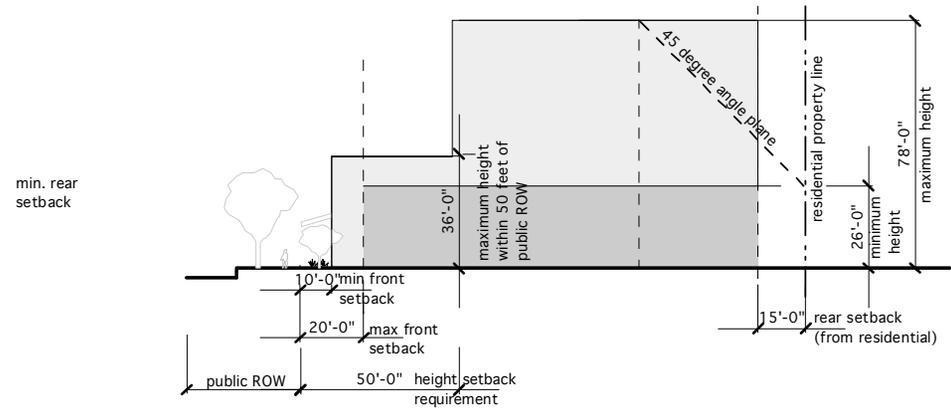
1. All development shall front platted roads or private drive.
2. Front setback from Central R.O.W.: 10 feet minimum; 20 feet maximum
3. Front setback from other arterials: 10 feet minimum; no maximum
4. All others: 5 feet minimum, 10 feet maximum.

### Side Setback

none/5 feet minimum abutting a residential zone

### Rear Setback

5 feet minimum/ 15 feet minimum abutting a residential zone



Building Section Diagram

## Building Height

1. Minimum height shall be 26 feet.
2. Heights shall be limited to 78 feet maximum.
3. Abutting R-1 through R-T zone or corresponding SU-1 zone, building heights shall not exceed a 45 degree angle plane that begins at a height of 26 feet measured from the residential property line
4. Within 50 feet of a public ROW, heights shall be limited to 36 feet.

## W66 - MAC Development Requirements

### *Additional Development Requirements*

#### All properties

1. The General Development Standards, p. 111, shall apply to this zone.
2. Where this zone or the General Development Standards of this Plan area silent, the requirements of the TOD-MAC in the Comprehensive Zoning Code shall apply.
3. For parcels with over 400 linear feet of frontage along Central, 1 curb cut shall be permitted every 300 linear feet.
4. No new parking shall be permitted between a building and a public right of way.
5. For lots greater than 150 feet in width, a maximum of 50% of the street frontage may consist of parking areas. All other parking areas shall be located behind a building.
6. For lots greater than 150 feet in width, a maximum of 30% of the street frontage may consist of parking areas. All other parking areas shall be located behind a building. Remaining street frontage shall consist of buildings, courtyards, patios and seating areas, site walls up to 4 feet in height and landscaping.
7. A ten foot minimum landscaped setback is required between the sidewalk and the parking area. Remaining street frontage shall consist of buildings, courtyards, patios and seating areas, site walls up to 3 feet in height and landscaping.
8. Stand alone ATM stations shall be walk-up only. See General Standards for standalone ATM station requirements.

#### For properties greater than 4 acres

1. In order to provide connectivity between public sidewalks, parking areas and development, no more than 400 linear feet of development shall front Central or Atrisco other public R.O.W.'s without a pedestrian passage from public R.O.W. to parking areas or a vehicular drive with sidewalks on both sides.
2. Blocks shall be no more than 4 acres in size.
3. Platted roadways shall separate blocks. Roadways including private drives shall not have more than two travel lanes a maximum of 10 feet in width. All roadways and private drives shall have on-street parking. On-street parking may be parallel, angled or perpendicular.
4. A minimum 6 foot wide sidewalk and 5 foot wide landscaped area, and stand up curb shall align both sides of all on site roadways and private drives. Cuts shall be permitted in curb to allow water to flow into planting areas, see Low Impact Development standards.

## W66 - MX Zone

*Intent:* The WR66 MX zone is a form based zone which builds upon the existing development pattern of this portion of the west Central corridor - small platted lots and buildings located close to and oriented to the street - in order to create a pedestrian oriented environment.

*Justification:* This section of the corridor is at the same elevation as the Community Activity Center and contains an existing development pattern conducive to pedestrian-oriented development where buildings line the public R.O.W. and parking is located to sides and rears of buildings to create a safe pedestrian environment.



**Permitted Uses**

1. Permissive uses in the C-2 and R-2 zone.
2. Nursing and Rest homes.

**Conditional Uses**

1. School, other than public.

**Prohibited Uses**

1. The sale of alcohol for off-premise consumption.
2. Drive through/up service windows are prohibited.
3. House, one per lot.
4. Conditional Uses in the C-2 zone are prohibited.

**Permitted Building Types**

1. Development shall occur in one or more of the following: Stacked flats; Terrace apartments; Courtyard apartments; Podium building; Flex building; Liner building, Stand alone commercial/office building; Civic institutional building; Structured parking - no ground floor uses, shall be located behind another structure; Structured parking - ground floor uses; Townhouse; Duplex,/Triplex/Fourplex, Loft.

**Usable Open Space**

1. A minimum 20% of the site area shall be designated as usable open space in the form of patios, plazas, exterior walkways, balconies, roof decks or courtyards.

**Public Space**

1. One public space area, a minimum of 200 square feet, shall be provided for every 10,000 square feet of building space.
2. The public space area shall be privately owned and maintained and should typically contain seating and shade.
3. A minimum of 50% of the required public space shall be provided in the form of aggregate space that encourages its use and that serves as the focal point for the development.
4. See General Development Standards for additional public space requirements.

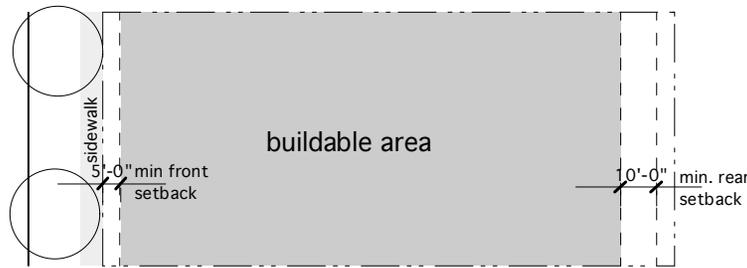
**Landscape Standards**

1. Landscape standards shall be per the City of Albuquerque Zoning Code (§ 14-16-3-10) except that the total landscape area required for each development shall be a minimum 10% of the net lot area.

**Off Street Parking Requirements**

1. For Residential Uses: 1.5 spaces/unit minimum
2. For all other uses: 1/1000 square feet minimum
3. Parking shall be located to the side or rear of building.
4. Parking area shall be setback a minimum of 10 feet from the property line.
5. Parking area shall be buffered from public ROW by a minimum 10 foot landscaped area and a site wall or continuous plant material a minimum of 3 feet high.
6. See Additional Development Requirements for maximum allowable parking frontage.

# W66 - MX Development Requirements



Site Layout Diagram

### Building Placement

#### Front Setback

Properties fronting Central: No Minimum\*

All others: 5 feet minimum\*

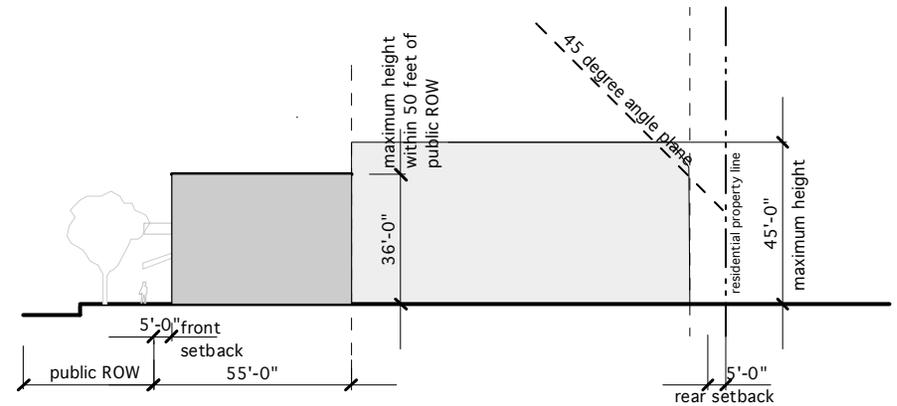
\*upper floors, balconies, portals, shading devices and signage may protrude up to five feet into setback area within the property line a minimum of 8 feet above sidewalk.

#### Side setback

No minimum

#### Rear setback

10 feet minimum



Building Section Diagram

### Building Height

1. Heights shall be limited to 45 feet maximum.
2. Within 75 feet of an abutting R-1 through R-T zone and corresponding SU-2 zone, building heights shall not exceed a 45 degree angle plane that begins at a height of 26 feet, measured from the residential property line.
3. Within 50 feet of Central Avenue, heights shall not exceed 36 feet.

### *Additional Development Requirements*

#### All properties

1. General Development Standards, p. 111, shall apply to this zone.
2. For lots 150 feet or less in width, a maximum of 50% of the street frontage may consist of parking and drive aisles at the minimum setback.
3. For lots greater than 150 feet in width, a maximum of 30% of the street frontage may consist of parking areas. All other parking areas shall be located behind a building. Remaining street frontage shall consist of buildings, courtyards, patios and seating areas, site walls up to 4 feet in height and landscaping.
4. A 5 foot minimum landscape buffer is required between parking areas and sidewalk. All other parking areas shall be located behind a building. Remaining street frontage shall consist of buildings, courtyards, patios and seating areas, site walls up to 4 feet in height and landscaping.
5. A minimum of 75% of the first floor building frontage shall be at the minimum front setback or at the sidewalk where applicable. A deviation up to 10 feet is permitted to accommodate building articulation, patios and building entrances. Remaining street frontage shall be landscaped, see General Development Standards for requirements.
6. Stand alone ATM stations shall be walk-up only. See General Development Standards for Stand alone ATM station requirements.

## W66 - RA Zone

**Intent:** The W66 River Activity (W66 RA) zone is a form based zone that provides for limited commercial development and public facilities near the Rio Grande, which serve retail, recreational and cultural needs of residents and visitors. The zone prescribes lower-density development that is designed to complement the natural environment of the river and Bosque, and the acequia system.

**Justification:** The W66 RA zone provides for a mix of non-residential land uses and for development design appropriate at this unique confluence in the city between urban Activity Centers and a Major Transit Corridor on the one hand, and the Open Space and waterways associated with the Rio Grande on the other.



### *Permitted Uses*

1. Permissive uses of the C-1 zone.
2. Sales, service and rental of bikes and outdoor recreational goods; outdoor restaurant seating; outdoor sales of fruit, vegetables, flowers and plants and incidental related signs; community garden; recreational facilities oriented toward outdoor activities: facility for horses and riding, kayaking, lake; outdoor storage or activity if part of a permissive use on the same premises that does not negatively impact neighboring uses or premises; bed and breakfast establishment; restaurant serving liquor; uses or activities in a tent except parking is not required on the premises.
3. Residential shall be permitted up to 30 du/acre.
4. Public parking areas shall be permitted.

### *Prohibited Uses*

1. The following uses shall be prohibited: Church; drive-up service windows; auto parts and sales; gasoline, oil, liquefied petroleum gas or other vehicle fuel sales; medical or dental lab; pawn shop; car washing; dry cleaning; taxidermy services.

### *Permitted Building Types*

1. Development shall occur in one or more of the following: Podium building, Flex building, Stand alone commercial/office building, Civic institutional building, Stacked flats, Courtyard apartments and loft unit.

### *Landscape Standards*

1. Landscape standards shall be per the City of Albuquerque Zoning Code (§ 14-16-3-10) except the total landscape area required for each development shall be a minimum 10% of the net lot area and lots adjoining ditches and drains along the Bosque shall use plants native to that environment (refer to City Open Space list).

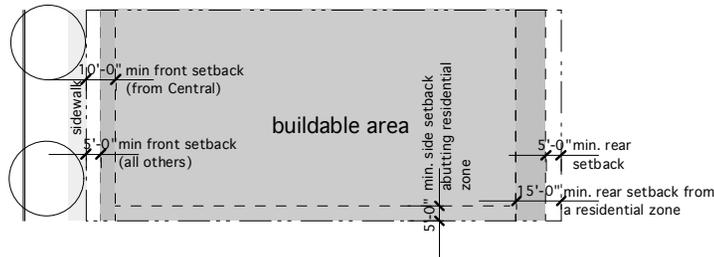
### *Usable Open Space Requirements*

1. Due to the proximity of a major open space area, usable open space requirements for residential use are intended to provide outdoor space associated with an individual unit. A minimum of 50 square feet of usable open space shall be provided per dwelling unit.
2. Usable Open Space may be in the form of patios, courtyards, balconies, and roof gardens.

### *Off Street Parking Requirements*

1. Required minimum parking calculations per the City of Albuquerque Zoning Code (§ 14-16-3-1) except:
  - a. Shared parking facilities. Off-street parking requirements may be met by shared parking facilities located within 800 feet.

# W 66 - RA Development Requirements



Site Layout Diagram

### Building Placement

#### Front Setback

Properties fronting Central: 10 feet minimum

All others: 5 feet minimum

Maximum front setback: 15 feet

\*\*upper floors, balconies, portals, shading devices and signage may protrude into the front setback within the property line a minimum of 8 feet above sidewalk.

#### Side Setback

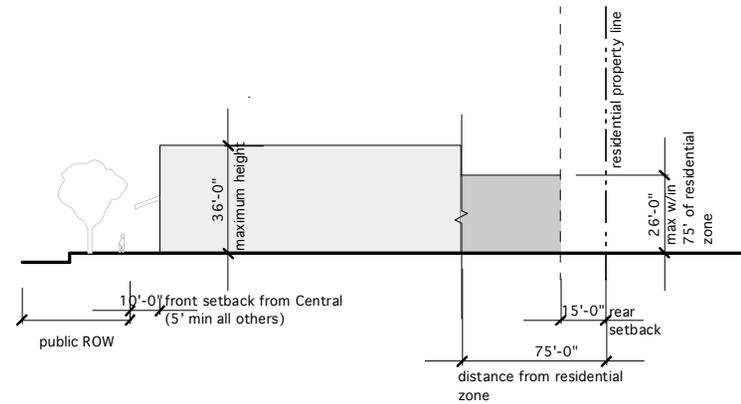
0 feet Minimum/5 feet minimum from residential zone.

#### Rear Setback

5 feet Minimum/15 feet where site abuts the rear of a lot in a residential zone.

#### Setback from abutting waterways (ditch and drain)

5 feet Minimum from MRGCD property or easement line, or 15 feet Minimum from toe of ditch or drain where neither such line exists.



Building Section Diagram

### Building Height

1. Heights shall be limited to 36 feet maximum

2. Within 75 feet of an abutting residential zone for houses, building heights shall not exceed 26 feet.

### *Additional Development Requirements*

#### All properties

1. General development standards, p. 111, shall apply to this zone.
2. In order to facilitate pedestrian circulation, no additional curb cuts shall be permitted. On parcels with no existing curb cut, one curb cut shall be permitted per 150 feet of street frontage.
3. No new parking shall be permitted between a building and a public right of way.
4. For lots greater than 75 feet in width, a maximum of 30% of the street frontage shall consist of parking areas.
5. A 10 foot wide minimum landscaped setback is required between the public realm and parking area. Remaining street frontage shall consist of buildings, courtyards, patios and seating areas, site walls up to 4 feet in height and landscaping.
6. Stand alone ATM stations shall be walk up only. See General Development Standards for Stand alone ATM requirements.
7. Development shall provide direct pedestrian access to the Bosque where possible.
8. Development shall be oriented to the Rio Grande and Bosque where possible. Facades which front the river and Bosque shall incorporate views of river, Bosque, mountains and acequias through the use of appropriately located glazing, balconies and outdoor pedestrian amenities including seating areas, courtyards and patios in order to take advantage of site location and to contribute to the development of an active outdoor recreation environment along the river and acequias.

# W66 SAC Zone

**Intent:** The WR66 Special Activity Center (W66 SAC) zone is a Form based zone which provides for a 1 to 4 story mixed use environment serving neighborhood and tourism needs that is sensitive to the character of the Old Town and BioPark area. This zone prescribes medium to high density residential, shopping, service, office, and entertainment uses along a Comprehensive Plan designated Major Transit Corridor, and is within walking or biking distance of residential areas.

**Justification:** The W66 SAC zone provides for a mixed use environment which is supportive of the unique conditions afforded by City cultural facilities, Historic Route 66 properties and Old Town.



- General SAC Development Characteristics
1. Garden District created through small catchment gardens enhance sidewalk area.
  2. Enhanced tourist amenities such as seating, lighting and wayfinding.
  3. Plazas and patios connect sidewalk to pad development.
  4. Minimum curb cuts along Central to facilitate pedestrian travel.
  5. North/South linkages to improve connectivity to adjacent neighborhoods
  6. Additional pedestrian crossings on Central.
  7. Soto Avenue developed as a one way with parking and bike lanes
  8. Buildings oriented to plazas and courtyards

SAC zone potential development diagram.

**Permitted Uses**

1. Uses permissive in the C-1 and R-2 zone.
2. Uses not limited to 30 du/acre; density is controlled by building and site envelope.
3. The following uses are permitted: Catering, Baking, Confectionery making, Jewelry or curio making, Pottery making.
4. Bed & breakfast establishment, hotel, restaurant serving liquor.
5. Nursing and Rest homes.

**Limited Uses**

1. Drive up service windows are based on a cap and replace system and shall be capped at 4/42 acres. A new Drive up service windows shall be permitted only to replace an existing Drive up service window use. Drive up service windows are not limited to a specific property.

**Conditional Uses**

1. Schools, other than public.

**Prohibited Uses**

1. The following uses shall be prohibited: House, one per lot; vehicle sales, service, repair, and storage; sale of building materials; commercial surface parking lot; sample dwelling unit.

**Permitted Building Types**

1. Development shall occur in one or more of the following: Stacked flats, Terrace apartments, Courtyard apartments, Podium building, Flex building, Courtyard building, Liner building, Stand alone commercial/office building, Civic institutional building, Structured parking - no ground floor uses, located behind another structure, Structured parking - ground floor uses, Townhouse, Duplex, /Triplex/Fourplex, Loft unit.

**Usable Open Space**

1. A minimum 20% of the site area shall be designated as usable open space in the form of patios, plazas, exterior walkways, balconies, roof decks or courtyards. Open space shall be oriented to the public realm where possible.

**Public Space**

1. One public space area, a minimum of 200 square feet, shall be provided for every 10,000 square feet of building space.
2. The public space area shall be privately owned and maintained and should typically contain seating and shade.
3. A minimum of 50% of the required public space shall be provided in the form of aggregate space that encourages its use and that serves as the focal point for the development.
4. See General Development Standards for additional public space requirements.

**Landscape Standards**

1. Landscape standards shall be per the City of Albuquerque Zoning Code (§ 14-16-3-10) except the total landscape area required for each development shall be a minimum 10% of the net lot area.
2. In order to maintain a continuity of materials in the pedestrian realm to reinforce the concept of a garden district, site walls within 25 feet of Central Avenue or Rio Grande Boulevard shall be exposed adobe construction.

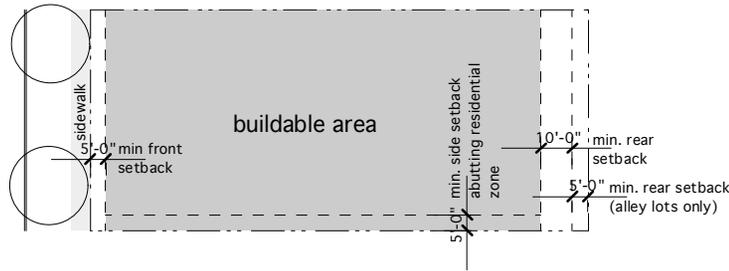


Adobe wall.

**Off Street Parking Requirements**

1. Required minimum parking calculations:
  - a. All uses: 1/1000 square feet
  - b. Shared parking facilities. Off-street parking requirements may be met by shared parking facilities located within 800 feet of the site.
  - c. See § 14-16-3-22(C)(5) for additional parking standards.

# W66 - SAC Development Requirements



Site Layout Diagram

## Building Placement

### Front Setback

Minimum setback: 5 feet from front property line to create Building Amenity Zone.

\*upper floors, balconies, portals, shading devices and signage may protrude into the front setback within the property line a minimum of 8 feet above sidewalk.

### Side Setback

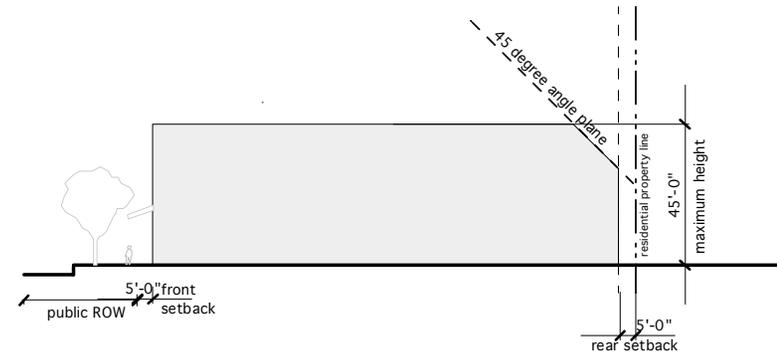
Internal side setback: no minimum.

Side setback adjoining residential zones: 5 feet minimum

### Rear Setback

Rear setback: 10 feet minimum on lots without alleys

5 feet minimum on lots with alleys



Building Section Diagram

## Building Height

Maximum height: 45 feet within 75 feet of an abutting R-1 through R-T zone and corresponding SU-2 zone, building heights shall not exceed a 45 degree angle plane that begins at a height of 26 feet, measured from the residential property line.

## W66 - SAC Development Requirements

### *Additional Development Requirements*

#### All properties

1. The General Development Standards, p. 111, shall apply to this zone.
2. For lots greater than 75 feet in width, a maximum of 50% of the street frontage shall consist of parking and driveway. Remaining street frontage shall consist of buildings, courtyards, patios and seating areas, site walls up to 4 feet in height and landscaping a minimum of 20 feet deep from the front property line.
3. A ten foot minimum setback shall be required between parking and the front setback, or the sidewalk where applicable.
4. No additional parking shall be permitted between a building and a public right of way.
5. No additional curb cuts shall be permitted.
6. For lots 5 acres or greater, Section 14-16-3-2 shall apply in addition to the above requirements. Where conflicts exist between this zone and the shopping center regulations, the regulations of this zone shall prevail.
7. Large truck parking shall be screened from street and adjacent buildings by trees and landscaping, architectural walls or other treatment approved by the Planning Director.
8. Service and loading shall not be located on any side of a structure that is adjoining residentially zoned lots.
9. See General Development Standards for Drive up service window requirements.
10. Stand alone ATM stations shall be walk-up only. See General Development Standards for Stand alone ATM station requirements.
11. See General Development Standards for Service Station requirements.
12. See General Development Standards for additional design standards.
13. Water Harvesting. In order to increase permeable area, mitigate against area flooding problems, and encourage the creation of a garden district, water harvesting on site is required. Water harvesting may consist of both active and passive measures. Passive measures include water catchment areas to collect surface runoff. Runoff from roofs should be collected into separate water catchment areas as this stormwater is cleaner than surface runoff from parking areas. Where possible catchment areas shall be located in the front setback or

in areas where their dual purpose as a ponding area and landscape amenity is most appreciated

14. Water catchment areas should be sized to achieve the first flush. See Drainage section.
15. Grading, hydrology, and landscaping plans shall be integrated to make maximum use of site storm water runoff for supplemental on site irrigation purposes. The landscape plan shall indicate use of all runoff, from individual catch basins around single trees to basins accepting flow from an entire vehicular use area or roof area
16. Storm water and runoff harvesting shall supplement drip irrigation and are required elements of the irrigation system for both new plantings and preserved vegetation
17. Catchment area shall be planted in a manner which supports the theme of a garden district. The following plants shall be used: Pending.

#### All development fronting Central Avenue

1. Development shall allow for a Building Amenities Zone within the first 5' of the front property line.
2. A minimum of 30% of the first floor building frontage shall be at the minimum front setback. A deviation up to 10 feet is permitted to accommodate building articulation, patios and building entrances. Remaining street frontage shall be landscaped.

This page intentionally left blank.

## General Development Standards for All Zones

### 7.0 General Development Standards

*This section includes mandatory standards and recommended guidelines that apply to new development within the sector plan area. (For exemptions, see Development Compliance.) Where a conflict exists between the Plan and the Comprehensive Zoning Code, the Plan will prevail. Where a conflict exists between the Plan and other applicable Rank III plans and Design Overlay Zones, such as the H-1 Historic Old Town zone, the stricter regulation will prevail.*

#### **General Development Standards Contents:**

- A. Building Types
- B. Frontage Types
- C. Building Standards
- D. Multifamily Design Standards
- E. General Site Standards
- F. Pedestrian Circulation and Connectivity
- G. Off Street Parking
- H. Public Spaces
- I. Site Lighting
- J. Signage
- K. Walls and Fences
- L. Screening of Mechanical Equipment, Service and Loading Areas.
- M. Landscape Standards
- N. Best Practices Guidelines for Site Development

## Building and Frontage Types

### A. Building Types

1. Permitted Building Types established by zone.
2. Building Types can be found in the Comprehensive Zoning Code § 14-16-3-22(C)(1).
3. The additions/modifications to the Comprehensive Zoning Code Building Types include Townhouse, Rowhouse and Loft.

### B. Frontage Types

Frontage Types are defined in the Comprehensive Zoning Code § 14-16-3-22(C)(2). Frontage Types are prescribed by Building Type.

Graphic Pending

#### 1. Townhouse

- i. **Description.** A Townhouse is an attached or semi-attached building type consisting of at least four attached units which have varying front setbacks creating semi-private terraces and entry gardens. Townhouse may also be known as Patio Houses. Townhouses may be one or two stories and may vary architecturally from one another, though typically maintain a similar style.
- ii. **Access.** Access to each unit shall be via Stoop, Porch or Patio fronting the street, courtyard, plaza or paseo. Loading and trash disposal shall be accessed from an alley or side driveway.
- iii. **Parking.** Parking shall be located in the rear portion of the lot or a common parking area may be located interior to the block. Garages may be attached, detached or connected by a breezeway and shall be setback from the front façade a minimum of ten feet.
- iv. **Frontage.** Each dwelling unit shall include a stoop, patio, portal or porch frontage type fronting the street or fronting a courtyard, plaza or paseo.
- v. **Exposure to light and air.** Each unit shall have at least two sides exposed to the outdoors. Units may abut other units at the property line and share common walls.

Graphic Pending

Graphic Pending

### 2. Rowhouse

Rowhouse form shall be per City of Albuquerque Form Based Zones “Rowhouse” with the following additions and exceptions:

- i. Description. A Rowhouse building type consists of at least four attached units divided from each other by common walls. Each unit shall have a separate entrance. Typically, Row Houses have aligned repeating front facades, though porches or stoops may project. Each unit shall be articulated architecturally as an individual unit. Row Houses may be one or two stories.
- ii. Access. Access to each unit shall be via Stoop, Porch or Patio fronting the street, plaza or paseo.

### 3. Loft

- i. Description. A loft is a live/work attached or semi-attached building type. A loft may be 1½, 2 or 3 stories. Lofts consist of a minimum of three units and can be used for live/work, work/work or live/live. The residence and work components are intended to be shared by the same user and should be directly linked via interior circulation. Live/work units may vary architecturally; however they should have similar stylistic qualities.
- ii. Access. Each Loft building type shall have only one entry on the front façade known as the primary entry. The primary entry to each dwelling unit shall have direct access to the street, plaza, paseo or to a court. Loading and trash disposal shall be accessed from an alley or side driveway.
- iii. Parking. Parking shall be located in the rear portion of the lot or a common parking area may be located interior to the block. Garages may be attached, detached or connected by a breezeway and shall be setback from the front façade a minimum of ten feet. If the Loft is intended to be used as live/work or work/work, enough dedicated parking spaces shall be available on site for employees.
- iv. Frontage. Each dwelling unit shall include a stoop, patio, portal and/or porch frontage type fronting the street, plaza, paseo or a courtyard.
- v. Exposure to light and air. Each unit shall have at least two sides exposed to the outdoors. Units may abut other units at the property line, and/or share common walls.

## General Development Standards for All Zones



Corner articulation.



Building articulation.



Balconies and entrances to street.

*The following standards are intended to create an environment that is visually appealing and pedestrian friendly, by requiring building articulation that creates visual interest, variety, relates to a pedestrian scale, and provides visibility both into and out of buildings, creates sidewalk activity and provides pedestrian comfort through sidewalk shading.*

### C. Building Standards

1. No specific architectural style is required and a variety of styles is encouraged. Architectural elements such as walls and fences shall provide a unifying theme for the development.
2. Parking garages visible from a street shall be designed to be complementary with adjacent buildings, through the use of similar building forms and materials.
3. Building entry ways on streets. At least one entrance for each commercial/retail space on a street facade shall have direct access to the street, a street-fronting court or plaza, or a paseo. All upper level non-residential and residential spaces shall have a first floor lobby or private stair with direct access to the street or a street-fronting court or plaza.
4. Corner lots. Corner lots shall address the corner in at least one of the following ways: location of main entrance at a corner; articulation at the corner of the building relating to the corner, i.e. curve, angle, step back or projection, tower element and/or Planning Director approved detail.
5. Upper floors shall have a minimum of 20% glazing on street-facing facades and shall meet the articulation requirements of each zone.
6. Building facades next to public spaces shall provide pedestrian features as listed in § 14-16-3-18 (C) of the Zoning Code.
7. Articulation on street facades. The design standards of the Zoning Code § 14-16-3-18(D) shall apply with the following modification and exception:
  - a. Wall plane projection or recesses shall occur at least every 75 feet (ref. § 14-16-3-18(D)(2)(a))
  - b. Section 14-16-3-18 (D)(2)(b), Vertical Change of Color or Material, shall not apply.
  - c. Art such as murals and sculptures will not require coordination with City Arts Program. (ref. § 14-16-3-18(D)(2)(e))
8. Balconies and portals shall have a minimum 8 feet of vertical clearance.
9. Uncolored standard concrete masonry unit (CMU) block is not allowed.
10. Reflective glass is not allowed unless the applicant can demonstrate that the glass would not reflect the sunlight glare or solar heat build up on adjacent residential properties or rights-of-way during the hours of 7-10am and 3-6 p.m.



Paseo



Buildings oriented to court.



Individual articulation of units through materials and setbacks.



Site walls and landscaping define units.

## D. Multi-family Design Standards

1. Buildings along the public R.O.W. or private drives shall orient to the street through the following:
  - a. Entrances to buildings or individual units shall be via stoops, patios, porches or courts.
  - b. Units and buildings shall front on public R.O.W., private drive, paseo (which leads directly to public R.O.W.) or courtyard (which is open or directly connected to a public R.O.W.).
  - c. Where sites are deep, allowing for development setback from the public R.O.W, development shall first meet minimum setback requirements of the zone before areas setback further from the public R.O.W may be developed.
2. Large (over 45,000 square feet footprint) multifamily designs are discouraged. Building mass and footprint shall be broken up through the use of smaller buildings, courtyards/courts, paseos and internal site streets.
3. In order to maintain an urban block pattern along public streets, buildings shall be no more than 400 linear feet long along a public R.O.W. and multiple buildings which front a public R.O.W on a single site shall be separated by a minimum of 50 feet to include a landscaped area with a paseo or bicycle path, a private or platted drive with sidewalks, a landscaped courtyard or play area.
4. Building articulation. A visible change in building setback, material or color shall occur a minimum of every 60 feet. Color or material change shall happen at clear breaks in mass or at inside corners.
5. Units with exterior entry ways shall have a minimum of 40 square feet of usable entry area dedicated to the unit. Minimum dimension of exterior entry way shall be 5 feet. Usable entry area may be in the form of a balcony, porch, patio, courtyard or terrace. No access path to another unit may be within the entry area. Usable entry way shall be buffered from abutting units through landscaping, site walls, or offset in building facade.
6. Units in multi-family buildings shall be individually articulated where possible. A minimum of two (2) of the following methods shall be utilized:
  - a. Building setbacks, step backs and offsets between units,
  - b. Visually separate balconies, terraces or patios,
  - c. Individual exterior entrances to units,
  - d. Majority entry element of a minimum of one of the following: a grand entry staircase, architecturally interesting overhang at entry point, public art piece, landscaped entry court, fountain, or other element approved by the Planning Director,
  - e. Color or material change to articulate individual units.
7. Gated communities shall not be permitted. Walls over 3 feet shall not be permitted within 15 feet of the public R.O.W. Gated access to interior courtyards shall not be considered a gated community.

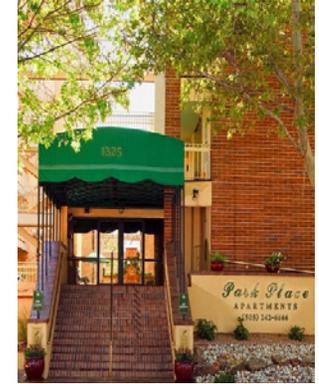
## Residential Development Guidelines



*Color and materials articulate individual units.*

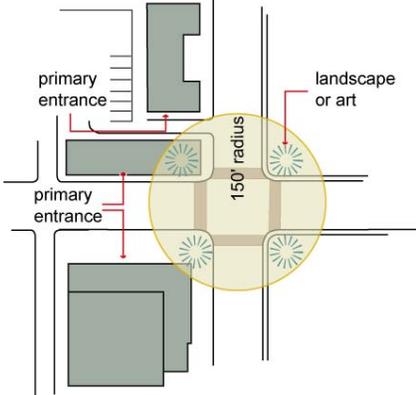


*Individual balconies*



*Grand entry way*

8. Parking shall not be visible from the public R.O.W. Where possible, parking shall be located behind buildings. Parking adjacent to a public R.O.W. shall be screened by a combination of site walls, trees and landscaping.
9. Parking areas shall be separated from residential areas by a 10 foot landscaped buffer with a minimum of 1 tree per 50 square feet and connected to residential area by a paved path a minimum of 5 feet in width. In no case shall a residential building be surrounded on more than two sides by parking.
10. Individual unit parking garages shall not front the public R.O.W. and shall be setback a minimum of 15 feet from the front facade. Double garage doors are not permitted. If a unit has more than two garage spaces, the third space shall be setback an additional 5 feet.
11. Carports shall not front the public R.O.W. Carports shall not front individual units and should be located behind residences where possible.
12. Parking structures shall not be accessed directly from the public R.O.W.
13. Sliding windows are discouraged.
14. Windows shall be recessed a minimum of 1 inch measured from building face to glazing.
15. Each unit shall have exposure to light and air on a minimum of two sides.
16. Sites with multiple residential buildings shall organize buildings around a common area, a paseo, or open space area. Pedestrian pathways shall connect all buildings on site.
17. Vehicular drop off areas shall be located to the side or rear of buildings.



Intersection with defined pedestrian crossing and primary entrance locations



Plan diagram showing pad site development with internal bays of parking, clear pedestrian access (orange lines) and street edge definition.

**E. General Site Standards**

The intent of the General Site Standards is to provide for minimum site development standards which will ensure cohesive layout of the project, continuity of design across all elements such as signs, outside storage, and landscaping within the property line, and a continuous street landscaping buffer along Central Avenue. Note that building placement and setbacks are determined by zone.

Site layout and design

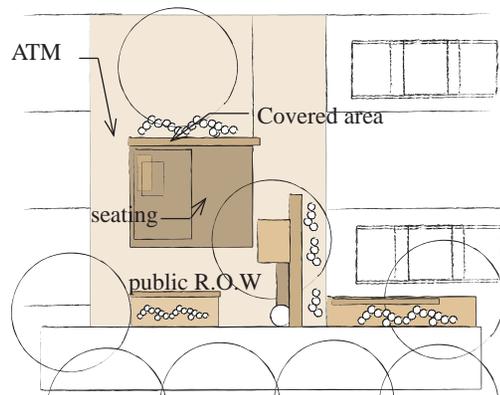
1. Development shall be designed in a manner to create the impression of a unified project and overall sense of a unique or identifiable place. A sense of entry or arrival shall be created at primary entry ways into the development. Building placement, landscaping, gates, entry monuments, specialty lighting and other design elements shall be used to create this design effect.

Guideline A. Linear “strip” development is discouraged. This type of development is characterized as one store deep with buildings and/or units arranged in a linear fashion rather than clustered.

Guideline B. Multiple buildings in single projects should be varied in size and mass.

- 2. Other design elements of the site, such as paving textures, seating, outdoor displays, walls and fences, and other amenities shall be compatible with the architectural and landscape treatment of the project.
- 3. Pad Development. The following design standards shall apply:
  - a. To the maximum extent practical, pad sites shall be clustered together to create consistent edges along streets and to provide safe and convenient pedestrian connections between buildings.
  - b. Spaces between adjacent pad site buildings shall provide pedestrian connections and amenities between sites such as a landscaped pedestrian walkway linking customer entrances between two or more pad site buildings; a public seating or outdoor eating area; sculptures or fountains; or other design features approved by the Planning Director.
  - c. Pad site buildings shall incorporate the same materials as those on the primary commercial building(s) in the development.

## General Development Standards for All Zones



Conceptual Stand Alone ATM Diagram

Walk up ATM

Graphics Pending

4. Stand alone ATM standards. Stand alone ATM's are automated teller machines which are not attached to a building. Stand alone ATM's shall be walk-up only. Drive-up ATM's shall only be permitted as part of a bank drive up facility. (See Drive through/up Standards) for additional information. Stand alone ATM's shall conform to the following standards:
  - a. Stand alone ATM may be permitted in front setback.
  - b. ATM Machine shall be covered by a shading element.
  - c. ATM Machine shall be incorporated into a public space a minimum of 50 sq ft. including at a minimum: 1 tree, 1 bench, 1 trash receptacle and 1 planted area.
  - d. Stand alone ATM shall be buffered from parking areas by a 3 foot minimum wall or plant material a minimum of 3 feet in maturity.
  - e. Stand alone ATM shall be connected to public R.O.W. or buildings on site via sidewalk.
5. Drive through/up Standards. Permissive Use determined by zone. Drive through/up shall not have queuing lane abutting Central Avenue. Drive through/up lane and window shall be buffered from the public R.O.W by a minimum of a 5 foot landscape area with a 3 foot site wall or building. Where possible, Drive through/up service windows and lanes shall be located on a non-street fronting side of a building.
6. Outdoor storage. No outdoor storage is allowed within 50 feet of the Central Avenue right-of-way line. All outdoor storage shall be enclosed on all sides by a minimum six foot high solid wall or fence.
7. Encroachments.
  - a. Encroachments in the public R.O.W. shall follow existing city regulations.
  - b. Building encroachments in the form of balconies or structural shading elements shall not extend within 2 feet of the curb.
8. Grading. Severe changes in grade shall be minimized. Retaining walls up to 4 feet area allowed. Any changes greater than 4 feet shall be permitted only by terracing at a 3:1 slope. A combination of these techniques is allowed.

# General Development Standards for All Zones



Permeable paving



Vegetative swale.



Hardscape broken up with bio retention areas.



Decorative grate and inlet.

- 9. Water harvesting areas. Surface runoff shall be directed into water collection areas located in parking lot landscape areas, landscape setback areas and patio or plaza areas where possible. The burden is on the applicant to demonstrate why water harvesting is not possible if water harvesting areas are not utilized.
- 10. Low Impact Development (LID) Requirements
  - a. All new development shall use two (2) or more of the following LID strategies.
  - b. All new development over 1 acre shall use three (3) or more of the following LID strategies.
  - c. LID strategies are as follows:
    - i. Impervious Surface. Minimize the amount of directly connected impervious material. Transition to permeable material under parking areas or create breaks in impervious material with open curbs of flush mount curbs that allow water to flow into landscaped areas.
    - ii. Internal Roadway Cross Sections. Minimize roadway cross sections where possible. Pedestrian zones should be separated from vehicular zones with a landscaped area that is bordered by open curbs or flush mount curbs to allow for water flow.

- iii. Intersection. Introduce traffic circles at large intersections that can be designed to accept storm water run off and act as a landscaped bio-retention area. At all intersections, minimize the radii to slow traffic and reduce paved area.
- iv. Looped Turnaround. Where possible along the corridor, cluster development and design a looped turn around for access. The center of loop can be bordered by flush mount curbs and act as a landscaped bio-retention area.
- v. Driveways. Where possible, share driveways.
- vi. Sidewalks. Slope/grade sidewalks horizontally so they drain towards flanking landscape.

## General Development Standards for All Zones



*Shade structure and fixed seating.*



*Children's play area.*



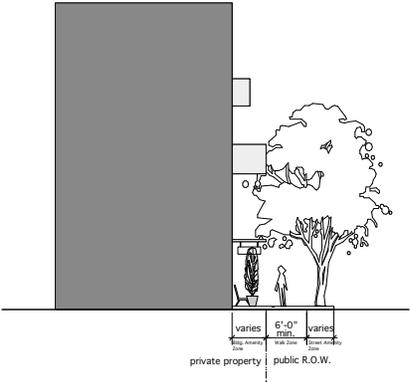
*Allee of trees.*



*Water element/fountain.*

11. Development Phasing. Construction phasing shall be indicated on the Site Development Plan. First phase development requirements: A Pedestrian Realm shall be constructed during the first phase of development. In addition to Pedestrian Realm requirements in the General Standards, undeveloped areas adjacent to the Pedestrian Realm shall include one or more of the following for every 400 linear feet of street frontage to provide visual interest until development build out.

- a. Water element (fountain, spray park, pond)
- b. Hardscaped plaza with colored scored concrete or pavers.
- c. Amphitheater/Fixed seating/movable seating 1/20 LF minimum
- d. Grove a minimum of 3 rows wide and 3 rows long
- e. 2 shrub + 1 tree /60 LF
- f. Shade structure
- g. Mural
- h. Public Art/Sculpture
- i. Children's play area
- j. Landscape Feature (planted area, garden)
- k. Allee of trees, 25 feet on center



Public Realm Diagram

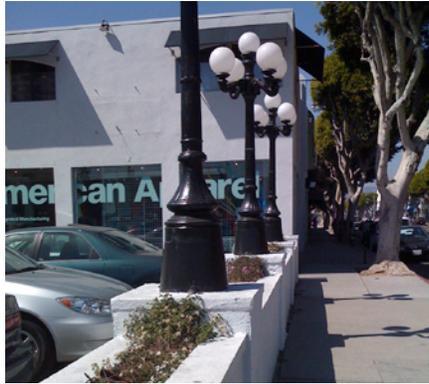
Graphics Pending

F. Pedestrian circulation and connectivity

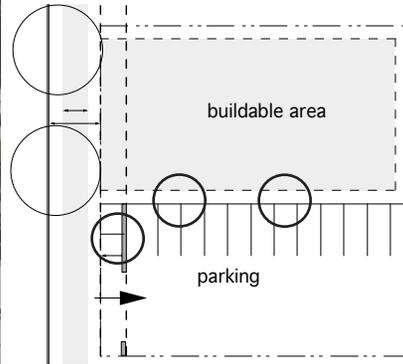
1. Sidewalks shall be located at the property line, except where necessary to meet existing adjacent sidewalk.
2. Public utilities shall not be located so as to interfere with clear pedestrian circulation.
3. Pedestrian Realm. The area from the back-of-curb dedicated to pedestrian use. The required dimensions of the Pedestrian Realm are per zone and shall contain the following areas subject to the following regulations:
  - a. Building Amenity Zone. Area adjacent to a building on private property that may be used to provide pedestrian amenities that stimulate street activity such as restaurant/café seating, retail vending, outdoor seating, fountain and public art.
  - b. Pedestrian Walk Zone. The area dedicated to pedestrian circulation within the public R.O.W. that shall remain clear at all times.
    - i. Sidewalk width. A clear pedestrian path of a minimum of six feet shall be maintained at all times.
    - ii. Sidewalk material. Sidewalks shall be a hard surface which may include concrete, brick, or pavers. Sidewalk material shall be slip resistant and of a permanent nature.
    - iii. Sidewalk alignment. The sidewalk shall be aligned where possible within the block.
    - iv. Sidewalk shall be located at property line.

- c. Street Amenity Zone. The area adjacent to the back of curb dedicated to street furnishings, bike racks, landscaping, telephones, information centers, lighting, signage, and transit facilities (benches and shelters).
    - i. Street trees. Street trees shall be located every 25 feet on center.
    - ii. The use of tree grates is permitted for all tree wells.
  - d. Limited R.O.W. Where public R.O.W. is too constrained to meet Pedestrian Realm requirements, Pedestrian Walk Zone shall be maintained and any additional R.O.W. shall be landscaped.
4. Internal Pedestrian Pathways.
  - a. For lots over one acre, separate pedestrian and vehicular access shall be provided from the public right-of-way to the building(s). Pedestrian access to buildings from the public right-of-way shall be shaded, illuminated for nighttime use, and not utilize driveways as walkways.
  - b. For sites with multiple buildings, pedestrian pathways shall connect each building and an unobstructed sidewalk measuring at least eight (8) feet in width shall be provided along the full length of all sides of buildings having customer entrances. Additional width shall be required if the sidewalk is used for seating, displays or other purposes.
5. Site Connectivity. See MAC and SAC connectivity in Chapter 5, Transportation recommendations.

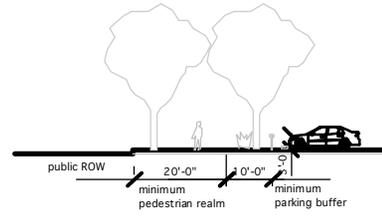
## General Development Standards for All Zones



Parking with wall planter as buffer.



Parking located to side of building.



Parking diagram with landscape buffer.



Parking with landscape buffer.

### G. Off-street parking

1. Buildings constructed prior to 1965 shall supply parking spaces only to the extent on-premise ground space is available.
  2. Adjacent and abutting on-street parking may count toward off-street parking requirements as permitted by zone.
  3. Standard City Transit reductions shall apply. See Zoning Code (§ 14-16-3-1(E)(6)(a)).
  4. Automobile and light truck space standards shall apply. See Zoning Code (§ 14-16-3-1(F)).
  5. Parking for Bicycles shall apply. See Zoning Code (§14-16-3-1(B)(3)).
  6. Pedestrian connections shall apply. See Zoning Code (§ 14-16-3-1(H)).
7. Off-street parking areas shall be designed and landscaped to minimize glare, reduce reflection and reduce the visual impact of large numbers of cars. Parking areas shall include the following landscaping elements:
    - a. A minimum of 15% of the parking lot area shall be landscaped. The landscaping shall consist of shade trees and shrubs and shall be distributed throughout the parking lot.
    - b. One tree shall be required per every 8 parking spaces. Tree planters shall be used at ends of double-loaded parking rows. Not more than 15 spaces side-by-side shall be allowed between planters. A minimum of 2 trees is required per parking area. No parking space may be more than 50 feet from a tree trunk.
    - c. Buildings shall be separated from parking areas and drives by a minimum 5 foot wide sidewalk and minimum 2 foot wide landscape buffer.

# General Development Standards for All Zones



Sports court.



Patio dining.



Play area.



Community Garden

## H. Public spaces

1. All pedestrian oriented and public spaces, including paseos and the pedestrian realm, shall comply with the following standards:
  - a. Include amenities such as benches, movable seating, drinking fountains, trash receptacles, or kiosks; A minimum of 1 bench and 1 trash receptacle is required. For public spaces greater than 100 sq ft, 1 additional seat is required for every additional 100 sq ft. 1 trash receptacle is required for every 10 seats or at least one trash receptacle every 300 lf along linear public spaces.
  - b. Consider solar angles and wind conditions in the design of the space;
  - c. Include areas along building edges for outdoor seating;
  - d. Be designed for security and be visible from the public right-of-way as much as possible;
  - e. Have pedestrian scale lighting a maximum of 16 feet in height.
  - f. Utilize distinctive paving materials, including permeable paving. Asphalt paving is prohibited.
2. Public spaces over 400 sq ft shall contain an outdoor plaza, patio, courtyard or pocket park with seating and shade covering a minimum of 25% of the area. A portion (generally at least 40%) of the square footage of this public space shall be landscaped with plant materials, including trees.
3. In addition, Public spaces over 10,000 sq ft shall contain a minimum of 1 of the following features:
  - a. Sculpture or other artwork.
  - b. Fountain or some other water feature.
  - c. Playground or other recreational amenity.
  - d. Any other amenity that meets the intent of this Plan and that meets the approval of the Planning Director.

## General Development Standards for All Zones

### I. Site Lighting

The intent of the site lighting standards is to ensure cohesive design, provide adequate lighting on a site for safety and to prohibit site lighting from creating a nuisance on adjacent property.

1. Exterior lighting shall be designed as part of the architectural and landscape character of a project. Fixture style and design shall be compatible and consistent with the building design.
2. Site lighting shall be per area lighting regulations in the Zoning Code (§ 14-16-3-9) with the following additions:
  - a. Pedestrian site lighting. Pedestrian street lights shall be located between 13 feet and 16 feet above grade with a maximum average spacing (per block face) of 60 feet on center. Pedestrian street lights must be placed two feet from the back of curb on each side of the street and travel lanes, unless otherwise indicated. Street lighting and street trees should not conflict.
  - b. Exterior building lights. On the street front elevation, exterior lights shall be mounted between six feet and 14 feet above adjacent grade.
  - c. Alley lighting. All lots with alleys shall have lighting fixtures within five feet of the alley's edge of pavement where it does not conflict with vehicle access and circulation. The fixture shall illuminate the alley, be between eight and 12 feet in height, and not cause glare into adjacent lots. When a structure in the lot is within five feet of the alley's edge, the lighting fixture shall be attached to the structure and not to a light pole.
  - d. Lighting elements. Lighting elements shall be compact fluorescent, metal halide, LED, or halogen only. No HID or fluorescent tube lights (excepting compact fluorescent bulbs) may be used on the exterior of buildings.
  - e. Floodlights and directional lights. Floodlights or directional lights may be used to illuminate alleys, parking garages and working (maintenance) areas, but must be shielded or aimed in such a way that they do not shine into other lots or the street.

### J. Signage

The intent of the signage standards is to provide for readable, attractive signs that do not dominate the streetscape, and to encourage the use of neon and iconic signs along Central Ave. that reinforce Route 66 identity. These standards prevail over any conflicting regulations that apply to signs as a permitted use in individual zones within the plan area.

1. Signage shall be per the C-1 zone, §14-16-3-5 General Sign Regulations shall apply with the following additions and exceptions:
  - a. Neon signs
    - i. Definitions.  
Neon sign. A sign that uses neon, argon, mercury or a similar gas to fill tubing made of glass or similar material, which is charged with electricity in order to create an illuminated tubular sign or illuminated elements of a sign that comprise at least 20% of the sign face area and include at minimum lettering and/or images. The tubing may contain an alternative illumination technology, such as, but not limited to, light-emitting diodes (LEDs). Any non-gaseous illumination technology, such as LEDs, must produce illumination that appears to be a continuous, uninterrupted line, similar to illumination produced by gaseous illumination technology.

Animated neon sign. A neon sign that uses changes in luminance in a sequential or radial manner to produce what appears to be movement of an element of the animated sign. Flashing and physical movement of a sign and/or its elements are not considered sequential or with radial changes in luminance.

- ii. Neon signs, including animated neon signs, are permitted on properties fronting Central Ave., or fronting on 98<sup>th</sup> Street, Unser Blvd or Coors Blvd if the signs are within 300 feet of the center line of Central Avenue, subject to regulations in §14-16-3-5 that protect residential zones. The following modifications to zone standards and general sign regulations also apply:
  - Location.  
Free-standing and all types of building-mounted neon signs are allowed. A free-standing sign shall be allowed on properties designated Developing Urban by the Comprehensive Plan.  
  
A neon sign may overhang or protrude horizontally up to 5 feet into the public right-of-way, except a marquee sign is allowed

## General Development Standards for All Zones

10 feet, if it is a minimum of 12-feet above the right-of-way at grade and does not impinge on required clearance for overhead utilities.

A neon sign shall not obscure portions of a building that contribute to architectural character including, but not limited to, doors and windows.

### Number.

One free-standing or projecting neon sign per premise frontage is permitted, except that corner lots of less than 5 acres are permitted only one free-standing or projecting sign. A free-standing sign is allowed on properties designated Developing Urban by the Comprehensive Plan.

One building-mounted neon sign per business or tenant façade fronting Central, 98<sup>th</sup> Street, Unser Blvd and Coors Blvd is permitted.

### Size.

Free-standing and projecting signs. Sign face area may be up to 50% over the sign area allowed for conventional signs but shall not exceed 150 square feet total after the bonus is applied on premises of less than 5 acres, or 225 sq ft on larger premises. Lettering is allowed a proportionate size increase.

Building-mounted signs, excluding projecting signs. Total sign face area may be up to 25% over the total sign face area allowed for conventional signs but shall not exceed 20% of the façade area after the bonus is applied. Lettering is allowed a proportionate size increase.

### Height.

Free-Standing Signs. Height shall not exceed the height of the main structure, excluding towers, on the property by more than 10% or it shall not exceed 30 feet, whichever is lower.

Building-Mounted Sign. Height shall not exceed the height of the building, excluding towers, to which the sign is mounted by more than 10% or it shall not exceed 45 feet, whichever is lower.

b. Historic signs. Historic signs that are associated with the historic use of a premise are exempt from provisions of the Zoning Code (14-

16-3-5(B)(1)(d)) and of this plan that would otherwise prohibit their continued display. For the purposes of this subsection a historic sign is defined as a sign that is determined by the Planning Director to be a designated City Landmark or listed or eligible for listing in the New Mexico Register of Cultural Properties either individually or as a contributing part of a property. A historic sign may be a neon sign.

c. Iconic signs. An iconic sign is a sign whose form suggests its meaning. Such a sign is unique and creates an image and/or defines a place. An iconic sign may be sculptural in style and demonstrates extraordinary aesthetic quality, creativity or innovation. These are signs that are different from the typical sign and have elements of highly recognizable or identifiable features, sign faces or are otherwise distinguished from an average square or rectangular box design. They typically have characteristics of art, going beyond simply advertising the “what and where”. The iconic sign typically refers to an object in symbolic form (as with the road sign which shows a man at work). An iconic sign may be a neon sign.

i. Modifications to zone standards. Where an iconic building-mounted or free-standing sign is proposed, the applicable height and size limitations of the zone and/or these general standards may be modified to allow such sign based on the definition listed above and at the discretion of the approving body.

d. Conventional signs, as distinct from neon, historic or iconic, shall be regulated by standards of the Zoning Code, with the following exceptions and additions.

e. The following signs and sign elements are specifically prohibited.

i. Any off-premise sign referring to a business or merchant not doing business on the premises where the sign is displayed, unless it is allowed as a neon or historic sign (see a. and b. above). Existing off-premise signs shall be allowed to remain, until removed. No existing off-premise signs shall be altered or enlarged.

ii. Any sign which is attached to the roof of a building or structure between the eaves and the ridge line of the roof, unless it is allowed wholly as a neon, historic or iconic sign (see a., b. and c. above).

## General Development Standards for All Zones

- iii. Any sign which is a roof sign as defined in the Comprehensive City Zoning Code, unless it is allowed as a neon, historic or iconic sign (see a., b. and c. above).
- iv. Any sign which is located (painted, affixed, etc) on a water tower, storage tank, smoke stack, utility pole or other similar structure.
- v. Any sign which is located (painted, affixed, etc) on trees, rocks or other natural features.
- vi. Any sign which emits smoke, visible vapors, particles, sound or odor.

### ***K. Walls and fences.***

- 1. As per City Zoning Code (§ 14-16-3-19) with the following exceptions:
  - a. Prohibited materials. Concertina or barbed wire, sheet metal fencing and razor ribbon are prohibited. Chain link fencing is not permitted in the front setback or where adjacent to a residential use
  - b. Street walls within 20 feet of the public street right-of-way shall not exceed three feet in height above the abutting grade on the street side, except walls used for screening of mechanical equipment, loading and service areas shall not exceed six feet.

### ***L. Screening of mechanical equipment, service and loading areas.***

- 1. Pursuant to City Zoning Code (§ 14-16-3-18(C)(6)(a)) with the following additions and exceptions:
  - a. Service and loading facilities shall be combined, where possible.
  - b. Trash containers shall not be visible from a public or private street and shall be recessed or screened by a six-foot high solid wall and/or gate.
  - c. Ground mounted mechanical equipment, and service and loading areas shall be accessed from alleys or rear access easements where possible.
  - d. Exterior mounted mechanical and electrical equipment shall be architecturally screened. Location of such equipment within the building or at ground level is preferable to roof-mounting, unless such location would adversely affect the streetscape, pedestrian circulation, or open space.

*M. Landscape Standards*

- 1. Landscape standards shall be per the City of Albuquerque Zoning Code (§ 14-16-3-10) with the following exceptions:
  - a. The mature spread of a tree’s canopy may count toward the 75% required vegetative ground cover (§ 14-16-3-10(G)(3)) for landscape areas;
  - b. The use of tree grates is permitted for all tree wells;
  - c. Usable open space in such forms as patios, plazas and courtyards, shall have a minimum landscape area of 15%;
  - d. Building setbacks not used for pedestrian activity shall have a minimum landscape area of 50%. Asphalt is not a permitted material within the landscaped setback area.
  - e. Landscaping on roof decks may be counted toward the required area landscaping.
  - f. Standard and special landscape buffers shall be a minimum of 6 feet wide ((§ 14-16-3-10(G)(3) & (40). The special landscape buffer shall be planted primarily (at least 50%) with evergreen trees or tall shrubs, and a minimum three (3) foot high screen wall adjacent to the parking area shall be required between parking areas and abutting residential zones.
- 2. All landscaped areas shall be served by underground irrigation systems unless otherwise approved by the Planning Director.

*n. Demolition*

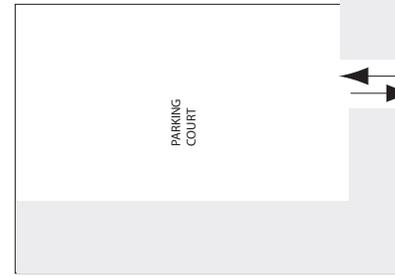
- 1. In order to limit the detrimental effect of demolition on remaining Route 66 historic resources in the plan area, a demolition review period as contained in Article 14-12-9 R.O.A. 1994 may be warranted when the demolition of a building is proposed. A review provides the time necessary to determine whether a structure that meets certain criteria shall be designated as a City Landmark consistent with Article 14-12-7, or for owners of such structures to consider alternative strategies for the preservation, rehabilitation, or restoration of an historic building. City residents and the neighborhood are required to be notified and asked to comment on the impending demolition of historic properties.

## General Development Standards for All Zones

### *o. Best Practices Guidelines for Development*

In addition to the requirements of the General Development Standards, the following Best Practices guidelines are encouraged to be incorporated in development.

1. Building entry ways on streets. At least one entrance for each commercial/retail space on a street facade should have direct access to the street or a street fronting court or plaza. All upper level nonresidential and residential spaces should have a first floor lobby with direct access to street or a street fronting court or plaza.
2. Corner lots. Corner lots should address the corner in at least one of the following ways: location of main entrance at a corner; articulation at the corner of the building relating to the corner, i.e. curve, angle, step back or projection, tower element and/or Planning Director approved detail.
3. Where possible, in order to minimize conflicts between pedestrian and vehicular circulation, drive aisles should be one-way.
4. The following site layout diagrams suggest development organization which provides access for both pedestrian and vehicles while maintain a street frontage which contributes to the public realm. These diagrams should guide development where applicable.



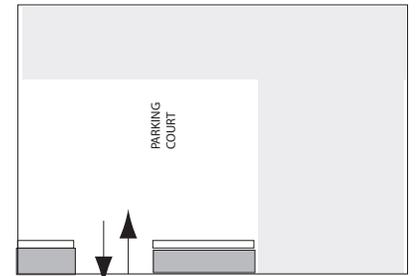
CORNER - A

CORNER LOT. BUILDING WRAPS CORNER. ACCESS TO PARKING FROM SIDE STREET.



CORNER - B

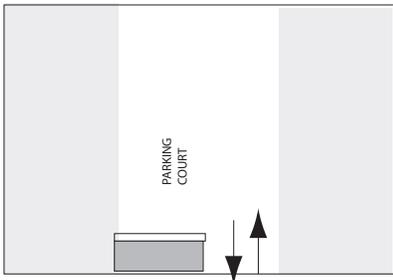
CORNER LOT. PLAZA AT CORNER. ACCESS TO PARKING FROM SIDE STREET.



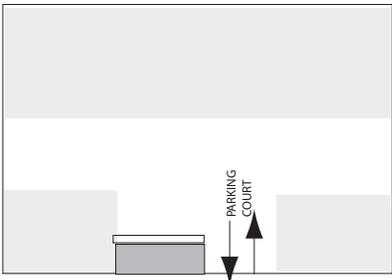
CORNER - C

CORNER LOT. BUILDING AT CORNER. ACCESS TO PARKING FROM MAIN STREET.

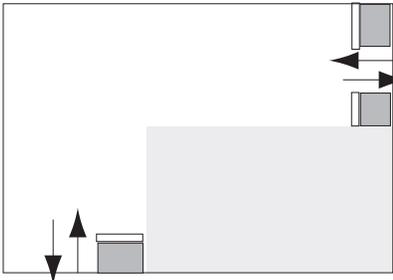
# General Development Standards for All Zones



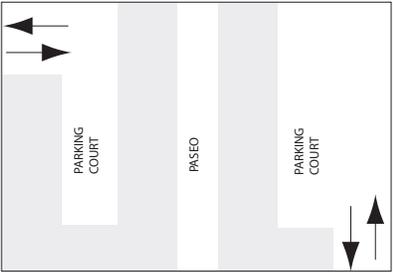
**COMMERCIAL - SIDE PARKING**  
 PARKING LOCATED AT SIDE OF BUILDING, BUFFERED BY LANDSCAPING AND STREET WALL.



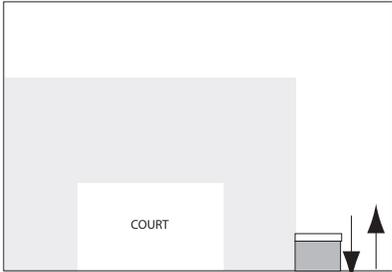
**COMMERCIAL - PAD SITES**  
 PARKING PRIMARILY LOCATED BETWEEN BUILDINGS. SOME PARKING TOWARD STREET BUFFERED BY LANDSCAPING AND STREET WALL.



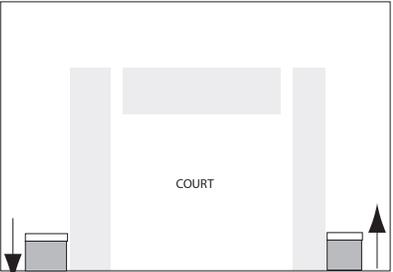
**COMMERCIAL - GENERAL**  
 ONE BAY OF PARKING LOCATED AT SIDE OF BUILDING. MAJORITY OF PARKING LOCATED IN REAR OF BUILDING.



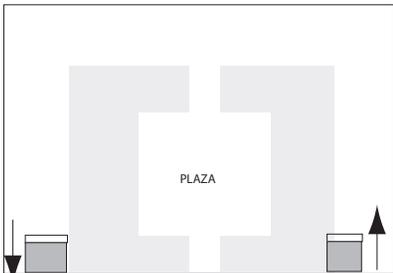
**PASEO**  
 BUILDINGS ORIENTED TO PASEO OR MAIN STREET. ACCESS FROM SIDE OR MAIN STREET. PASEO IS A PEDESTRIAN ONLY STREET



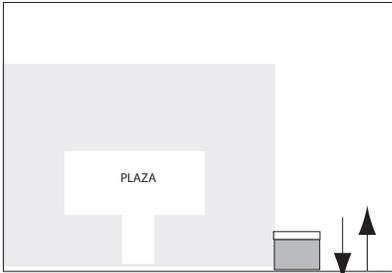
**COURT- A**  
 BUILDINGS ORIENTED TO COURT OR MAIN STREET. ACCESS FROM SIDE OR MAIN STREET. PLAZA FOR PEDESTRIAN USE.



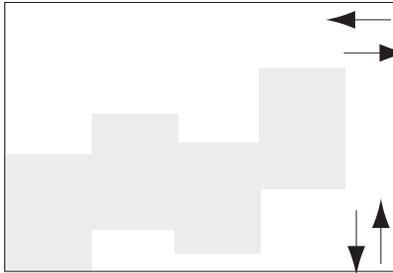
**COURT- B**  
 BUILDINGS ORIENTED TO COURT OR MAIN STREET. ACCESS FROM SIDE OR MAIN STREET. PLAZA FOR PEDESTRIAN USE.



**PLAZA -**  
 BUILDINGS ORIENTED TO PLAZA OR MAIN STREET. ACCESS FROM SIDE OR MAIN STREET. PLAZA FOR PEDESTRIAN USE.



**PLAZA -**  
 BUILDINGS ORIENTED TO PLAZA OR MAIN STREET. ACCESS FROM SIDE OR MAIN STREET. PLAZA FOR PEDESTRIAN USE.



**STEPPED**  
 BUILDINGS STEPPED BACK TO CREATE FRONT PATIOS. ACCESS TO PARKING FROM FRONT OR SIDE.

This page intentionally left blank.