

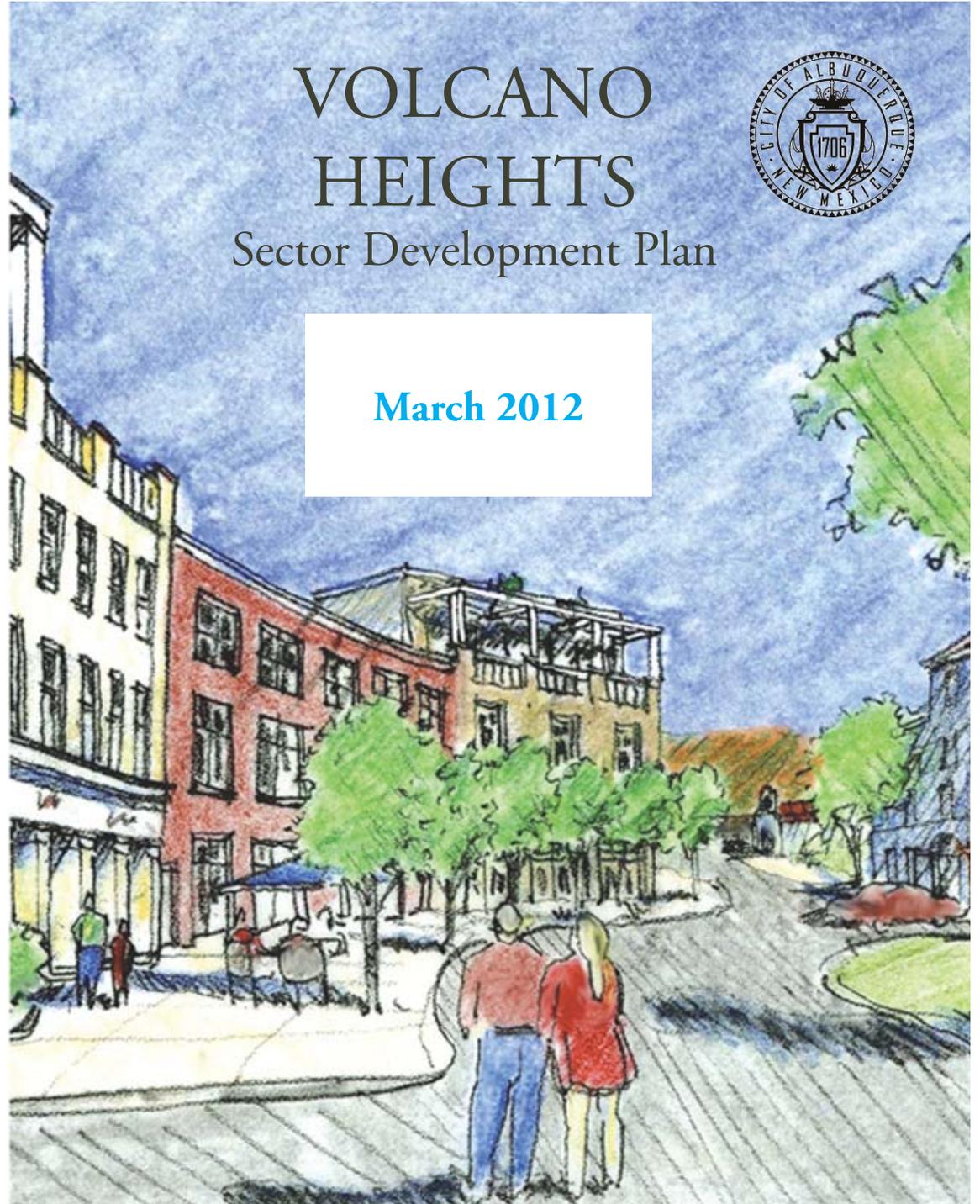


VOLCANO HEIGHTS

Sector Development Plan



March 2012



Volcano Heights Sector Development Plan

City of Albuquerque

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- Appendix B. Sector Planning Process
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Text in red indicates language or graphics still pending planning efforts or staff review.

Because this is a working draft, internal cross referencing may not be up-to-date.]

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List of Acronyms

AMAFCA:	Albuquerque Metropolitan Area Flood Control Authority	ONC:	City Office of Neighborhood Coordination
AMPA:	Albuquerque Metropolitan Planning Area	PID:	Public Improvement District
BID:	Business Improvement District	RAC:	Roadway Access Committee
BRT:	Bus Rapid Transit	RI/RO:	Right-in / Right-out (Intersection)
BTZ:	Build-to Zone	ROW:	Right-of-Way
CABQ:	City of Albuquerque	SAD:	Special Assessment District
DMD:	City Department of Municipal Development	SDP:	Sector Development Plan
DOZ:	Design Overlay Zone	SF:	Square Feet
DPM:	Development Process Manual	SIA:	Subdivision Improvement Agreement
DRB:	Development Review Board	TCC:	Transportation Coordinating Committee
EIFS:	Exterior Insulating Finishing System	TIF:	Tax Increment Financing
EPC:	Environmental Planning Commission	TIDD:	Tax Increment Development District
FAABS:	Future Albuquerque Area Bikeways and Streets	TPTG:	Transportation Program Task Group
GSF:	Gross Square Footage	VHET:	Volcano Heights Escarpment Transition Zone
ITS:	Intelligent Transportation Systems	VHMX:	Volcano Heights Mixed-use Zone
LID:	Low Impact Design	VHNT:	Volcano Heights Neighborhood Transition Zone
LRV:	Light Reflective Value	VHRC:	Volcano Heights Regional Center Zone
MPO:	Metropolitan Planning Organization	VHRT:	Volcano Heights Review Team
MPOS:	Major Public Open Space	VHSDP:	Volcano Heights Sector Development Plan
MRCOG:	Mid-Region Council of Governments	VHTC:	Volcano Heights Town Center
MRMPO:	Mid-Region Metropolitan Planning Organization	WSSP:	West Side Strategic Plan
MTB:	Metropolitan Transportation Board		
MTP:	Metropolitan Transportation Plan		
NMDOT:	New Mexico Department of Transportation		
NOD:	Notice of Decision		
NSF:	Net Square Footage		
NWMEP:	Northwest Mesa Escarpment Plan		

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CHAPTER I

executive summary

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1.0 Executive Summary

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1.0 Executive Summary

1.1. Plan Area

Volcano Heights is one of three Sector Development Plan areas in Volcano Mesa. (See **Exhibit 1.1**).

The Volcano Heights Sector Development Plan (SDP) is bordered by Paseo del Norte to the north before it curves southeast, the Petroglyph National Monument on the east, Volcano Cliffs SDP boundary on the south, and Universe Boulevard on the west. The Plan area includes approximately 570 acres and surrounds the intersection of Paseo del Norte and Unser Boulevard, two vital, regional traffic arteries.

1.2. Purpose

The purpose of the Plan is to support pedestrian-friendly and transit-supportive development with particular emphasis on employment, while buffering pre-existing single-family neighborhoods and sensitive lands on the borders of the Plan area from higher-density development toward the center of the Plan area.

1.3. Intent

The intent of this Plan is to encourage development that creates an attractive, vibrant Major Activity Center that respects and honors the unique cultural, historical, geological, and volcanological setting, while providing employment, services, amenities, housing, and transportation choices for the larger region as part of a sustainable community on Albuquerque's West Side.

The zoning and corresponding standards are created to support economic development, sustainable tax base, and job creation by establishing the predictability of private development along corridors and across property lines to support and leverage investment in Volcano Heights.

The Plan aims to encourage attractive development through the use of recognized principles of urban design and to allow property owners flexibility in land use, while providing predictability through a higher level of detail in proscribed site development standards, building design, and form.

1.4. Zoning Strategy

All zones in Volcano Heights allow a mix of residential and non-residential development in order to provide maximum flexibility to property owners to adapt to market conditions and to encourage a mix of housing and services within walking and biking distance.

East of the intersection of Paseo del Norte and Unser Boulevard, the Plan envisions a Town Center with employment-oriented development and entertainment uses organized around a planned transit corridor. Development lining these vital regional auto transportation corridors is intended to support auto-oriented, destination retail and businesses accessed from local roads. Surrounding the Town Center and Regional Center, the Plan offers pedestrian-oriented, Mixed Use development with convenient access between new and existing neighborhoods and shopping, employment, services, and amenities throughout the Plan area. One Village Center is included at the intersection of Paseo del Norte and Universe Boulevard, mirroring a Village Center in Volcano Trails.

The Plan includes two types of transition zones as buffers between development densities and intensities: (1) Neighborhood Transitions to ensure new development compatible with pre-existing neighborhoods to the north and south of the Plan area and (2) an Escarpment Transition east of the Plan area to ensure development compatible with sensitive lands within the Petroglyph National Monument.

CHAPTER I: INTRODUCTION

1.0 Executive Summary

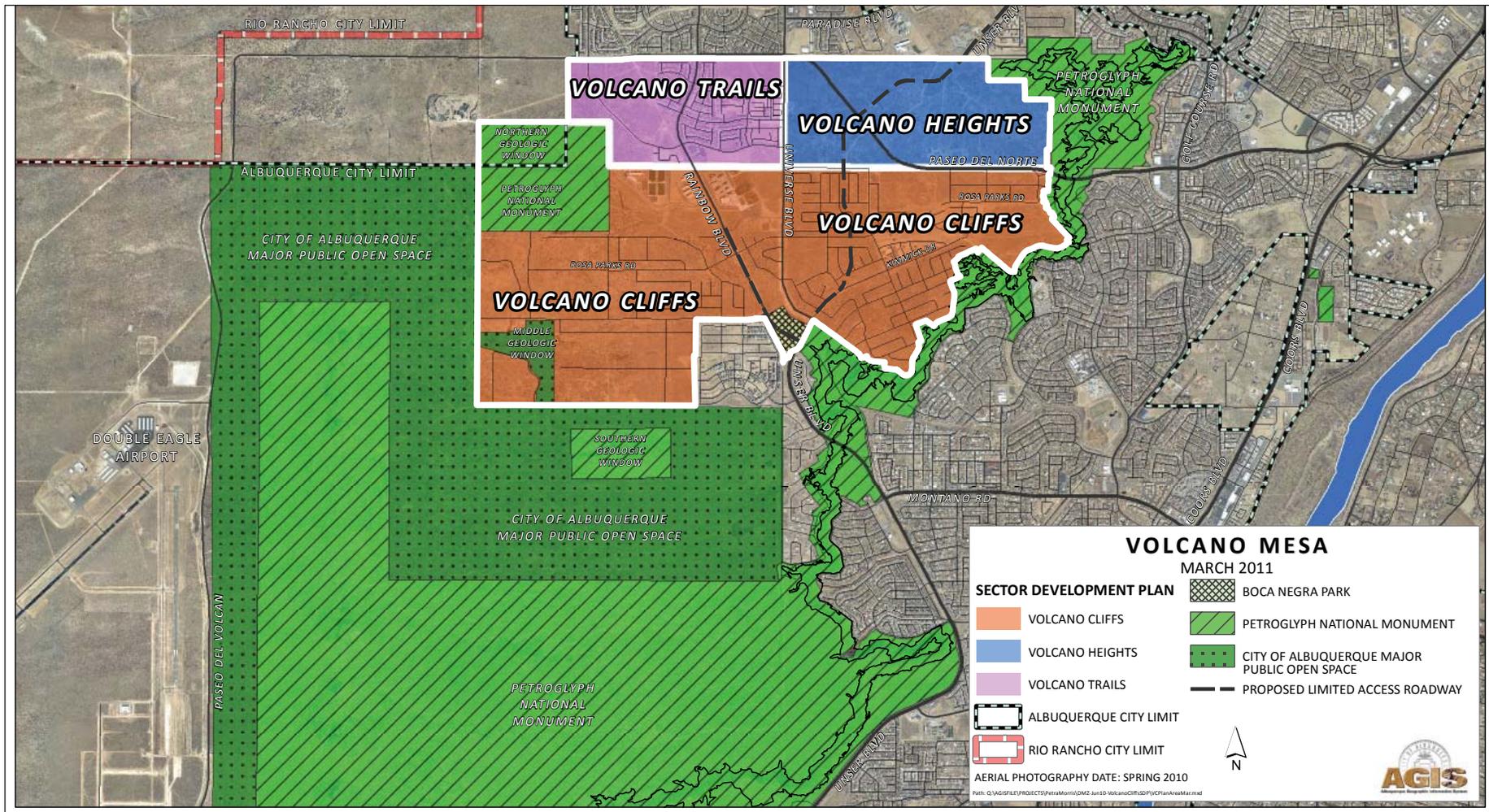
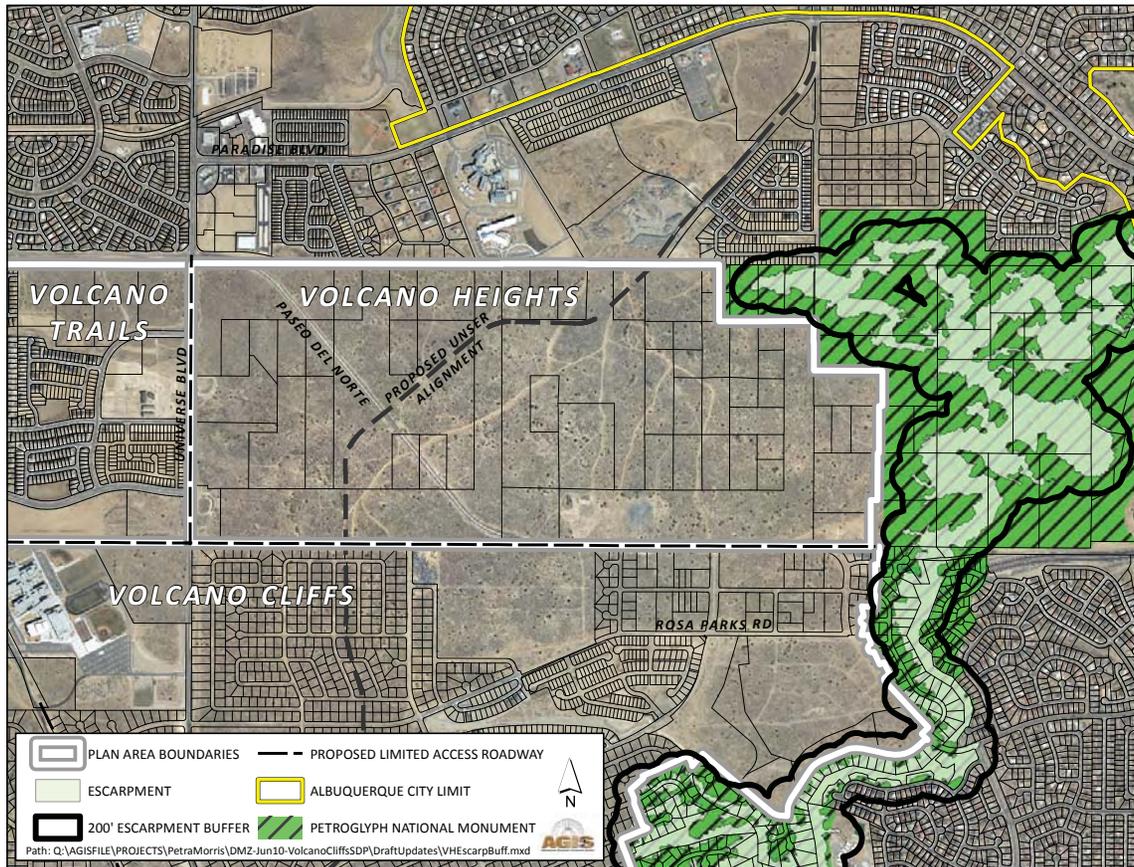


Exhibit 1.1 – Volcano Mesa Sector Development Plan Areas

CHAPTER I: INTRODUCTION

1.0 Executive Summary



As Volcano Heights is a unique landscape that shares a border with the Petroglyph National Monument, the Plan takes an innovative approach toward height limits for buildings and structures to concentrate opportunities for height and density toward the center of the Plan Area and step heights down toward the edges with Transition zones. The Plan offers an optional bonus height system as an incentive to preserve and provide public access to rock outcroppings and other sensitive lands. See **Section 7.3 Optional Bonus Heights**.

1.5. Development Standards Strategy

The Plan implements the vision for Volcano Heights as established by the Zoning Map. (See **Exhibit 5.1**) The Zoning requires minimum standards to guide property owners, developers, and the City on the form, character, and intensity of desired future development within key locations and along key corridors in the Plan area. Six distinct Character Zones enumerate specific site, building design, and other development standards as basic building blocks to create sense of place. Clear graphic standards are provided for location, height, and building design elements. Such standards promote predictability of adjacent developments, flexibility in land use, walkable and transit-supportive mixed-use development, transitions to existing neighborhoods, and protection of natural and cultural resources.

Exhibit 1.2 – Volcano Heights Plan Area

1.0 Executive Summary

1.6. Plan Components

The VHSDP shall provide the official zoning map. It establishes the development standards for all properties within the Plan area including the following:

1.6.1. **Character Zones:** The Zoning Map designates all property in Volcano Heights as one of six “Character Zones.” Each Character Zone is intended to create a distinct urban form. Each Character Zone establishes allowable land uses and site development and building design standards, including building and parking location, height, massing, glazing, materials, and design.

1.6.2. Street Designations

- (i) **Mandatory Streets:** The Mandatory Street Map designates streets as Type ‘A’ Streets or ‘B’ Streets within the Plan area. All streets, whether Type ‘A’ or ‘B,’ that are mapped in **Exhibit 4.1** in **Section 4.4** are considered Mandatory Streets.
- (ii) **Non-mandatory Streets:** Non-mandatory streets are those local roads constructed to serve development projects and contribute to the local street network grid. These streets are also to be designated ‘A’ or ‘B’ streets, with a menu of cross section options and typical cross sections provided in **Section 4.7**, **Table 4.2**, and **Exhibit 4.19** and **Exhibit 4.20**.

1.6.3. Site Development Standards, Building Design Standards, and General Regulations

The Plan provides development standards with text and graphics for Character Zones, Frontage Types, site development, landscape, building design, signage, open space, lighting, construction mitigation, and rainwater management.

1.7. Using This Plan

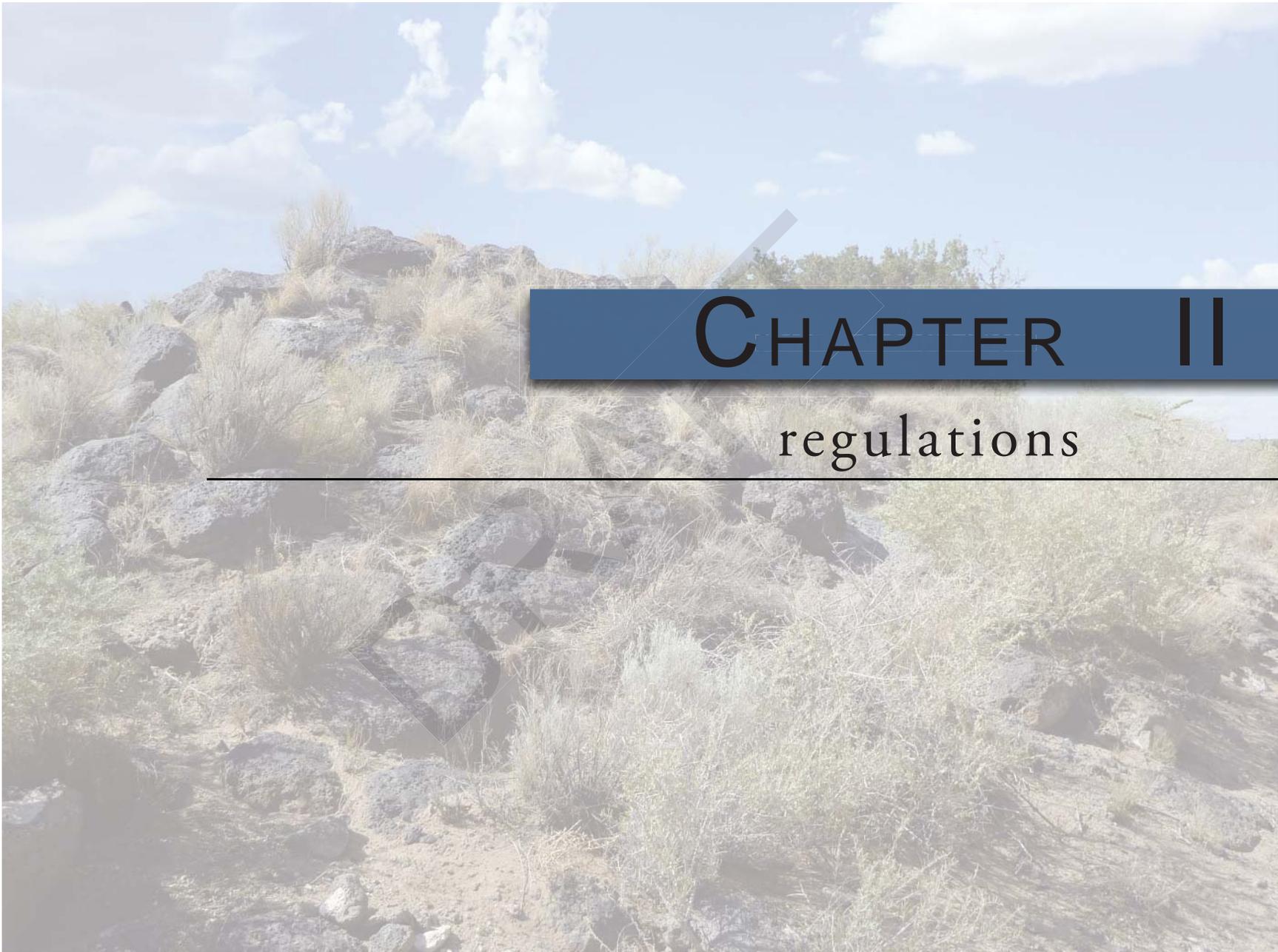
This Plan is organized into three chapters plus an appendix. **Chapter I** summarizes the purpose and intent of the Plan. Only **Chapter II** includes regulations with the power of law, including zoning and all design and development regulations and standards. **Chapter III** provides policy and guidance for City staff and developers. The Appendix includes supplementary information, including pre-existing conditions prior to the Plan’s adoption.

The following basic steps should be followed to determine the uses and development standards applicable on property within Volcano Heights, including where the building can sit on the lot, the limits on its three-dimensional form, the range of uses allowed, and the palette of materials that will cover it:

- 1.7.1. Review **Table 3.1** to evaluate the applicability of the uses and design standards/regulations based on the size and scope of the proposed project.
- 1.7.2. Locate the subject property in **Exhibit 4.2** and identify:
 - (i) the Character Zone(s) in which the property is located; and,
 - (ii) the Street Type designation along all its Mandatory Street frontages.

1.0 Executive Summary

- 1.7.3. Review **Section 4 Street and Streetscape Standards** for relevant Mandatory Streets and options for Non-mandatory streets proposed to serve the project.
- 1.7.4. Review **Table 5.1 – Land Use Table by Character Zone** to determine allowed uses.
- 1.7.5. Examine **Section 6** Site Development and Building Design Standards to determine the applicable development standards and any Special Frontage standards for each Character Zone.
- 1.7.6. Refer to **Section 7** for Site Development Standards that apply generally to all zones.
- 1.7.7. Refer to **Section 8** for Building Design Standards that apply generally to all zones.
- 1.7.8. Refer to **Section 9** for Sign Standards that apply generally to all zones.
- 1.7.9. Refer to **Section 10** for Open Space Standards that apply generally to all zones.
- 1.7.10. Refer to **Section 11** for General Regulations that apply to all zones.
- 1.7.11. Development projects shall also be subject to the Development Process Manual (DPM) and other City ordinances and regulations not in conflict with policies and regulations in this Plan.



CHAPTER II

regulations

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2.0 Authority

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2.0 Plan Authority



2.1. Plan Authority

The Volcano Heights SDP (“The Plan” or “VHSDP”) is a Rank III plan that includes both regulations (i.e. law) and policies (i.e. guidance) as one of the instruments to implement the goals, objectives, and policies of the Rank I Albuquerque-Bernalillo Comprehensive Plan, and more specifically the Rank II West Side Strategic Plan (WSSP), which designates a Major Activity Center within Volcano Heights as part of the Volcano Mesa Amendment. The Plan supplants the application of all provisions of the City Zoning Code as it pertains to land within the Plan area prior to the Plan’s adoption unless otherwise stated in this Plan.

2.1.1. Rank Plans

The City of Albuquerque uses a system of ranked plans, starting with the Rank I Albuquerque / Bernalillo County Comprehensive Plan, which sets the vision, goals, and overall policies from a City-wide perspective. There are also lower-ranked plans that must comply with the intent, policies, and goals of higher-ranked plans. Rank II Plans, such as the WSSP or the Arroyos Facility Plan, are exclusively policy documents that provide more detail and give more direction about large but distinct areas within Albuquerque. Rank III Plans, including Sector Development Plans (SDP) such as this Volcano Heights SDP, take the most detailed look at smaller areas and can include both policy (i.e. direction) and regulations (i.e. law).

This Rank III VHSDP is intended to further and comply with the policies and intents of the adopted plans in **Table 2.1**.

TABLE 2.1 – RELEVANT RANKED PLANS

Relevant Ranked Plans	Area	Policy / Regulation
Rank I: Albuquerque / Bernalillo County Comprehensive Plan	Entire Albuquerque Area	Policy
Rank II: Area / Facility Plans <ul style="list-style-type: none"> West Side Strategic Plan Facility Plan for Arroyos COA Major Public Open Space Facility Plan Trails and Bikeways Facility Plan Facility Plan for Electric Service Transmission and Subtransmission Facilities 	Relevant Albuquerque Areas, including Volcano Mesa	Policy
Rank III: <ul style="list-style-type: none"> Northwest Mesa Escarpment Plan (NWMEP) 	Specific Area	Policy / Regulation

2.0 Plan Authority

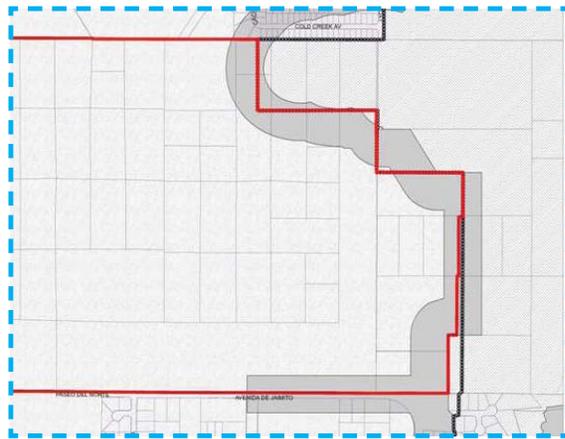
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2.1.2. Northwest Mesa Escarpment Plan (NWMEP)

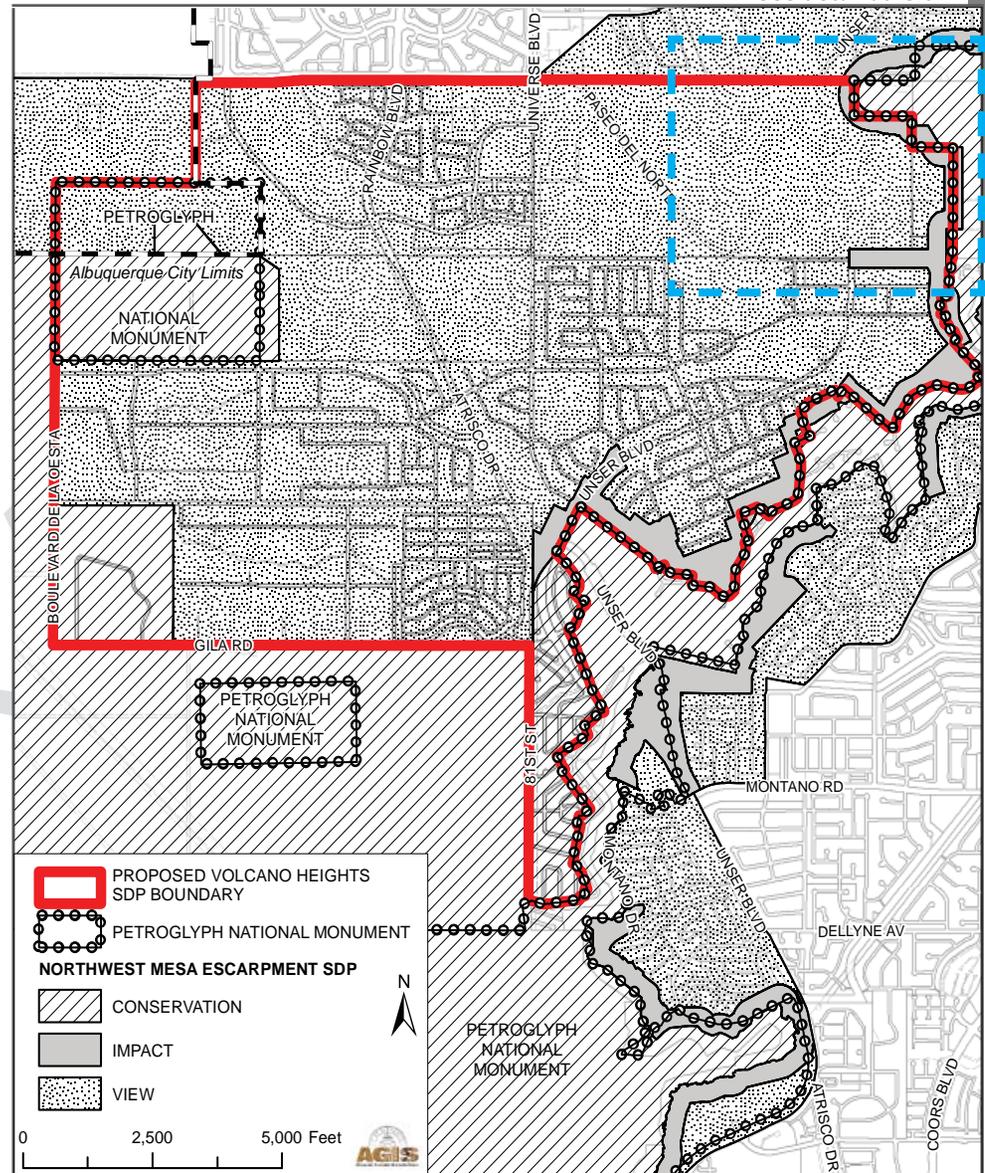
The Rank III NWMEP provides policy and regulatory guidance for development within its boundaries. Adopted in 1988, it sets regulations and policies to control design, construction mitigation, view preservation, and drainage to protect sensitive lands in and around the Escarpment, parts of which became the Petroglyph National Monument.

The NWMEP designates three distinct areas and distinguishes regulations and policies accordingly: Conservation, Impact, and View. Volcano Heights is included in the NWMEP boundary and contains a small portion of Impact Area along the boundary of the Petroglyph National Monument to the east, while the rest of Volcano Heights is designated as View Area. (See Exhibit 2.1, NWMEP and WSSP Volcano Mesa Amendment Boundary.)



Impact Area Detail for Volcano Heights

Exhibit 2.1 – NWMEP and WSSP Volcano Mesa Amendment Boundaries



2.0 Plan Authority

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- (i) Because the VHSDP and the NWMEP are both Rank III plans with overlapping boundaries, where a policy or regulation conflicts, the VHSDP prevails, unless otherwise stated in the Plan. Where one plan is silent, the policies/regulations of the other plan prevail.
- (ii) Maximum heights by right in Town Center, Regional Center, Mixed-Use, and Neighborhood Transition Zones in Volcano Heights comply with the 40-foot maximum height allowed in the NWMEP View area, although the NWMEP measures from natural grade, and this Plan measures from approved grade, which in some cases may include a minimum amount of fill necessary for construction or to connect to infrastructure.
- (iii) Portions of Escarpment Transition Zone that are also within the NWMEP Impact Area have structure height restrictions of 15 feet per the NWMEP, with the exception that heights shall be measured from approved grade. No variances or deviations shall be granted.
- (iv) No bonus heights shall be granted within the Escarpment Transition or Neighborhood Transition Zones.

In order to balance the intent of respecting and protecting the fabric of unique cultural and geological features of this area with the intent of creating opportunities for a regional employment center and Major Activity Center, the VHSDP includes an optional bonus height system that allows additional structure height for the Center and Mixed-Use Zones. Projects must meet additional criteria intended to provide commensurate benefits to both the natural and built environments as a trade-off for bonus height and density.

2.1.3. **Unser Boulevard Design Overlay Zone (DOZ):** The Unser Boulevard DOZ, adopted in 1992, sets design regulations regarding signs between I-40 and the Sandoval County line following the previous alignment of Unser Boulevard. [Update pending, Map pending]

Because the VHSDP and the Unser Boulevard DOZ are both Rank III plans with overlapping boundaries, where a policy or regulation conflicts, the more restrictive prevails, unless otherwise stated in this Plan. Where one plan is silent, the policies/regulations of the other plan prevail.

2.1.4. **City of Albuquerque Subdivision Ordinance:** [Language pending]



3.0 Administration

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3.0 Administration

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3.1. Applicability

- 3.1.1. The uses, buildings, and structures on all land within Volcano Heights shall conform to this Plan. **Table 3.1** shall determine which sections of the Plan apply to any proposed development based on the type and scope of the proposed development.
- 3.1.2. Provisions of this Plan are activated by “shall” when required, “should” when recommended, and “may” when optional.
- 3.1.3. The provisions of this Plan, when in conflict, shall take precedence over those of other City of Albuquerque codes, ordinances, regulations, and standards as amended except as noted herein. Where this Plan is silent, relevant City of Albuquerque codes, ordinances, regulations, and standards shall apply.
- 3.1.4. **Where in conflict with this Plan, the development standards under City Zoning Code Section 14-16-3-18 General Building and Site Design Regulations for Non-Residential Uses, as amended, shall not apply to property within Volcano Heights. Development standards not addressed in this Plan shall be governed by the City Zoning Code to the extent they are not in conflict with the intent or provisions of this Plan. [confirm language]**
- 3.1.5. Terms used throughout this Plan are defined below in **Section 3.5** of this Plan. For those terms not defined in this Plan, City Zoning Code Section 14-16-1-5 shall apply. Terms not defined in either section shall be accorded commonly accepted meanings. In the event of conflict, the definitions of this Plan shall take precedence.
- 3.1.6. Where in conflict, numerical metrics shall take precedence over graphic metrics.
- 3.1.7. **Projects Subject to Review:** All private development and redevelopment projects are subject to administrative review by the Planning Department and/or official review by the Development Review Board (DRB) and/or Environmental Planning Commission (EPC) for compliance with the Plan. All buildings, streets, and public spaces by public entities are also subject to review. **Table 3.1** establishes the standards for the applicability of this Plan to all development and redevelopment.

3.0 Administration

TABLE 3.1 – APPLICABLE PLAN SECTIONS BY DEVELOPMENT TYPE

Plan Section	4.0 Streetscape Standards	5. 1 Land Use Table	6.X.2 Building Placement	6.X.4 Height Standards	6.X. 6 Parking & Service	6.X.7.i Encroachments	6.X.7.iii Single-Family Buffer	6.X.9-12 Building Design Standards by Zone	8.0 General Building Design Standards	9.0 Signage	10.0 Civic/Open Space	10.6 Landscaping & Lighting
Type of Development												
Commercial (retail, office, restaurant), lodging, mixed-use building, apartment/multi-family building (3 or more units per lot), and live-work buildings												
New Construction	X	X	X	X	X	X	X	X	X	X	X	X
Change of Use/Expansion of use with no expansion of building		X			X	X			X	X		
Expansion of Building Area												
0% - 49% increase in building area <i>(Standards in applicable sections shall apply only to the expansions)</i>		X	X	X	X	X	X	X	X	X	X	X
Any increase in building value with NO increase in building area		X										
50% or greater increase in building area AND less than 50% increase in building value <i>(Standards in applicable sections shall apply only to the expansions)</i>		X	X	X	X	X	X	X	X	X	X	X
50% or greater increase of building area AND 50% or greater increase in building value <i>(Standards in applicable sections shall apply the entire building including retrofitting of the existing building if non-conforming)</i>	X	X	X	X	X	X	X	X	X	X	X	X
Expansion of parking area only (not in conjunction with a building or use expansion)												
Up to 10 spaces					X				X	X		X
11 or more additional spaces					X				X	X		X
Façade changes to existing buildings												
'A' Street façade	X					X		X	X	X		X
All other street façades						X		X	X	X		X



3.0 Administration

TABLE 3.1 – APPLICABLE PLAN SECTIONS BY DEVELOPMENT TYPE (Cont'd)

Plan Section	4.0 Streetscape Standards	5.1 Land Use Table	6.X.2 Building Placement	6.X.4 Height Standards	6.X.6 Parking & Service	6.X.7.i Encroachments	6.X.7.iii Single-Family Buffer	6.X.9-12 Building Design Standards by Zone	8.0 General Building Design Standards	9.0 Signage	10.0 Civic/Open Space	10.6 Landscaping & Site Lighting
Type of Development												
Commercial (retail, office, restaurant), lodging, mixed-use building, apartment/multi-family building (3 or more units per lot), and live-work buildings (cont'd)												
Signage												
Refacing or changing a panel on an existing sign												
Modification of an existing sign where the cost of the modification is valued at <u>less than</u> 50% of the replacement value of the sign												
Modification of an existing sign where the cost of the modification is valued at <u>more than</u> 50% of the replacement value of the sign										X		
New sign or complete replacement of an existing sign										X		
Residential Buildings (single-family attached and detached buildings)												
New construction	X	X	X	X	X	X		X	X	X	X	X
Change of Use with no expansion of building		X			X				X	X		X
Expansion of use/structure (any building addition, deck, porch, etc.)		X	X	X	X	X		X	X			X
Expansion of use/structure (new accessory building/structure on the lot)		X	X	X		X		X	X			X
Façade changes												
'A' Street façade						X		X	X			X
All other street façades						X		X	X			

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3.0 Administration

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3.2. Development Review and Approval Process

3.2.1. **Intent / Purpose:** In order to encourage and support development in Volcano Heights, review and approval of proposed projects should be conducted as speedily as possible. As such, this Plan proposes several opportunities to streamline approval, despite the significant efforts needed to coordinate infrastructure for development in the short-term.

3.2.2. **Pre-Application Review Discussions:** As the first step in proposing a new development project, all potential applicants shall schedule and attend a discussion with the Planning Director or his/her designee. The Planning Department holds Pre-Application Review Discussions in order to provide informal guidance to property owners and/or developers during the conceptual design phase. These discussions are available for appointments on a weekly basis. This discussion will help determine whether major infrastructure will be needed, options for infrastructure planning and coordination, and opportunities for streamlining approvals. **Based on conceptual plans, a staff planner may be assigned to the project to help applicants navigate necessary approval processes. [confirm language]**

- 3.2.3. **Significant Infrastructure Coordination:** Significant Infrastructure is determined on a case-by-case basis but generally applies to a major street, drainage or utility facility, etc. that is necessary for the subject property as well as other nearby properties to develop. Applicants have two process options to coordinate Significant Infrastructure:
- (i) A Site Development Plan for Subdivision may be submitted to the Development Review Board (DRB). This submittal includes a Subdivision Improvement Agreement (SIA), which documents financial guarantees of funds available to provide infrastructure.
 - a. Once approved by the DRB, Site Plans for Subdivision for projects less than 5 acres may proceed directly to Building Permit.
 - b. Once approved by the DRB, Site Plans for Subdivision 5 acres or greater shall require a Site Plan for Building Permit. Projects may submit a Site Development Plan for Building Permit concurrently with a Site Plan for Subdivision submittal.
 - (ii) Applications for a Special Assessment District (SAD), Tax Increment Development District (TIDD), or Public Improvement District (PID) may be submitted to the City Council for approval, per the following City Ordinances.
 - a. SAD: Albuquerque Code of Ordinances Section 6-8.
 - b. TIDD: Albuquerque Code of Ordinances Section 4-10.
 - c. PID: Albuquerque Code of Ordinances Section 6-9.

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d. Because these processes include significant infrastructure planning and provide a financial mechanism to fund infrastructure improvements, projects that receive City Council approval are eligible for Administrative Approval of Site Development Plans for Building Permit.

3.2.4. **DRB Sketch Plan Review:** All applicants shall schedule and attend a discussion with the DRB before submitting a Site Development Plan for Subdivision. The DRB holds weekly Sketch Plan Reviews for no fee.

3.2.5. **Administrative Review and Approval:** Once infrastructure is either in place or coordinated with financial guarantees in place per **Section 3.2.3** of this Plan, Site Development Plans for Building Permit that fully comply with all standards of the Plan shall be processed administratively by the Planning Director or his/her designee and proceed to Building Permit.

The Planning Director or his/her designee shall be responsible for the following:

- (i) Scheduling and conducting Pre-Application Review Discussions;
- (ii) Assigning Staff to follow the project through the necessary approvals;
- (iii) Reviewing Site Plans for Building Permit for compliance with the intent, policies, and requirements of the Plan;
- (iv) Determining whether a Site Plan for Building Permit deviates from any standard within the thresholds for:

- a. Minor Deviation per **Section 3.2.10** and **Table 3.2** below and therefore eligible for Administrative Approval;
 - b. Major Deviation per **Section 3.2.11 and Table 3.3** below and therefore required to obtain approval by the EPC;
 - c. Non-compliance without meeting the intent of this Plan and therefore required to modify the Project or amend this Sector Development Plan per City Zoning Code 14-16-4-3; OR
 - d. Non-compliance yet still meeting the intent of this Plan and therefore eligible for an SU-1 Site Development Plan approval for a use not listed in this Plan OR eligible for approval by the EPC with an Exception per **Section 3.2.13** in this Plan.
- (v) Determining the public notice required for proposed projects, based on the approval process determined above
 - a. Public Notice for Site Plans for Building Permit per **Table 3.4** OR
 - b. Public Notice for Site Plans for Subdivision per **Table 3.5**.
 - (vi) Mailing Public Notice to relevant Property Owners as noted by (PO) in **Table 3.4** or **Table 3.5**;
 - (vii) Determining whether convening the Volcano Heights Review Team is appropriate;
 - (viii) Convening a Volcano Heights Review Team of relevant City staff and/or members of the public;

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- (ix) Approving Site Development Plans for Building Permit that fully comply with the intent, policies, and requirements of the Plan;
- (x) Approving minor deviations to previously approved Site Development Plans for Building Permit that comply with this Plan and all applicable City ordinances, as per City Zoning Code Section 14-16-2-22(6) and **Section 3.2.10** in this Plan.
- (xi) Forwarding any major deviations to the EPC per **Section 3.2.11** below; and
- (xii) Forwarding any appeals to the decision of the Planning Director to the City Council, per City Zoning Code Section 14-16-4-4.

3.2.6. **Volcano Heights Review Team (VHRT):** As part of the Administrative Review Process, a Volcano Heights Review Team (“the Review Team” or VHRT) may be convened by the Planning Director or his/her designee for projects that require interpretation or discretionary judgment with respect to the project’s compliance with standards. This non-judicial Review Team shall be charged with working cooperatively and creatively with the applicant to solve problems and resolve conflicts regarding elements of a proposed development project that seem to meet the intent and policies of this Plan but face logistic challenges in meeting its numeric or text regulations.

The VHRT may include, but is not limited to, the following City staff and/or agency representatives:

- Planning Director or his/her designee
- Staff Planner assigned to proposed project;
- City Planning & Zoning;
- City Hydrologist;
- City Department of Municipal Development (DMD);
- City Open Space Division;
- City Parks and Recreation;
- City ONC;
- Albuquerque-Bernalillo Water Utility Authority (ABCWUA);
- Mid-Region Council of Governments (MRCOG);
- Public Service Company of New Mexico (PNM);
- New Mexico Gas Company;
- Telecommunications Companies;
- Adjacent Property owners;
- Potential tenants;
- Neighborhood Association representatives; and/or
- Merchants’ Associations representatives.

3.2.7. **Development Review Board (DRB) Approval**
 The DRB shall be responsible for the following:

- (i) Conducting Sketch Plan reviews weekly;
- (ii) Reviewing/approving Site Plans for Subdivisions that comply with this Plan, the DPM, and City ordinances;
- (iii) Reviewing/approving minor deviations from any dimensional standard in a Site Development Plan for Subdivision per **Section 3.2.10** of this Plan;
- (iv) Reviewing/approving Site Development Plans for Building Permit that involve 4 or more feet of fill; and

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- (v) Reviewing/approving Site Development Plans for Building Permit that involve construction within the Impact Area as defined by the NWMEP.

3.2.8. Environmental Planning Commission (EPC)

Approval: The EPC shall be responsible for the following:

- (i) Major deviations from the standards beyond Minor Deviations as defined in **Table 3.2** below require review and approval by the Environmental Planning Commission (EPC), which includes more opportunity for public review and comment. Major Deviation thresholds shall be allowed per **Table 3.3**. The EPC shall not approve projects that fail to meet the intent of this Plan.
- (ii) Reviewing/approving requests for Exceptions to the regulations of this Plan that still meet the Plan's intent, per **Section 3.2.13** in this Plan.

3.2.9. City Council Appeal and Approval

- (i) The City Council shall be the final appeal body for proposed projects, per City Zoning Code Section 14-16-4-3.
- (ii) The City Council shall be the body to approve SAD, TIDD, and/or PID applications. See **Section 3.2.3.ii** in this Plan.

3.2.10. **Minor Deviations:** As defined in **Table 3.2**, the DRB shall have the authority to approve minor deviations from Site Development Plans for Subdivision and the Planning Director or his/her designee shall have the authority to approve Minor Deviations to Site Development Plans for Building Permit that:

- (i) Meet the intent of this Plan;
- (ii) Help to preserve a rock outcrop or other cultural or natural feature deemed significant by the City Open Space Division;
- (iii) Do not materially change the circulation and building location on the site;
- (iv) Do not increase the building area permitted;
- (v) Do not change the relationship between the buildings and the street;
- (vi) Do not allow greater height of any building without a commensurate benefit to the natural environment and built environment;
- (vii) Do not eliminate regulations intended as protections for single-family development existing at the time of this Plan's adoption; or
- (viii) Do not change any required element of the Zoning Map or Mandatory Streets Map beyond the thresholds established in **Table 3.2** below.

Any appeals shall be heard by the City Council, per City Zoning Code Section 14-16-4-4.

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3.2.11. **Major Deviations:** The EPC shall have the authority to approve major deviations, as defined in **Table 3.3**, that:

- (i) Meet the intent of this Plan;
- (ii) Help to preserve a rock outcrop or other cultural or natural feature deemed significant by the City Open Space Division;
- (iii) Do not materially change the circulation and building location on the site;
- (iv) Do not increase the building area permitted;
- (v) Do not change the relationship between the buildings and the street;
- (vi) Do not allow greater height of any building without a commensurate benefit to the natural environment and built environment;
- (vii) Do not eliminate regulations intended as protections for single-family development existing at the time of this Plan's adoption; or
- (viii) Change any required element of the Zoning Map, Mandatory Streets Map, or regulatory standard within the thresholds established by **Tables 3.2 or 3.3** below.

Any appeals shall be heard by the City Council, per City Zoning Code Section 14-16-4-4.

3.2.12. **Non-compliance:** This Plan distinguishes projects that meet the intent of this Plan, as determined by the Planning Director or his/her designee from those that do not.

- (i) Projects that meet the intent of this Plan but require deviations beyond those considered Major per **Table 3.3** in this Plan shall follow one of the relevant processes below:
 - a. Request and justify a variance because of unique parcel conditions through the process outlined in City Zoning Code 14-16-4-2;
 - b. Request and justify a zone change to SU-2 for SU-1 per City Zoning Code 14-16-2-22 to accommodate a compatible use not listed as permissive or conditional in Table of this Plan; OR
 - c. Request and justify an Exception to a design standard (e.g. Site Development or Building Design Standards) per **Section 3.2.13** in this Plan.
- (ii) Projects that do not meet the intent of this Plan shall require either modification to the project or amendment of this Plan, per **Section 3.3** of this Plan and City Zoning Code Section 14-16-4-3.

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3.2.13. Exceptions from Design and/or Subdivision Ordinances' Development Standards

- (i) As long as a proposed project meets the intent of this Plan, the EPC shall be authorized to approve Site Development Plan submittals with appropriate exceptions to adopted standards and regulations, provided a project meets at least one of the following conditions, as justified by the Applicant:
 - a. The project includes elements of exceptional civic, architectural, or environmental design. These design elements should provide a benefit to the City, adjacent property owners, and/or the built or natural environment that justifies the requested exception.
 - b. The exception will help to preserve and/or enhance significant views, rock outcroppings, or other natural features.
 - c. The exception will help to coordinate and/or enhance development or improvements on adjacent properties.
 - d. The exception is needed to accommodate a Major Employer. [Confirm language]
- (ii) In order to justify requests for exceptions, the applicant shall submit sufficient design sketches, photographs, and other detailed information as may be necessary to demonstrate the case and solution requested for an exception to the adopted regulations.

- (iii) Site Development Plans may require additional analysis and/or documentation, such as, but not limited to: site elevations, site sections, and building heights showing impacts to views and/or relationship to nearby sensitive lands.

3.2.14. **Project Approval:** The Planning Director, DRB, or EPC shall approve Site Development Plans for public and private projects that meet the intent of the Volcano Heights Sector Development Plan. This approval is required as a condition for the following:

- (i) Acceptance by the Planning Department of an application for a grading permit;
- (ii) Acceptance by the Planning Department of an application for a building permit for construction of a new structure;
- (iii) Acceptance by the Planning Department of an application for a building permit for expansion of an existing structure;
- (iv) Acceptance by the Planning Department of an application for a building permit for exterior renovation, remodeling, or other alteration of existing structure;
- (v) Construction of a surface parking lot; and
- (vi) Acceptance by the Planning Department of an application for a sign permit.

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3.2.15. **Review of Phased Projects:** To minimize review steps for phased projects, the DRB may approve a Site Development Plan for Subdivision for all phases provided that the applicant submits drawings that depict the initial phase as well as all future phases. The Site Development Plan is valid for seven years from the date of the approval and may be amended administratively per City Zoning Code Section 14-16-2-22.

3.3. Amendments to the Sector Development Plan and/or Zoning Map: Amendments and/or changes to the Zoning Map, Mandatory Street Map, and property boundaries shall be considered by the EPC for recommendation of approval to the City Council for sites 10 acres or less. For sites 10 acres or greater and for all text amendments, the Land Use Hearing Officer shall consider such changes and recommend approval or denial for final action by the City Council in accordance with the procedure set out in Section 14-16-4-3 of the City Zoning Code.

3.4. Public Notification

3.4.1. Notification for Site Plans for Building Permit shall be required as per **Table 3.4.**

3.4.2. Notification for Site Plans for Subdivision shall be required as per **Table 3.5.**

3.4.3. **Applicant Responsibilities:** When required (as noted with "NA" in Table 3.4 or Table 3.5), the Applicant shall give written Notification of the Application for Site Development Plan to any recognized Neighborhood Association, non-recognized neighborhood organization, and/or homeowner association which covers, abuts, or is across the public right-of-way from the project site.

(i) The Office of Neighborhood Coordination (ONC) shall provide the Applicant with a complete list of the names and addresses for all people who require Notification, including any interested parties in the area who have registered with ONC to receive notice. The Applicant shall not be held responsible for notifying people who are not on the ONC list.

(ii) Notification shall include a reduced version of the proposed Site Development Plan, including the Landscape Plan; the date, time, and location of the DRB or EPC hearing; contact information for the Applicant; and the name, e-mail, and phone number of the Staff Planner.

(iii) Notification shall be sent by certified mail, return receipt requested, and postmarked at least ten (10) days in advance of the hearing.

(iv) The Applicant shall give copies of the ONC Notification list and proof of mailings to the Approval Body for the hearing record. Failure by the Applicant to show proof of Notification shall be grounds for a deferral of the Applicant's proposal at the Hearing until proper proof of Notification is provided by the Applicant.

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3.4.4. **Planning Director Responsibilities:** When required (as noted with “PO” in Table 3.4 or Table 3.5), the Planning Director or his/her designee shall provide mailed Notification to property owners within two-hundred feet (200) of the boundaries of the proposed project site, excluding public rights-of-way.

Notification shall be postmarked ten (10) days prior to the hearing and shall include reduced copies of the proposed Site Development Plan, the Landscape Plan, and contact information for the Planning Department and the Applicant.

TABLE 3.2 – MINOR DEVIATION CRITERIA

Standard	Minor Deviation Allowed	Criteria
Location/geometry of Mandatory Streets	10% deviation from the Centerline of the street. In the case of avoiding natural and/or culturally significant features, a greater allowance is permitted on a case by case basis and may require a signed agreement with affected adjacent owners.	<ul style="list-style-type: none"> • Shall not introduce a curve beyond what an automobile can navigate safely as defined in the City’s Development Process Manual (DPM) Section X. [ref] • Any change to the location of a Mandatory Street shall be reviewed by the Planning Director or his/her designee for approval. • Any changes proposed to avoid rock outcroppings or other natural and/or culturally significant features shall be coordinated with City Open Space Division.
Mandatory Street Cross Sections	Any dimensional standard change (increase or decrease) up to 10%.	<ul style="list-style-type: none"> • Shall not eliminate any element from a cross section. • Shall not alter the total right-of-way (ROW) of the paved portion of the cross section. • Shall not decrease travel lanes below 10 feet or increase travel lanes to more than 12 feet. • Shall require justification by the applicant. • May include criteria for variances per City Zoning Code 14-16-4-2(C).
	Adding medians to an ‘A’ Street.	<ul style="list-style-type: none"> • When incorporating a median, the median shall provide enough width to allow vegetation to be planted and sustain itself within the entire median, including turn bays.
	Adding or removing reverse-angle parking to/from an ‘A’ Street	<ul style="list-style-type: none"> • On-Street parking may be parallel or reversed angle parking. Where reversed angled parking is used, the ROW shall be increased to add that element while still maintaining appropriate dimensions for all other elements. A median is recommended with reverse-angle parking.
Non-Mandatory Street Cross Section	Any changes in arrangement of the streets cross section shall be reviewed by the Planning Director or his/her designee to ensure compatibility with the intent of this Plan.	<ul style="list-style-type: none"> • Reference to ITE Manual for Complete Streets and Context-sensitive Solutions and/or Context Sensitive Design is encouraged.

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TABLE 3.2 – MINOR DEVIATION CRITERIA (Cont'd)

Standard	Minor Deviation Allowed	Criteria
Site Development Standards		
Build-to-zones/setbacks	No more than a 20% change in the maximum or minimum setback. In the case of avoiding natural and/or culturally significant features, a greater allowance is permitted on a case-by-case basis.	<ul style="list-style-type: none"> Changes to the build-to zones and setbacks may only be due to any changes to the street cross sections, changes in the width of a sidewalk, or changes to avoid natural and/or culturally significant features. In no case shall the sidewalk be less than 6 feet in width.
Building Frontage	No more than a 15% reduction in the required building frontage along each block of an 'A' Street.	<ul style="list-style-type: none"> Any reduction in the required building frontage shall be to accommodate porte-cocheres for drop-off and pick-up.
Street screen	Waiver of street screen requirement along a 'B' Street.	<ul style="list-style-type: none"> Requirement for a street screen may only be waived along a 'B' Street frontage of any interim surface parking lot (off-street) that is intended to be in-filled with a parking structure within 2 years. Applicant may be required to show financial records or sketch plans indicating intent and/or planning. In no case shall any portion of the surface parking have frontage along an 'A' Street without a required street screen. In no case shall the (off-street) surface parking lot be located at a street intersection for a minimum depth of 20' along each street (regardless of Street Type).
Non-dimensional standard	Any non-dimensional standard deemed minor by the Planning Director or his/her designee.	<ul style="list-style-type: none"> Any change shall be based on specific development context that poses a challenge to meet the standards OR a demonstration that the proposed change improves the project's ability to meet the Plan's intent(s).
Building Design Standards	Any dimensional standard change (increase or decrease) up to 10%.	<ul style="list-style-type: none"> Any change shall be based on specific development context that poses a challenge to meet the standards OR a demonstration that the proposed change improves the project's ability to meet the Plan's intent(s).
	Any non-dimensional standard deemed minor by the Planning Director or his/her designee.	<ul style="list-style-type: none"> Any change shall be based on specific development context that poses a challenge to meet the standards OR a demonstration that the proposed change improves the project's ability to meet the Plan's intent(s).
Streetscape Standards	Street tree planting, street lighting, and other streetscape standards may be adjusted based on the development context and street cross section.	<ul style="list-style-type: none"> Any changes to the streetscape standards shall be based on specific development context such as vegetation, natural features, drainage, and fire access and is subject to approval by the Planning Director or his/her designee and/or relevant City department.



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TABLE 3.3 – MAJOR DEVIATION CRITERIA

<i>Standard</i>	<i>Major Deviation Allowed</i>	<i>Criteria</i>
Site Development Standards		
Build-to-zones/setbacks	A change in the maximum or minimum setback between 20-50%. In the case of avoiding natural and/or culturally significant features, a greater allowance is permitted on a case-by-case basis.	<ul style="list-style-type: none"> • Changes to the build to zones and setbacks may only be due to any changes to the street cross sections or changes in the width of a sidewalk. • In no case shall the sidewalk be less than 6 feet in width.
Building Frontage	A reduction in the required building frontage along each block of an 'A' Street between 15-30%.	<ul style="list-style-type: none"> • Any reduction in the required building frontage shall be to accommodate porte-cocheres for drop-off and pick-up.
Building Design Standards	Any dimensional standard change (increase or decrease) between 10-40%.	<ul style="list-style-type: none"> • Any change shall be based on specific development context that poses a challenge to meet the standards OR a demonstration that the proposed change improves the project's ability to meet the Plan's intent(s).
	Any non-dimensional standard deemed major by the Planning Director or his/her designee.	<ul style="list-style-type: none"> • Any change shall be based on specific development context that poses a challenge to meet the standards OR a demonstration that the proposed change improves the project's ability to meet the Plan's intent(s).



TABLE 3.4 – APPROVAL PROCESS AND NOTIFICATIONS: SITE PLANS FOR SUBDIVISION

Zones	Proposed Project	Compliance	Approval Process	Notification			
Transition Zones	Any Project < 5 acres	Fully Compliant	DRB			NR	
		Minor Deviation(s)	DRB			NR	
		Major Deviation(s)		EPC	NA	PO	
	Any Project 5+ acres	Fully Compliant	DRB		NA	PO	
		Minor Deviation(s)	DRB		NA	PO	
		Major Deviation(s)		EPC	NA	PO	
Center Zones + VHMx	Any Project < 5 acres	Fully Compliant	DRB			NR	
		Minor Deviation(s)	DRB			NR	
		Major Deviation(s)		EPC	NA	PO	
	Any Project 5+ Acres	Fully Compliant	DRB				NR
		Minor Deviation(s)	DRB				NR
		Major Deviation(s)		EPC	NA	PO	

Approval Process Abbreviations

DRB = Development Review Board

EPC = Environmental Planning Commission

Notification Abbreviations

NA = Neighborhood Associations *(Includes relevant Merchants Associations registered with the Office of Neighborhood Coordination. Notice shall be the responsibility of the Applicant.)*

PO = Property Owners within 200 feet *(Notice shall be the responsibility of the Planning Director or his/her designee.)*

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TABLE 3.5 – APPROVAL PROCESS AND NOTIFICATIONS: SITE PLANS FOR BUILDING PERMIT

Zones	Proposed Project	Compliance	Approval Process			Notification		
Transition Zones	Residential Project < 5 acres	Fully Compliant	AA					NR
		Minor Deviation(s)	AA	VHRT				NR
		Major Deviation(s)			EPC	NA	PO	
	Residential Project 5+ acres OR Project with Non-residential Component (any size)	Fully Compliant	AA	VHRT		NA	PO	
		Minor Deviation(s)	AA	VHRT		NA	PO	
		Major Deviation(s)		VHRT	EPC	NA	PO	
Center Zones + VHMX	Any Project < 5 acres	Fully Compliant	AA	VHRT				NR
		Minor Deviation(s)	AA	VHRT				NR
		Major Deviation(s)		VHRT	EPC	NA	PO	
	Any Project 5+ Acres	Fully Compliant	AA	VHRT				NR
		Minor Deviation(s)	AA	VHRT				NR
		Major Deviation(s)		VHRT	EPC	NA	PO	

Approval Process Abbreviations

- AA = Administrative Approval
- VHRT = Volcano Heights Review Team
- EPC = Environmental Planning Commission

Notification Abbreviations

- NA = Neighborhood Associations *(Includes relevant Merchants Associations registered with the Office of Neighborhood Coordination. Notice shall be the responsibility of the Applicant.)*
- PO = Property Owners within 200 feet *(Notice shall be the responsibility of the Planning Director or his/her designee.)*

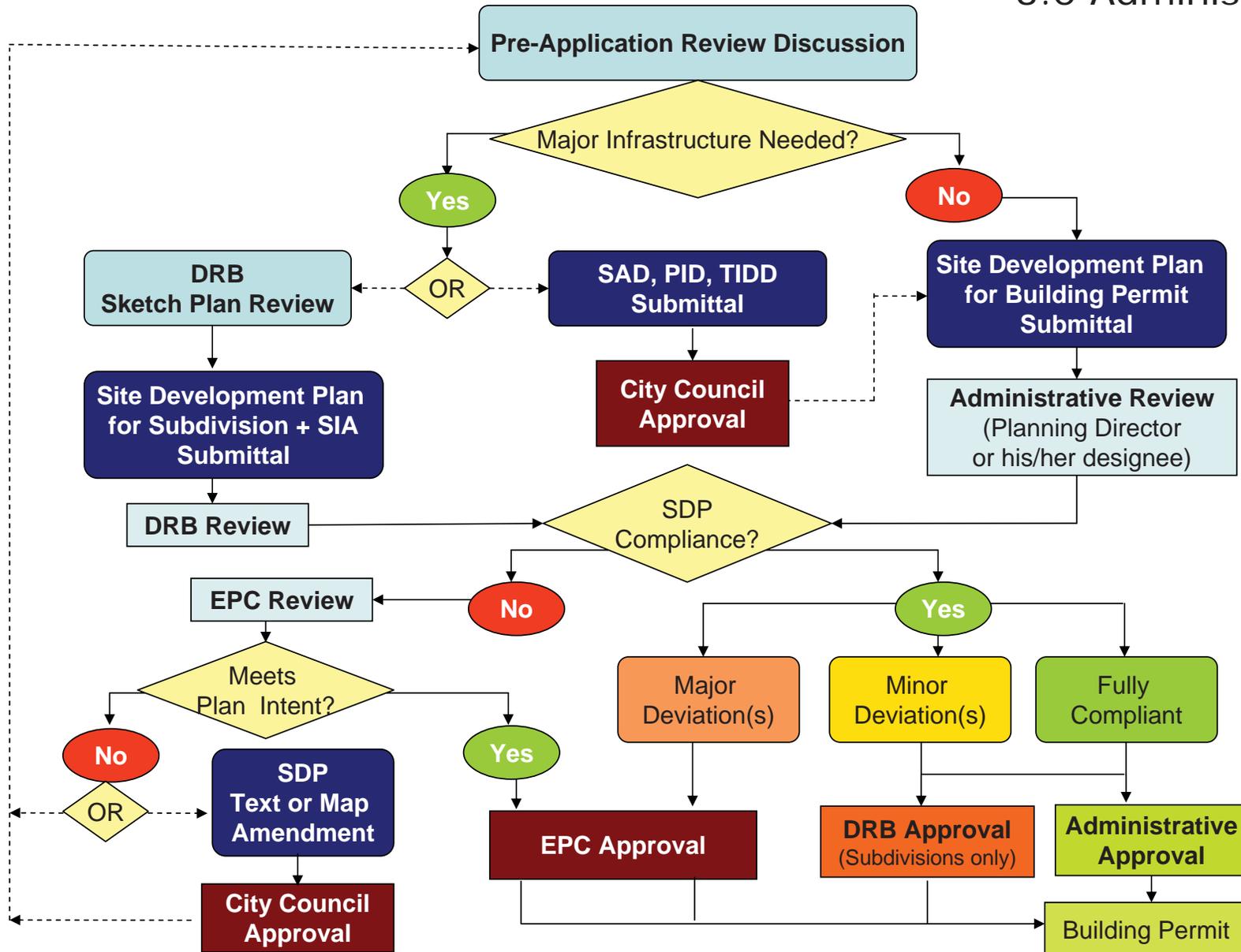


Exhibit 3.1 – Volcano Heights Approval Process Diagram

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TABLE 3.6 - APPROVAL PROCESS BY DEVELOPMENT TYPE

Type of Request	Administrative Review (PRD)	Administrative Approval (AA)	DRB Decision	EPC Decision	City Council Decision	Building Permit (Front Counter)	Applicable Sections/Criteria
1. Ordinary building maintenance (includes changes to the exterior and/or interior)						X	<ul style="list-style-type: none"> Interior changes may require building, electrical, and/or other city permits Exterior changes affecting more than 120 SF require a building permit.
2. Renovation or alteration of a building interior with no exterior façade changes						X	<ul style="list-style-type: none"> Interior changes may require building, electrical, and/or other city permits.
3. Renovation, alteration of, or addition to an existing building	X	X				X	<ul style="list-style-type: none"> Any decision on Major Deviations from dimensional elements limited to building height and building setbacks shall be made by the EPC.
4. Demolition of a building	X					X	<ul style="list-style-type: none"> Requires demolition permit from the City.
5. New construction with < 4 ft. fill	X		Minor Deviat.	Major Deviat.		X	
6. New construction with 4+ ft. fill	X		X			X	<ul style="list-style-type: none"> Requires sign-off from City hydrologist.
7. Construction of a parking garage	X	Minor Deviat.		Major Deviat.		X	<ul style="list-style-type: none"> Building Design Standards shall apply.
8. Construction of a new surface parking lot or expansion of an existing surface parking lot	X		X			X	
9. Construction of a Major Civic Venue	X		Minor Deviat.	Major Deviat.		X	
10. Change of use within an existing building or structure (with no exterior façade changes)	X	X				X	

Notes:

1. Appeals per City Zoning Code Section 14-16-4-4
2. Deviat. = Deviation [See **Table 3.2** and **Table 3.3**]

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TABLE 3.6 – APPROVAL PROCESS BY DEVELOPMENT TYPE (Cont'd)

Type of Request	Administrative Review (PRD)	Administrative Approval (AA)	DRB Decision	EPC Decision	City Council Decision	Building Permit (Front Counter)	Applicable Sections/Criteria
11. New sign or alteration of an existing sign	X	Minor Deviat.				X	<ul style="list-style-type: none"> Sign permits shall be required for all new and altered signs within any character zone. Signs may be approved administratively for all signs that conform to Sign Standards.
12. Master Sign Plan approval	X		X	Major Deviat.			<ul style="list-style-type: none"> See #11 above.
13. Character zone boundary change request	X			Recomm.	X		<ul style="list-style-type: none"> All requests to change the underlying zoning boundaries shall be processed as an SDP Zoning Map Amendment per City Zoning Code Section 14-16-4-3. Zone changes shall not eliminate any Character Zone from the Plan area or change the overall boundary of the Plan area.
14. Request to change zoning standards or design standards	X			Recomm.	X		<ul style="list-style-type: none"> All requests to change the underlying zoning and/or design regulations shall be processed as an SDP amendment to zoning per City Zoning Code Section 14-16-4-3.

Notes:

1. Appeals per City Zoning Code Section 14-16-4-4
2. Deviat. = Deviation [See **Table 3.2** and **Table 3.3**]
3. Recomm. = Recommendation to the City Council



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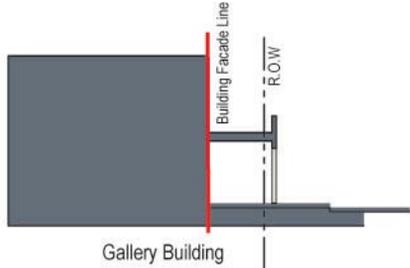
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3.5. Definitions

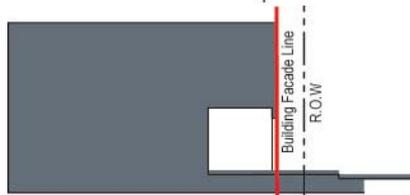
In addition to Definitions in the City Zoning Code Section 14-16-1-5, the following terms shall have the corresponding interpretations.



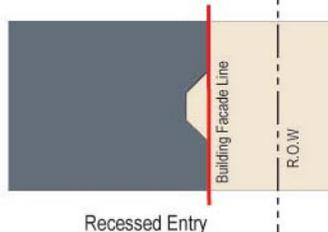
Mezzanine on the West Side



Gallery Building



Arcade Building



Recessed Entry

Sample Building Façade Lines

Abutting: The condition under which two properties touch, without separation from a public right-of-way.

Adjacent: The condition under which two properties are next to each other, whether abutting or separated from a public right-of-way.

Alley: A thoroughfare designated by the City as public right-of-way, which affords secondary means of access to an abutting property.

Appeal Process: The process by which a declaratory ruling made by the DRB or EPC or Administrative Denial may be challenged.

Applicant: An applicant is a person applying for Site Development Plan Approval. The person may be the property owner or owner's representative.

Approval: An action taken by the Planning Director (or his/her designee) issuing an Administrative Approval as noted in a Notice of Decision (NOD) or by the DRB signing and dating a Sector Development Plan as Approved.

Articulation: A means of breaking up large expanses of blank wall both horizontally and vertically by adding changes of relief (i.e. how far a portion of the façade projects from or is recessed into the main façade surface), alternating building materials, and/or the placement of windows, portals, and other external features.

Attics / Mezzanines: The interior part of a building contained within a pitched roof structure or a partial story between two main stories of a building.

Awning: A decorative feature extending from the exterior of a Building that may serve as a shelter from the sun, rain, or wind.

Auto-Related Sales and Service Uses: Establishments that provide retail sales and services related to automobiles including, but not limited to, cars, tires, batteries, gasoline, etc.

Block Size: Block size is measured between centerlines of the vehicular streets that frame the block.

Bikeway: Any bicycle lane, bicycle route, and/or bicycle trail.

Build-to Zone (BTZ): The area within which the principal building's front façade is to be built. The BTZ is measured from the required minimum setback. Where appropriate, parking is allowed within the BTZ, given the required screening.

Building: [Pending]

Building Façade Line: The vertical plane along a lot where the building's front façade is actually located.

3.0 Administration



Building Frontage: The percentage of front façade located within the front Build-to Zone as a proportion of the block frontage along an 'A' or 'B' street. Parks, plazas, improved forecourts, and pedestrian breezeway frontages shall be considered as buildings for the calculation of building frontage.

Building Height: The height of a building measured from approved grade to to the eave line in the case of a pitched roof or to the roof line in the case of a parapet. See also "Structure Height."

Building Permit: An official certificate of Entitlement issued by the City to an Applicant in order to construct, enlarge, or alter a structure.

Bus Rapid Transit (BRT): A public transportation system with some dedicated infrastructure and additional resources that enables faster, more efficient service than an ordinary bus line. These systems approach the service quality of rail transit with the cost savings and flexibility of bus transit.

Caliper: The diameter of a tree trunk measured six inches above grade.

Character Zone: A zone within Volcano Heights that creates an urban form distinct from other zones in the Plan area. Character Zones are identified in the Zoning Map in **Section 5.1.**

Civic/Open Space: See Open Space, Civic.

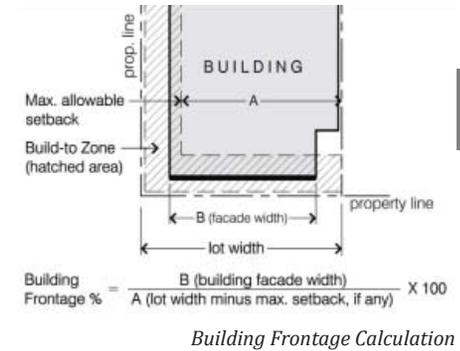
Clear Sight Triangle: An area of unobstructed vision at Street intersections between 3 and 8 feet above the gutter line and within a triangular area at the street corner, bounded by:

- The Street property lines of the corner lot and a line connecting points 25 feet distant from the intersection of the property lines of such a lot or
- The curb lines of an intersection and a line connecting points 35 feet distant from the corner of the intersection, such corner determined by projecting the curb lines out to a specific point, whichever is less. **[confirm Site Development Standards compatibility]**

Colonnade: A row of regularly spaced columns supporting a major horizontal element above.

Commercial Ready: A space constructed at a minimum ground floor height as established in each character zone that may be used for noncommercial uses and/or be converted into retail/commercial use. The space must comply with all building and construction codes for retail use in place at the time of site plan approval. Commercial ready spaces are intended to provide additional flexibility to use space according to market demand.

Commercial Surface Parking Lot: Typically for-fee parking as the primary use of the property. This does not include commercial garages or required off-street parking that supports an associated, primary commercial use located on the same property, such as store or office parking.



3.0 Administration



Action Buzz Neighborhood Garden



Sawmill Courtyard



Brickyard District Courtyard



Forecourt in East Downtown

Community Garden: A small- to medium-size garden cultivated by members of an area for small-scale agricultural uses for the benefit of the same people. It may consist of individually tended plots on a shared parcel or may be communal (everyone shares a single plot).

Comprehensive Plan: Rank I Albuquerque-Bernalillo County Comprehensive Plan that establishes the blueprint for the long-term growth and development of the metropolitan region.

Cornice: The uppermost section of the trim along the top of a wall or just below a roof.

Courtyard: A landscaped open space in the center of the block with no street frontage, surrounded by walls or buildings on all sides. It shall be large enough to allow for public activities and have sunlight during midday. It should be designed to connect to adjacent buildings or to the public sidewalk through a pedestrian passage and should incorporate water harvesting.

Denial: A refusal by any relevant approval body to approve a Site Development Plan because of non-compliance with the intent, requirements, regulations, and/or standards of a Sector Development Plan and/or applicable City codes.

Development Review Board (DRB): A 5-member board of City staff (designees for the Planning Director, Parks and Recreation, and ABCWUA; City Engineer; and Traffic Engineer) charged with administering the Subdivision Ordinance and granting Approval or Denial of Site Development Plans. [DRB Hearings are weekly.]

DRB Hearing: The quasi-judicial Hearing held for the purpose of reviewing Site Development Plans and taking public comment on proposed projects.

Encroachment: Any structural or non-structural element such as a sign, awning, canopy, terrace, or balcony, that breaks the plane of a vertical or horizontal regulatory limit, extending into a Setback, into the Public ROW, or above a height limit.

Exceptional Project: Any project incorporating elements of exceptional civic, architectural, or environmental design that benefit the City, adjacent properties, and/or the built or natural environments.

Façade: Any separate external face of a building, including parapet walls and recessed walls. Where separate faces are oriented in the same direction, or in directions within 45 degrees of one another, they are considered part of a single façade.

Full Service Grocery: A retail establishment that primarily sells food but may also sell other convenience and household goods, which occupies at least 5,000 square feet (SF) but not more than 20,000 SF of gross floor area.

Forecourt: Similar to a Courtyard but located in the front of a building such that the forecourt is surrounded on two or three sides by wings of the building. See also Court Parking.

Gallery: An extension of the main façade of the building at or near the front property line. The gallery may overlap the public sidewalk.

3.0 Administration



Living Fence on the West Side

Gross Square Footage: Gross developable area is exclusive of site constraints such as undevelopable land and parks but inclusive of streets and parking.

Institutional Uses Use related to non-profit organizations dedicated to religious, educational, healthcare, or social functions.

Intent: A clarifying statement that sets forth a broad desired outcome. A statement of intent does not require specific actions unless the mandatory word “shall” is specifically used.

Live-Work Unit: A dwelling unit that is also used for work purposes, such as professional office, artist’s workshop, or studio, located on the street level and constructed as separate condominium units or as a single unit. The ‘live’ component may be located on the street level (behind the work component) or any other level of the building. Live-work unit is distinguished from a home occupation otherwise defined by this Plan in that the work use is not required to be incidental to the dwelling unit, non-resident employees may be present on the premises, and customers may be served on site.

Living Fence: A landscaping treatment with vegetation dense and/or tall enough to screen abutting uses, particularly parking areas.

Lot: A separate tract or parcel of land platted and placed in the County Clerk’s record in accordance with laws and ordinances.

Low Impact Design (LID): An approach to managing rainwater runoff that emphasizes conservation and use of on-site natural features to protect water quality. Using engineered small-scale hydrologic controls, LID works to replicate the pre-development condition by infiltrating, filtering, storing, evaporating, and detaining runoff close to its source. Frequently used LID techniques include bioretention cells (rain gardens), cisterns, green roofs, pervious paving, bioswales, and commercial filter systems.

Major Employer: A company with 200+ employees in a particular location or campus.

Mandatory Streets: A minimal network of streets needed to support future development within Volcano Heights. Mandatory Streets are those mapped in **Exhibit 4.1**. Street locations have some flexibility per **Table 3.2** and **Table 3.3** but shall retain the general grid pattern and a minimum level of connectivity to serve pedestrians and disperse auto traffic. Mandatory Streets include required cross sections and frontage standards per **Section 4.6**.

Merchants Association: [Pending]

Neighborhood Association: An association recognized by the City of Albuquerque Office of Neighborhood Coordination (ONC). Membership in a Neighborhood Association is defined by each individual Neighborhood Association’s by-laws. Merchants’ Associations registered with the ONC are included in notification requirements for the purposes of this Plan.

3.0 Administration

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Net Square Footage (NSF):

Buildable land left after subtracting undevelopable land from the total property (i.e. gross square footage). Undevelopable land shall be defined as land that is not suitable for cut or fill and includes, but is not limited to, significant Rock Outcroppings as defined by this Plan.

Open Space, Civic:

Publicly accessible areas such as plazas, paseos, and streetscapes within the Build-to Zone along public rights-of-way. These spaces may count as either Usable or Detached Open Space. For all residential and mixed uses, privately accessible open spaces such as courtyards, porches, and balconies may count as open space. [confirm language]

Notice of Decision (NOD):

The official document issued by the Planning Director or his/her designee, the DRB, or the EPC approving Site Development Plans.

Open Space, Detached:

Outdoor space required by zone to balance development densities in a developing area. Detached Open Space shall be provided via dedication, on-site, or cash-in-lieu per City Zoning Code 14-16-3-8. See requirements in **Section 10.3**. Management responsibilities for dedicated lands shall be determined as part of the developer’s dedication agreement. Detached Open Space requirements are in addition to, and calculated and administered separately, from Usable Open Space or Impact Fees. Private open space amenities shall be the responsibility of property owners/ developers for the cost of construction, and ongoing maintenance and liability shall also be privately funded, such as by a property owner or merchant association. To be dedicated to the City for ongoing maintenance and liability responsibility, they must meet City standards and be acceptable to the relevant City department.

Notification:

Notification is the requirement of an Applicant to notify, by certified/ return receipt mail, two duly authorized representatives of any Neighborhood Association or other association registered with the Office of Neighborhood Coordination within, contiguous to, or across public right-of-way from the area covered by the Site Development Plan. See **Table 3.4** and **Table 3.5** for projects requiring the Planning Director or his/her designee to notify property owners within 200 feet of the project boundary.

Open Space:

A generic term for any outdoor space or amenity, regardless of location, ownership, or management responsibility—e.g., landscaped medians, buffers, paseos, setbacks, courtyards, community gardens, or balconies. Open space is required through various means in order to provide a psychological and physical respite from development densities. Healthy places balance density vs. openness, urban vs. natural environments.

3.0 Administration

Open Space, Major Public (MPOS): Publicly-owned spaces managed by City Open Space Division. These are typically greater than five acres and may include natural resources, preserves, recreational facilities, dedicated lands, or trail corridors.

Open Space, Usable: Outdoor space to be preserved on-site to help ensure livable conditions on each site. See Section 10.4. Management responsibilities to be determined as part of the developer’s dedication agreement. On-site open space in non-residential and mixed use developments shall be accessible to the public, with the exception of balconies, porches, and courtyards.

Open Space Network: The totality of Major Public Open Space managed by the City Open Space Division, comprising native or minimally-developed areas such as public rights-of-way, trail corridors, and environmentally sensitive lands that are preferably (but not necessarily) visually or physically linked.

Open Space, Private: Any open space owned, managed, and maintained privately. Private open space may be accessible to the public via public access easement. Private open space may count as Usable or Detached Open Space, depending on whether its provided on the same site as the proposed project.

Parapet: A low wall that serves as a vertical barrier at the edge of a roof, terrace, or other raised area; in an exterior wall, the part entirely above the roof.

Park: A civic/open space available for active or passive recreation and accessible to the public.

Parking Court: A small parking area surrounded by three wings of a building, served by one-way access from an ‘A’ Street, with angled parking spaces screened by vegetation or living fence.

Paseo: A civic space dedicated to pedestrian movement located between blocks, buildings or along alleys. Paseos may terminate public streets within the Plan area.

Patio: An outdoor space for dining or recreation that adjoins a residence or non-residential use and is often paved. It may also be a roofless inner courtyard within a residence or non-residential use.

Permeable Pavement: Hardscape material used as a means of allowing water and air to penetrate to underlying soil or gravel reservoir for storm water control, reduction in heat island, and the health of planted materials, e.g. pave stones, matrix materials, permeable asphalt, or permeable concrete.

Planning Director: The Director of the Planning Department or his/her designee.

Playgrounds: Civic/open spaces designed and equipped for children’s recreation. These playgrounds should serve as quiet, safe places – protected from the street and typically located where children do not have to cross major streets to access. Playgrounds may be fenced. An open shelter, play structures, interactive art and/or fountains may be included. A larger playground may be incorporated into a park, whereas a more intimate playground may be incorporated into a recreation area.



Parking Court in Nob Hill [replace]



Paseo in Downtown



Patio on the West Side



Playground on the West Side

3.0 Administration



Portal on the West Side



Rock Outcropping in Volcano Heights



Roof Terrace in East Downtown

Plaza: A primarily hardscaped, civic/open space with formal landscaping, available for civic purposes and commercial activities. A plaza shall be defined spatially by buildings but may have street frontage on up to two sides.

Pilaster: A pier or pillar attached to the wall or portion of the wall that projects slightly to resemble a column, often with capital and base.

Portal: A portion of the ground-level, main façade of the building that is recessed, with a colonnade supporting the upper floors of the building. Portals are intended to provide weather protection for pedestrians to access buildings with ground-floor commercial or retail uses.

Porte Cocheres: A roofed structure covering a driveway at the entrance of a building to provide shelter while entering or leaving a vehicle.

Private Open Space Amenity: Could include a pool (swimming, lap pool, spa area), play courts (basket ball), or picnic areas with shade structures.

Property Owners Association: [Pending]

Public Improvement District (PID): A method of funding subdivision improvements, such as roads, public buildings, drainage infrastructure and recreational facilities through special assessments added to property taxes.

Recreation Area: A civic/open space intended for passive or active recreation, spatially defined by landscaping rather than building frontages.

Retail Sales: Retail establishments are the final step in the distribution of merchandise. They are organized to sell items in small quantities to many customers. Establishments in stores operate as fixed point-of-sale locations, which are designed to attract walk-in customers. Retail establishments often have displays of merchandise and sell to the general public for personal or household consumption, though they may also serve businesses and institutions. Some establishments may further provide after-sales services, such as repair and installation. Included in, but not limited to this category, are durable consumer goods sales and service, consumer goods, other grocery, food, specialty food, beverage, dairy, etc., and health and personal services.

Right-of-Way (ROW): The area of land acquired by the city, county, or state primarily for the use of the public for the movement of people, goods, and vehicles.

Rock Outcropping: Bedrock or other stratum a minimum of 6 feet high on its steepest side as measured from the adjacent 10% slope line and in excess of 500 SF in surface area.

Roof Garden: [Pending]

Roof Terraces: Flat areas on top of a building that are accessible for use as a recreation space for the residents and users of the building.

3.0 Administration

Sensitive Lands: Areas with environmental concerns that warrant special consideration for nearby development. Sensitive lands include, but are not limited to, the volcanic Escarpment; the Petroglyph National Monument; Major Public Open Space; or archaeologically, culturally, and/or geologically significant areas.

Service Uses: A category for limited personal service establishments that offer a range of personal services including (but not limited to) clothing alterations, shoe repair, dry cleaners, laundry, health and beauty spas, tanning and nail salons, hair care, etc.

Setback: The required minimum distance between the property line and the building façade or structure.

Secondary Accessory Dwelling Unit: A minor second dwelling unit up to 650 SF associated with a single-family detached dwelling unit permitted anywhere on the lot except the front yard.

Senior Housing Facility: [language pending]

Sign, Building Blade: A pedestrian-oriented sign affixed perpendicular to the corner of a building or along the front façade of a building above the ground floor to provide identification for the whole building.

Sign, Freestanding: Includes both permanent and temporary signs placed within a building's front yard. Freestanding signs may be Pole or Monument Signs.

Sign, Marquee: A sign structure placed over the entrance to a theatre or other public gathering venue. It has signage stating either the name of the establishment or, in the case of theatres or other public venues, the name of the event, artist, and other details of the event appearing at that venue. The marquee is often identifiable by a surrounding cache of light bulbs, usually yellow or white, that flash intermittently or in sequence as chasing lights. Marquee signs may often be combined with Building Blade signs.

Sign, Monument: Any freestanding sign connected to the ground with no clear space for the full width of the sign between the bottom of the sign and the surface of the ground. A monument sign may include a sign face and sign structure and may also include a sign base and sign cap.

Sign, Sandwich Board: A portable sign consisting of two panels of equal size, which are hinged at the top or one panel with a support and placed on the ground or pavement so as to be self-supporting.

Sign, Tenant Blade: A pedestrian-oriented sign smaller than a Building Blade Sign, affixed perpendicular to the building façade under a canopy or awning or immediately over a tenant space to provide identification for individual tenants within a building.



Blade Sign in Downtown



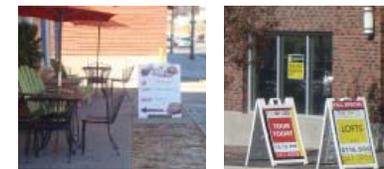
Monument Sign on the West Side



Blade Sign and Marquee in Downtown



Tenant Blade Signs in East Downtown



Sandwich Board Signs in East Downtown

CHAPTER II: REGULATIONS

3.0 Administration



Single-loaded Roads Next to Open Space in Albuquerque



Sprayground Features in Albuquerque

Signage Plan: A plan submitted along with a Site Development Plan indicating the dimensions, location, colors, lighting, motion, and materials of all proposed signage. Elevation drawings of all signs shall be included on the Signage Plan.

Significant Infrastructure: Determined on a case-by-case basis but generally including a major street, drainage or utility facility, etc. necessary to develop the subject property as well as other nearby properties.

Single-loaded Road: A road with development only on one side. In this Plan, a single-loaded road is the recommended transition from the Petroglyph Monument Boundary to development in the Escarpment Transition Zone.

Site Development Plan: A shortened phrase referring to a Site Development Plan for Building Permit. [Site Development Plan cannot be used interchangeably with “Site Development Plan for Subdivision.”] Where used as a plural in this Plan (i.e. “Site Development Plans”) without a qualifier (i.e. “for Building Permit” or “for Subdivision”), it refers to both Site Development Plans for Building Permit and Site Development Plans for Subdivision.

Site Development Plan for Building Permit: As per City Zoning Code Section 14-16-1-5, an accurate Plan at a scale of at least 1 inch to 100 feet, which covers a specific site. Submittal requirements vary based on the the size of the development, proposed uses, and existing conditions surrounding the site. Typically, Site Development Plans for Building Permit identify land uses, exact structure locations, structure elevations and dimensions, parking, loading facilities, any energy conservation features of the Site Plan and the proposed schedule for development including any phases. A Site Development Plan for Building Permit may also require a Landscape Plan and a Signage Plan.

Site Development Plan for Subdivision: As per City Zoning Code Section 14-16-1-5, a Site Development Plan for Subdivision includes information used to plat a lot for future development.

Site Development Standards: Shall be established for each Character Zone to specify the height, bulk, orientation, and arrangement of elements for all new construction and redevelopment.

Soffit: The exposed underside of any architectural element, especially a roof, or the underside of a structural component such as a beam, arch, or recessed area.

Sprayground: A fully automated water feature for people of all ages to play. Spraygrounds may be private or public.

3.0 Administration



Stepback: An indentation on a Façade abutting the right-of-way intended to eliminate the “canyon effect” of buildings along a roadway. A stepback is the upper portion of a Building that is offset in comparison to the lower portion of the Building.

Storage: A space or place where goods, materials, or personal property is placed and kept for more than 24 consecutive hours.

Street Designations: The Street Designations Map in **Section 4.5** shows the character zones and street type designations applicable to the Volcano Heights Plan area subject to the standards in this Plan.

Street, Neighborhood: A street that is primarily for access to abutting properties and carries relatively low traffic volumes.

Streetlights, Cobrahead: A typical streetlight installation with a light fixture resembling a cobra head mounted on a distribution pole high enough to light a roadway. Cobrahead lights are typically used to serve auto-oriented streets, as the tall distribution poles are out of scale with the pedestrian realm.

Streetlights, Column: A typical streetlight installation for retail areas. The light fixture is mounted on a distribution pole at a height at the pedestrian scale to serve the pedestrian as well as the auto realm.

Street Screen: A freestanding wall or living fence built along the frontage line or in line with the building façade along the street. It may mask a parking lot or a loading/service area from view or provide privacy to a side yard and/or strengthen the spatial definition of the public realm.

Structure: Anything constructed or erected above ground-level that requires location on the ground but not including a tent, vehicle, vegetation, public utility pole or line, or attached to something having a location on the ground. [A Building is a Structure but a Structure is not necessarily a Building.]

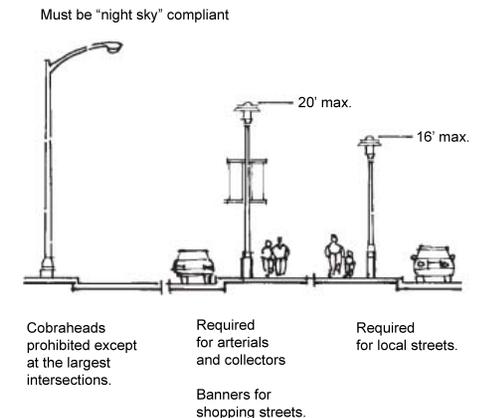
Structure Height: [Pending]

Tax Increment Development District (TIDD): The geographic area where a Tax Increment (i.e. a piece or portion of future gains of taxes used to finance current improvements) is generated and located.

Trail: [Language pending]

Transition Zones: Areas intended to buffer existing lower-density and single-family residential development and sensitive lands from higher-density, higher-intensity uses toward the center of the Plan area. Transition Zones include limits on building heights and scale, as well as business operations for conditional uses. [confirm language]

Transom: A bar of wood or stone across the top of a door or window.



Sample Streetlights

3.0 Administration

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Water Harvesting:

A water conservation method used to capture, divert, and/or store rainwater for plant irrigation and other uses. A simple system usually consists of an area to catch water (i.e. catchment area or cistern) and a means to distribute water using gravity. Water is directed to landscape holding areas, concave or plated areas with “edges” to retain water, which can be used immediately by nearby plants.

Zoning Map:

A regulatory map that shows the character zones applicable to the Volcano Heights Plan area subject to the standards in this Plan.

DRAFT

4.0 Streets and Streetscape Standards

DRAFT

4.0 Streets and Streetscape Standards

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4.1. Intent/Purpose: Streets in Volcano Heights need to support the overall intent for each corridor. They should balance all forms of mobility while maximizing convenience for residents and visitors. Streets are also used to convey drainage and support water harvesting. Auto-oriented streets serve more regional trips as well as providing access for service, trucking, and maintenance for non-residential uses.

4.2. Applicability: Street and streetscape standards shall apply to all streets and development within Volcano Heights. Property owners and/or developers are responsible for constructing all streets within Volcano Heights per the standards in this Plan. Streetscape standards shall address all elements between the building face and edge of the curb. Typical streetscape elements addressed are street trees, lighting, street furniture and pedestrian amenities, and materials. Maintenance of all streetscape shall be according to the standards in City Zoning Code Chapter 6, Article 5. See **Section 13.3.12** in this Plan for Policies relating to roles and responsibilities for Maintenance.

Street sections and frontage standards take precedence over Site Development Standards for each Character Zone in order to provide a consistent, predictable built environment along corridors, across property lines, and over time.

4.3. How to Use These Standards

4.3.1. Mandatory Streets: The character of streets in Volcano Heights will vary based on location. The Mandatory Street Map designates the minimal, required street network within the Plan area. These streets have been planned to coincide with the existing 20-foot access easement on the edge of each parcel as much as possible. This section specifies the typical configuration of each Mandatory street type.

The specifications address vehicular lane width, parkway widths, right-of-way (ROW) widths, number of travel lanes, on-street parking, and pedestrian accommodation.

- (iv) Once fully built by developers, Paseo del Norte will become the purview of the New Mexico Department of Transportation (NMDOT), while the remaining streets, once constructed by the developers, will become City streets.
- (v) The Mandatory Cross Sections depict the required elements and associated dimensions for each Mandatory Street. The order of these elements may change to suit local conditions, particularly when a Mandatory Street is intended for Bus Rapid Transit (BRT). A study by the Mid-Region Council of Governments (MRCOG) is analyzing whether the BRT will run in the median, the outside, or the inside lanes, or a combination of the above based on adjacent land uses, right-of-way, etc. The final location of these lanes shall be determined during the road design process.
- (vi) See **Section 3.2** and **Table 3.2** and **Table 3.3** for allowable deviations from the Mandatory Street network and/or cross section requirements.

4.3.2. Non-Mandatory Streets: This section specifies standards for all new streets in Volcano Heights. New streets shall be addressed on a project-by-project basis and shall be reviewed by the City Department of Municipal Development (DMD).

4.4. Mandatory Streets and Street Designations Map: See **Exhibit 4.1**.

4.0 Streets and Streetscape Standards

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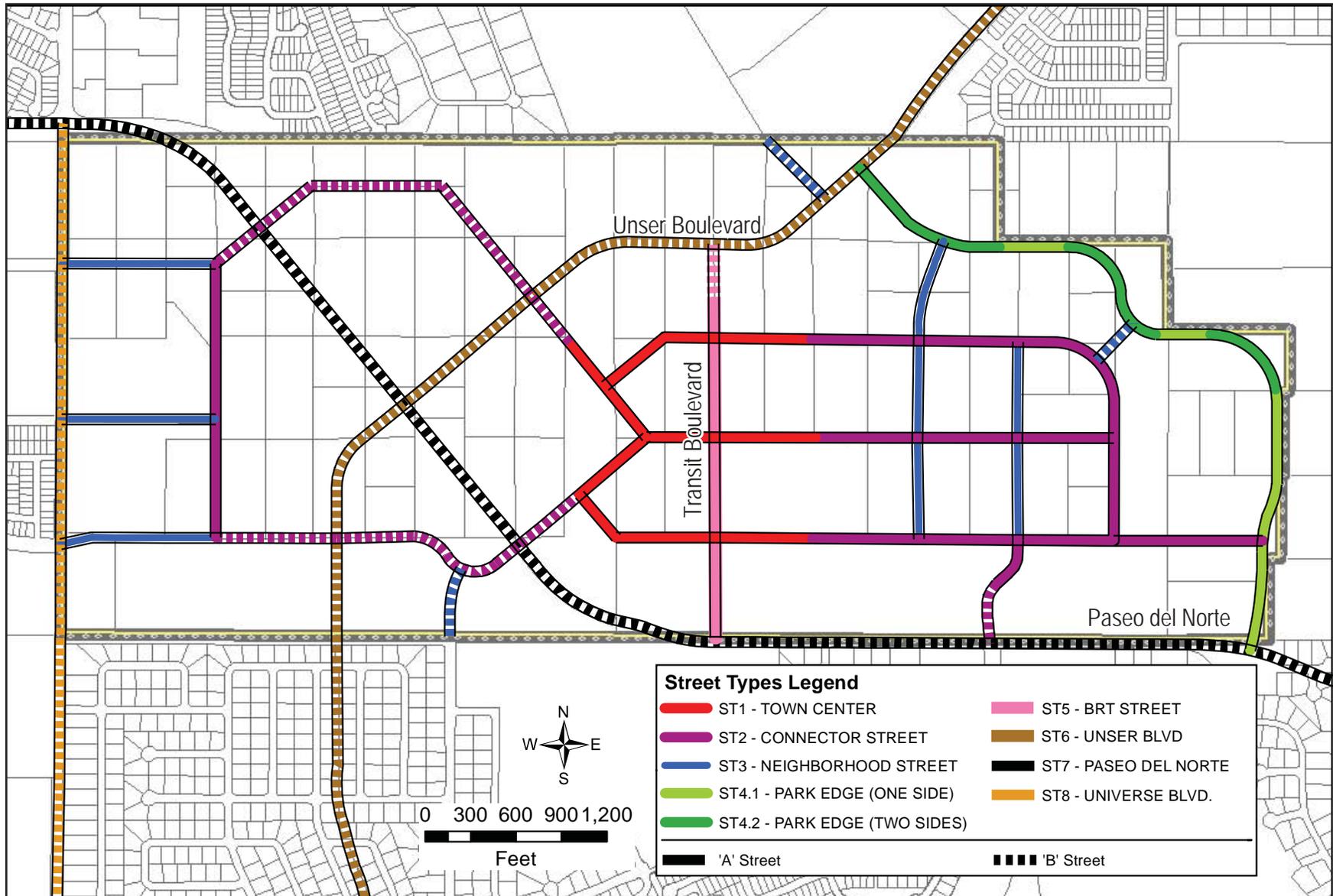


Exhibit 4.1 – Mandatory Streets and Designations Map

4.0 Streets and Streetscape Standards

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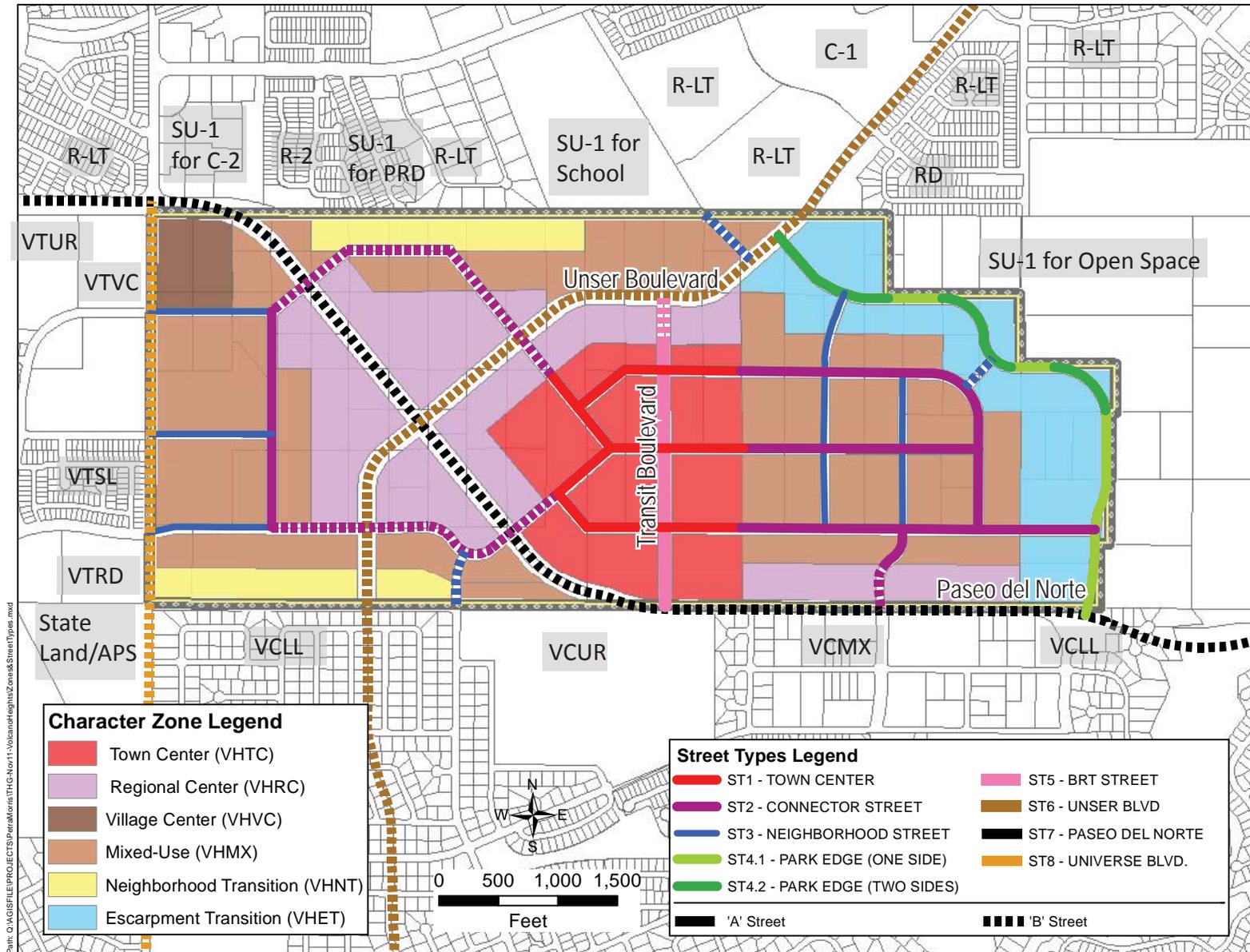


Exhibit 4.2 – Character Zones and Street Types

4.0 Streets and Streetscape Standards

4.4.1. **Recommended Limited-Access Intersections:** Much of the development in Volcano Heights will be highly dependent on additional access from the regional limited-access roads, Unser Boulevard and Paseo del Norte. This Plan recommends that the City submit a request for Access Modification through the official process outlined in **Appendix A** for the access points shown in **Exhibit 4.3**. See additional Transportation Policies in **Section 13.3**.

LEGEND

-  Full intersection recognized by Future Albuquerque Area Bikeways and Streets (FAABS)
-  Full intersection recommended by this Plan
-  Right-in / Right-out intersection recognized by FAABS

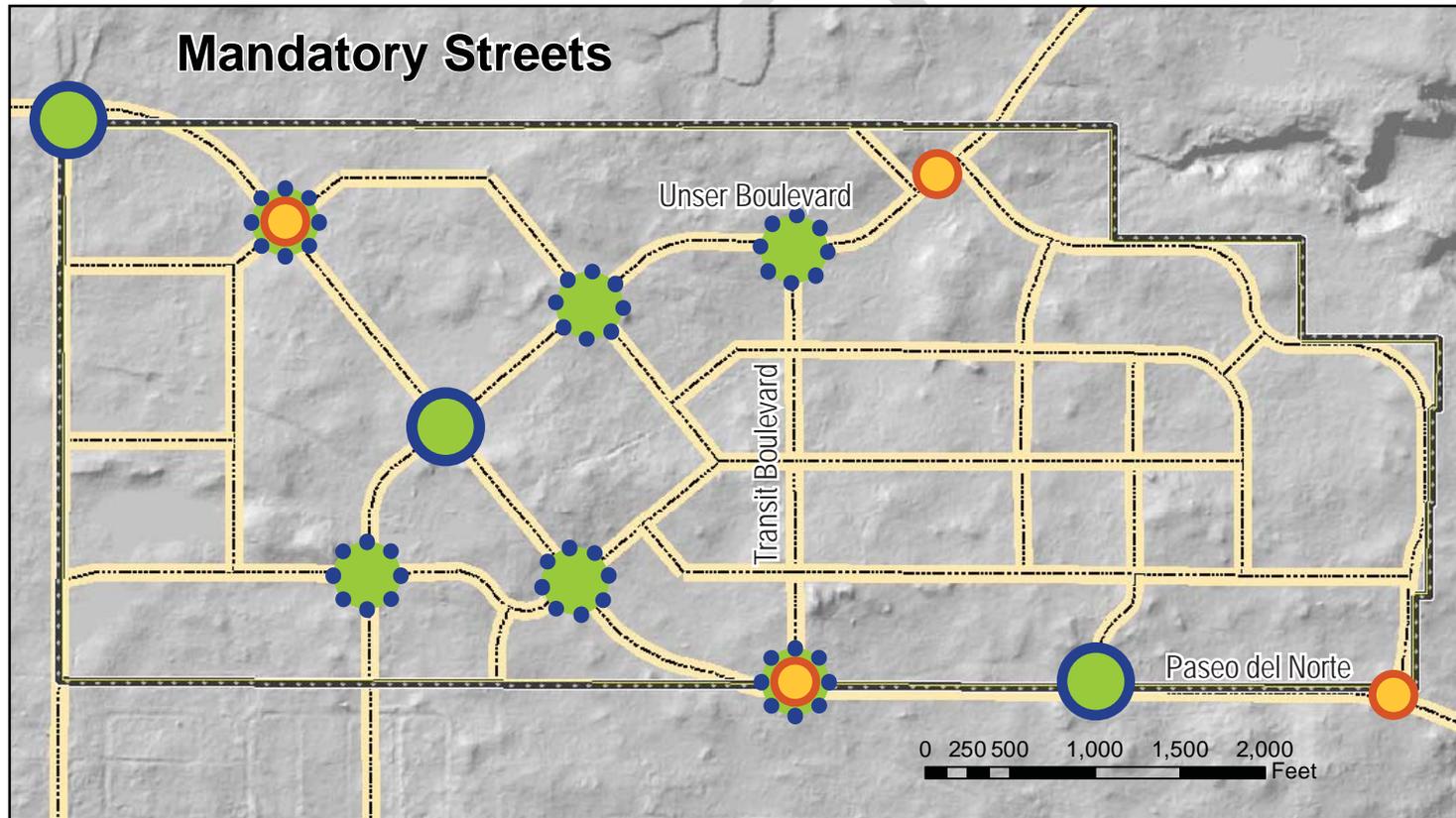


Exhibit 4.3 – Recommended Intersections

4.0 Streets and Streetscape Standards

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4.5. Street Designations: The following Street Designations shall be established for all streets within the Plan area:

4.5.1. 'A' Streets: 'A' Streets are intended to provide the most pedestrian-friendly development context. Buildings along 'A' Streets shall be held to the highest standard of pedestrian-oriented design. These streets are the main connectors for local development and existing neighborhoods.

- (i) Development on 'A' Streets may incorporate a parking court, surrounded on three sides by the development, served via one-way access, and dimensions not to exceed 110 feet wide and 150 feet deep.
- (ii) In order to provide pedestrian connectivity where blocks are more than 300 feet long, private walkways shall be provided every 300-500 feet. [\[confirm language\]](#)
- (iii) In order to support their purpose as pedestrian and cyclist-friendly corridors as well as supportive of retail and neighborhood services, 'A' Streets shall incorporate design speeds within 5 miles per hour of posted speeds. [\[confirm language\]](#)
- (iv) More 'A' Street standards are included in individual Mandatory Streets standards in **Section 4.6**, as well as Non-mandatory 'A' Streets in **Section 4.7.3**.

4.5.2. 'B' Streets: 'B' Streets are intended to accommodate more auto-oriented uses, surface parking, and service functions on a site with automobile orientation. All streets that are not designated as 'A' Streets shall be 'B' Streets. Standards are included in individual Mandatory Streets standards for Unser Boulevard in **Section 4.6.6** and Paseo del Norte in **Section 4.6.7** and Non-mandatory 'B' Street standards in **Section 4.7.5**.

4.5.3. Alleys: Alleys are an optional way to provide vehicle, parking, and service access to local development while screening these vehicle uses from the public realm. Alleys can be either Residential or Commercial.

- (i) Typically narrower than 'B' Streets and with fewer curb cuts, alleys can be a functional element within a commercial block and can provide a pleasant walking option in residential areas. See more standards in **Sections 4.7** and **4.9** in this Plan.
- (ii) See **Section 13.3.12** in this Plan for Policies relating to roles and responsibilities for alley maintenance.

4.0 Streets and Streetscape Standards

4.6. Mandatory Cross Sections and Frontage Standards

4.6.1. Street Type 1: Town Center

- (i) **Intent/Purpose:** These streets are intended to be the most pedestrian-friendly while supporting multiple modes of transportation circulating throughout the Plan area and surrounding region.
- (ii) **Cross Section:** See **Exhibit 4.4.**

(iii) Frontage Standards

- a. Front Setback: 5 feet
- b. Side Setback: 0 feet
- c. Build-to Zone (BTZ): 5-10 feet
- d. Frontage requirement: 80%
- e. Civic/Open Space within the BTZ shall count toward the frontage requirement.
- f. A Commercial Ready ground floor shall be required for buildings along Mandatory 'A' Streets and Civic Spaces.
- g. Any paved area within the setback shall match the material of the sidewalk and be constructed at the same grade level.
- h. Where a surface parking lot is visible from the public ROW, a street screen shall be used.
- i. See **Section 4.9** for other Streetscape Standards.
- j. Encroachments are regulated by Character Zone and can be found in **Section 6** of this Plan.

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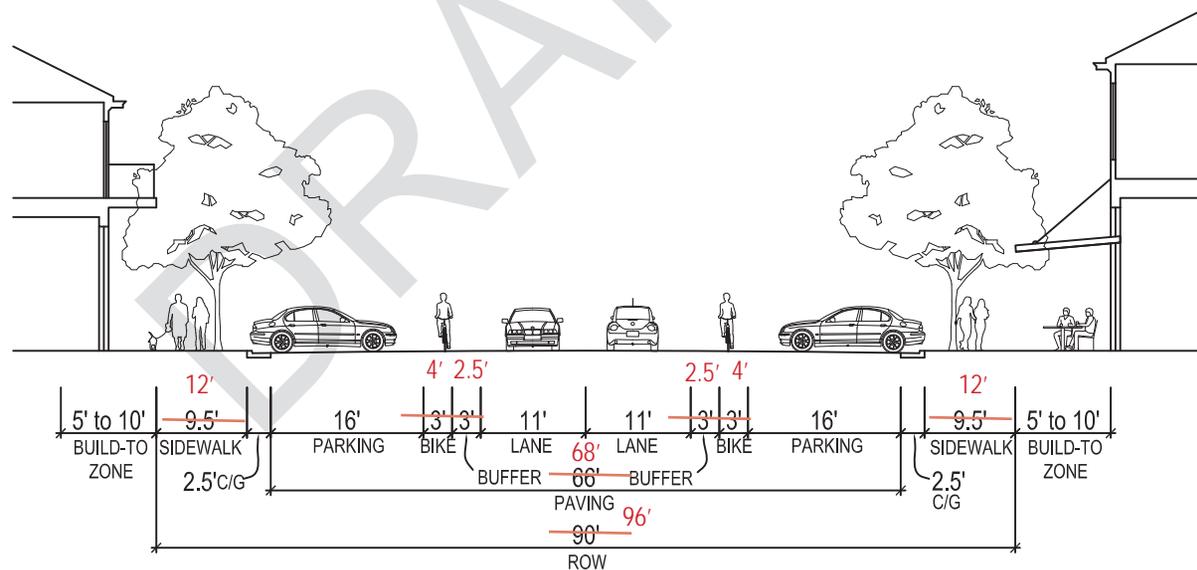


Exhibit 4.4 – Street Type 1: Town Center Cross Section
[Update pending]

4.0 Streets and Streetscape Standards

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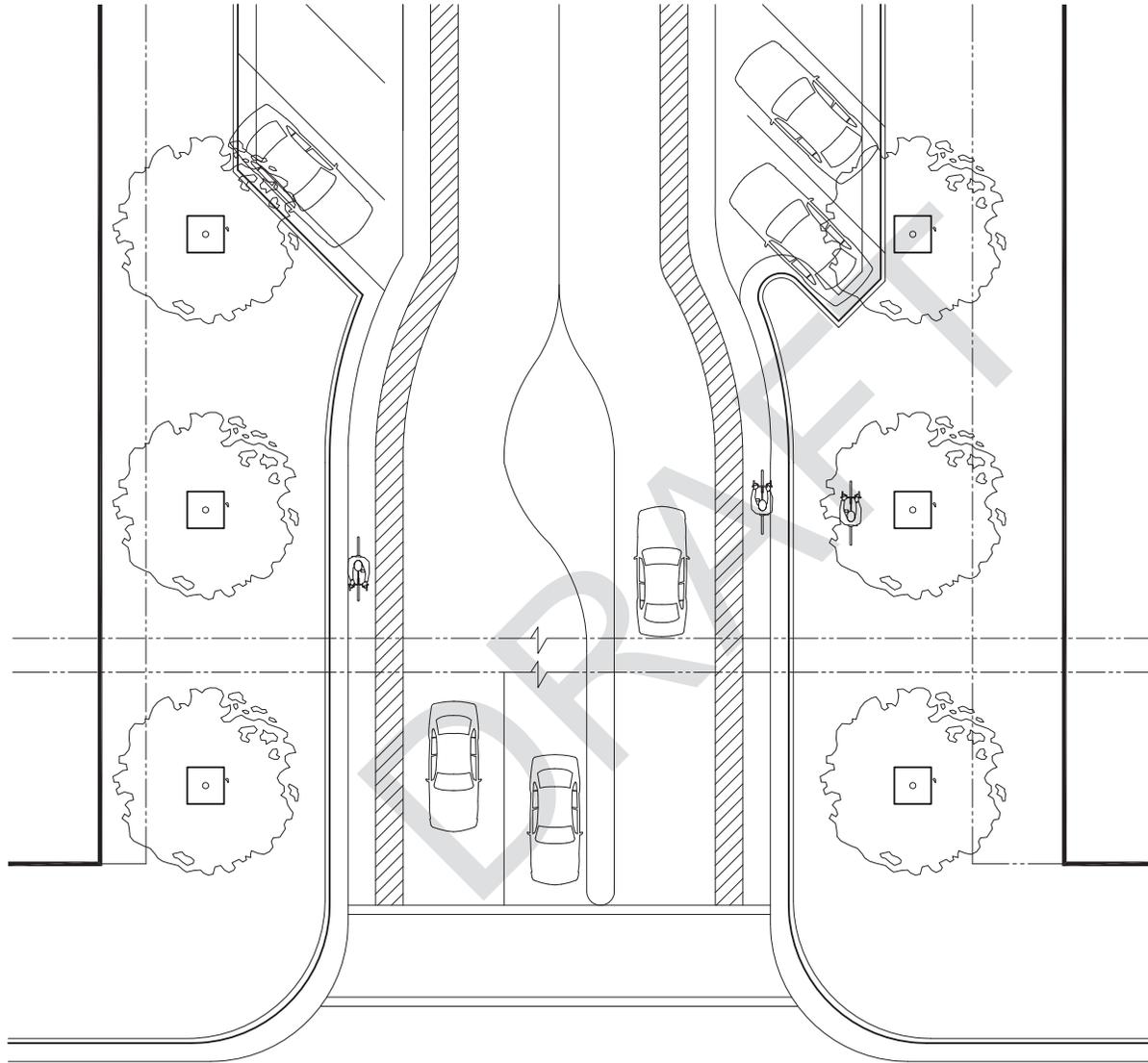


Exhibit 4.5 – Street Type 1: Typical Intersection (Plan Perspective) [Update pending]

4.0 Streets and Streetscape Standards

4.6.2. **Street Type 2: Connector Street**

- (i) **Intent/Purpose:** These streets are intended to access neighborhood streets and promote multi-modal transportation to reach businesses and residences within the Plan area.
- (ii) **Cross Section:** See **Exhibit 4.6.**

(iii) **Frontage Standards**

- a. Front Setback: 5 feet
- b. Side Setback: 0 feet
- c. Build-to Zone (BTZ) along 'A' Streets and Civic Spaces: 5-15 feet
- d. Build-to Zone (BTZ) along 'B' Streets: 5-75 feet
- e. Frontage requirement along 'A' Streets and Civic Spaces: 60%
- f. Frontage requirement along 'B' Streets: 30%
- g. Civic/Open Space within the BTZ shall count toward the frontage requirement.

- h. Any paved area intended for pedestrians, within the setback, shall match the material of the sidewalk and be constructed at the same grade level.
- i. Where a surface parking lot is visible from the public ROW, a street screen shall be used.
- j. See **Section 4.9** in this Plan for other Streetscape Standards.
- k. Encroachment regulations are regulated by Character Zone and can be found in **Section 6** of this Plan.

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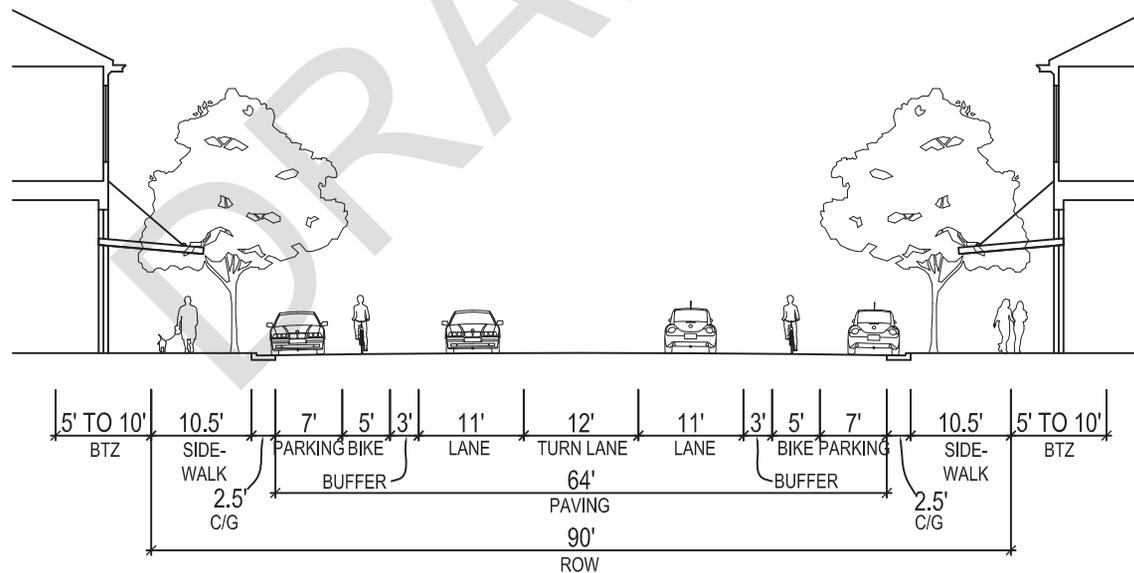


Exhibit 4.6 – Street Type 2: Connector Street Cross Section

4.0 Streets and Streetscape Standards

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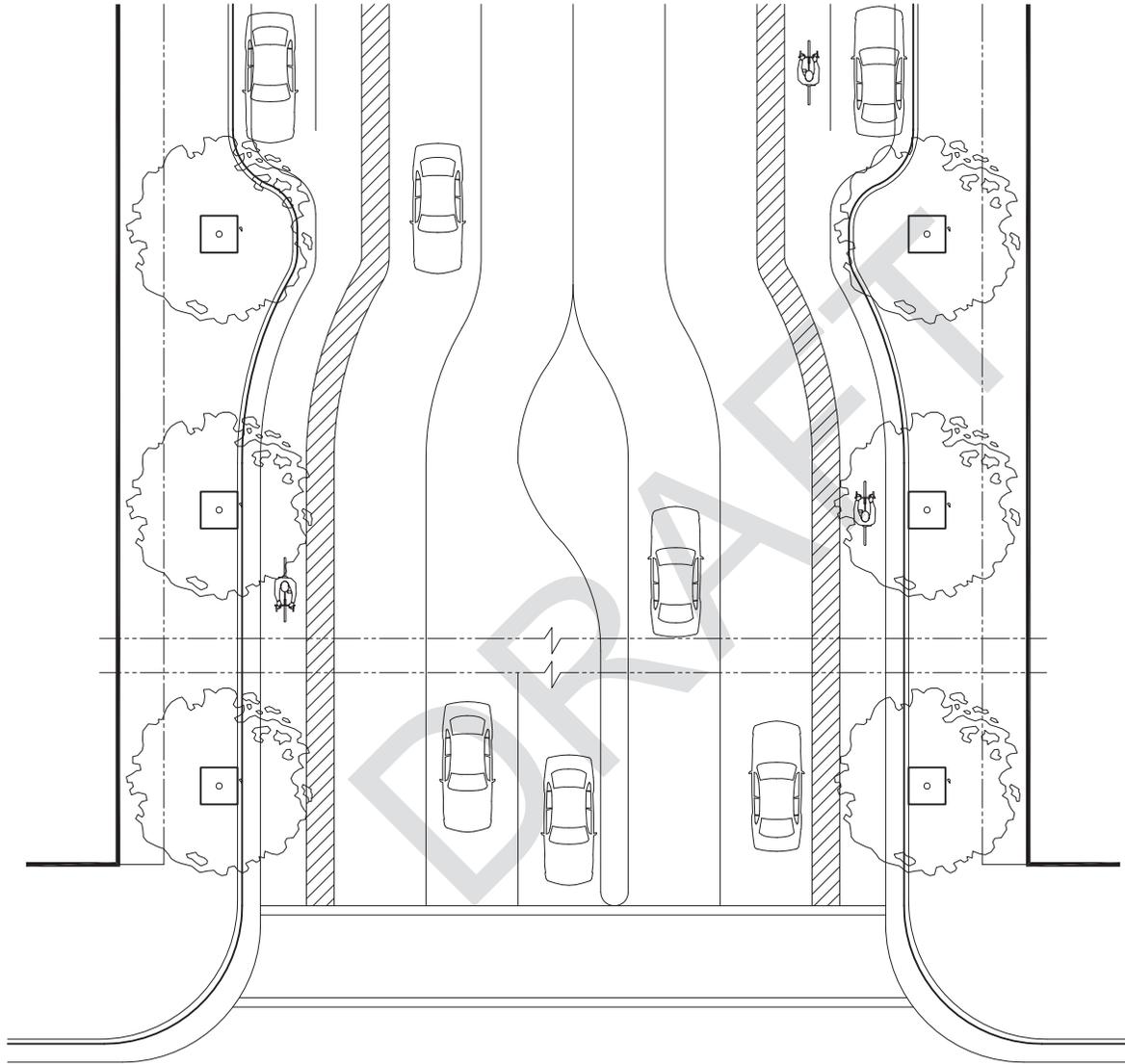


Exhibit 4.7 – Street Type 2: Typical Intersection (Plan Perspective) [Update pending]

4.0 Streets and Streetscape Standards



- 4.6.3. **Street Type 3: Neighborhood Street**
 - (i) **Intent/Purpose:** These streets are intended to access local uses, predominantly businesses and residences within the Plan area.
 - (ii) **Cross Section:** See **Exhibit 4.8.**
 - (iii) **Frontage Standards**
 - a. Front Setback: 5 feet
 - b. Side Setback: 0 feet
 - c. Build-to Zone (BTZ) along 'A' Streets and Civic Spaces: 5-15 feet
 - d. Build-to Zone (BTZ) along 'B' Streets: 5-25 feet
 - e. Frontage requirement along 'A' Streets and Civic Spaces: 60%
 - f. Frontage requirement along 'B' Streets: 30%
 - g. Civic/Open Space within the BTZ shall count toward the frontage requirement.
 - h. Any paved area intended for pedestrians, within the setback, shall match the material of the sidewalk and be constructed at the same grade level.
 - i. Where a surface parking lot is visible from the public ROW, a street screen shall be used.
 - j. See **Section 4.9** in this Plan for other Streetscape Standards.
 - k. Encroachment regulations are regulated by Character Zone and can be found in **Section 6** of this Plan.

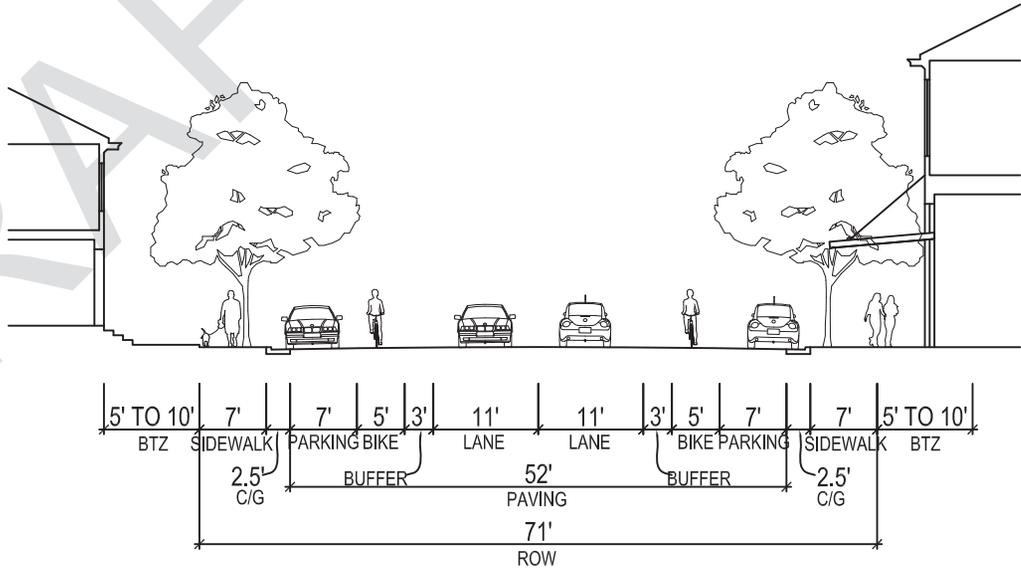


Exhibit 4.8 – Street Type 3: Neighborhood Street Cross Section

4.0 Streets and Streetscape Standards

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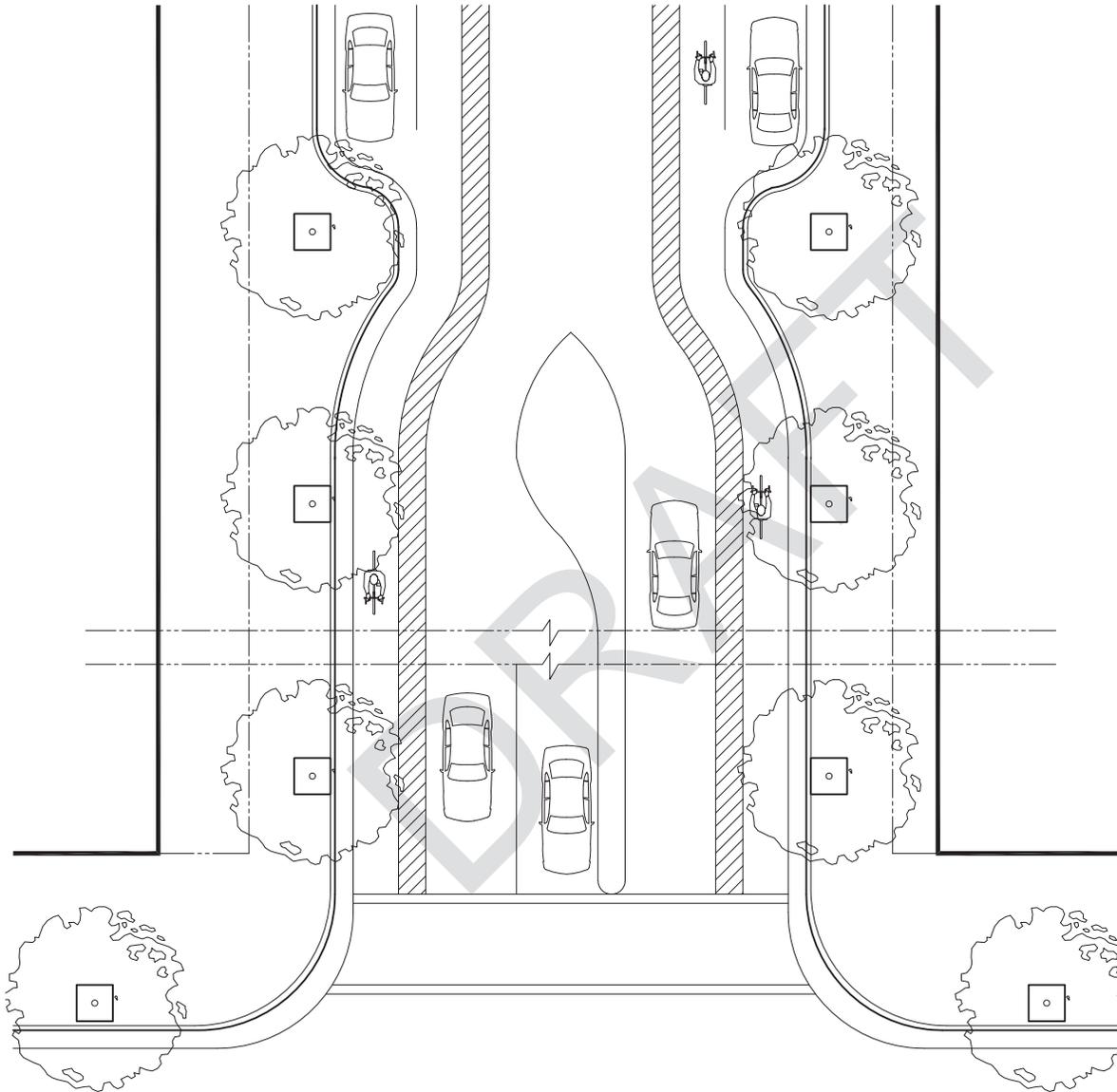


Exhibit 4.9 – Street Type 3: Typical Intersection (Plan Perspective) [Update pending]

4.0 Streets and Streetscape Standards

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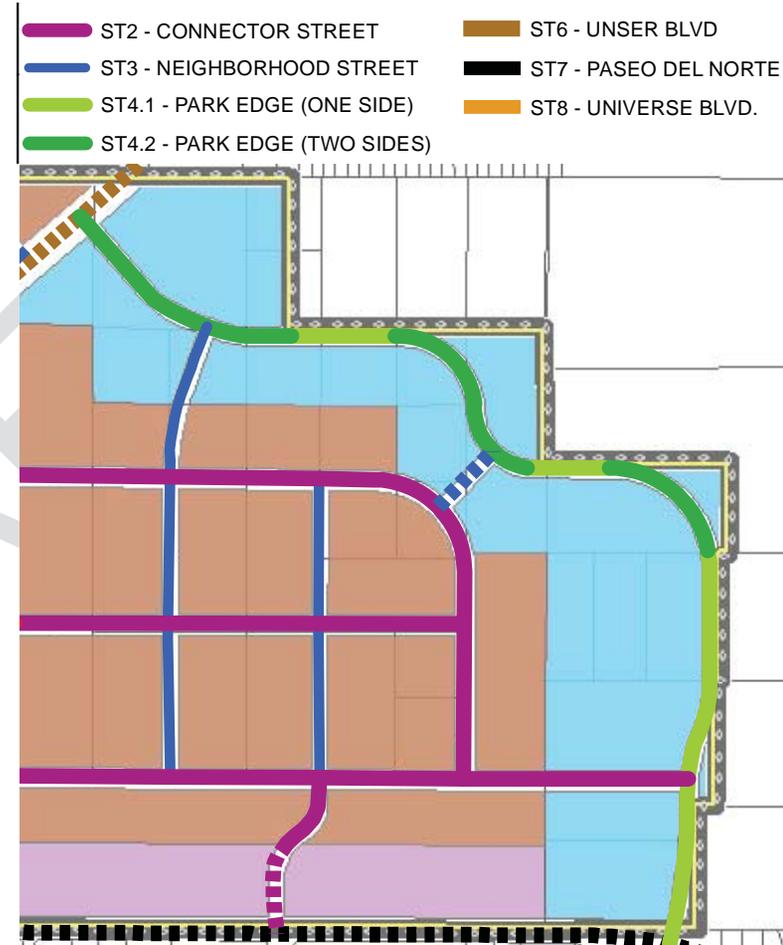
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4.6.4. Street Type 4: Park Edge

(i) **Intent/Purpose:** These streets are intended to access local uses, predominantly residences and businesses within the Plan area, by multiple modes of transportation, including on-street bicycle lanes as well as connections to an off-street trail along the Petroglyph National Monument edge. The Major Public Open Space is best preserved as a public amenity by designing a single-loaded road along Open Space edges. Landscaping and medians act as transitions from the built environment to sensitive lands within the Monument.

(ii) **Cross Sections:** Two cross sections are provided for the Park Edge Street.

- a. Where the street abuts the Petroglyph National Monument and/or where development is only intended on the east side of the road, it shall be single-loaded, and Street Type 4.1 shall be constructed.
- b. Where development will occur on both sides of the street, Street Type 4.2 shall be used. (See **Exhibit 4.10** and **Exhibit 4.11**.)
- c. The Park Edge and additional local streets in the SU-2 Volcano Heights Escarpment Transition (VHET) zone shall be sited to provide regular but controlled pedestrian access to the Petroglyph National Monument. Access shall be determined by the National Park Service Monument Visitor Plan and/or by the City Open Space Division in lieu thereof.



Detail of Exhibit 4.2 – Character Zones and Street Types: Park Edge Road

4.0 Streets and Streetscape Standards

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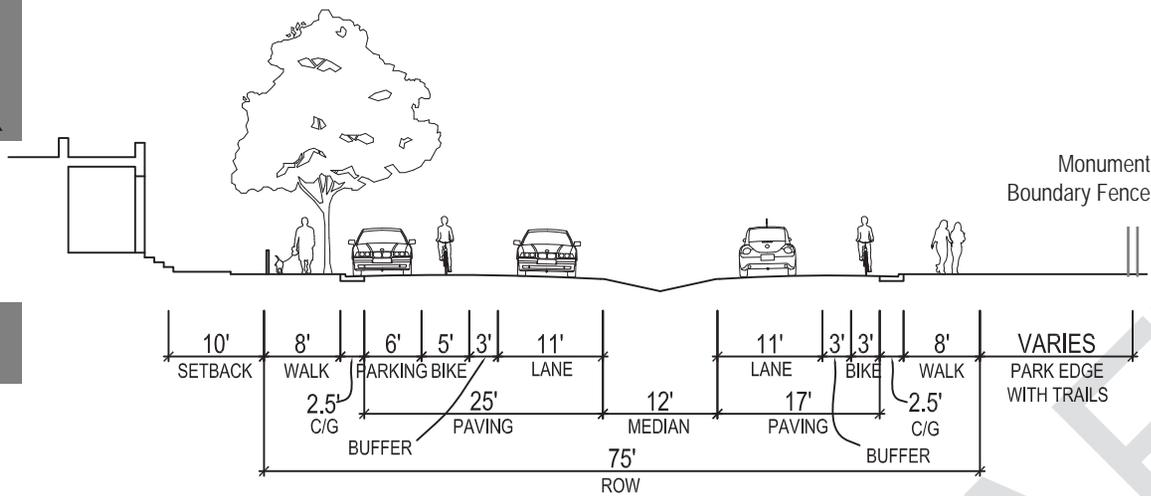


Exhibit 4.10 – Street Type 4.1: Park Edge Single-Loaded Cross Section

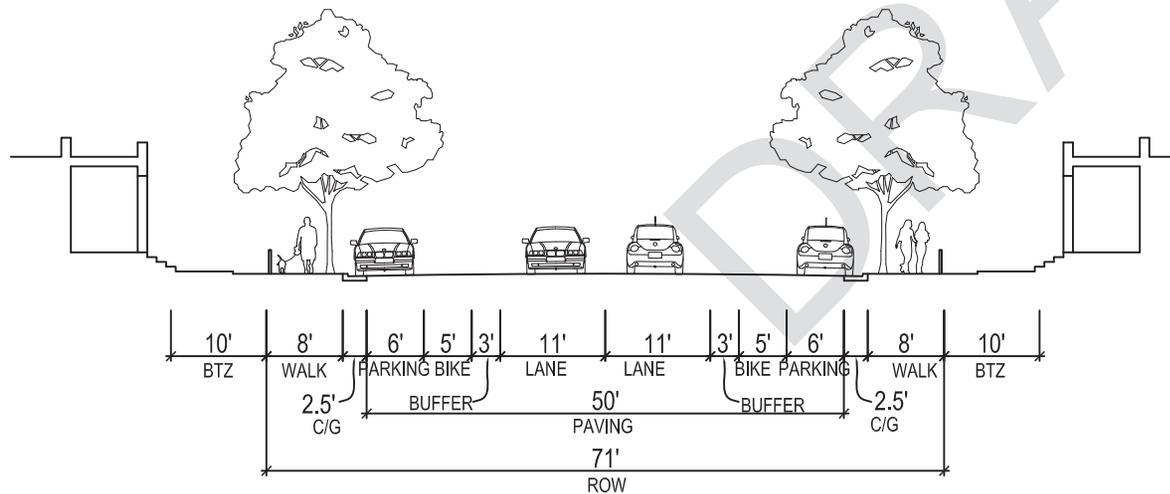


Exhibit 4.11 – Street Type 4.2: Park Edge Double-Loaded Cross Section

(iii) Frontage Standards

- a. Front Setback: 5 feet
- b. Side Setback: 0 feet
- c. Build-to Zone (BTZ) along ‘A’ Streets and Civic Spaces: 5-20 feet
- d. Build-to Zone (BTZ) along ‘B’ Streets: 5-75 feet
- e. Frontage requirement along ‘A’ Streets and Civic Spaces: 60%
- f. Frontage requirement along ‘B’ Streets: 30%
- g. Civic/Open Space within the BTZ shall count toward the frontage requirement.
- h. Where a surface parking lot is visible from the public ROW, a street screen shall be used.
- i. See **Section 4.9** in this Plan for other Streetscape Standards.
- j. Encroachment regulations are regulated by Character Zone and can be found in **Section 6** of this Plan.

4.0 Streets and Streetscape Standards

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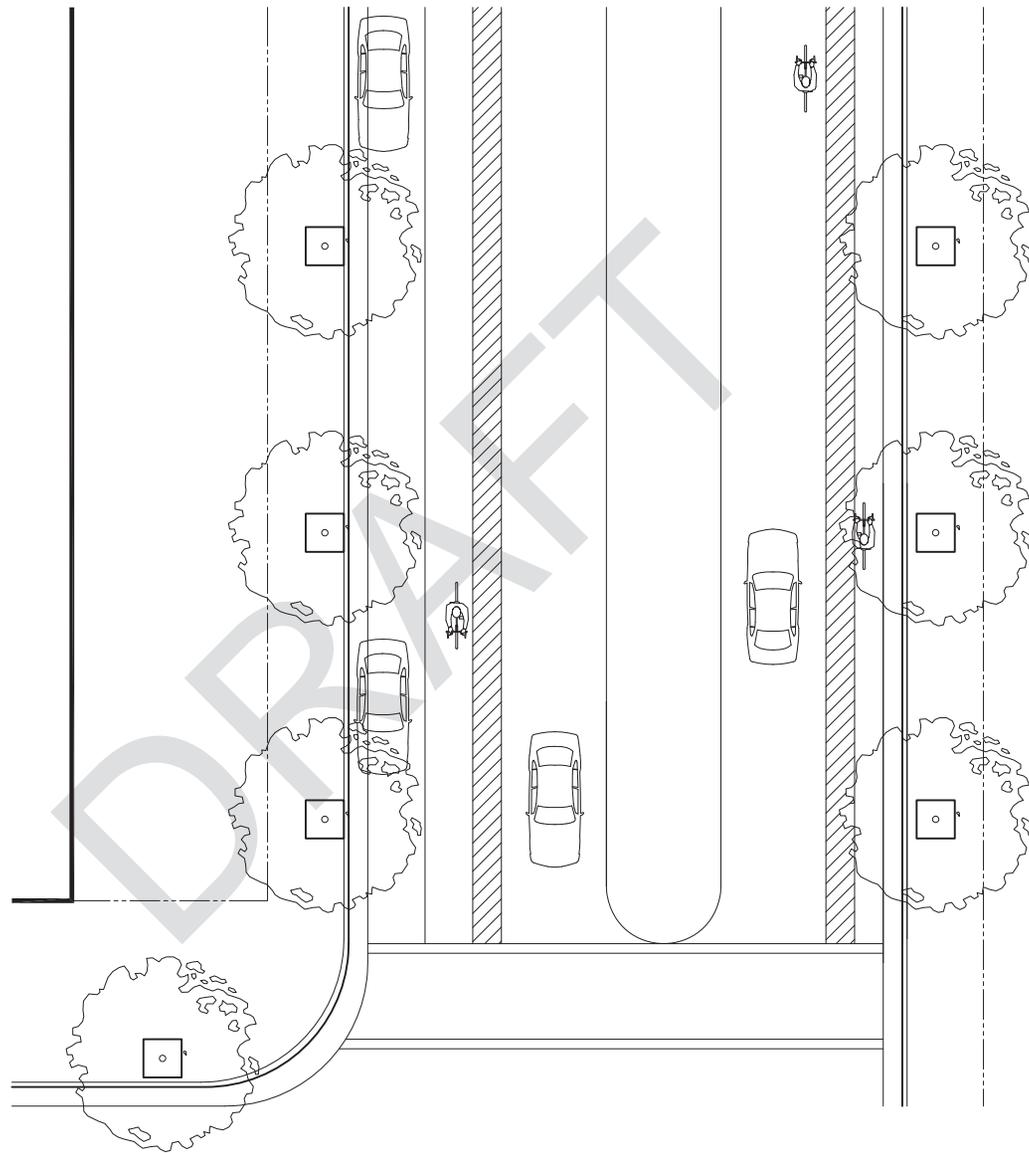


Exhibit 4.12 – Street Type 4.1: Typical Intersection (Plan Perspective) [Update pending]

4.0 Streets and Streetscape Standards

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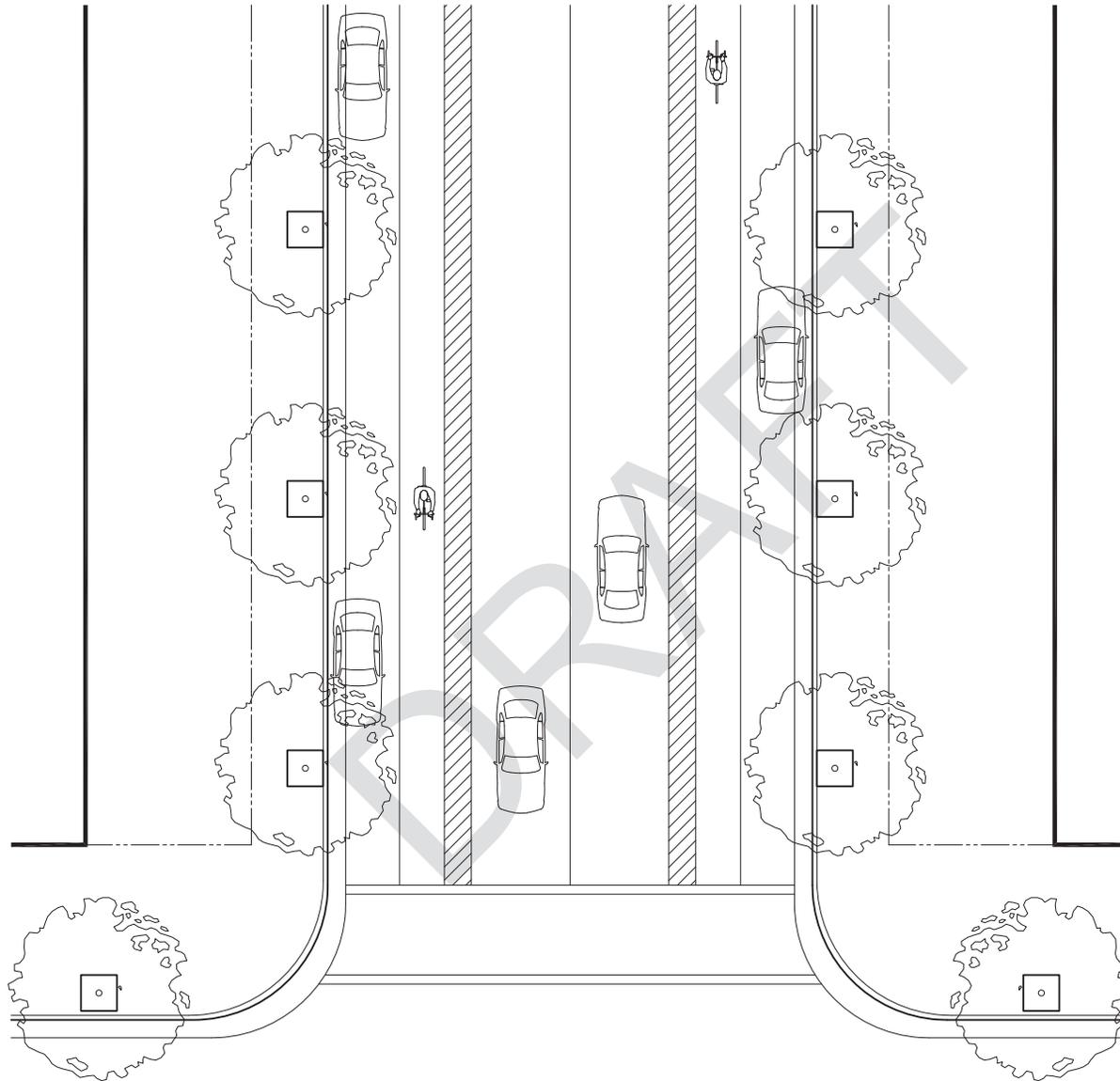


Exhibit 4.13 – Street Type 4.2: Typical Intersection (Plan Perspective) [Update pending]

4.0 Streets and Streetscape Standards

4.6.5. **Street Type 5: Transit Corridor**

- (i) **Intent/Purpose:** The Transit Corridor serves multiple modes of transportation, including the proposed Bus Rapid Transit. The walkable, dense, urban Town Center is organized around this Transit Corridor, which acts as a “Main Street” for Volcano Heights.
- (ii) **Cross Section:** See **Exhibit 4.14**.
- (iii) **Frontage Standards**
 - a. Front Setback: 5 feet
 - b. Side Setback: 0 feet

- c. Build-to Zone (BTZ) along ‘A’ Streets and Civic Spaces: 5-15 feet
- d. Build-to Zone (BTZ) along ‘B’ Streets: 5-75 feet
- e. Frontage requirement along ‘A’ Streets and Civic Spaces: 60%
- f. Frontage requirement along ‘B’ Streets: 30%
- g. Civic/Open Space within the BTZ shall count toward the frontage requirement.
- h. A Commercial Ready ground floor shall be required for buildings along Mandatory ‘A’ Streets and Civic Spaces.
- i. Any paved area intended for pedestrians, within the setback, shall match the material of the sidewalk and be constructed at the same grade level.
- j. Where a surface parking lot is visible from the public ROW, a street screen shall be used.
- k. See **Section 4.9** in this Plan for other Streetscape Standards.
- l. Encroachment regulations are regulated by Character Zone and can be found in **Section 6** of this Plan.
- m. Beginning 500 feet from intersections on Paseo del Norte, Unser Boulevard, and any other potential station locations, 36 feet in the right-of-way shall be dedicated for BRT lanes and/or station platforms.

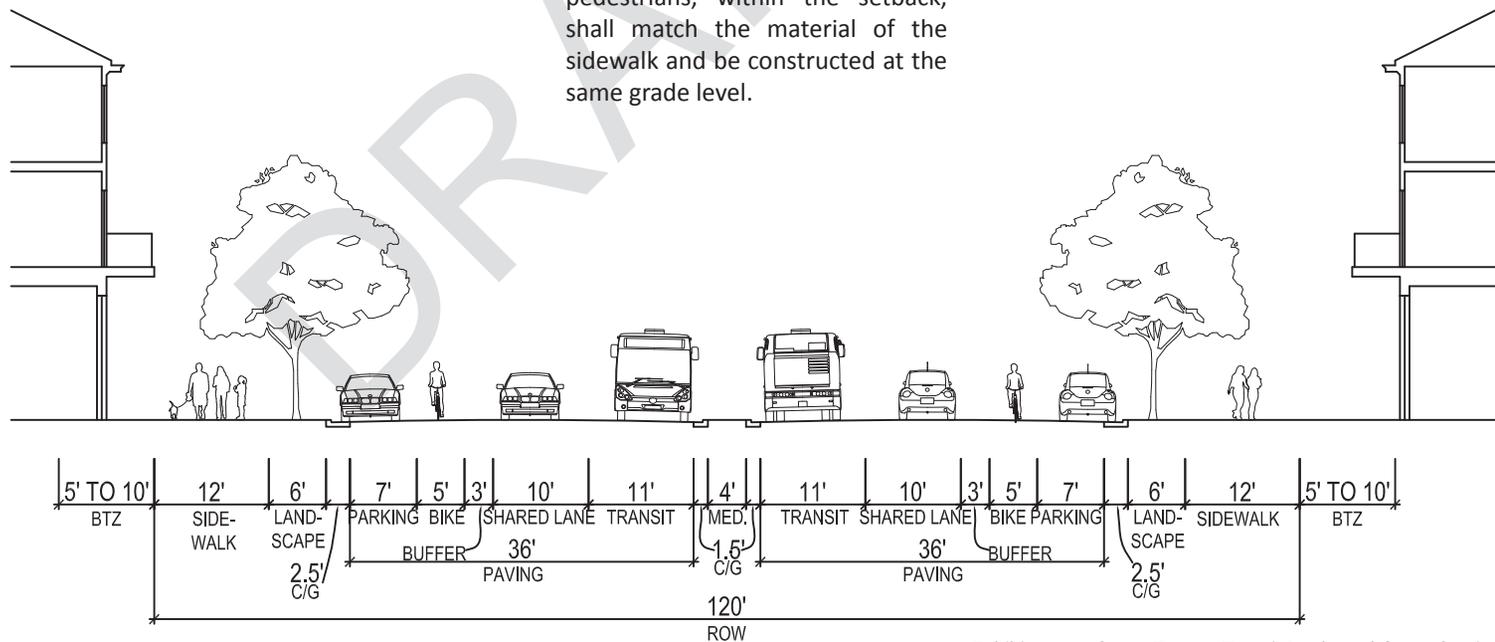


Exhibit 4.14 – Street Type 5: Transit Boulevard Cross Section



4.0 Streets and Streetscape Standards

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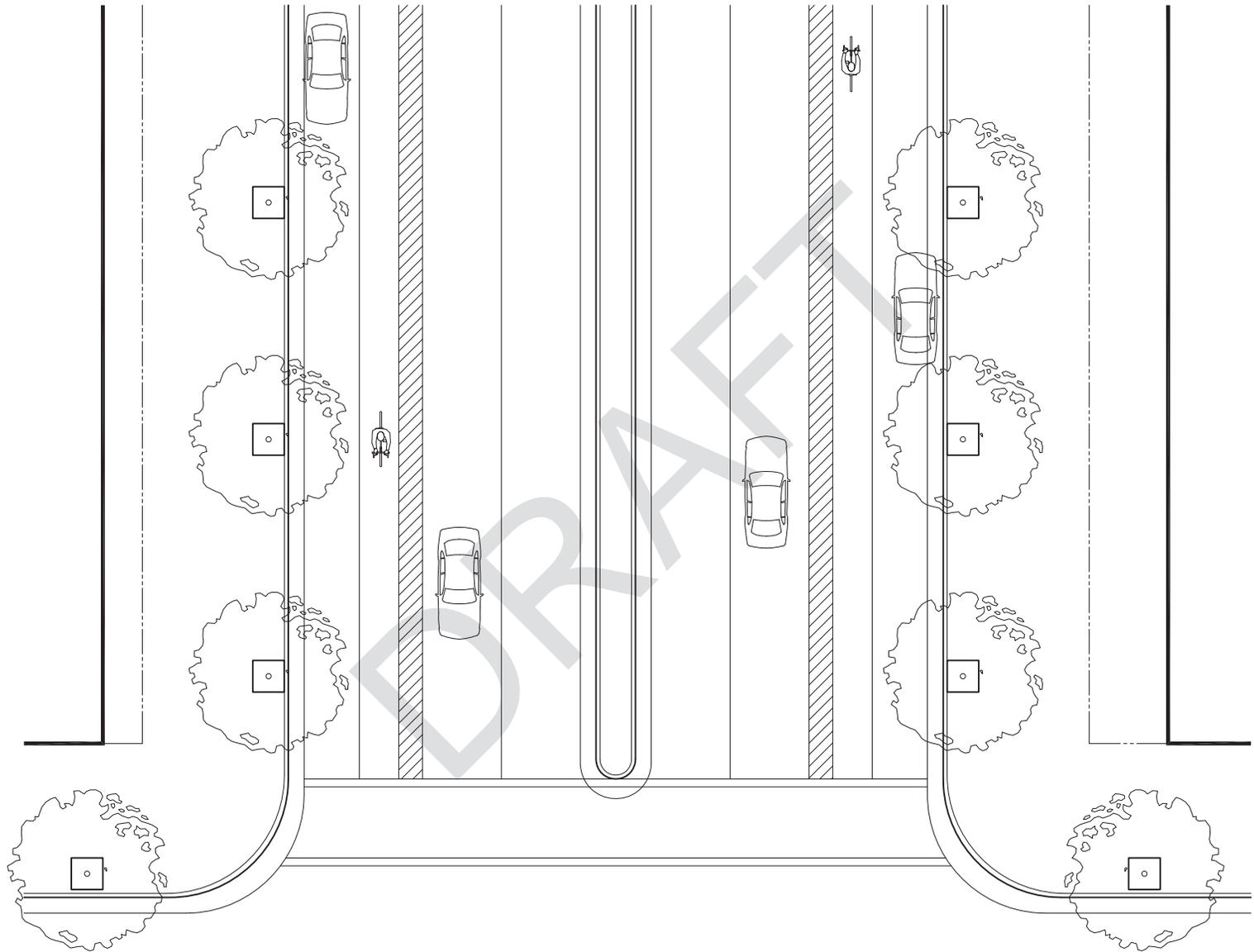


Exhibit 4.15 – Street Type 5: Typical Intersection (Plan Perspective)

4.0 Streets and Streetscape Standards

4.6.6. **Street Type 6: Unser Boulevard**

- (i) **Intent/Purpose:** Unser Boulevard is primarily a regional road, serving residents and businesses within Volcano Heights as well as the surrounding region.
- (ii) **Cross Section:** See Exhibit 4.16.
- (iii) **Frontage Standards**
 - a. Front Setback: 5 feet
 - b. Side Setback: 0 feet
 - c. Build-to Zone (BTZ) along 'B' Streets: 5-75 feet
 - d. Frontage requirement along Civic Spaces: 60%

- e. Frontage requirement along 'B' Streets: 30%
- f. Civic/Open Space within the BTZ shall count toward the frontage requirement.
- g. Any paved area intended for pedestrians, within the setback, shall match the material of the sidewalk and be constructed at the same grade level.
- h. Where a surface parking lot is visible from the public ROW, a street screen shall be used.

- i. Outdoor storage of vehicles or other products sold shall not exceed 50% of a lot's frontage.
- j. See **Section 4.9** in this Plan for other Streetscape Standards.
- k. Encroachment regulations are regulated by Character Zone and can be found in **Section 6** of this Plan.
- l. Beginning 500 feet from an intersection with Paseo del Norte, the transit boulevard, or any potential station locations, 36 feet in the right-of-way shall be dedicated for BRT lanes and/or station platforms.

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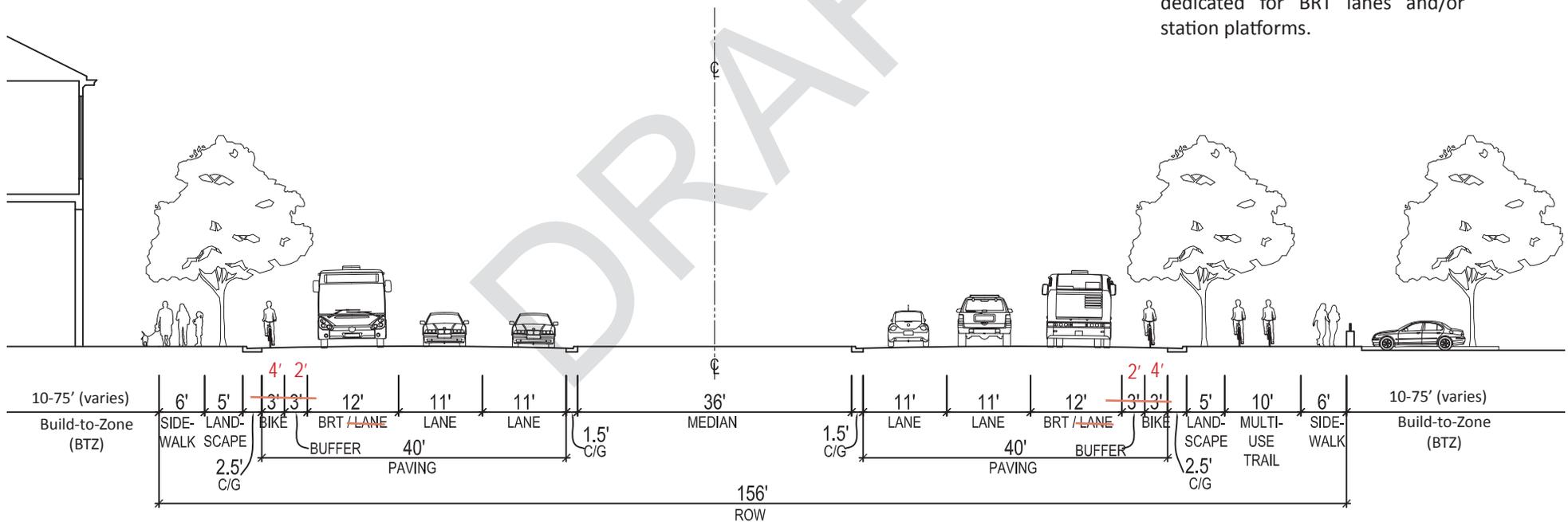


Exhibit 4.16 – Street Type 6: Unser Boulevard Cross Section
 [Update Pending]

4.0 Streets and Streetscape Standards

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4.6.7. Street Type 7: Paseo del Norte

(i) **Intent/Purpose:** Paseo del Norte is primarily a regional road, serving residents and businesses within Volcano Heights as well as the surrounding region.

(ii) **Cross Section:** See Exhibit 4.17.

(iii) **Frontage Standards**

- a. Front Setback: 5 feet
- b. Side Setback: 0 feet
- c. Build-to Zone (BTZ) along 'B' Streets: 5-75 feet

- d. Frontage requirement along Civic Spaces: 60%
- e. Frontage requirement along 'B' Streets: 30%
- f. Civic/Open Space within the BTZ shall count toward the frontage requirement.
- g. Any paved area intended for pedestrians, within the setback, shall match the material of the sidewalk and be constructed at the same grade level.

- h. Where a surface parking lot is visible from the public ROW, a street screen shall be used.
- i. Outdoor storage of vehicles or other products sold shall not exceed 50% of a lot's frontage.
- j. See **Section 4.9** for other Streetscape Standards.
- k. Encroachment regulations are regulated by Character Zone and can be found in **Section 6** of this Plan.
- l. Beginning 500 feet from an intersection with Unser Boulevard, the transit boulevard, or any potential station locations, 36 feet in the right-of-way shall be dedicated for BRT lanes and/or station platforms.

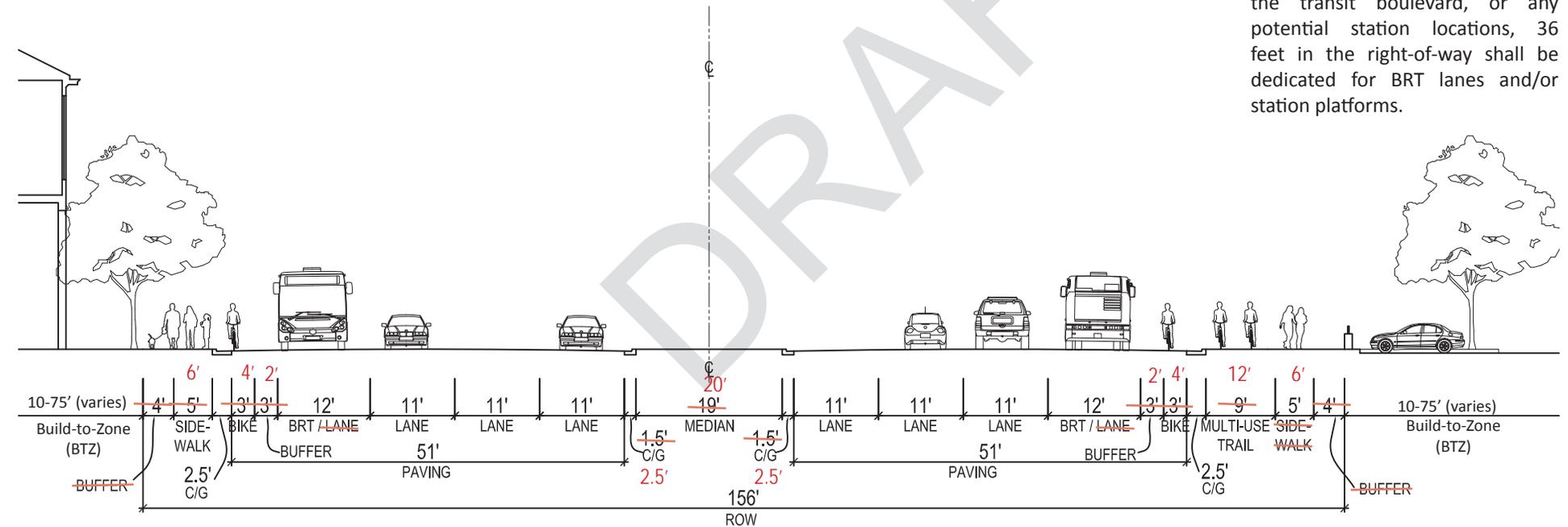


Exhibit 4.17 – Street Type 7: Paseo del Norte Boulevard Cross Section [Update pending]

4.0 Streets and Streetscape Standards

4.6.8. **Street Type 8: Universe Boulevard**

- (i) **Intent/Purpose:** Universe Boulevard is a major arterial carrying significant traffic volumes to facilitate regional movement. At the same time, it connects many predominantly residential areas and therefore must be safe and comfortable for pedestrian and cyclists.
- (ii) **Cross Section:** See **Exhibit 4.18**.
- (iii) **Frontage Standards**
 - a. Front Setback: 5 feet
 - b. Side Setback: 0 feet
 - c. Build-to Zone (BTZ) along 'B' Streets: 5-75 feet
 - d. Frontage requirement along Civic Spaces: 60%
 - e. Frontage requirement along 'B' Streets: 30%
 - f. Civic/Open Space within the BTZ shall count toward the frontage requirement.
 - g. Any paved area intended for pedestrians, within the setback, shall match the material of the sidewalk and be constructed at the same grade level.
 - h. Where a surface parking lot is visible from the public ROW, a street screen shall be used.
 - i. See **Section 4.9** for other Streetscape Standards.
 - j. Encroachment regulations are regulated by Character Zone and can be found in **Section 6** of this Plan.

[Revised cross section to show the following cross section measurements]

6'	setback
10'	sidewalk
6'	tree strip
2.5'	curb/gutter
5'	bike lane
2.5'	buffer
11'	lane
11'	lane
16'	median
11'	lane
11'	lane
2.5'	buffer
5'	bike lane
2.5'	curb/gutter
6'	tree strip
10'	sidewalk
6'	setback
124'	Total ROW

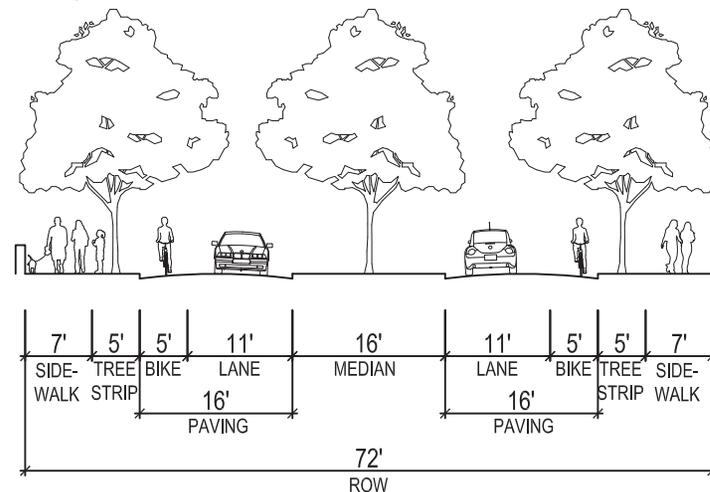


Exhibit 4.18 – Street Type 8: Universe Boulevard Cross Section
 [Update pending]



4.0 Streets and Streetscape Standards

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4.7. Non-Mandatory Street Design Standards

- 4.7.1. The platting of new dead-end streets and culs-de-sac that terminate the road is prohibited.
 - (i) Stub streets or “knuckle” culs-de-sac are allowed where necessary to reach no more than 4 parcels beyond a corner or intersection.
 - (ii) Mid-block “bubble” culs-de-sac without throats are allowed.
 - (iii) Where dead-ends cannot be avoided, pedestrian/bike connections shall be provided to open space and/or road networks beyond the dead-end.

4.7.2. Required ‘A’ vs. ‘B’ Streets

- (i) **New development shall include non-mandatory roads to serve projects, per the requirements in Table 4.1.**
 - a. ‘A’ vs. ‘B’ Percentage:** The percentages given are the minimum for ‘A’ Streets and the maximum for ‘B’ Streets. For example, for a project within Town Center, a minimum of 1 of every 2 non-mandatory streets shall be planned as ‘A’ Streets. If more than half the streets are planned as ‘A’ Streets, the ‘B’ Street percentage would be reduced accordingly. In Regional Center, for example, at least 1 of every 4 roads shall be an ‘A’ Street.
 - b. Connectivity:** Non-mandatory roads added in Volcano Heights shall maintain or improve street connectivity to serve pedestrians as well as dispersing auto traffic.
- (ii) The first development in shall determine non-mandatory road designation. Roads shall be platted as such during the TIDD/SAD/PID and/or Site Development Plan for Subdivision process.

TABLE 4.1 – NON-MANDATORY ROAD REQUIREMENTS

Character Zone	Non-mandatory Roads	
	‘A’ Street <i>(min.)</i>	‘B’ Street <i>(max.)</i>
Town Center	50%	50%
Regional Center	25%	75%
Village Center	25%	75%
Mixed Use	25%	75%
Neighborhood Transition	0%	100%
Escarpment Transition	25%	75%

4.0 Streets and Streetscape Standards



- 4.7.3. **Non-mandatory Cross Sections**
- (i) Non-mandatory streets shall be assembled per **Table 4.2**.
 - (ii) The elements may be arranged to best meet local conditions and intended character of the corridor.
 - (iii) Where a non-mandatory road crosses property lines, adjacent property owners shall be required to coordinate and **sign a development agreement for the agreed-to the cross section as the first project is being planned.**

- (iv) Typical Cross Sections for 'A' Streets and 'B' Streets are illustrated in **Exhibit 4.19** and **Exhibit 4.20**, respectively. Typical Cross Sections for Residential and Commercial Alleys are illustrated in **Exhibit 4.21** and **Exhibit 4.22**, respectively.

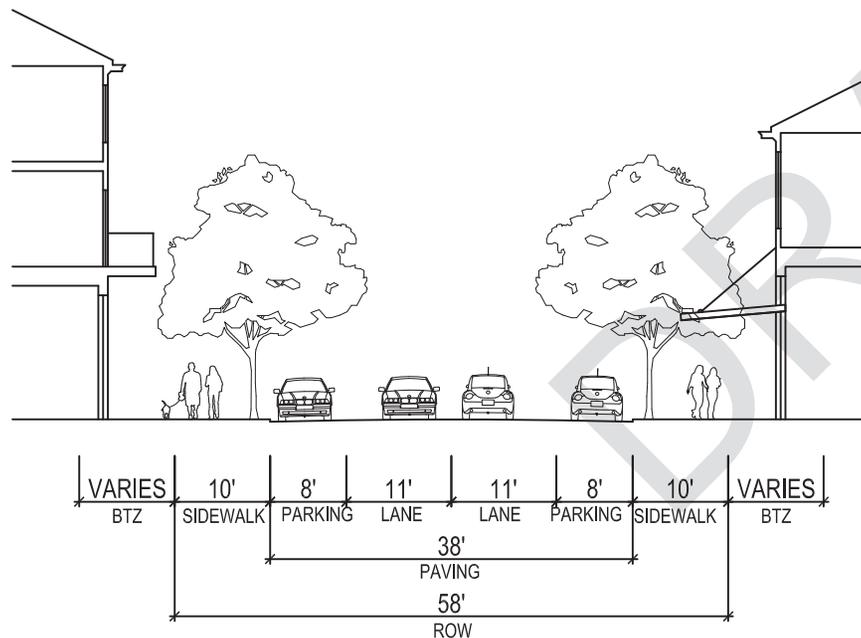
TABLE 4.2 – TYPICAL NON-MANDATORY STREET AND ALLEY CROSS SECTION OPTIONS

Street Elements	Street ROW	# Vehicular Lanes	Vehicular Travel Lane Widths (max.)	Bike Lane	On-Street Parking	Pedestrian Sidewalk Width (min.) * includes easement(s)	Parkway/ Tree Well
'A' Street	58-96 feet	2-3 <i>(includes center turn lane)</i>	10 (11) feet	None	6-18 feet <i>(reverse-angle parking requires 16-18 feet)</i>	12 feet	<i>(optional within sidewalk width)</i>
'B' Street	58-96 feet	2-4	11 (13) feet	None	8 feet	6 feet	<i>(optional with sidewalks at least 8 feet wide)</i>
Commercial Alley	20-36 feet <i>(all paved)</i>	N/A	N/A	None	None	None	None
Residential Alley	20-30 feet <i>(12 feet minimum paved)</i>	N/A	N/A	None	None	None	None

4.0 Streets and Streetscape Standards

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TYPICAL 'A' STREET

Exhibit 4.19 – Typical 'A' Street Cross Section

4.7.4. Typical 'A' Street

- (i) **Intent/Purpose:** Non-mandatory 'A' Streets are intended to provide the primary pedestrian access to development. They should be pedestrian-friendly and attractive, including streetscape amenities and landscaping. See **Exhibit 4.19**.
- (ii) Streets shall have 10-foot minimum sidewalks but 12 feet where possible. Larger sidewalks are encouraged as long as the primary building is positioned abutting the sidewalk.
- (iii) Drive lanes shall have a typical dimension of 11 feet maximum but may be decreased to no less than 10 feet wide for each lane where transit is not anticipated.
- (iv) On-Street parking may be parallel or reversed angle parking. Where reversed angled parking is used, the ROW shall be increased to add that element (typically requiring 16-18 feet) while still maintaining appropriate dimensions for all other elements. A median is also recommended with reverse angle parking.
- (v) When incorporating a median, the median shall provide enough width to allow vegetation to be planted and sustain itself within the entire median, including turn bays.
- (vi) Streetscaping shall refer to **Section 4.2** of this Plan.
- (vii) Street lighting shall be per **Section 4.9** of this Plan.

4.0 Streets and Streetscape Standards



4.7.5. **Typical 'B' Street**

- (i) **Intent/Purpose:** Non-mandatory 'B' Streets are intended to provide the primary automobile access to development. They should serve both potential customers and residents as well as service, delivery, and maintenance vehicles. See **Exhibit 4.20**.
- (ii) Streets shall have 6-foot minimum sidewalks but 8 feet where possible. Where street trees are added to the cross section within the allowance for the sidewalk, the sidewalk portion, including the tree well, shall be no less than 10 feet wide.
- (iii) Drive lanes shall have a typical dimension of 10 feet but may be increased to no more than 11 feet wide for each lane.
- (iv) On-Street parking shall be parallel.
- (v) Parking visible from the public ROW along the 'B' Street shall have a street screen of masonry, metal railing, vegetation or a combination of these. This street screen shall be a minimum of 3 feet and no more than 6 feet tall.
- (vi) Medians are not permitted on 'B' Streets.
- (vii) Streetscaping shall be per **Section 4.2** of this Plan.
- (viii) Street lighting shall be per **Section 4.9** of this Plan.

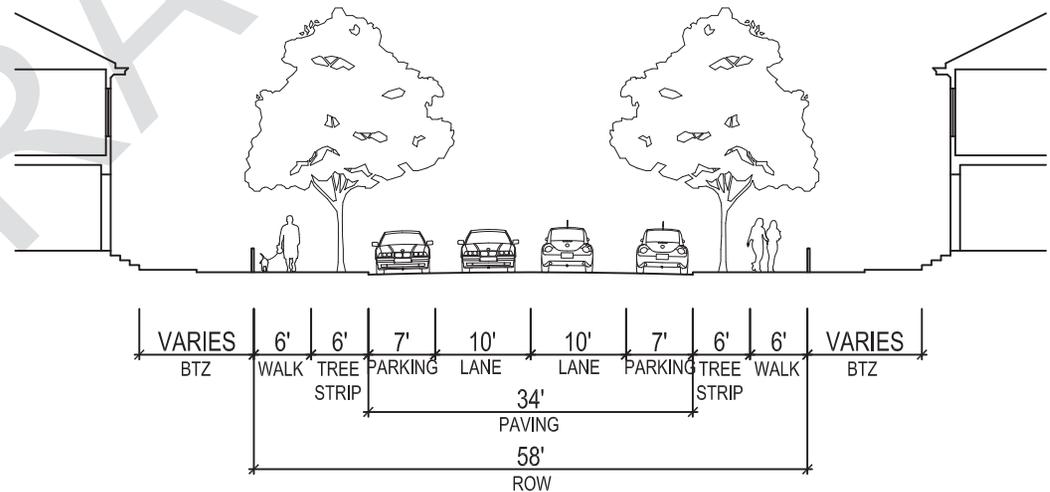


Exhibit 4.20 – Typical 'B' Street Cross Section

4.0 Streets and Streetscape Standards

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4.7.6. Typical Alleys

(i) Residential

- a. **Purpose / Intent:** Provides access to service areas in back, parking, and outbuildings. Contains utility easements.
- b. **ROW width:** 20 feet
- c. **Pavement width:** 12 feet minimum
- d. **Design speed:** 10 miles per hour

(ii) Commercial

- a. **Purpose / Intent:** Provides access to service areas in back, parking, and outbuildings. Contains utility easements.
- b. **ROW width:** 20 feet
- c. **Pavement width:** 20 feet minimum
- d. **Design speed:** 15 miles per hour

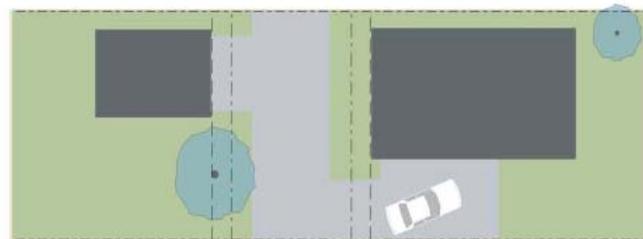
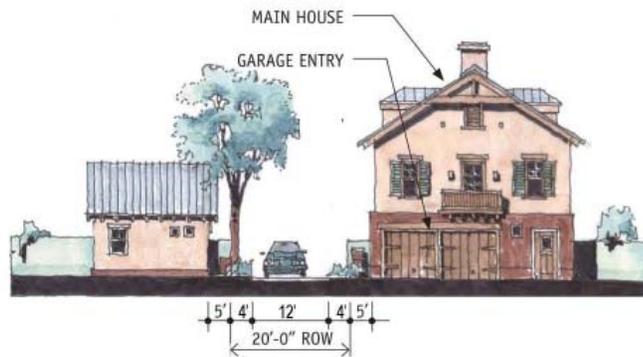


Exhibit 4.21 – Typical Residential Alley

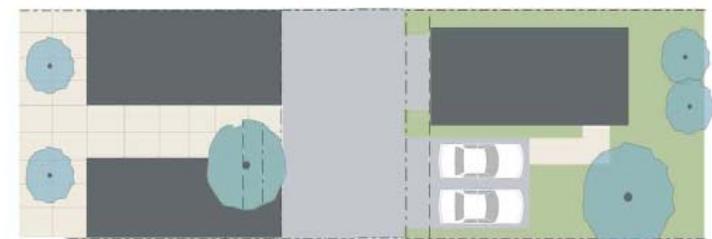
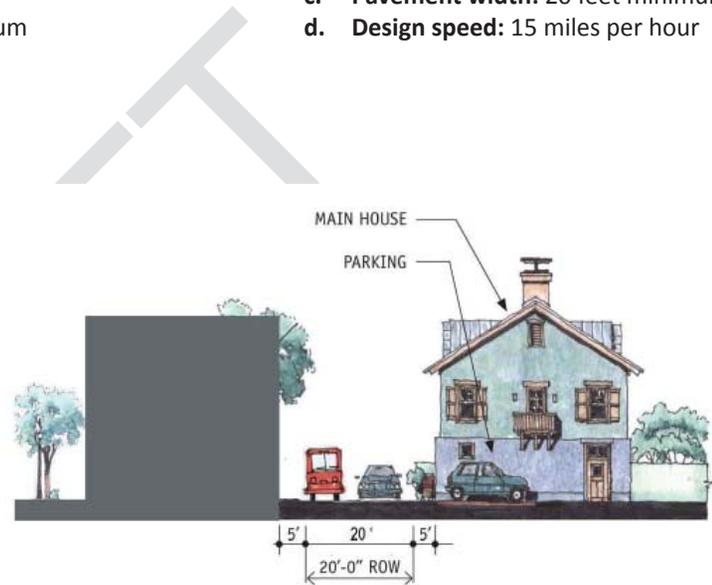


Exhibit 4.22 – Typical Commercial Alley

4.0 Streets and Streetscape Standards



4.8. Street Tree Standards

- 4.8.1. **Purpose/Intent:** Street trees are an amenity first for pedestrian comfort and enjoyment, next for enhancement of abutting properties, and finally for passing motorists, cyclists, or transit riders. Street trees shall be chosen and incorporated to serve the following intents:
 - (i) Enclose or frame the space of the street with a canopy.
 - (ii) Provide shade.
 - (iii) Provide a buffer between traffic and pedestrian to enhance the feeling of safety for the pedestrian.
 - (iv) Provide an aesthetic accompaniment to nearby architecture.
 - (v) Reduce the heat island effect created by paved surfaces.
 - (vi) Aid in storm water management through transpiration.
 - (vii) Work within their context (i.e. region- and climate-appropriate, native species that are disease resistant and drought-tolerant).
- 4.8.2. Street trees shall be required on all 'A' Streets.
- 4.8.3. Street tree location and selection shall be coordinated with the Planning Director or his/her designee.
- 4.8.4. In order to ensure that street trees are appropriate for the region and climate and are not an invasive species, street trees shall be selected from the Native Plant List A in **Section 11.1.1** of this Plan. Low-water xeric species may be selected with approval from the Planning Director or his/her designee. Street trees should be disease resistant and drought tolerant.

- 4.8.5. Where available, street trees shall be planted within the tree strip. Otherwise, street trees shall be planted between the sidewalk and the street curb. Street trees may be planted within the furnishing zone on 'A' Streets.
- 4.8.6. All street trees shall be maintained by property owner, per the Street Tree Ordinance, Section 8-5-1, City of Albuquerque Revised Ordinances (1974).
- 4.8.7. Spacing shall be an average of 30 feet on center (measured per block face) along all streets. Spacing may be adjusted as appropriate to accommodate optimum root growth for native species. On narrow streets, spacing may vary on each side of the street.
- 4.8.8. Each planting area shall be no less than 36 square feet (SF). The tree well shall be no less than 25 SF. On 'A' Streets, trees shall be in a grated or permeable planting square a minimum of 4 feet wide. Metal tree grates are preferred for 'A' Streets.
- 4.8.9. The minimum caliper size for each tree shall be 3 inches and shall be a minimum of X feet [pending] in height at planting. Caliper size for a multi-trunk tree shall be the total of the diameter of the largest trunk and half (1/2) the diameter of each additional trunk, measured at a height of 4 1/2 feet above the ground.
- 4.8.10. Minimum height of base of canopy should be 8 feet, at time of maturity, for vertical clearance of pedestrians and vehicles. On 'A' Streets, the base of the canopy should be a minimum of 10 feet so as to not obscure windows and signage.

4.0 Streets and Streetscape Standards

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- 4.8.11. Street trees should be a minimum box size of 36 inches.
- 4.9. Streetscape Standards**
 - 4.9.1. **Groundcover:** When clearly visible from the street and alleys, all unpaved ground areas shall be planted with low growing shrubs or ground cover, ornamental grasses, or a combination thereof selected from Plant List A or B in **Section 11.1** of this Plan. Turf grass must be installed as solid sod, not seed.
 - 4.9.2. **Streetscape Maintenance:** Maintenance of all landscape materials shall be the responsibility of the abutting property owner and shall meet the requirements of Chapter 6, Article 6 in the City Zoning Code.
 - 4.9.3. **Sidewalk Design:** Sidewalks in the public right-of-way shall be designed to facilitate pedestrian accessibility and efficient travel. Curvilinear sidewalks are strongly discouraged. Where possible, planting strips should be used to buffer pedestrians from vehicle traffic and set the pedestrian path back from driveway ramps.
 - 4.9.4. **Sidewalk Locations:** The location of sidewalks shall reflect the desired character and density of the surrounding land uses. In Center and Mixed-Use Zones, sidewalks are essential to creating a vibrant, pedestrian lifestyle.
 - 4.9.5. **Grading:** Bikeways and other amenities within the right-of-way shall be provided in such a way as to minimize the extent of disturbance to slopes and vegetation and the need for cut and fill.

4.10. Street Lighting Standards

4.10.1. Purpose/Intent:

- (i) Outdoor lighting should create and encourage a pedestrian-friendly environment, which is especially beneficial to residential neighborhoods and neighborhood business districts. Pedestrian-scale lights should improve walkway illumination for pedestrian traffic and enhance community safety and business exposure.
- (ii) Street lighting should be designed to be appropriate to location and context. Lamp post height, lamp head, lighting source, and spacing should all be calibrated. The light standard selected should be compatible with the design of the street and dominant architectural style of adjacent buildings

- 4.10.2. Pedestrian-scale lighting, with the top of fixture being no more than 20 feet from the ground, shall be provided along all streets except alleyways.
- 4.10.3. All new alleys shall have lights mounted on outbuildings or garages. These lights should be connected to a circuit separate from the main building.
- 4.10.4. Street lights shall be placed at 30 feet on center and in-line with street trees. Street trees and light poles shall be alternated along the street.
- 4.10.5. All street lighting shall be “full-cutoff” or “fully shielded” to minimize light pollution and save energy, per the New Mexico Night Sky Ordinance [74-12-1 to 74-12-10 NMSA 1978].

4.0 Streets and Streetscape Standards



- 4.10.6. Lamp post height should be designed to be proportional to the width of the street. Street lamps shall be 12 to 15 feet high along 'A' Streets. 'B' Streets shall incorporate 15-foot lampposts. See **Section 10.7** for parking area lighting requirements.
- 4.10.7. For those intersections that require more light, the 20-foot lamppost can be instituted for safety, but should be used only if necessary.
- 4.10.8. The minimum clearance from a street light pole to the face of curb shall be 2 feet. The minimum clearance from a street light pole to the edge of a sidewalk shall be 1 foot. The minimum clearance from a street light pole to the centerline of a street shall be 20 feet.
- 4.10.9. Light posts shall be placed within the tree planting strip where available.
- 4.10.10. All lamp posts shall have a base, middle and top.
- 4.10.11. Shoe box style lighting shall not be used, except in large parking areas. Metal halide lights are encouraged. High pressure sodium lights are discouraged since they visually render all colors the same.
- 4.10.12. Cobraheads shall only be used on high traffic volume streets. A supplemental non-cobrahead light shall be mounted between 12 to 14 feet to light sidewalks, pedestrian paths, or multi-use trails.
- 4.10.13. Column streetlights shall be used on 'A' Streets.

4.10.14. Multi-head column streetlights shall be used on 'A' streets, Unser Boulevard, and Paseo del Norte.

4.11. Street Furniture and Materials Standards

- 4.11.1. **Purpose/Intent:**
 - (i) Street furniture should create and encourage a pedestrian-friendly environment, which is especially beneficial to residential neighborhoods and neighborhood business districts.
 - (ii) Street furniture should be designed to be appropriate to location and context. Street furniture should be compatible with the design of the street and dominant architectural style of adjacent buildings.
 - (iii) Materials for paving and street furniture should be selected based on durability, quality, and minimal maintenance requirements.
- 4.11.2. Street furniture and streetscape amenities are the responsibility of the property owner and/or property owner association or merchants association. The cost for construction, maintenance, and liability shall be borne by private entities.
- 4.11.3. Street furniture shall not be placed within the Public ROW without the approval of the relevant City agency.
- 4.11.4. Trash receptacles and bike racks shall be required along 'A' Streets. A minimum of one each per block face shall be required.

4.0 Streets and Streetscape Standards

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- 4.11.5. Where provided, street furniture and pedestrian amenities such as benches shall be placed to maximize pedestrian access and circulation along 'A' Streets. All street furniture shall be located in such a manner as to allow a clear sidewalk passageway of a minimum of 6 feet.
- 4.11.6. Materials selected for paving and street furniture shall be of durable quality and require minimal maintenance.

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5.0 Zoning

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5.1. Adoption of Volcano Heights Zoning: The Volcano Heights Zoning Map is hereby adopted as the official zoning map. Within any area subject to the approved Zoning Map, this Plan becomes the mandatory regulation.

5.1.1. NWMEP: Because the VHSDP and the NWMEP are both Rank III plans with overlapping boundaries, where a policy or regulation conflicts, the VHSDP shall prevail, unless otherwise stated in the Plan. Where one plan is silent, the policies/regulations of the other plan shall prevail.

5.1.2. Zone Lines and Property Lines: Where zone lines do not correspond to property lines or where they are not divided and defined by a Mandatory Road, they shall begin at or be measured from the nearest Mandatory Road centerline.

(i) Regional Center: Where this zone applies to a portion of the property only, the zone shall begin 400 feet from the centerline of Unser Boulevard or Paseo del Norte, whichever is closer to the property. [confirm]

(ii) Mixed Use: Where this zone applies to a portion of the property only, the zone shall begin 300 feet from the centerline of the nearest Mandatory Road. [confirm]

(iii) Escarpment Transition: Where this zone applies to a portion of the property only, the zone shall begin 300 feet from the centerline of the nearest Mandatory Road. [confirm]

5.2. Establishment of Character Zones

The Zoning Map designates the following Character Zones.

5.2.1. Town Center: The Volcano Heights Town Center (VHTC) Zone is intended to implement the City’s vision for an urban Town Center. Development in this zone is intended to create a major employment center with office, entertainment, urban residential, and supporting retail uses. Development can be a mix of employment centers, destination retail, and entertainment, restaurant, and urban residential uses. Development within the Town Center Zone shall meet the Site Development and Building Design Standards in Section 6.1 of this Plan.



Town Center Character Zone

5.2.2. Regional Center: The Volcano Heights Regional Center (VHRC) Zone is intended to provide an appropriate transition into Volcano Heights from the regional, limited-access Paseo del Norte and Unser Boulevard. This area is also intended for large-format and destination retail and office development. Development within the Regional Center Zone shall meet the Building Form and Development Standards in Section 6.2 of this Plan.

[Graphic Pending]

Regional Center Character Zone

5.2.3. Village Center: Volcano Heights Village Center (VHVC) Zone is intended for smaller-scale neighborhood oriented retail and office development with gateway elements at key intersections. Development within the Village Center Zone shall meet the Site Development Standards in Section 6.3 of this Plan.



Village Center Character Zone

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Mixed-Use Character Zone

5.2.4. **Mixed-Use:** The Volcano Heights Mixed Use (VHMX) Zone is intended to provide the most flexibility of use and development standards surrounding the Regional and Town Centers. VHMX also serves to buffer Transition zones from the auto-oriented uses of the Regional Center and the more intensive urban uses and taller buildings allowed within the Town Center. Development within the Mixed Use Zone shall meet the Site Development Standards in **Section 6.4** of this Plan.

This zone is intended for single-family residential, live-work, and townhouse uses as well as smaller-scale office, and neighborhood retail in key locations. Development within the Escarpment Transition shall meet the Site Development and Building Design Standards in **Section 6.6** of this Plan.

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Neighborhood Transition Character Zone

5.2.5. **Neighborhood Transition:** The Volcano Heights Neighborhood Transition (VHNT) Zone is intended to provide transitions of building scale and uses from the more dense, urban Mixed Use zone to adjoining neighborhoods north and south of the Plan area.

5.3. Table of Land Uses: Due to the emphasis on urban form over land uses, general use categories have been identified by Character Zone in **Table 5.1**. Uses not listed but substantially similar may be permitted upon approval of the Planning Director or his/her designee.

This zone is intended for single-family residential, live-work, and townhouse uses as well as smaller-scale office, and neighborhood retail in key locations, particularly intersections. Development within the Transition Zone shall meet the Site Development Standards in **Section 6.5** of this Plan.

5.4. Criteria for Conditional Uses: All uses listed as Conditional (C) in **Table 5.1** shall also meet the following standards in **Table 5.2**.



Escarpment Transition Character Zone

5.2.6. **Escarpment Transition:** The Volcano Heights Escarpment Transition (VHET) Zone is intended to protect sensitive lands in the eastern portion of the Plan area adjacent to the Petroglyph National Monument from the higher-density and intensity mixed uses. Properties adjacent to or abutting the Monument are subject to more rigorous height and design guidelines in order to comply with the regulations and intention of the “Impact” designation in the Rank III NWMEP that also covers this area.



TABLE 5.1 – LAND USE TABLE BY CHARACTER ZONE

	Character Zone	Town Center	Regional Center	Village Center	Mixed-Use	Neighborhood Transition	Escarpment Transition
	Land Use						
	Commercial Uses (Office, Retail, Sales and Service Uses)						
CU-1	Retail Sales or Service with no drive through facility (includes alcohol sales). <i>Excluded from this category are retail sales and service establishments geared toward the automobile.</i>	P	P	P	P	C	C
CU-2	Auto-related Sales or Service establishments	C	P	C	C	NP	NP
CU-3	Finance and Real Estate establishments including banks, credit unions, real estate, and property management services, with no drive through facility	P	P	P	P	NP	C
CU-4	Offices for business, professional, administrative, and technical services such as, but not limited to, accountants, architects, lawyers, doctors, insurance sales/services , etc.	P	P	P	P	C	C
CU-5	Research laboratory headquarters, laboratories and associated facilities	P	P	P	P	NP	NP
CU-6	Food Service Uses such as full-service restaurants, cafeterias, bakeries and snack bars with no drive-through facilities*	P	P	P	P	C	C
CU-7	Pet and animal sales or service (including vet clinic)	P	P	P	P	NP	C
CU-8	Any permitted use with a drive through facility	C	P	C	C	NP	NP

P= Permitted by right NP= Not Permitted C = Permitted with Specific Criteria P/A = Permitted Accessory Use NA= Not applicable as established in **Table 5.2**

* Included in this category is café seating within a public or private sidewalk area with no obstruction of pedestrian circulation. Also included in this category is the sale of alcoholic beverages.

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TABLE 5.1 – LAND USE TABLE BY CHARACTER ZONE (Cont'd)

	Character Zone	Town Center	Regional Center	Village Center	Mixed-Use	Neighborhood Transition	Escarpment Transition
	Land Use						
	Arts, Entertainment, and Recreation Uses						
AU-1	Art galleries	P	P	P	P	C	C
AU-2	Art, antique, furniture or electronics studio (retail, repair or fabrication; excludes auto electronics sales or service)	P	P	P	P	C	C
AU-3	Games establishments (includes arcades)	P	P	P	P	NP	NP
AU-4	Theater, cinema, dance, or music establishment	P	P	P	P	NP	NP
AU-5	Museums and other special purpose recreational institutions	P	P	P	P	NP	NP
AU-6	Adult-oriented Businesses	NP	P	NP	NP	NP	NP
AU-7	Fitness, recreational sports, gym, or athletic club	P	P	P	P	C	C
AU-8	Parks, greens, plazas, squares, and playgrounds	P	P	P	P	P	P

P= Permitted by right NP= Not Permitted C = Permitted with Specific Criteria as established in **Table 5.2** P/A = Permitted Accessory Use NA= Not applicable

A* = Accessory use to not exceed 25% of the primary use building square footage

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TABLE 5.1 – LAND USE TABLE BY CHARACTER ZONE (Cont'd)

	Character Zone	Town Center	Regional Center	Village Center	Mixed-Use	Neighborhood Transition	Escarpment Transition
	Land Use						
	Educational, Public Administration, Health Care and Other Institutional Uses						
EU-1	Business associations, professional membership organizations, social & fraternal organizations	P	P	P	P	NP	NP
EU-2	Child day care and preschools	P	P	P	P	P	P
EU-3	Schools, libraries, and community halls	P	P	P	P	P	P
EU-4	Universities and Colleges	P	P	P	P	NP	NP
EU-5	Technical, trade, and specialty schools	P	P	P	P	NP	NP
EU-6	Hospitals and nursing establishments	P	P	P	P	NP	NP
EU-7	Civic uses	P	P	P	P	P	P
EU-8	Social services and philanthropic organizations	P	P	P	P	C	C
EU-9	Rehabilitative Clinics	P	P	P	P	NP	NP
EU-10	Public administration uses (including local, state, and federal government uses, public safety, health and human services)	P	P	P	P	C	C
EU-11	Religious Institutions	P	P	P	P	C	C
EU-12	Funeral homes	C	P	P	P	NP	NP

P= Permitted by right NP= Not Permitted C = Permitted with Specific Criteria P/A = Permitted Accessory Use NA= Not applicable as established in **Table 5.2**

A* = Accessory use to not exceed 25% of the primary use building square footage

CHAPTER II: REGULATIONS

5.0 Zoning

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TABLE 5.1 – LAND USE TABLE BY CHARACTER ZONE (Cont'd)

	Character Zone	Town Center	Regional Center	Village Center	Mixed-Use	Neighborhood Transition	Escarpment Transition
	Land Use						
	Manufacturing, Transportation, Communication, and Utility Uses						
MU-1	Food and textile product manufacturing	C	P	NP	P	NP	NP
MU-2	Wood, paper, and printing products manufacturing	NP	P	NP	NP	NP	NP
MU-3	Machinery, electronics, and transportation equipment manufacturing	NP	P	NP	P	NP	NP
MU-4	Miscellaneous manufacturing (included in this category are jewelry, silverware, personal metal goods, flatware, dolls, toys, games, musical instruments, office supplies, and signs.)	NP	P	NP	C	NP	NP
MU-5	Wholesale trade establishment	NP	P	NP	P	NP	NP
MU-6	Warehouse and storage services	NP	P	NP	P	NP	NP
MU-7	Transportation services (air, rail, road, truck and freight)	NP	P	NP	NP	NP	NP
MU-8	Publishing (newspaper, books, periodicals, software)	P	P	P	P	NP	NP
MU-9	Motion picture and sound recording	P	P	P	P	NP	C
MU-10	Telecommunications and broadcasting (radio, TV, cable, wireless communications, telephone, etc)	P	P	P	P	NP	NP
MU-11	Information services and data processing	P	P	P	P	NP	NP
MU-12	Utilities and utility services (electric, natural gas, alternative)	NP	P	NP	P	NP	NP

P= Permitted by right NP= Not Permitted C = Permitted with Specific Criteria P/A = Permitted Accessory Use NA= Not applicable
as established in **Table 5.2**

A* = Accessory use to not exceed 25% of the primary use building square footage

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TABLE 5.1 – LAND USE TABLE BY CHARACTER ZONE (Cont'd)

	Character Zone	Town Center	Regional Center	Village Center	Mixed-Use	Neighborhood Transition	Escarpment Transition	
	Land Use							
	Residential Uses							
RU-1	Home Occupations	P	P	P	P	P	P	
RU-2	Multi-family residential							
RU-2 G		Ground floor	C	P	C	P	C	P
RU-2-U		Upper floors	P	P	P	P	C	P
RU-3	Residential Lofts	P	P	P	P	P	P	
RU-4	Single-family residential attached dwelling unit (Townhomes)	P	NP	P	P	P	P	
RU-5	Single-family residential detached	NP	NP	NP	NP	P	P	
RU-6	Secondary accessory dwelling unit	NP	NP	P	P	P	P	
RU-7	Live-work unit	P	NP	P	P	P	P	
RU-8	Senior housing facility	C	C	P	P	C	C	

P= Permitted by right NP= Not Permitted C = Permitted with Specific Criteria P/A = Permitted Accessory Use NA= Not applicable as established in **Table 5.2**

A* = Accessory use to not exceed 25% of the primary use building square footage

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TABLE 5.1 - LAND USE TABLE BY CHARACTER ZONE (Cont'd)

	Character Zone	Town Center	Regional Center	Village Center	Mixed-Use	Neighborhood Transition	Escarpment Transition
	Land Use						
	Other Uses						
OU-1	Model homes for sales and promotion**	P	P	P	P	P	P
OU-2	Hotels	P	P	P	P	NP	NP
OU-3	Bed and Breakfast	NP	NP	NP	P	P	P
OU-4	Commercial surface parking lot (primary use of property)	C	C	C	C	NP	NP
OU-5	Parking, structured	P	P	P	P	NP	NP
OU-6	Private attached garage	NP	NP	NP	P	P	P
OU-7	Private detached garage	NP	NP	NP	P	P	P
OU-8	Sales from kiosks (includes food carts)	C	C	C	C	NP	C
OU-9	Veterinary clinic	P	P	P	P	NP	NP
OU-10	Community garden	C	C	C	P	P	P
OU-11	Antennas including cell, accessory, and mounted on top of buildings. <i>(Excluded from this category are freestanding and commercial antennas and equipment buildings)</i>	P	P	P	P	P	P
OU-12	Wind energy equipment	P/A	P/A	P/A	P/A	P/A/C	P/A/C
OU-13	Solar energy equipment	P/A	P/A	P/A	P/A	P/A	P/A
OU-14	Special Event	P	P	P	P	C	C

P= Permitted by right NP= Not Permitted C = Permitted with Specific Criteria P/A = Permitted Accessory Use NA= Not applicable as established in Table 5.2

A* = Accessory use to not exceed 25% of the primary use building square footage

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TABLE 5.2 – CONDITIONAL USE CRITERIA

Use	Zone	Location & Design Criteria
Non-Residential Uses		
CU-1	Retail Sales or Service with no drive-through facility (includes alcohol sales)	Transition Zones <ul style="list-style-type: none"> • Building size limited to 5,000 net square feet • Character complements nearby single-family development • Scale and architectural style complementary to nearby single-family development • Location restricted to any corner of the intersection of two streets • Hour of operation limited to 7 a.m. - 10 p.m. • Deliveries restricted to between 6 a.m. - 11 p.m. • Surface parking lot restricted to no more than 10 spaces • Any buildings associated with the use shall also have a pedestrian entrance at an 'A' Street
CU-2	Auto-related Sales and Service	Town Center and Village Center <ul style="list-style-type: none"> • Gas pumps, canopies, and/or service bays shall not be located along any 'A' Street frontage. • No more than 50% of a lot's frontage along a Mandatory Street shall be occupied by gas pumps, canopies, and/or service bays. • Any buildings associated with the use shall also have a pedestrian entrance at an 'A' Street. • No outdoor storage of vehicles or other products sold shall be permitted along 'A' Streets or Mandatory Streets. All auto-related sales displays shall be inside storefronts or along 'B' Streets.
		Mixed Use <ul style="list-style-type: none"> • Gas pumps, canopies, and/or service bays shall not be located along any 'A' Street frontage. • No more than 50% of a lot's frontage along a Mandatory Street shall be occupied by gas pumps, canopies, and/or service bays. • Any buildings associated with the use shall also have a pedestrian entrance at an 'A' Street. • Outdoor storage of vehicles or other products sold shall not be permitted along 'A' Streets. Along Paseo del Norte or Unser Boulevard, outdoor storage of vehicles or other products sold shall not exceed 50% of a lot's frontage. There shall be no such limitation along non-mandatory 'B' Streets or alleys.
CU-3	Finance and Real Estate establishments	Escarpment Transition <ul style="list-style-type: none"> • Building size limited to 5,000 net square feet • Character complements nearby single-family development • Scale and architectural style complementary to nearby single-family development • Any buildings associated with the use shall also have a pedestrian entrance at an 'A' Street



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TABLE 5.2 – CONDITIONAL USE CRITERIA (Cont'd)

	Use	Zone	Location & Design Criteria
CU-4	Offices	Transition Zones	<ul style="list-style-type: none"> • Building size limited to 5,000 net square feet • Character complements nearby single-family development • Scale and architectural style complementary to nearby single-family development • Any buildings associated with the use shall also have a pedestrian entrance at an 'A' Street
CU-6	Food Service Uses	Transition Zones	<ul style="list-style-type: none"> • Building size limited to 5,000 net square feet • Character complements nearby single-family development • Scale and architectural style complementary to nearby single-family development • Location restricted to any corner of the intersection of two streets • Hour of operation limited to 7 a.m. - 10 p.m. • Deliveries restricted to between 6 a.m. - 11 p.m. • Off-street parking lots required but restricted to no more than 25 spaces • Any buildings associated with the use shall also have a pedestrian entrance at an 'A' Street
CU-7	Pet and animal sales or service	Escarpment Transition	<ul style="list-style-type: none"> • Building size limited to 5,000 net square feet • Character complements nearby single-family development • Scale and architectural style complementary to nearby single-family development • Any buildings associated with the use shall also have a pedestrian entrance at an 'A' Street • Any overnight pet boarding shall be indoors
CU-8	Any permitted use with a drive-through facility	Town Center, Village Center, Mixed Use	<ul style="list-style-type: none"> • All drive-through access (driveways) shall be from a 'B' Street only. If the lot has no access to any 'B' Street, then access may be from an 'A' Street with approval by the Planning Director or his/her designee. • Drive through lanes and/or canopies shall not have frontage or be located along any 'A' Street. • Drive through areas shall be screened by a 4-foot high Street Screen along 'B' Streets. • Drive through facilities shall meet the Building Design Standards in Section 8 of this code.
AU-1	Art galleries	Transitions Zones	<ul style="list-style-type: none"> • Building size limited to 5,000 net square feet • Character complements nearby single-family development • Scale and architectural style complementary to nearby single-family development • Any buildings associated with the use shall also have a pedestrian entrance at an 'A' Street • Off-street parking lots restricted to no more than 10 spaces
AU-2	Art, antique, furniture or electronics studio	Transition Zones	<ul style="list-style-type: none"> • Building size limited to 5,000 net square feet • Character complements nearby single-family development • Scale and architectural style complementary to nearby single-family development

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TABLE 5.2 – CONDITIONAL USE CRITERIA (Cont'd)

Use	Zone	Location & Design Criteria
AU-6 Fitness, recreational sports, gym, or athletic club	Transition Zones	<ul style="list-style-type: none"> • Size limited to small end of what's typical • Off-street parking lot required but restricted to small end of what's typical • Character complements nearby single-family development • Scale and architectural style complementary to nearby single-family development • Any buildings associated with the use shall also have a pedestrian entrance at an 'A' Street
EU-8 Social services and philanthropic organizations	Transition Zones	<ul style="list-style-type: none"> • Building size limited to 5,000 net square feet • Character complements nearby single-family development • Scale and architectural style complementary to nearby single-family development • Any buildings associated with the use shall also have a pedestrian entrance at an 'A' Street
EU-10 Public administration uses	Transition Zones	<ul style="list-style-type: none"> • Character complements nearby single-family development • Scale and architectural style complementary to nearby single-family development
EU-11 Religious Institutions	Transition Zones	<ul style="list-style-type: none"> • Size limited to small end of what's typical • Off-street parking required but limited to small end of what's typical • Character complements nearby single-family development • Scale and architectural style complementary to nearby single-family development • Any buildings associated with the use shall also have a pedestrian entrance at an 'A' Street
EU-12 Funeral homes	Town Center	<ul style="list-style-type: none"> • Size limited to small end of what's typical (French's at University, Barelax) • Off-street parking required but limited to small end of what's typical • Character complements nearby single-family development • Scale and architectural style complementary to nearby single-family development • Any buildings associated with the use shall also have a pedestrian entrance at an 'A' Street

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5.0 Zoning

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TABLE 5.2 – CONDITIONAL USE CRITERIA (Cont'd)

	Use	Zone	Location & Design Criteria
MU-1	Food and textile product manufacturing	Town Center	<ul style="list-style-type: none"> • Scale and architectural style complementary to walkable, urban built environment • Off-street parking required but may be provided off-site • Any buildings associated with the use shall also have a service entrance on an 'B' Street for deliveries and maintenance equipment
MU-4	Miscellaneous manufacturing	Mixed-Use	<ul style="list-style-type: none"> • Scale and architectural style complementary to walkable, urban built environment • Off-street parking required but may be provided off-site • Any buildings associated with the use shall also have a service entrance on an 'B' Street for deliveries and maintenance equipment • Activities shall be buffered appropriately to mitigate noise and emission impact on neighboring properties.
MU-9	Motion picture and sound recording	Escarpment Transition	<ul style="list-style-type: none"> • Building size limited to 5,000 net square feet • Character complements nearby single-family development • Scale and architectural style complementary to nearby single-family development • Activities shall be buffered appropriately to mitigate noise impact on neighboring properties.
Residential Uses			
RU-2-G	Multi-family residential (Ground Floor)	Town Center, Village Center, and Neighborhood Transition	<ul style="list-style-type: none"> • Residential uses are permitted on the ground floors of building with all other street and interior driveway frontages. Where possible, along 'A' Streets ground floor of Multi-family buildings should be a commercial use. • All ground floors of building with commercial uses along 'A' Street frontage shall be built to Commercial Ready standards. • Off-street parking required as per City Zoning Code Section XX-X-XX
RU-2-U	Multi-family residential (Upper floors)	Neighborhood Transition	<ul style="list-style-type: none"> • Character complements nearby single-family development • Scale and architectural style complementary to nearby single-family development • Upper stories shall be set back from the first floor a minimum of 12 feet. • Off-street parking required as per City Zoning Code Section XX-X-XX
RU-8	Senior housing facility	Town Center, Regional Center, and Transition Zones	<ul style="list-style-type: none"> • Shall be designed as a multi-family building, and any service entrances shall be along a 'B' Street or alley. • Any buildings associated with the use shall also have a pedestrian entrance at an 'A' Street. • Off-street parking required as per City Zoning Code Section XX-X-XX. • [Pending]

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TABLE 5.2 – CONDITIONAL USE CRITERIA (Cont’d)

	Use	Zone	Location & Design Criteria
	Other Uses		
OU-4	Commercial surface parking lot (primary use of property)	Town Center, Regional Center, Village Center, and Mixed Use	<ul style="list-style-type: none"> • Shall be permitted as an interim use of property (permits provided in 3-year increments). • Applications for new surface lots shall include phasing of infill building concepts on the lot. • New surface parking shall be set back a minimum of 30 feet from the edge of the right-of-way of ‘A’ Streets. • New surface parking shall not be located at a street intersection for a minimum of 30 feet along each street.
OU-10	Sales from Kiosks	Center Zones, Mixed Use, and Escarpment Transition	<ul style="list-style-type: none"> • Permits required for food? Other things? • Hours of operation? • Clean-up? • Deliveries? • [See existing food cart language] • [Language pending]
OU-12	Community Garden	Center Zones	<ul style="list-style-type: none"> • Shall be no larger than 1.0 acre. • Gardens shall be enclosed by a fence on all open sides. • Fences should be installed straight and plumb, with vertical supports at a minimum of 8 feet on center. • Chicken wire is permitted if used with another permitted fencing material. Where used, chicken wire shall be continuously supported along all edges. <p><i>Fencing Materials:</i></p> <ul style="list-style-type: none"> • Permitted: wood, chicken wire, wrought iron, painted galvanized steel • Not permitted: chain link, barbed or razor wire, vinyl, plywood
OU-14	Wind energy equipment	Escarpment Transition	<ul style="list-style-type: none"> • [Language pending]
OU-16	Special Event	Transition Zones	<ul style="list-style-type: none"> • [Language pending]



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6.0 Site Development and Building Design Standards Specific to Zones

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6.0 Site Development and Building Design Standards



6.1. Town Center Zone

6.1.1. Illustrations and Intent

Note: These are provided as illustrations of intent. The illustrations and statements on this page are advisory only and do not have the power of law. Refer to the standards on the following pages for the specific Building Form and Development Standards.

The Town Center Zone Site Development Standards are intended to address development along a proposed Bus Rapid Transit (BRT) corridor near the center of the Volcano Heights Plan area. Development standards in this character zone are intended to take advantage of its location along a transit corridor and promote a dense, urban, pedestrian-friendly development pattern accessible via all transportation modes.

With the smallest block sizes of all the zones and the tallest potential structure heights, this character zone generally accommodates development of neighborhood-serving and destination retail, employment centers, urban residential and entertainment uses. The goal is to encourage transit-oriented development within a quarter-mile radius of the proposed Transit Corridor and eventual Transit Center.

In addition, Town Center development sites shall be planned in such a manner as to accentuate the intersections with taller buildings that are closer to the street. The required site development pattern of building pads, parking, driveways, and service areas is intended to provide opportunities for short-term development while allowing for planned urban block infill development in the long-term.



6.0 Site Development Standards SU-2 VHTC

6.1.2 Building Placement	(i) Build-To-Zone (BTZ)	Notes		
<p>Legend</p> <ul style="list-style-type: none"> Property Line (dashed line) Setback Line (dotted line) Building Area (yellow) Landscape Zone (green) Build-to Zone (hatched) Sidewalk (grey) 	<p>Front – ‘A’ Street and Civic Space 0 – 5 feet (see #1) A</p> <p>Front - ‘B’ Streets 0 – 75 feet B</p>	<p>#1 – Area between the building and the edge of the BTZ at the sidewalk shall be paved flush with the public sidewalk.</p> <p>#2 – Side and rear setbacks shall be based on minimum fire separation required between buildings, if applicable.</p> <p>#3 – Corner building street facades shall be built to the BTZ for a minimum of 30 feet from the corner along both streets or the width of the corner lot, whichever is less. Recessed entrances are permitted as long as the upper floors meet the BTZ standards.</p> <p>#4 – Floor to floor heights shall not apply to parking structures.</p> <p>#5 – Attics and mezzanines less than 7 feet (avg.) in height shall not be counted as a story.</p> <p>#6 – Any frontage along all ‘A’ Streets and Civic Space not defined by a building at the BTZ shall be defined by a 4-foot high Street Screen. Furthermore, service areas along all streets and alleys shall be defined by a Street Screen that is at least as high as the service equipment being screened. Required Street Screens shall be of either the same building material as the principal structure on the lot or masonry or a living screen composed of shrubs planted to be opaque at maturity. Species shall be selected from the Plant List in Section 11.1 of this Plan. The required Street Screen shall be located at the setback line along the corresponding frontage.</p> <p>#7 – Corner buildings may exceed the maximum building height by 15% for 20% of the building’s frontage along each corresponding street façade.</p>		
	(ii) Setback			
	Front (‘A’ Street and Civic Space) 5 feet (min.) I		<p>Front (‘B’ Streets) 10 feet (min.) J</p> <p>Side or Rear (distance from property line) 0 feet (see #2 and 6.1.7(iii)) C D</p>	
	(iii) Building Frontage Required			
	% of building built to ‘A’ Street/Civic Space BTZ 80% (min.) (see #3 and #6) E		<p>% of building built to ‘B’ Street BTZ 30% (min.) (see #3 and #6) F</p>	
	6.1.3 Block Standards (for development/redevelopment greater than 50% of the block)			
	Block Face Dimensions 300 feet (min.) 500 feet (max.)		<p>Block Perimeter 1600 feet (max.)</p>	
	6.1.4 Height Standards			
			Principal Building Standards [rename]	
			Maximum structure height 3 stories or 40 feet (max.) (See also Section 7.2) K	<p>First floor-to-floor height 15 feet (min.) (see #4) M</p> <p>Ground Floor finish level 12 inches max. above sidewalk (for ground floor Commercial Ready buildings) L</p> <p>Upper floor-to-floor height 10 feet (min.) (see #4) N</p>
6.1.5 Frontage Requirements				
<p>Ground floors of all buildings fronting on Mandatory ‘A’ Streets shall be built to Commercial Ready standards including first floor-to-floor height, ingress and egress, handicap access, and first floor elevation flush with the sidewalk.</p>				

6.0 Site Development Standards

<p>6.1.6 Parking and Service Access</p>	<p>(i) Parking Location</p>	<p>6.1.7 Other Standards</p>																					
	<table border="1"> <tr> <th colspan="2">Surface/At Grade Parking</th> <th></th> </tr> <tr> <td>'A' Street/ Civic Space setback</td> <td>Shall be located behind the principal building</td> <td>O</td> </tr> <tr> <td>'B' Street setback</td> <td>Min. of 3 feet behind building facade line along that street</td> <td>Q</td> </tr> <tr> <td>Side and Rear setback (<i>distance from property line</i>)</td> <td>0 feet min. (see # 2 and 6.1.7(iii))</td> <td>P R</td> </tr> <tr> <th colspan="2">Above Grade Parking</th> <th></th> </tr> <tr> <td>Setback along 'A' & 'B' Streets or Civic Space</td> <td>May be built up to the building façade line along that street</td> <td>O*</td> </tr> <tr> <td>Side and Rear setbacks (<i>distance from property line</i>)</td> <td>0 feet min. (see # 2 and 6.1.7(iii))</td> <td>P</td> </tr> </table>	Surface/At Grade Parking			'A' Street/ Civic Space setback	Shall be located behind the principal building	O	'B' Street setback	Min. of 3 feet behind building facade line along that street	Q	Side and Rear setback (<i>distance from property line</i>)	0 feet min. (see # 2 and 6.1.7(iii))	P R	Above Grade Parking			Setback along 'A' & 'B' Streets or Civic Space	May be built up to the building façade line along that street	O*	Side and Rear setbacks (<i>distance from property line</i>)	0 feet min. (see # 2 and 6.1.7(iii))	P	<p>(i) Encroachments</p> <ol style="list-style-type: none"> Canopies, signs, awnings and balconies may encroach over the sidewalk as long as the vertical clearance is a minimum of 8 feet. In no case shall an encroachment be located over an on-street parking or travel lane. Building projections on all other facades may not be closer than 5 feet to any adjacent property line. <p>(ii) Arcades and Colonnades</p> <ol style="list-style-type: none"> Arcades and Colonnades are permitted within the build-to-zone and shall be a minimum of 6 feet in depth. The minimum interior clearance height within an arcade or colonnade shall be 12 feet <p>(iii) Buffers for Detached Single-family Housing</p> <ol style="list-style-type: none"> A Buffer for detached single-family housing shall be established on all development sites adjacent to single family sites at 25 feet parallel to any lot line that is common with a single-family residential lot. Building height within this Buffer shall not exceed 2 floors or 26 feet and is not eligible for height bonus. This standard shall apply to any parking structures located within the Buffer. A privacy fence (masonry or opaque vegetation) 6 feet high shall be required when abutting a single-family residential lot and shall be optional for all other adjacencies.
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<p>Legend</p> <ul style="list-style-type: none"> Property Line (dashed line) Building Footprint (yellow) Surface Parking Area (solid brown) Above Grade Parking Area (diagonal hatching) 	<p>(ii) Required Off-Street Parking Spaces</p> <table border="1"> <tr> <td>Non-residential uses within 250 feet of single-family residential</td> <td>1 space per every 300 gross square feet</td> </tr> <tr> <td>Residential Uses</td> <td>1 space/unit minimum</td> </tr> </table>	Non-residential uses within 250 feet of single-family residential	1 space per every 300 gross square feet	Residential Uses	1 space/unit minimum	<p>See also Section 10.7.1.</p>																	
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<p>(i) Doors and Windows:</p> <ol style="list-style-type: none"> There shall be no blank walls greater than 25 feet in width of any building along 'A' Streets, Mandatory 'B' Streets, and Civic Spaces. For non-mandatory 'B' Streets there is no requirement. Doors and windows on ground floor of all buildings shall be between 50% and 90% of the ground floor façade area along all 'A' Streets and Civic Spaces. Along 'B' Streets there is no requirement. Windows on upper floor facades along all 'A' Streets and Civic Spaces shall be a minimum of 25% of each upper floor façade area, which is measured between 3 feet and 9 feet above each finished floor. Primary entrance doors for all buildings shall be on 'A' Streets or Civic Spaces. Windows and doors on facades directly facing toward a lot line shared by any single-family residential lot and less than 10 feet from the lot line shall have sills higher than 6 feet on the ground floor unless a privacy fence (masonry or vegetation) or wall of at least 6 feet in height is constructed to obscure any direct views into adjacent properties. 	<p>Parking driveway width</p> <p>City DPM standards on Arterial Roadways and 25 feet max. on all other streets</p> <ul style="list-style-type: none"> Driveways and off-street loading and unloading shall not be located on 'A' Streets or Civic Spaces. Porte cocheres may be permitted on 'A' Streets to provide drop-off and valet service. Shared driveways and cross access easements are encouraged between lots to minimize curb cuts. If driveway and/or off-street service loading and unloading access is provided from an 'A' Street, such access shall be deemed as temporary, and cross access easements along the rear of the property shall be required when adjoining properties are undeveloped. 	<p>Property Line</p>																					

6.0 Building Design Standards SU-2 VHTC

Building Design Standards Specific to Town Center Character Zone

The following standards shall be used for new development or redevelopment within the Town Center Zone.

6.1.9. Massing and Façade Composition

- (i) Buildings generally have a rectangular layout scheme with single or multiple components with mostly flat front and square, round, or octagonal corner towers.
- (ii) Articulation along the Base façade shall maintain a prevalent rhythm of 20-30 feet or multiples thereof along all 'A' Streets.
- (iii) This façade rhythm may be expressed by changing materials or color, using design elements such as columns and pilasters, or varying the setback of portions of the building façade.
- (iv) Building façades may be symmetrical or asymmetrical, but the central part of the building shall be expressed with well-balanced façade compositions.
- (v) Courtyards and plazas should be incorporated.
- (vi) Doors or windows shall form regular patterns of openings, some accentuated by balconies.
- (vii) The ground floor should be taller and include heavier massing (i.e. clad in masonry or with a high proportion of fenestration to opaque wall area) than floors above. A portal, arcade, or colonnade may be incorporated and need not be massive if built at the ground floor.
- (viii) Commercial and Mixed-Use building facades shall be designed with a distinct base, middle, and cap and shall maintain the alignment of horizontal elements along the block.
- (ix) Buildings are capped with wide and continuous eaves supported by open rafters and decorative brackets or finished with elaborate parapet walls covered by tile and very shallow eaves.
- (x) Storefronts should be inset with recessed entry or under an arcade or fit into arch openings, covered with canvas awnings.
- (xi) Awnings, blade signs, arcades, galleries, café seating, and balconies should be used along commercial storefronts fronting an 'A' Street or Civic Space to add pedestrian interest.
- (xii) If the residential building is setback less than 10 feet from the front property line, the grade of the slab or first floor elevation shall be elevated at least 18 inches above the grade of the sidewalk. If the residential structure is set back 10 feet or more from the property line and is not elevated above the grade of the sidewalk, a 3-foot high fence shall be provided at the front property line.
- (xiii) Chain link fences and plastic vinyl fences shall not be permitted within the Town Center Character Zone.

6.0 Building Design Standards

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6.1.10. **Building Materials**

- (i) At least 75% of a building's base façade along all 'A' Streets and Civic Spaces shall be composed of stucco using a 3-step process, masonry, or brick.
- (ii) No more than 25% of a building's base façade along 'A' Streets or Civic Spaces shall use other accent materials such as wood, architectural metal panel, split-face concrete block, tile, or pre-cast concrete panels. Exterior Insulating Finishing System (EIFS) shall not be permitted along any 'A' Street or Civic Space façade.
- (iii) In addition to those permitted for the base façade, upper floors may include glass curtain wall, split-face concrete, or cementitious fiber board.
- (iv) All facades along 'B' Streets or alleys shall be of a similar finished quality and color that blend with the front of the building. Building materials for these facades may be any of the primary and accent façade materials listed above.
 - a. EIFS shall be limited to no more than 10% of the upper floor facades along 'B' Streets and alleys.
 - b. Cementitious-fiber clapboard shall be prohibited on mixed-use or residential frontages. On the upper floors of any commercial frontage, no more than 20% of a 'B' Street or alley façade shall be cementitious-fiber clapboard (not sheet), which shall have at least a 50-year warranty.

- (v) Other primary building materials shall be considered on a case-by-case basis by the Volcano Heights Review Team.
- (vi) Roofing materials for pitched roofs visible from any public right-of-way shall be predominantly barrel clay tile, copper, factory finished standing seam metal, slate, synthetic slate, or similar materials.

6.1.11. **Windows**

- (i) Glazing along 'A' Streets shall not be opaque.
- (ii) Glazing shall not have highly reflective coatings or finishes, so that mirror effect is reduced.
- (iii) Windows shall be recessed to give a thick wall impression. Windows may be arched on the ground level and flat top on upper floors. Windows shall be vertically proportioned with multiple panes in both casement and double hung design. Generally separated by a wall from other windows, wall openings shall be punched through the wall rather than grouped with other windows.
- (iv) Ornamental arches of various designs shall be deeper on the ground floor and shallower on upper floors.

6.0 Building Design Standards SU-2 VHTC

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6.1.12. **Architectural Details and Other Elements**

At least two of the following detail elements shall be incorporated to provide visual interest:

- (i) Elaborate detailing around principal openings,
- (ii) Decorative windows, metal railings at balconies,
- (iii) Canvas awnings, Roof towers, Masonry screen products for see-through walls or portions of walls, and/or
- (iv) Tower elements.

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6.0 Site Development Standards

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6.2. Regional Center Zone

6.2.1. Illustrations and Intent

Note: These are provided as illustrations of intent. The illustrations and statements on this page are advisory only and do not have the power of law. Refer to the standards on the following pages for the specific Site Development Standards.

The Regional Center Zone site development standards are intended to address development along Paseo del Norte and Unser Boulevard in the central portion of the Volcano Heights Plan area. Development standards in this character zone are intended to acknowledge the primary regional traffic function of these roads while also capitalizing on the commercial and retail opportunities for auto-oriented development provided by the significant visibility and high-traffic flow of these regional arteries.

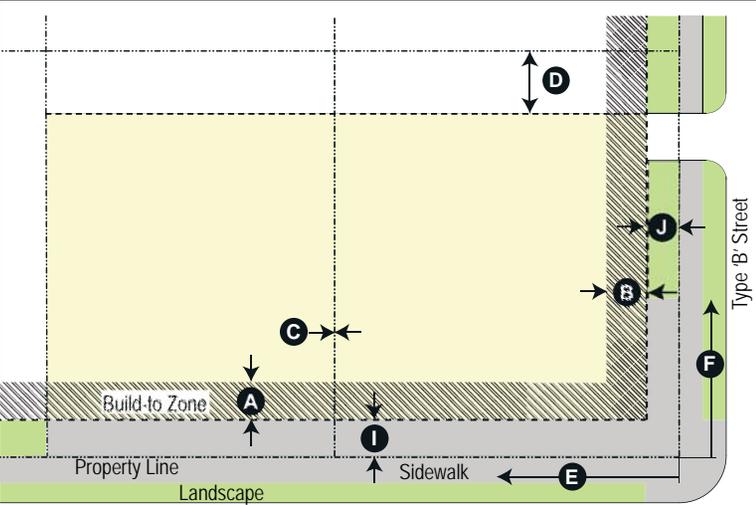
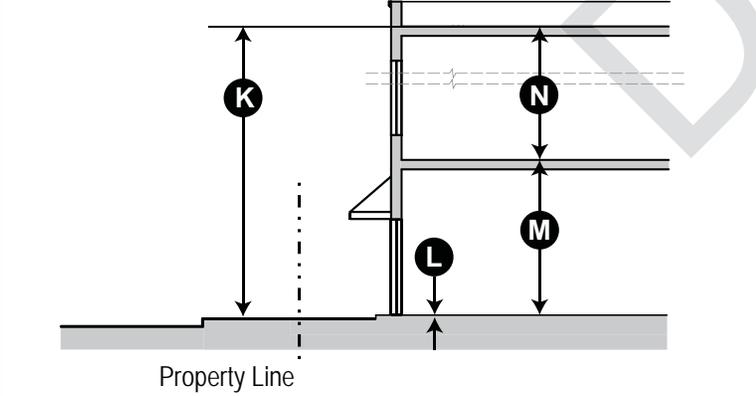
Generally, this character zone allows for a wide range of retail, office, service, and light industrial uses more dependent on the automobile. The goal is to encourage employment and destination retail that serves a broader, regional area as well as new residents and existing neighbors.

In addition, Regional Center Zone sites shall be planned in such a manner as to encourage buildings to be closer to the street at intersections in order to provide attractive, quality development along corridors in the short-term, as well as leaving room for urban block infill as the area becomes more dense and/or redevelops over time.

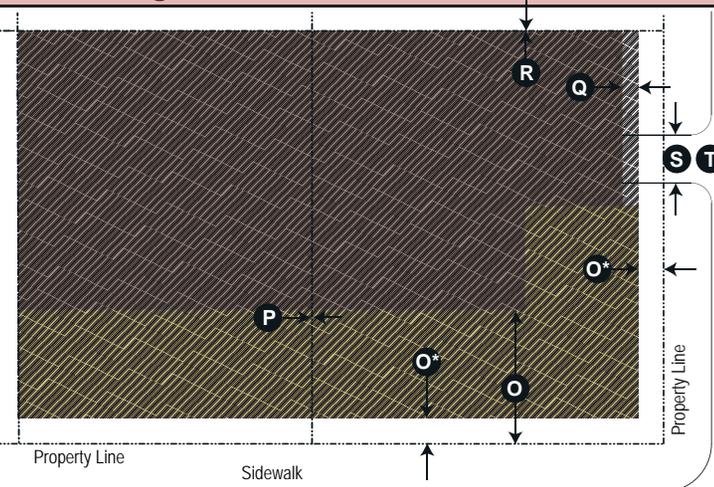
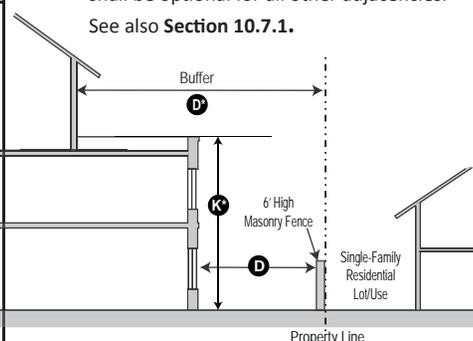


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6.0 Site Development Standards SU-2 VHRC

6.2.2 Building Placement	(i) Build-To-Zone (BTZ)	Notes	
 <p>Legend</p> <ul style="list-style-type: none"> Property Line Setback Line Type 'A' Street / Civic Space Building Area Landscape Zone Build-to-Zone Sidewalk 	Front - 'A' Street and Civic Space 0 – 5 feet (see #1) A	#1 – Area between the building and the edge of the BTZ at the sidewalk shall be paved flush with the public sidewalk.	
	Front - 'B' Streets 0 – 75 feet B	#2 – Side and rear setbacks shall be based on minimum fire separation required between buildings, if applicable.	
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	Front ('A' Street and Civic Space) 5 feet (min.) I	#3 – Corner building street facades shall be built to the BTZ for a minimum of 25 feet from the corner along both streets or the width of the corner lot, whichever is less. Recessed entrances are permitted as long as the upper floors meet the BTZ standards.	
	Front ('B' Streets) 10 feet (min.) J		
	Side or Rear (distance from property line) 0 feet (see #2 and 6.2.7(iii)) C D	#4 – Floor to floor heights shall not apply to parking structures.	
	(iii) Building Frontage Required		
	% of building built to 'A' Street/Civic Space BTZ 60% (min.) (see #3 and #6) E	#5 – Attics and mezzanines less than 7 feet (avg.) in height shall not be counted as a story.	
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	6.2.3 Block Standards (for development/redevelopment greater than 50% of the block)		
Block Face Dimensions 300 feet (min.) 1000 feet (max.)			
Block Perimeter 1600 feet (max.)			
6.2.4 Height Standards			
	Principal Building Standards [rename]		
	Maximum structure height 3 stories or 40 feet (max.) (See also Section 7.2) K	#7 – Corner buildings may exceed the maximum building height by 15% for 20% of the building's frontage along each corresponding street façade.	
	First floor-to-floor height 15 feet (min.) (see #4) M		
	Ground Floor finish level 12 inches max. above sidewalk (for ground floor Commercial Ready buildings) L		
	Upper floor-to-floor height 10 feet (min.) (see #4) N		
6.2.5 Frontage Requirements			
Ground floors of all buildings fronting on Mandatory 'A' Streets shall be built to Commercial Ready standards including first floor-to-floor height, ingress and egress, handicap access, and first floor elevation flush with the sidewalk.			

6.0 Site Development Standards

6.2.6 Parking and Service Access	(i) Parking Location	6.2.7 Other Standards																					
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6.0 Building Design Standards SU-2 VHRC

Building Design Standards Specific to Regional Center Character Zone

The following standards shall be used for new development or redevelopment within the Regional Center Character Zone.

6.2.9. Massing and Façade Composition

- (i) Buildings shall maintain a rhythm of façade articulation between 30-50 feet along all 'A' Streets or Civic Spaces.
- (ii) This articulation rhythm may be expressed by changing materials or color; using design elements such as fenestration, columns, and pilasters; or varying the setback of portions of the façade.
- (iii) Buildings shall be designed and built in tripartite architecture with a distinct Base, Middle, and Cap.
- (iv) An expression line or equivalent architectural element shall delineate the Base and Cap of all buildings. A parapet cornice shall delineate the caps of façades that do not use a pitched roof.
- (v) Retail storefront buildings shall include a transom, display window area, and bulkhead at the base.
- (vi) Storefronts on façades that span multiple tenants shall use architecturally compatible materials, colors, details, awnings, signage, and lighting fixtures.
- (vii) Building entrances may be defined and articulated by architectural elements such as lintels, pediments, pilasters, columns, porticos, porches, overhangs, railings, balustrades, and others as appropriate. All building elements should be compatible with the architectural style, materials, colors, and details of the building as a whole. Entrances to upper level uses may be defined and integrated into the design of the overall building facade.
- (viii) Buildings shall generally maintain the alignment of horizontal elements along the block.
- (ix) Architectural features emphasizing the corners of buildings, such as pedimented, gabled parapets; cornices; awnings; blade signs; arcades; colonnades; projecting roofs, and balconies may be used along commercial storefronts to add pedestrian interest.
- (x) Buildings located on axis with a terminating street or at the intersection of streets shall be considered feature buildings. Such buildings shall be designed with features that take advantage of that location, such as an accentuated entry and a unique building articulation off-set from the front wall planes that rises above the main building eave or parapet line.
- (xi) Chain link fences and plastic vinyl fences shall not be permitted in the Regional Center Character Zone.

6.0 Building Design Standards



6.2.10. **Commercial and Mixed-Use Building Materials**

- (i) The following materials shall be permitted as principal building materials along all 'A' Street or Civic Space fronting façades:
 - a. Masonry (stucco using a three-step process, brick, stone, cast stone, glass, or glass block)
 - b. Split face concrete block or pre-cast or poured-in-place concrete
 - c. Cementitious fiber clapboard (not sheet) with at least a 50-year warranty
 - d. Architectural metal panel (not including galvanized or unfinished metal).
 - e. Other primary building materials approved on a case-by-case basis by the Volcano Heights Review Team, particularly for buildings over 26 feet.
- (ii) No more than 50% of each façade along any 'A' Street shall use accent materials such as other metal finishes, wood, or EIFS.
- (iii) All façades along 'B' Streets or alleys shall be of a similar finished quality and color that blend with the front of the building. Building materials for these façades may be any of the primary and accent façade materials listed above.
 - a. EIFS shall be limited to 40% of any 'B' Street or alley-facing façade.
- (iv) Roofing materials visible from any public right-of-way shall be clay barrel tile, copper, factory finished standing seam metal, slate, synthetic slate, or similar materials.

6.2.11. **Residential Building Materials**

- (i) The following shall be permitted finishes for all street fronting façades (except alleys) of residential buildings. No more than three different materials shall be used on any single façade:
 - a. Cementitious-fiber clapboard (not sheet) with at least a 50-year warranty;
 - b. Masonry (stucco utilizing a three-step process, brick; stone; or man-made stone)
- (ii) The following may only be allowed up to 40% as an accent material:
 - a. EIFS or similar material over a cementitious base, rock, glass block and tile.
- (iii) Side and rear façades shall be of finished quality and of the same color and materials that blend with the front of the building.
- (iv) Other primary building materials shall be considered on a case-by-case basis by the Volcano Heights Review Team, particularly for buildings above 26 feet.
- (v) Roofing materials visible from any public right-of-way shall be clay barrel tile, copper, factory finished standing seam metal, slate, synthetic slate, or similar materials.

6.0 Building Design Standards SU-2 VHRC

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6.2.12. **Windows**

- (i) Glazing along 'A' Streets shall not be opaque.
- (ii) Glazing shall not have highly reflective coatings or finishes, so that mirror effect is reduced.
- (iii) Windows shall be recessed in walls to emphasize their thickness.

6.2.13. **Architectural Details and Other Elements**

At least one of the following detail elements shall be incorporated to provide visual interest:

- (i) Elaborate detailing around principal openings;
- (ii) Decorative windows, metal railings at balconies;
- (iii) Awnings, Roof towers, Masonry screens for see-through walls or portions of walls; and/or
- (iv) Free-standing arbors, canopies, or towers.

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6.3. Village Center Zone

6.3.1. Illustrations and Intent

Note: These are provided as illustrations of intent. The illustrations and statements on this page are advisory only and do not have the power of law. Refer to the standards on the following pages for the specific regulatory Site Development Standards.

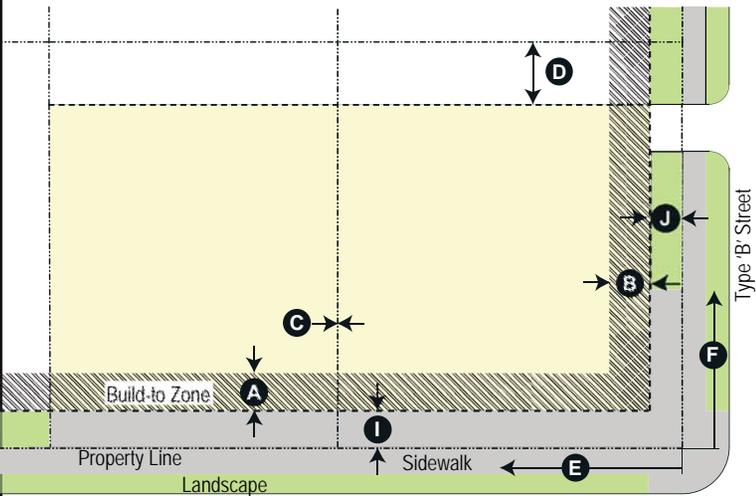
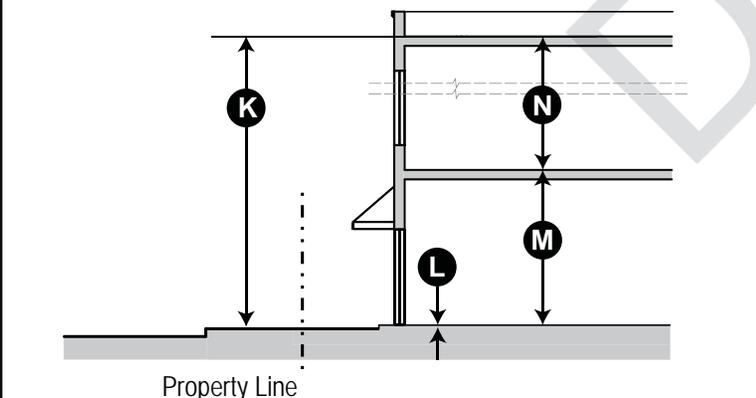
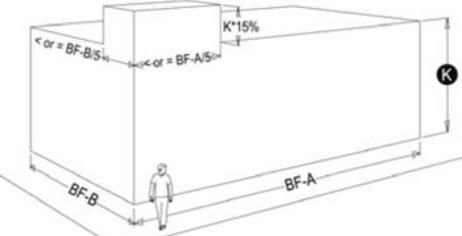
The Village Center Zone site development standards are intended to address development at a proposed node on the northwest corner of the Plan area, mirroring a Village Center across Universe Boulevard in Volcano Trails. Development standards in this character zone are intended to create conditions for a Village Center to serve development in the surrounding Mixed Use Zone.

Generally, this character zone accommodates higher-density development near an intersection of mandatory roads. The goal is to provide slightly higher-density development to add gravity for surrounding development in the Mixed Use zone. The limited size of the zone should accommodate a range of commercial, retail, and urban residential development with smaller footprints than the surrounding Mixed Use but eligible for heights up to 60 feet.

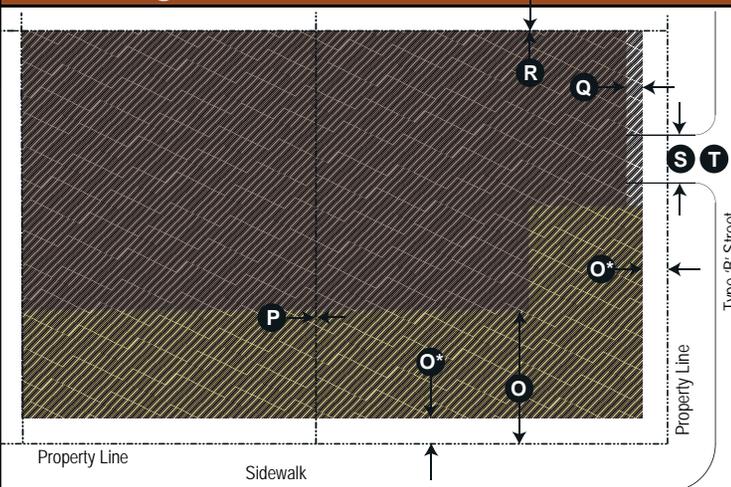
In addition, the required site development pattern of building pads, parking, driveways, and service areas is intended to provide opportunities for short-term development while allowing for planned urban block infill development in the long-term.



6.0 Site Development Standards SU-2 VHVC

6.3.2 Building Placement	(i) Build-To-Zone (BTZ)	Notes	
 <p>Legend</p> <ul style="list-style-type: none"> Property Line (dashed line) Setback Line (dotted line) Type 'A' Street / Civic Space (yellow area) Building Area (light green area) Landscape Zone (green area) Build-to Zone (hatched area) Sidewalk (grey area) 	Front - 'A' Street and Civic Space 0 – 5 feet (see #1) A	#1 – Area between the building and the edge of the BTZ at the sidewalk shall be paved flush with the public sidewalk.	
	Front - 'B' Streets 0 – 20 feet B	#2 – Side and rear setbacks shall be based on minimum fire separation required between buildings, if applicable.	
	(ii) Setback		
	Front ('A' Street and Civic Space) 5 feet (min.) I	#3 – Corner building street facades shall be built to the BTZ for a minimum of 30 feet from the corner along both streets or the width of the corner lot, whichever is less. Recessed entrances are permitted as long as the upper floors meet the BTZ standards.	
	Front ('B' Streets) 10 feet (min.) J		
	Side or Rear (distance from property line) 0 feet (see #2 and 6.3.7(iii)) C D	#4 – Floor to floor heights shall not apply to parking structures.	
	(iii) Building Frontage Required		
	% of building built to 'A' Street/Civic Space BTZ 75% (min.) (see #3 and #6) E	#5 – Attics and mezzanines less than 7 feet (avg.) in height shall not be counted as a story.	
	% of building built to 'B' Street BTZ 30% (min.) (see #3 and #6) F	#6 – Any frontage along all 'A' Streets and Civic Space not defined by a building at the BTZ shall be defined by a 4-foot high Street Screen. Furthermore, service areas along all streets and alleys shall be defined by a Street Screen that is at least as high as the service equipment being screened. Required Street Screens shall be of either the same building material as the principal structure on the lot or masonry or a living screen composed of shrubs planted to be opaque at maturity. Species shall be selected from the Plant List in Section 11.1 of this Plan. The required Street Screen shall be located at the setback line along the corresponding frontage.	
	6.3.3 Block Standards (for development/redevelopment greater than 50% of the block)		
Block Face Dimensions 300 feet (min.) 800 feet (max.)			
Block Perimeter 2000 feet (max.)			
6.3.4 Height Standards			
	Principal Building Standards [rename]		
	Maximum structure height 3 stories or 40 feet (max.) (See also Section 7.2) K	#7 – Corner buildings may exceed the maximum building height by 15% for 20% of the building's frontage along each corresponding street façade.	
	First floor-to-floor height 15 feet (min.) (see #4) M		
	Ground Floor finish level 12 inches max. above sidewalk (for ground floor Commercial Ready buildings) L		
	Upper floor-to-floor height 10 feet (min.) (see #4) N		
6.3.5 Frontage Requirements			
Ground floors of all buildings fronting on Mandatory 'A' Streets shall be built to Commercial Ready standards including first floor-to-floor height, ingress and egress, handicap access, and first floor elevation flush with the sidewalk.			

6.3.6 Parking and Service Access



Legend

- Property Line
- Surface Parking Area
- Building Footprint
- ▨ Above Grade Parking Area

6.3.8 Façade Elements

- (i) **Doors and Windows:**
- There shall be no blank walls greater than 25 feet in width of any building along 'A' Streets, Mandatory 'B' Streets, and Civic Spaces. For non-mandatory 'B' Streets there is no requirement.
 - Doors and windows on ground floor of all buildings shall be between 50% and 90% of the ground floor façade area along all 'A' Streets and Civic Spaces. Along 'B' Streets there is no requirement.
 - Windows on upper floor facades along all 'A' Streets and Civic Spaces shall be a minimum of 25% of each upper floor façade area, which is measured between 3 feet and 9 feet above each finished floor.
 - Primary entrance doors for all buildings shall be on 'A' Streets or Civic Spaces.
 - Windows and doors on facades directly facing toward a lot line shared by any single-family residential lot and less than 10 feet from the lot line shall have sills higher than 6 feet on the ground floor unless a privacy fence (masonry or vegetation) or wall of at least 6 feet in height is constructed to obscure any direct views into adjacent properties.

(i) Parking Location

Surface/At Grade Parking	
'A' Street/ Civic Space setback	Shall be located behind the principal building O
'B' Street setback	Min. of 3 feet behind building facade line along that street Q
Side and Rear setback (distance from property line)	0 feet min. (see # 2 and 6.2.7(iii)) P R
Above Grade Parking	
Setback along 'A' & 'B' Streets or Civic Space	May be built up to the building façade line along that street O*
Side and Rear setbacks (distance from property line)	0 feet min. (see # 2 and 6.3.7(iii)) P

(ii) Required Off-Street Parking Spaces

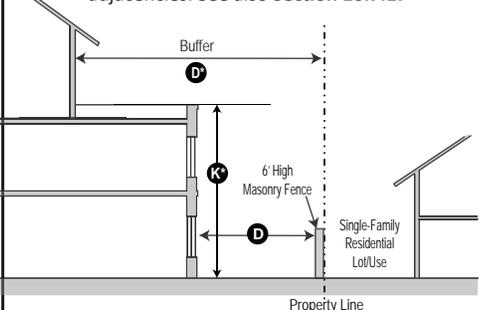
Non-residential uses within 250 feet of single-family residential	1 space per every 300 gross square feet
Residential Uses	1 space/unit minimum

(iii) Driveways and Service Access

- | | |
|---|--|
| Parking driveway width | City DPM standards on Arterial Roadways and 25 feet max. on all other streets S |
| <ul style="list-style-type: none"> Driveways and off-street loading and unloading shall not be located on 'A' Streets or Civic Spaces. Porte cocheres may be permitted on 'A' Streets to provide drop-off and valet service. Shared driveways and cross access easements are encouraged between lots to minimize curb cuts. If driveway and/or off-street service loading and unloading access is provided from an 'A' Street, such access shall be deemed as temporary, and cross access easements along the rear of the property shall be required when adjoining properties are undeveloped. | T |

6.3.7 Other Standards

- (i) **Encroachments**
- Canopies, signs, awnings and balconies may encroach over the sidewalk as long as the vertical clearance is a minimum of 8 feet. In no case shall an encroachment be located over an on-street parking or travel lane.
 - Building projections on all other facades may not be closer than 5 feet to any adjacent property line.
- (ii) **Arcades and Colonnades**
- Arcades and Colonnades are permitted within the build-to-zone and shall be a minimum of 6 feet in depth.
 - The minimum interior clearance height within an arcade or colonnade shall be 12 feet
- (iii) **Buffers for Detached Single-family Housing**
- A Buffer for detached single-family housing shall be established on all development sites adjacent to single family sites at 25 feet parallel to any lot line that is common with a single-family residential lot.
 - Building height within this Buffer shall not exceed 2 floors or 26 feet and is not eligible for height bonus. This standard shall apply to any parking structures located within the Buffer.
 - A privacy fence (masonry or opaque vegetation) 6 feet high shall be required when abutting a single-family residential lot and shall be optional for all other adjacencies. See also **Section 10.7.1**.



6.0 Building Design Standards SU-2 VHVC

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Building Design Standards Specific to Village Center Character Zone

The following standards shall be used for new development or redevelopment within the Village Center Zone.

6.3.9. Massing and Façade Composition

- (i) Buildings generally have a rectangular layout scheme with single or multiple components with mostly flat front and square, round, or octagonal corner towers.
- (ii) Buildings shall maintain a prevalent façade articulation rhythm of 20-30 feet or multiples thereof along all 'A' Streets or Civic Spaces
- (iii) This articulation rhythm may be expressed by changing materials or color, using design elements such as columns and pilasters, or varying the setback of portions of the building façade.
- (iv) Building facades may be symmetrical or asymmetrical, but the central part of the building shall be expressed with well-balanced façade compositions.
- (v) Doors or windows shall form regular patterns of openings, some accentuated by balconies.
- (vi) The ground floor should be taller and include heavier massing than floors above. An arcade or colonnade may be incorporated.
- (vii) Commercial and Mixed-Use building facades shall be designed with a distinct base, middle, and cap and shall maintain the alignment of horizontal elements along the block.
- (viii) Storefronts are inset with recessed entry, under the portal, arcade, or fit into arches openings, often with canvas awnings.
- (ix) Awnings, blade signs, arcades, galleries, café seating and balconies should be used along commercial storefronts fronting an 'A' Street or Civic Space to add pedestrian interest.
- (x) If the residential building is setback less than 10 feet from the front property line, the grade of the slab or first floor elevation shall be elevated at least 18 inches above the grade of the sidewalk. If the residential structure is setback 10 feet or more from the property line and is not elevated above the grade of the sidewalk, a 3-foot high fence shall be provided at the front property line.
- (xi) Chain link fences and plastic vinyl fences shall not be permitted in the Village Center Character Zone.

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6.0 Building Design Standards



6.3.10. **Building Materials**

- (i) At least 60% of a building’s façade along all ‘A’ Streets or Civic Spaces shall be composed of masonry, three-step process stucco, or brick.
- (ii) No more than 40% of a building’s façade along all ‘A’ Streets or Civic Spaces shall use other accent materials wood, architectural metal panel, split-face concrete block, tile, or pre-cast concrete panels. EIFS shall not be permitted along any ‘A’ Street or Civic Space façade.
- (iii) All facades along ‘B’ Streets or alleys shall be of a similar finished quality and color that blend with the front of the building. Building materials for these facades may be any of the primary and accent façade materials listed above.
 - a. EIFS shall be limited to no more than 10% of the upper floor facades along ‘B’ Streets and alleys.
 - b. Cementitious-fiber clapboard (not sheet) with at least a 50-year warranty shall only be used on the upper floors of any ‘B’ Street or alley façades.
- (iv) Other primary building materials shall be considered on a case-by-case basis by the Volcano Heights Review Team, particularly for buildings above 26 feet.
- (v) Roofing materials visible from any public right-of-way shall be clay tile, copper, factory finished standing seam or corrugated metal, metal shingle, slate, synthetic slate, or similar materials.

6.3.11. **Windows**

- (i) Glazing along ‘A’ Streets shall not be opaque.
- (ii) Glazing shall not have highly reflective coatings or finishes, so that mirror effect is reduced.
- (iii) Windows shall be recessed to give a thick wall impression.
- (iv) Windows shall be vertically proportioned with multiple panes in both casement and double hung design.
- (v) Ornamental arches of various designs shall be deeper on the ground floor and shallower on upper floors.

6.3.12. **Architectural Details and Other Elements**

- At least two of the following detail elements shall be incorporated to provide visual interest:
- (i) Elaborate detailing around principal openings;
 - (ii) Decorative windows, metal railings at balconies;
 - (iii) Awnings, Roof towers, Masonry screens for see-through walls or portions of walls; and/or
 - (iv) Free-standing arbors, canopies, or towers.

6.0 Site Development and Building Design Standards SU-2 VHMX

6.4. Mixed-Use Zone

6.4.1. Illustrations and Intent

Note: These are provided as illustrations of intent. The illustrations and statements on this page are advisory only and do not have the power of law. Refer to the standards on the following pages for the specific Site Development Standards.

The Mixed-use Zone site development standards are intended to address development throughout Volcano Heights. Development standards in this zone are intended to take advantage of the large parcels with access to regional connectors and a proposed Transit Corridor.

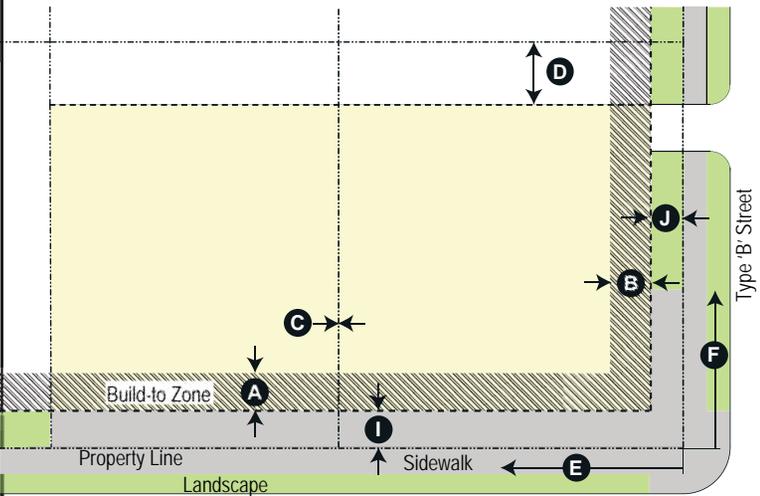
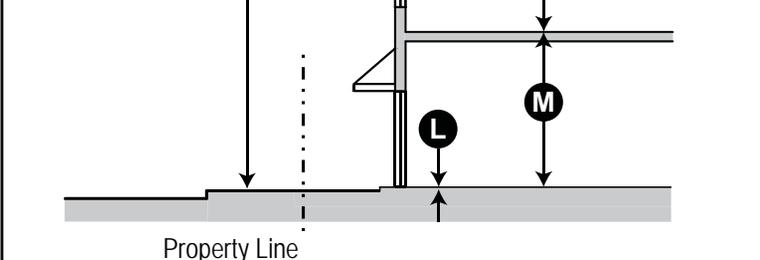
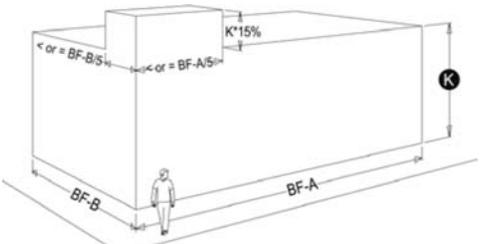
Generally, this zone may accommodate large-format office sites with surface parking within the interior of the lot/block and screened from public view along 'A' Streets. The goal is to minimize the impact of large, surface parking lots and encourage an "office park" look.

In addition, required site development pattern of building pads, parking, driveways, and service areas is intended to provide opportunities for short-term development while allowing for planned urban block infill development in the long-term as the market can accommodate it.

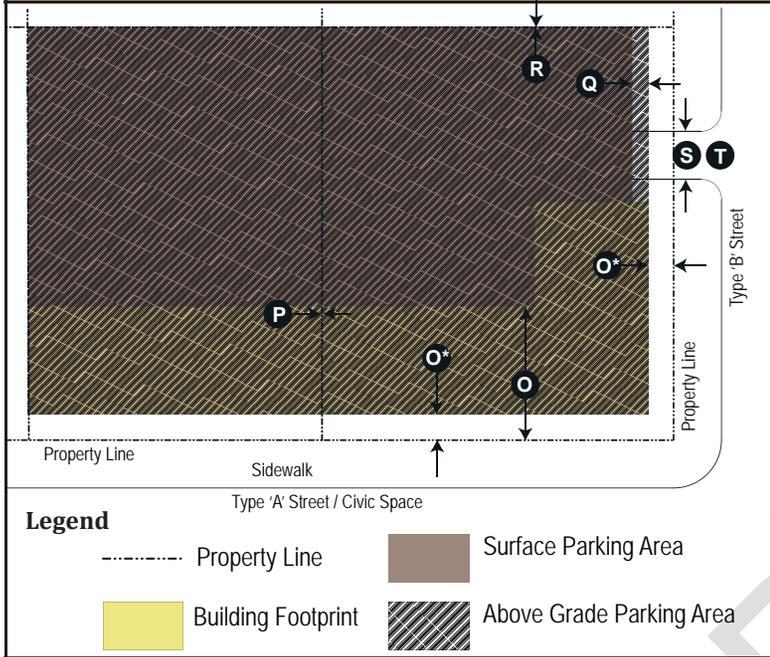
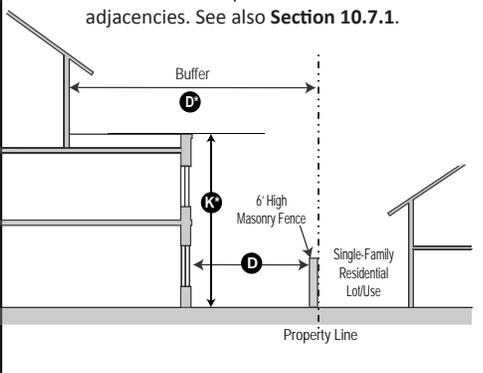


SU-2 Volcano Heights Mixed Use

6.0 Site Development Standards

6.4.2 Building Placement	(i) Build-To-Zone (BTZ)	Notes	
 <p>Legend</p> <ul style="list-style-type: none"> Property Line (dashed line) Setback Line (dotted line) Building Area (yellow) Landscape Zone (green) Build-to-Zone (hatched) Sidewalk (grey) 	Front – ‘A’ Street and Civic Space 0 – 15 feet (see #1) A	#1 – Area between the building and the edge of the BTZ at the sidewalk shall be paved flush with the public sidewalk.	
	Front – ‘B’ Streets 0 – 75 feet B	#2 – Side and rear setbacks shall be based on minimum fire separation required between buildings, if applicable.	
	(ii) Setback		
	Front (‘A’ Street and Civic Space) 5 feet (min.) I	#3 – Corner building street facades shall be built to the BTZ for a minimum of 25 feet from the corner along both streets or the width of the corner lot, whichever is less. Recessed entrances are permitted as long as the upper floors meet the BTZ standards.	
	Front (‘B’ Streets) 10 feet (min.) J		
	Side or Rear (distance from property line) 0 feet (min.) (see #2 and 6.4.7(iii)) C D	#4 – Floor to floor heights shall not apply to parking structures.	
	(iii) Building Frontage Required		
	% of building built to ‘A’ Street/Civic Space BTZ 50% (min.) (see #3 and #6) E	#5 – Attics and mezzanines less than 7 feet (avg.) in height shall not be counted as a story.	
	% of building built to ‘B’ Street BTZ 25% (min.) (see #3 and #6) F	#6 – Any frontage along all ‘A’ Streets and Civic Space not defined by a building at the BTZ shall be defined by a 4-foot high Street Screen. Furthermore, service areas along all streets and alleys shall be defined by a Street Screen that is at least as high as the service equipment being screened. Required Street Screens shall be of either the same building material as the principal structure on the lot or masonry or a living screen composed of shrubs planted to be opaque at maturity. Species shall be selected from the Plant List in Section 11.1 of this Plan. The required Street Screen shall be located at the setback line along the corresponding frontage.	
	6.4.3 Block Standards (for development/redevelopment greater than 50% of the block)		
Block Face Dimensions 300 feet (min.) 1200 feet (max.)			
Block Perimeter 4000 feet (max.)			
6.4.4 Height Standards			
	Principal Building Standards <i>[rename]</i>		
	Maximum structure height 2 stories or 26 feet (max.) (See also Section 7.2) K	#7 – Corner buildings may exceed the maximum building height by 15% for 20% of the building’s frontage along each corresponding street façade.	
	First floor-to-floor height 15 feet (min.) (see #4) M		
	Ground Floor finish level 12 inches max. above sidewalk (for ground floor Commercial Ready buildings) L		
	Upper floor-to-floor height 10 feet (min.) (see #4) N		
6.4.5 Frontage Requirements			
Ground floors of all buildings fronting on Mandatory ‘A’ Streets shall be built to Commercial Ready standards including first floor-to-floor height, ingress and egress, handicap access, and first floor elevation flush with the sidewalk.			

6.0 Site Development Standards SU-2 VHMX

6.4.6 Parking and Service Access	(i) Parking Location	6.4.7 Other Standards															
 <p>Legend</p> <ul style="list-style-type: none"> Property Line Building Footprint Surface Parking Area Above Grade Parking Area 	<p>(i) Parking Location</p> <p>Surface/At Grade Parking</p> <table border="1"> <tr> <td>'A' Street/ Civic Space setback</td> <td>Shall be located behind the principal building</td> <td>O</td> </tr> <tr> <td>'B' Street setback</td> <td>Min. of 3 feet behind building facade line along that street</td> <td>Q</td> </tr> <tr> <td>Side and Rear setback (distance from property line)</td> <td>0 feet min. (see # 2 and 6.4.7(iii))</td> <td>P R</td> </tr> </table> <p>Above Grade Parking</p> <table border="1"> <tr> <td>Setback along 'A' & 'B' Streets or Civic Space</td> <td>May be built up to the building facade line along that street</td> <td>O*</td> </tr> <tr> <td>Side and Rear setbacks (distance from property line)</td> <td>0 feet min. (see # 2 and 6.4.7(iii))</td> <td>P</td> </tr> </table>	'A' Street/ Civic Space setback	Shall be located behind the principal building	O	'B' Street setback	Min. of 3 feet behind building facade line along that street	Q	Side and Rear setback (distance from property line)	0 feet min. (see # 2 and 6.4.7(iii))	P R	Setback along 'A' & 'B' Streets or Civic Space	May be built up to the building facade line along that street	O*	Side and Rear setbacks (distance from property line)	0 feet min. (see # 2 and 6.4.7(iii))	P	<p>6.4.7 Other Standards</p> <p>(i) Encroachments</p> <ol style="list-style-type: none"> Canopies, signs, awnings and balconies may encroach over the sidewalk as long as the vertical clearance is a minimum of 8 feet. In no case shall an encroachment be located over an on-street parking or travel lane. Building projections on all other facades may not be closer than 5 feet to any adjacent property line. <p>(ii) Arcades and Colonnades</p> <ol style="list-style-type: none"> Arcades and Colonnades are permitted within the build-to-zone and shall be a minimum of 6 feet in depth. The minimum interior clearance height within an arcade or colonnade shall be 12 feet <p>(iii) Buffers for Detached Single-family Housing</p> <ol style="list-style-type: none"> A Buffer for detached single-family housing shall be established on all development sites adjacent to single family sites at 25 feet parallel to any lot line that is common with a single-family residential lot. Building height within this Buffer shall not exceed 2 floors or 26 feet and is not eligible for height bonus. This standard shall apply to any parking structures located within the Buffer. A privacy fence (masonry or opaque vegetation) 6 feet high shall be required when abutting a single-family residential lot and shall be optional for all other adjacencies. See also Section 10.7.1.
	'A' Street/ Civic Space setback	Shall be located behind the principal building	O														
'B' Street setback	Min. of 3 feet behind building facade line along that street	Q															
Side and Rear setback (distance from property line)	0 feet min. (see # 2 and 6.4.7(iii))	P R															
Setback along 'A' & 'B' Streets or Civic Space	May be built up to the building facade line along that street	O*															
Side and Rear setbacks (distance from property line)	0 feet min. (see # 2 and 6.4.7(iii))	P															
<p>(ii) Required Off-Street Parking Spaces</p> <table border="1"> <tr> <td>Non-residential uses within 250 feet of single-family residential</td> <td>1 space per every 400 gross square feet</td> </tr> <tr> <td>Residential Uses</td> <td>1 space/unit minimum</td> </tr> </table>	Non-residential uses within 250 feet of single-family residential	1 space per every 400 gross square feet	Residential Uses	1 space/unit minimum													
Non-residential uses within 250 feet of single-family residential	1 space per every 400 gross square feet																
Residential Uses	1 space/unit minimum																
6.4.8 Façade Elements	(iii) Driveways and Service Access																
<p>(i) Doors and Windows:</p> <ol style="list-style-type: none"> There shall be no blank walls greater than 50 feet in width of any building along 'A' Streets, Mandatory 'B' Streets, and Civic Spaces. For non-mandatory 'B' Streets there is no requirement. Doors and windows on ground floor of all buildings shall be a minimum of 30% of the ground floor façade area along all 'A' Streets and Civic Spaces. Along 'B' Streets there is no requirement. Windows on upper floor facades along all 'A' Streets and Civic Spaces shall be a minimum of 20% of each upper floor façade area, which is measured between 3 feet and 9 feet above each finished floor. Primary entrance doors for all buildings shall be on 'A' Streets or Civic Spaces. Windows and doors on facades directly facing toward a lot line shared by any single-family residential lot and less than 10 feet from the lot line shall have sills higher than 6 feet on the ground floor unless a privacy fence (masonry or vegetation) or wall of at least 6 feet in height is constructed to obscure any direct views into adjacent properties. <p>(ii) Façade Articulation: Along Unser Boulevard and Paseo del Norte, façades shall incorporate a corner element, articulated by a horizontal break of 3 feet max.</p>	<p>(iii) Driveways and Service Access</p> <table border="1"> <tr> <td>Parking driveway width</td> <td>City DPM standards on Arterial Roadways and 25 feet max. on all other streets</td> <td>S</td> </tr> </table> <ul style="list-style-type: none"> Driveways and off-street loading and unloading shall not be located on 'A' Streets or Civic Spaces. Porte cocheres may be permitted on 'A' Streets to provide drop-off and valet service. Shared driveways and cross access easements are encouraged between lots to minimize curb cuts. If driveway and/or off-street service loading and unloading access is provided from an 'A' Street, such access shall be deemed as temporary, and cross access easements along the rear of the property shall be required when adjoining properties are undeveloped. 	Parking driveway width	City DPM standards on Arterial Roadways and 25 feet max. on all other streets	S													
Parking driveway width	City DPM standards on Arterial Roadways and 25 feet max. on all other streets	S															

6.0 Building Design Standards

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Building Design Standards Specific to Mixed-Use Character Zone**6.4.9. Façade Composition**

- (i) Buildings shall maintain a façade articulation rhythm of 20-30 feet along all 'A' Streets and Civic Spaces and 30-50 feet for façades along Paseo del Norte or Unser Boulevard.
- (ii) This articulation rhythm may be expressed by changing materials or color; using design elements such as fenestration, columns, and pilasters; or varying the setback of portions of the façade.
- (iii) Buildings shall be designed and built in tripartite architecture with a distinct Base, Middle, and Cap.
- (iv) Retail storefront buildings shall include a transom, display window area, and bulkhead at the base.
- (v) Storefronts on façades that span multiple tenants shall use architecturally compatible materials, colors, details, awnings, signage, and lighting fixtures.
- (vi) Building entrances should be defined and articulated by architectural elements such as lintels, pediments, pilasters, columns, porticos, porches, overhangs, railings, balustrades, and others as appropriate. All building elements should be compatible with the architectural style, materials, colors, and details of the building as a whole. Entrances to upper level uses may be defined and integrated into the design of the overall building facade.

- (vii) Buildings should generally maintain the alignment of horizontal elements along the block.
- (viii) Architectural features emphasizing the corner of the building, such as use of pediments, gabled parapets; cornices; awnings; blade signs; arcades; colonnades; and balconies may be used along commercial storefronts to add pedestrian interest.

6.4.10. Commercial and Mixed-Use Building Materials

- (i) At least 60% of each building's Base façade (excluding doors and windows) along any 'A' Street shall be finished in one of the following materials:
 - a. Masonry (three-step process stucco, brick, stone, cast stone, glass, or glass block)
 - b. Hardi plank
 - c. Split face concrete block or pre-cast, or poured in place concrete
 - d. Cementitious fiber clapboard
 - e. Other primary building materials approved by the Volcano Heights Review Team on a case-by-case
- (ii) No more than 40% of each façade along any 'A' Street shall use accent materials such as wood, architectural metal panel, or EIFS.
 - a. Cementitious-fiber clapboard shall be prohibited on mixed-use frontages. On the upper floors of any commercial frontage, no more than 20% of a 'B' Street or alley façade shall be cementitious-fiber clapboard (not sheet), which shall have at least a 50-year warranty.

6.0 Site Development Standards SU-2 VHMX

R

- (iii) Roofing materials visible from any public right-of-way shall be factory-finished standing seam metal, slate, synthetic slate, or similar materials.
- (iv) Glazing along 'A' Streets shall not be opaque.
- (v) Glazing shall not have highly reflective coatings or finishes, so that mirror effect is reduced.
- (iv) Roofing materials visible from any public right-of-way shall be clay barrel tile, copper, factory finished standing seam metal, slate, synthetic slate, or similar materials.
- (v) Glazing along 'A' Streets shall not be opaque.
- (vi) Glazing shall not have highly reflective coatings or finishes, so that mirror effect is reduced.

6.4.11. Residential Building Materials

- (i) The following shall be permitted finishes for all street fronting facades (except alleys) of residential buildings. No more than three different materials shall be used on any single façade:
 - a. Cementitious-fiber clapboard (not sheet) with at least a 50-year warranty
 - b. Lap sided wood
 - c. Masonry (three-step process stucco, brick; stone; man-made stone)
 - d. Architectural Metal Panels
 - e. Hardi plank
 - f. Other primary building materials approved by the Volcano Heights Review Team on a case-by-case
- (ii) The following may only be allowed up to 40% as an accent material:
 - a. EIFS or similar material over a cementitious base, rock, glass block, and tile.
- (iii) Side and rear facades shall be of finished quality and of the same color and materials that blend with the front of the building.

6



6.5. Neighborhood Transition
6.5.1. Illustrations and Intent

Note: These are provided as illustrations of intent. The illustrations and statements on this page are advisory only and do not have the power of law. Refer to the standards on the following pages for the specific Site Development Standards.

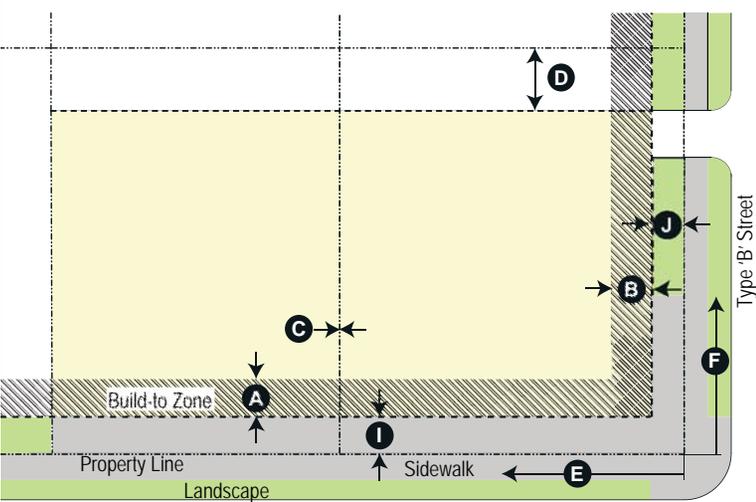
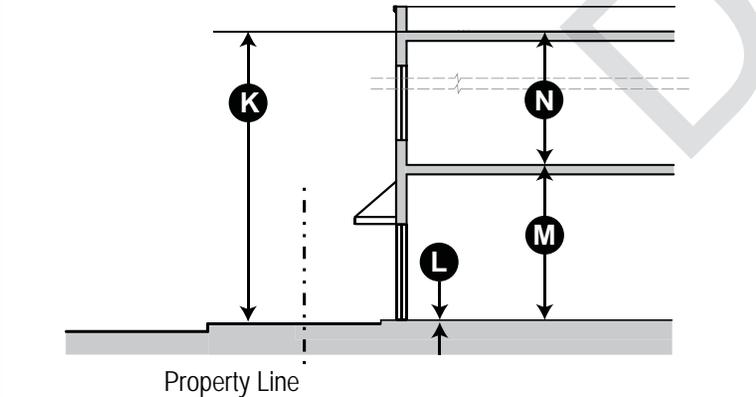
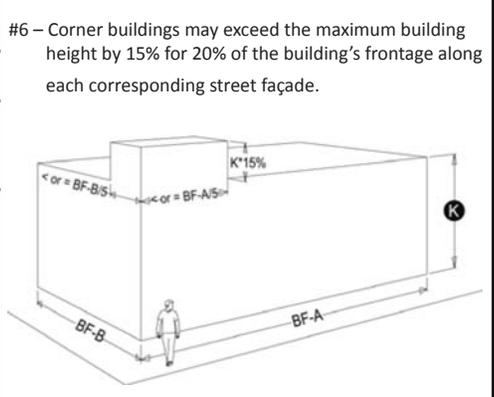
The Neighborhood Transition Zone site development standards are intended to address development between proposed Volcano Heights character zones and existing residential areas. Development standards in this zone are intended to appropriately blend the areas between new construction and existing buildings outside the Plan area.

Generally, this zone may accommodate small-scale, low-rise commercial and urban residential development. The goal is to have a reasonable buffer between existing buildings and any higher-density new construction.

In addition, the site shall be planned in such a manner as to facilitate low-intensity, residential-scale office, retail, and live-work buildings in a 1-2 story environment.



6.0 Site Development Standards SU-2 VHNT

6.5.2 Building Placement	(i) Build-To-Zone (BTZ)	Notes	
 <p>Legend</p> <ul style="list-style-type: none"> Property Line (dashed line) Setback Line (dotted line) Building Area (yellow) Landscape Zone (green) Build-to Zone (hatched) Sidewalk (grey) 	<p>Front – ‘A’ Street and Civic Space 0 – 10 feet (see #1) A</p> <p>Front – ‘B’ Streets 0 – 20 feet B</p>	<p>#1 – Area between the building and the edge of the BTZ at the sidewalk shall be paved flush with the public sidewalk.</p>	
	<p>(ii) Setback</p>	<p>#2 – Side and rear setbacks shall be based on minimum fire separation required between buildings, if applicable.</p>	
	<p>Front (‘A’ Street and Civic Space) 5 feet (min.) I</p>	<p>#3 – Corner building street facades shall be built to the BTZ for a minimum of 25 feet from the corner along both streets or the width of the corner lot, whichever is less. Recessed entrances are permitted as long as the upper floors meet the BTZ standards.</p>	
	<p>Front (‘B’ Streets) 10 feet (min.) J</p>		
	<p>Side or Rear (distance from property line) 0 feet (min.) (see #2 and 6.5.7(iii)) C D</p>		
	<p>(iii) Building Frontage Required</p>	<p>#4 – Attics and mezzanines less than 7 feet (avg.) in height shall not be counted as a story.</p>	
	<p>% of building built to ‘A’ Street/Civic Space BTZ 75% (min.) (see #3 and #6) E</p>	<p>#5 – Any frontage along all ‘A’ Streets and Civic Space not defined by a building at the BTZ shall be defined by a 4-foot high Street Screen. Furthermore, service areas along all streets and alleys shall be defined by a Street Screen that is at least as high as the service equipment being screened. Required Street Screens shall be of either the same building material as the principal structure on the lot or masonry or a living screen composed of shrubs planted to be opaque at maturity. Species shall be selected from the Plant List in Section 11.1 of this Plan. The required Street Screen shall be located at the setback line along the corresponding frontage.</p>	
	<p>% of building built to ‘B’ Street BTZ 30% (min.) (see #3 and #6) F</p>		
	<p>6.5.3 Block Standards (for development/redevelopment greater than 50% of the block)</p>	<p>Block Face Dimensions 200 feet (min.) 600 feet (max.)</p> <p>Block Perimeter 2000 feet (max.)</p>	
	<p>6.5.4 Height Standards</p>	<p>Principal Building Standards [rename]</p>	<p>#6 – Corner buildings may exceed the maximum building height by 15% for 20% of the building’s frontage along each corresponding street façade.</p>
	<p>Maximum structure height 2 stories or 26 feet (max.) K</p> <p>First floor-to-floor height 12 feet (min.) (see #4) M</p> <p>Ground Floor finish level 12 inches max. above sidewalk (for ground floor Commercial Ready buildings) L</p> <p>Upper floor-to-floor height 10 feet (min.) (see #4) N</p>		

6.0 Site Development Standards

<p>6.5.6 Parking and Service Access</p>	<p>(i) Parking Location</p>	<p>6.5.7 Other Standards</p>																														
<p>Legend</p> <ul style="list-style-type: none"> Property Line Building Footprint Surface Parking Area Above Grade Parking Area 	<table border="1"> <tr> <th colspan="2">Surface/At Grade Parking</th> <th></th> </tr> <tr> <td>'A' Street/ Civic Space setback</td> <td>Shall be located behind the principal building</td> <td>O</td> </tr> <tr> <td>'B' Street setback</td> <td>Min. of 3 feet behind building facade line along that street</td> <td>Q</td> </tr> <tr> <td>Side and Rear setback (distance from property line)</td> <td>0 feet min. (see # 2 and 6.5.7(iii))</td> <td>P R</td> </tr> <tr> <th colspan="2">Above Grade Parking</th> <th></th> </tr> <tr> <td>Setback along 'A' & 'B' Streets or Civic Space</td> <td>May be built up to the building façade line along that street</td> <td>O*</td> </tr> <tr> <td>Side and Rear setbacks (distance from property line)</td> <td>0 feet min. (see # 2 and 6.5.7(iii))</td> <td>P</td> </tr> <tr> <th colspan="2">(ii) Required Off-Street Parking Spaces</th> <th></th> </tr> <tr> <td>Non-residential uses within 250 feet of single-family residential</td> <td>1 space per every 300 gross square feet</td> <td></td> </tr> <tr> <td>Residential Uses</td> <td>1 space/unit minimum</td> <td></td> </tr> </table>	Surface/At Grade Parking			'A' Street/ Civic Space setback	Shall be located behind the principal building	O	'B' Street setback	Min. of 3 feet behind building facade line along that street	Q	Side and Rear setback (distance from property line)	0 feet min. (see # 2 and 6.5.7(iii))	P R	Above Grade Parking			Setback along 'A' & 'B' Streets or Civic Space	May be built up to the building façade line along that street	O*	Side and Rear setbacks (distance from property line)	0 feet min. (see # 2 and 6.5.7(iii))	P	(ii) Required Off-Street Parking Spaces			Non-residential uses within 250 feet of single-family residential	1 space per every 300 gross square feet		Residential Uses	1 space/unit minimum		<p>(i) Encroachments</p> <ol style="list-style-type: none"> Canopies, signs, awnings and balconies may encroach over the sidewalk as long as the vertical clearance is a minimum of 8 feet. In no case shall an encroachment be located over an on-street parking or travel lane. Building projections on all other facades may not be closer than 5 feet to any adjacent property line. <p>(ii) Arcades and Colonnades</p> <ol style="list-style-type: none"> Arcades and Colonnades are permitted within the build-to-zone and shall be a minimum of 6 feet in depth. The minimum interior clearance height within an arcade or colonnade shall be 14 feet. <p>(iii) Privacy Fence</p> <ol style="list-style-type: none"> A privacy fence (masonry or opaque vegetation) 6 feet high shall be required when abutting a single-family residential lot and shall be optional for all other adjacencies. See also Section 10.7.1.
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6.0 Building Design Standards SU-2 VHNT & VHET

Building Design Standards Specific to Neighborhood and Escarpment Building Design Standards Specific to Transition Character Zones

6.5.9. Façade Composition

- (i) Buildings shall maintain a façade articulation rhythm of 20-30 feet along all 'A' Streets.
- (ii) This articulation rhythm may be expressed by changing materials or color; using design elements such as fenestration, columns, and pilasters; or varying the setback of portions of the façade.
- (iii) Buildings shall be designed and built in tripartite architecture with a distinct Base, Middle, and Cap.
- (iv) Building entrances may be defined and articulated by architectural elements such as lintels, pediments, pilasters, columns, porticos, porches, overhangs, railings, balustrades, and others as appropriate. All building elements should be compatible with the architectural style, materials, colors, and details of the building as a whole. Entrances to upper level uses may be defined and integrated into the design of the overall building facade.
- (v) Buildings shall generally maintain the alignment of horizontal elements along the block.
- (vi) Garages for Residential Buildings shall be located on alleys at the rear of residential buildings; pull-through detached garages are allowed if the garage door is set back behind the rear façade of the main structure.

- (vii) All garage doors shall be divided into single bays separated by at least a 16-inch pier or column. Front-loaded garages on residential lots less than 40 feet wide shall not be allowed. Townhouses and courtyard apartments shall use rear-loaded garages.
- (viii) Residential, front-loaded garages or carports shall be no greater than 12 feet wide and set back at least 20 feet measured from the front façade of the main structure closest to the garage/ carport or rotated 90 degrees with windows on the wall facing the street.
- (ix) On corner lots, the garage may be rotated with windows facing an 'A' Street or Civic Space with driveway access from the 'B' Street or alley.

6.5.10. Commercial and Mixed-Use Building Materials

- (i) The following primary building materials shall be permitted for façades (excluding doors and windows) along any 'A' Street or Civic Space:
 - a. Masonry (stucco using a 3-step process, brick, stone, cast stone, glass, split-face concrete, pre-cast concrete panels, tile or glass block)
 - b. Cementitious-fiber clapboard (not sheet) with at least a 50-year warranty
 - c. Wood
 - d. Architectural Metal Panels
 - e. Other materials will be considered as primary building materials on a case-by-case basis and approved by the Volcano Heights Review Team.

6.0 Building Design Standards

- (ii) EIFS shall not be permitted along any 'A' Street or Civic Space facade.
- (iii) All building façades along all other streets or alleys shall be of a similar finished quality and color that blend with the front of the building. Building materials for these façades may be any of the primary materials listed above.
 - a. EIFS shall be limited to 25% of all other facades.
- (iv) Roofing materials visible from any public right-of-way shall be factory finished standing seam metal, slate, synthetic slate, or similar materials.
- (v) Glazing along 'A' Streets shall not be opaque.
- (vi) Glazing shall not have highly reflective coatings or finishes, so that mirror effect is reduced.
- (ii) The following may only be allowed up to 40% as an accent material:
 - a. EIFS or similar material over a cementitious base, rock, glass block, and tile.
- (iii) Side and rear facades shall be of finished quality and of the same color and materials that blend with the front of the building.
- (iv) Roofing materials visible from any public right-of-way shall be clay barrel tile, copper, factory finished standing seam metal, slate, synthetic slate, or similar materials.
- (v) An enclosed garage or carport shall be designed and constructed of the same material as the primary building.
- (vi) Glazing along 'A' Streets shall not be opaque.
- (vii) Glazing shall not have highly reflective coatings or finishes, so that mirror effect is reduced.

6.5.11. Residential Building Materials

- (i) The following shall be permitted finishes for all street fronting facades (except alleys) of residential buildings. No more than three different materials shall be used on any single façade:
 - a. Cementitious-fiber clapboard (not sheet) with at least a 50-year warranty
 - b. Lap sided wood
 - c. Masonry (three-step process stucco, brick; stone; man-made stone)
 - d. Architectural Metal Panels
 - e. Hardi plank
 - f. Other primary building materials approved by the Volcano Heights Review Team on a case-by-case basis.

6.0 Site Development and Building Design Standards SU-2 VHET

6.6. Escarpment Transition

6.6.1. Illustrations and Intent

Note: These are provided as illustrations of intent. The illustrations and statements on this page are advisory only and do not have the power of law. Refer to the standards on the following pages for the specific Site Development Standards.

The Escarpment Transition Zone site development standards are intended to address development between proposed character zones and the Petroglyph National Monument east of the Plan boundary. Development standards in this district are intended to blend the areas between development and Open Space, as well as stepping down the intensity of uses and building heights closer to the Monument boundary.

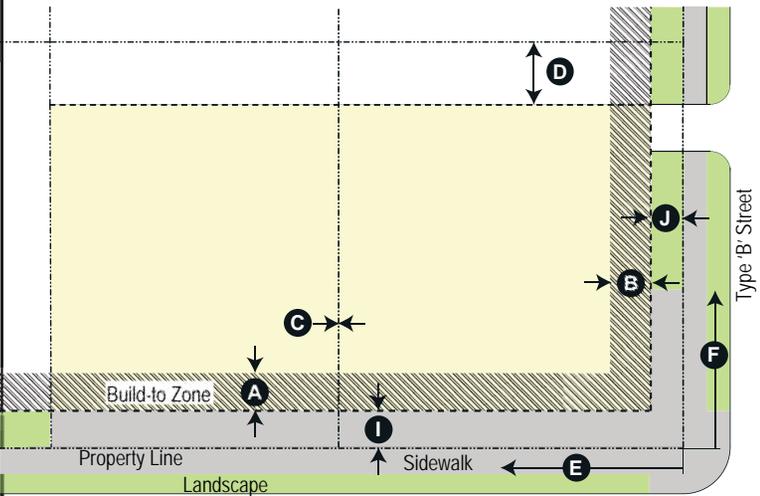
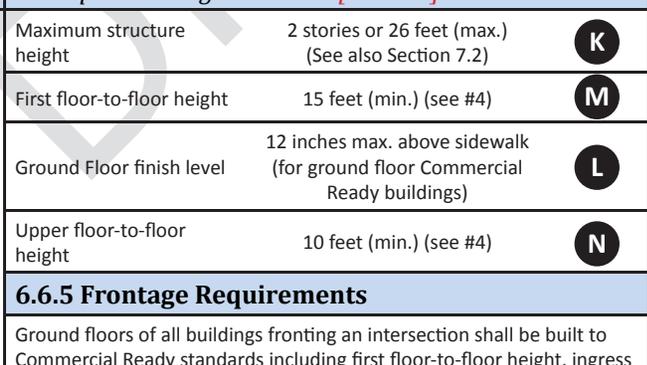
Generally, this district may accommodate low-rise commercial, single-family development, and limited urban residential development. The goal is to have a reasonable buffer between development and sensitive lands to the east.

In addition, Escarpment Transition sites shall be planned in such a manner as to facilitate low-intensity, residential-scale office, retail, and live-work buildings in a 1-2 story environment that accommodates natural topography, terrain, and natural environment.

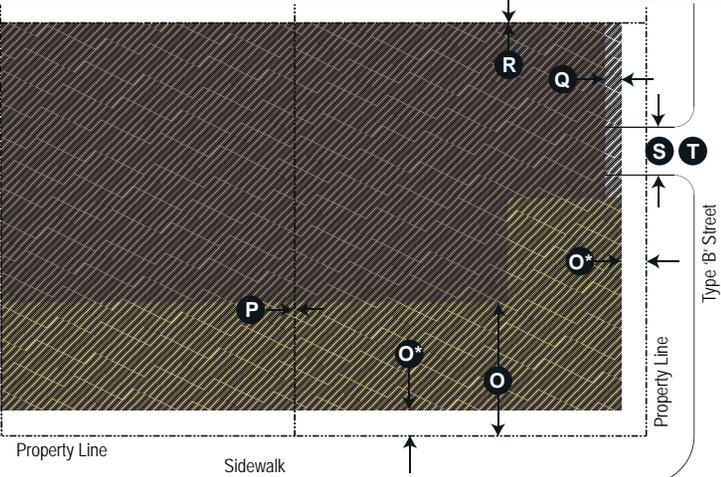
- 6.6.2. Building Design Standards Specific to Escarpment Transition shall be as per **Section 6.5.9-6.5.12** in this Plan.



6.0 Site Development Standards

6.6.2 Building Placement	(i) Build-To-Zone (BTZ)	Notes	
 <p>Legend</p> <ul style="list-style-type: none"> Property Line (dashed line) Setback Line (dotted line) Type 'A' Street / Civic Space (yellow area) Building Area (light green area) Landscape Zone (green area) Build-to Zone (hatched area) Sidewalk (grey area) 	Front - 'A' Street and Civic Space 0 – 5 feet (see #1) A	#1 – Area between the building and the edge of the BTZ at the sidewalk shall be paved flush with the public sidewalk.	
	Front - 'B' Streets 0 – 20 feet B		
	(ii) Setback		#2 – Side and rear setbacks shall be based on minimum fire separation required between buildings, if applicable.
	Front ('A' Street and Civic Space) 5 feet (min.) I		
	Front ('B' Streets) 10 feet (min.) J	#3 – Corner building street facades shall be built to the BTZ for a minimum of 25 feet from the corner along both streets or the width of the corner lot, whichever is less. Recessed entrances are permitted as long as the upper floors meet the BTZ standards.	
	Side or Rear (distance from property line) 0 feet (min.) (see #2 and 6.6.7(iii)) C D		
	(iii) Building Frontage Required		
	% of building built to 'A' Street/Civic Space BTZ 75% (min.) (see #3 and #6) E		
	% of building built to 'B' Street BTZ 30% (min.) (see #3 and #6) F	#4 – Attics and mezzanines less than 7 feet (avg.) in height shall not be counted as a story.	
	6.6.3 Block Standards (for development/redevelopment greater than 50% of the block)		#5 – Any frontage along all 'A' Streets and Civic Space not defined by a building at the BTZ shall be defined by a 4-foot high Street Screen. Furthermore, service areas along all streets and alleys shall be defined by a Street Screen that is at least as high as the service equipment being screened. Required Street Screens shall be of either the same building material as the principal structure on the lot or masonry or a living screen composed of shrubs planted to be opaque at maturity. Species shall be selected from the Plant List in Section 11.1 of this Plan. The required Street Screen shall be located at the setback line along the corresponding frontage.
Block Face Dimensions 250 feet (min.) 600 feet (max.)			
Block Perimeter 2000 feet (max.)			
6.6.4 Height Standards	Principal Building Standards [rename]		
	Maximum structure height 2 stories or 26 feet (max.) (See also Section 7.2) K		
	First floor-to-floor height 15 feet (min.) (see #4) M		
	Ground Floor finish level 12 inches max. above sidewalk (for ground floor Commercial Ready buildings) L		
	Upper floor-to-floor height 10 feet (min.) (see #4) N		
6.6.5 Frontage Requirements		#6 – Corner buildings may exceed the maximum building height by 15% for 20% of the building's frontage along each corresponding street façade.	
Ground floors of all buildings fronting an intersection shall be built to Commercial Ready standards including first floor-to-floor height, ingress and egress, handicap access, and first floor elevation flush with the sidewalk.			

6.0 Site Development Standards SU-2 VHET

6.6.6 Parking and Service Access	(i) Parking Location	6.6.7 Other Standards																					
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7.0 Site Development Standards General to All Zones

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7.0 Site Development Standards General to All Zones

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7.1. Gated Communities: In order to meet the intent of a walkable, urban environment that serves as a Major Activity Center, gated and/or walled developments are prohibited.

7.2. Structure Heights

7.2.1. Structures within the Impact Area as designated by the Rank III Northwest Mesa Escarpment Plan shall be limited to 15 feet. No exceptions or variances shall be granted.

7.2.2. Structures within 200 feet of either the Rank III Northwest Mesa Escarpment Plan Impact Area or a Park Edge Street shall be limited to 18 feet, with a second story up to 26 feet permitted for no more than 50% of the building footprint.

7.2.3. In Town Center only, if a developer can show documentation that a major employer (i.e. 200+ employees on-site) will be part of the project, that project is exempt from height maximums and bonus height criteria, subject to a Development Agreement with the City, including clawback provisions in the case the employer leaves the Plan area before 5 years have passed. This provision is to be approved by the City Economic Development Department.

7.2.4. Structure height shall be measured from approved grade to the eave line in the case of a pitched roof or to the roof line in the case of a parapet. See also **Section 7.4.**

7.2.5. A setback may occur at a minimum height of 26 feet, but no building shall be built taller than 40 feet without a Setback. The minimum depth of a setback shall be 12 feet.

7.2.6. Cupolas may extend 10 feet beyond height limits. Flagpoles may extend 6 feet above the height limit. Equipment on the roof or visible from the public right-of-way shall be screened by louvered or metal screen, or by a parapet for flat roofs. Screened equipment shall not exceed 6 feet beyond height limits. Screened equipment on the roof shall be set back 15 feet from façades. Equipment Screening shall not be made of chain link or vinyl materials.

7.3. Optional Bonus Heights

7.3.1. **Purpose / Intent:** This Plan offers an optional bonus height system in order to balance the intent to create an urban, walkable Major Activity Center and the intent to respect and honor the unique cultural, historical, geological, and volcanological area. In order to go beyond height maximums in the Center and Mixed-Use Zones (which correspond to maximum heights in the City's NWMEP), development projects shall provide commensurate benefits to both the Natural and Built Environments.

7.3.2. Structure height maximums are set by Site Development Standards for each zone. All non-Transition Zones are eligible to opt in to a voluntary bonus height system per **Table 7.1** and **Table 7.2**, with associated criteria in **Table 7.3.**

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7.0 Site Development Standards General to All Zones

- 7.3.3. In order to earn a height bonus in non-Transition Zones, projects shall incorporate required criteria to earn points for both the Built Environment and Natural Environment, per **Table 7.1**, **Table 7.2**, and **Table 7.3**. A project may shift up to 10 points between the Natural and Built Environment totals in order to better meet the intent of this Plan and/or accommodate site context.
- 7.3.4. The Planning Director or his/her designee shall certify and approve projects that meet the criteria for height bonuses in **Table 7.3**. The Volcano Heights Review Team may be consulted to assess whether individual standards have been met to satisfy the bonus criteria. See **Section 3.2.6**.
- 7.3.5. Projects that incorporate a bonus shall be required to provide a Site Development Plan for Building Permit or Site Development Plan for Subdivision that documents bonus criteria. Either Site Development Plan shall be eligible for administrative approval by the Planning Director. Such projects are subject to code enforcement to ensure ongoing provision of bonus criteria in the future, including maintenance of private amenities accessible by the public and preservation of natural vegetation in perpetuity.

7.4. Grading

- 7.4.1. **Purpose / Intent:** This Plan restricts grading in order to protect natural topography and distinct topographic and natural character to the extent possible. Natural terrain and geological features make the area unique, desirable, and of value to residents and the larger community.
- 7.4.2. Cut and fill slopes shall be no steeper than 3:1 on average, and retaining walls shall not exceed 4 feet in height unless incorporated within a building’s foundation or approved by the City Hydrologist.
- 7.4.3. Graded areas shall maintain the character of the natural terrain by varying gradients, undulating contours, and rounding the toe and crest of any slope greater than 10 feet in height.
- 7.4.4. Fill shall be limited to the minimum required for site development and drainage. When more than 4 feet of fill is proposed, a project shall require DRB approval in order to ensure coordination with the City Hydrologist, Open Space Division, and other relevant City staff.
- 7.4.5. Fill shall not exceed the existing highest natural grade point on site, unless approved by the City Hydrologist for required drainage. Fill height shall be measured from the natural grade.



TABLE 7.1 – TOTAL POINTS FOR HEIGHT BONUS BY CHARACTER ZONE

Height Bonus Criteria	Height Bonus			
	VHTC	VHRC	VHVC	VHMX
Bonus Heights	75 ft.	60 ft.	60 ft.	40 ft.
I. Natural Environment Bonus Criteria	50 pts.	35 pts.	25 pts.	50 pts.
II. Built Environment Bonus Criteria	50 pts.	40 pts.	25 pts.	50 pts.
Total Points Required to Receive Height Bonus	100 pts.	75 pts.	50 pts.	100 pts.

7.0 Site Development Standards General to All Zones

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TABLE 7.2 – BONUS HEIGHT POINTS PER CRITERION

Criteria for Optional Height Bonuses	Points
I. Natural Environment Bonus Criteria	
a. Rock outcropping dedications	25
b. Rock outcropping private preservation*	20
Public Access Easement (<i>Optional Bonus</i>)	5
c. Open space dedications	20
For dedications abutting rock outcroppings or other features deemed significant by the City Open Space Division (<i>Optional Bonus</i>)	5
d. Private park construction	20
e. Public park land dedication of at least 2 acres	15
f. Private walkway connection to rock outcropping, parks, trails, rock outcroppings, or other features deemed significant by the City Open Space Division	10
Up to 2 additional connections to features deemed significant by the City Open Space Division (<i>Optional Bonus</i>)	5 each
g. Landscaping / natural buffers in addition to what's required by Zone	10
h. Shared drainage/swales on adjacent developments / Low Impact Design	10
i. View preservation	10
j. Community garden	10
k. Interpretive signage	5
l. Preservation of native vegetation	5
m. Other benefits to the natural environment	5

Criteria for Optional Height Bonuses	Points
II. Built Environment Bonus Criteria	
a. Plazas / courtyards	25
b. Living roof or accessible roof garden	25
c. Commercial-ready ground floor	25
d. Transit shelters on transit corridors	20
e. LEED certification	20
f. Transit Center within 500 feet of proposed project	15
g. Solar panels	15
h. Permeable paving	10
i. Permanent streetscape/plaza furniture and/or features	10
j. Public art	10
k. Rooftop water harvesting / cisterns	5
l. Other sustainable building practices	5

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7.0 Site Development Standards General to All Zones

TABLE 7.3 – STANDARDS FOR OPTIONAL HEIGHT BONUS CRITERIA

I. Natural Environment Bonus Criteria	
a.	Dedicating a rock outcropping shown in Exhibit 10.1 shall meet the criteria for these points. The land shall be dedicated to the City following the process outlined in City Zoning Code Section 14-16-3-8(A) or by plat, deed, or other legal instrument acceptable to City Real Property.
b.	Private preservation may be accomplished through Transfer of Development Rights (TDR), conservation easement, or platting as private open space. The additional 5 points for public access may be accomplished through granting a public access easement that stays in place in the event of transfer of property ownership.
c.	Open space dedications, either on-site or offsite, shall be one-half acre or larger. Points may be granted at a rate of 5 points per acre up to a total of 20 points.
d.	A private park shall be one-half acre or larger and maintained by the property owner or property owner association. A public access easement shall be granted and filed with the City. [Language pending]
e.	Land dedicated for City public parks shall be 2 acres or larger and must meet the approval of the City Parks and Recreation Department.
f.	Private walkways constructed according to criteria in Table 10.3 (ix) shall meet the criteria for these points. Private walkways that provide connections to rock outcroppings or other features deemed significant by City Open Space Division, including those features mapped in Exhibit 10.1 , shall be eligible for an additional 5 bonus points per feature connected.
g.	[Language pending]
h.	Shared swales shall be designed for the secondary purpose of additional open space OR to water landscaping on the site or streetscaping. They shall be privately constructed and maintained.
i.	Developments that can graphically demonstrate an arrangement of roadways, building placement, and/or building setback to preserve views toward the Sandia Mountains, volcanoes, or City vistas across the Rio Grande shall meet the criteria for these points.
j.	See Table 10.3 (vi) .
k.	Projects that include at least 2 educational, cultural, and/or historical signs or markers pertaining to Volcano Heights that are accessible to pedestrians and cyclists shall meet the criteria for these points. Signs shall be a minimum of 18 by 18 inches and a maximum of 24 by 36 inches.
l.	Projects that preserve native vegetation, especially juniper trees, are eligible for 5 bonus points per 100 square feet up to 50 total points. Native vegetation in usable and/or detached open space on-site may be counted toward this bonus. Applicant shall provide a site plan, photographs, and other relevant documentation of original site conditions.
m.	Applicants may demonstrate other benefits to the natural environment. These points may be granted per the discretion of the Planning director or his/her designee.



7.0 Site Development Standards General to All Zones

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TABLE 7.3 – STANDARDS FOR OPTIONAL HEIGHT BONUS (Cont'd)

II. Built Environment Bonus Criteria	
a.	[Pending]
b.	[Pending - Hydrology] Improving rainwater quality by removing sediment and floatables.
c.	[Pending]
d.	Transit shelters shall be built per City ABQRide standards and in coordination with ABQRide staff. Transit shelters built on private land (i.e. not in the public ROW) may also be counted as Usable or Detached Open Space. [Confirm language]
e.	LEED certification for buildings or for Neighborhood Design are both eligible for points toward the Height Bonus. Individual LEED elements matching other criteria in this Table shall also be eligible for points toward the Height Bonus.[Pending]
f.	[Pending]
g.	[Pending]
h.	[Pending]
i.	Permanent furniture/features includes Interactive public amenities such as but not limited to lifesize gameboards, tables & seating, low water-use fountains, amphitheaters, labyrinths, shade structures, etc.
j.	[Pending]
k.	[Pending]
l.	[Pending]

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8.0 Building Design Standards General to All Zones

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8.0 Building Design Standards General to All Zones

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8.1. Purpose/Intent: The Building Design Standards for the Volcano Heights Sector Development Plan shall establish a coherent urban character and encourage enduring and attractive development. All development plans shall comply with the standards below.

Three key design principles for Volcano Heights support the preservation, sustainability, and visual quality of different development character zones. The key design principles include the following:

- Buildings shall use building elements and details to achieve a pedestrian-oriented public realm within the Town and Village Centers and Mixed-Use and Transition Zones, along 'A' Streets, and at 'A' street intersections.
- Compatibility is not meant to be achieved through uniformity, but through variations in building elements to achieve individual building identity.
- Building façades shall include architectural details and ornamentation to create variety and interest.

The design of buildings and their relationship to adjacent streets shall depend on the context of the development. Generally, the corridor becomes more pedestrian-oriented farther from the Paseo del Norte/ Unser Boulevard intersection within each development quadrant, along the proposed Bus Rapid Transit Corridor, and particularly within the Town Center. Generally, buildings shall be located and designed to provide visual interest and create enjoyable, human-scaled spaces between and among buildings.

8.2. Building Orientation

8.2.1. Buildings shall be oriented toward 'A' Streets, where the lot has frontage along an 'A' Street. All other buildings shall be oriented toward Civic Spaces. Where a building does not front on an 'A' Street or Civic Space, the building shall be oriented toward a 'B' Street.

8.2.2. Primary entrances to buildings shall be located on the street along toward which the building is oriented. At intersections, corner buildings may have their primary entrances oriented at an angle to the intersection.

8.2.3. All primary entrances shall be oriented to the public sidewalk for ease of pedestrian access. Secondary and service entrances may be located from internal parking areas or alleys.

8.2.4. Garages, carports, or new surface parking for Residential Buildings shall be located and accessed from 'B' Streets or alleys at the rear of residential buildings.

8.2.5. Service entrances shall be screened from abutting single-family residences as well as the public right-of-way as per **Section 8.8.1** in this Plan.

8.0 Building Design Standards General to All Zones



8.3. Building Massing and Scale

- 8.3.1. Commercial and mixed-use buildings shall be simply massed with flat or low pitched roofs with parapets, projecting roofs, or both. Commercial and mixed-use buildings located on a corner have flexibility for corner treatments but must adhere to the frontage requirements in the Site Development Standards in **Section 6**.
- 8.3.2. Residential Buildings shall have relatively flat fronts and simple roofs with most building wing articulations set at the rear of the structure. Window projections, stoops, porches, balconies, and similar extensions are exempt from this standard.

8.4. Building Color

- 8.4.1. In order to minimize the visual impact of development, colors shall be regulated per NWMEP [Appendix E].
- 8.4.2. Stucco, block, and/or brick shall have integral color.
- 8.4.3. In keeping with New Mexico tradition, accent colors on front doors, window sashes, and other incidental elements are allowed.
- 8.4.4. The color of residential garage doors shall blend with or complement the exterior wall color.

8.5. Design of Parking Structures

- 8.5.1. All frontages of parking structures located on 'A' Streets or Civic Spaces shall not have parking uses on the ground floor to a minimum depth of 30 feet along the street frontage.
- 8.5.2. The amount of street frontage devoted to a parking structure shall be minimized by placing the shortest dimension along a street edge.
- 8.5.3. Parking structure façades on all 'A' Streets or Civic Spaces shall be designed with both vertical (façade rhythm of 20 feet to 30 feet) and horizontal (aligning with horizontal elements along the block) articulation.
- 8.5.4. Where above-ground structured parking is located at the perimeter of a building with frontage along an 'A' Street or Civic Space, it shall be screened in such a way that cars on all parking levels are completely screened from view from all adjacent public streets. Parking garage ramps shall not be visible from any public street. Ideally, ramps should not be located along the perimeter of the parking structure. Architectural screens shall be used to articulate the façade, hide parked vehicles, and shield lighting.
- 8.5.5. When parking structures are located at corners, corner architectural elements shall be incorporated such as corner entrances, signage, and glazing.



8.0 Building Design Standards General to All Zones



8.5.6. Parking structures and adjacent sidewalks shall be designed so pedestrians are clearly visible to entering and exiting automobiles and, in turn, can see the automobiles.

8.6.3. Three-car garages are not permitted on lots equal to or less than 48 feet wide. Three-car garages on lots greater than 48 feet wide shall have a third garage setback of 3 feet minimum from the primary garage façade.

8.6. Design of Residential Garages

8.6.1. Garages shall not dominate the front façade. Street-fronting garages shall be per the requirements of **Table 8.1**. Garages shall not exceed 50% of the total front façade.

8.6.4. Property owners intending to build garages on the property lot line shall submit a platted and filed maintenance easement agreement signed by the adjacent property owner prior to being issued a building permit.

8.6.2. Garage doors shall be set back a minimum of 1 foot from the garage façade to create a “shadow box” that minimizes the prominence of the garage door.

8.6.5. The color of garage doors shall blend with or complement the exterior wall color in order to minimize the prominence of the garage door.

8.6.6. See **Table 8.1** for additional garage requirements.

TABLE 8.1 - GARAGE TYPES

Lot Width	Allowable Garage Types*	Front Garage Setback from Main Facade	Side Garage Setback from Property Line	Rear Garage Setback from Property Line
40+ Feet	A,B,C,D,E,F	10 ft. Minimum	5 ft. Minimum	2 ft. Minimum 5 ft. Maximum
Less than 40 Feet	A,B,C,E	10 ft. Minimum	None	2 ft. Minimum 5 ft. Maximum

Note 1: Garage Type D shall have a minimum of 5 linear feet of fenestration on the street façade and be articulated to resemble main structure.

Note 2: Garage Type F may be accessed from either front or side.

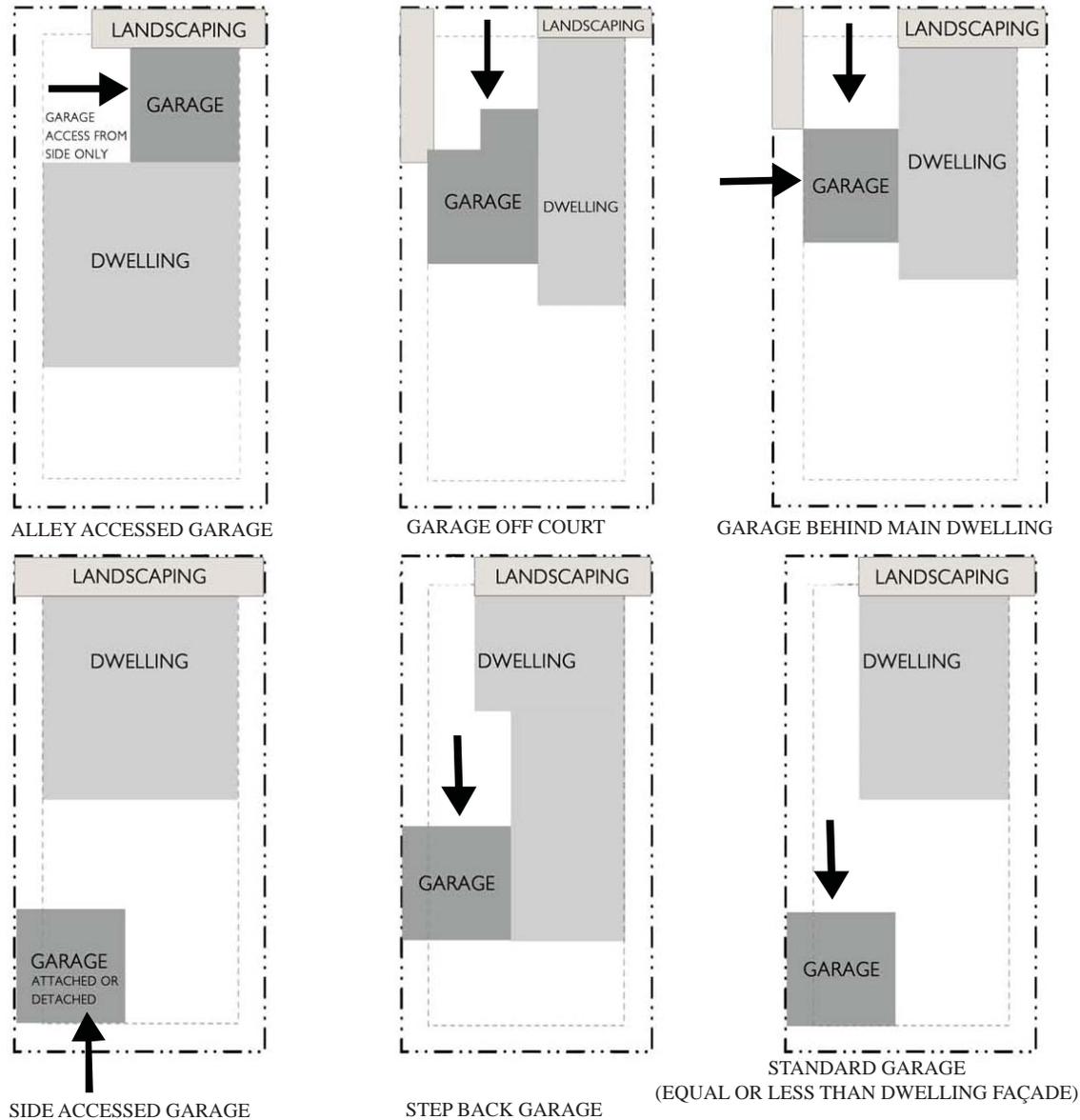
Note 3: Driveway access from street, including curb cut, is limited to 12 feet for Garage Types B, C, D, E, and F except where providing access from alleys.

Note 4: Where alleys are available, residential garages must be accessed via the alley.

Note 5: Where there is no setback from the property line, gutters and downspouts shall drain to the street or water harvesting area to avoid impact to abutting lots.

* See Exhibit 8.1 – Garage Type Diagrams

8.0 Building Design Standards General to All Zones



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STANDARD GARAGE (EQUAL OR LESS THAN DWELLING FAÇADE)

Exhibit 8.1 – Garage Types Diagram

8.0 Building Design Standards General to All Zones

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8.7. Design of Automobile Related Building Site Elements

- 8.7.1. Drive-through lanes for commercial uses shall not be located along 'A' Street. Along Paseo del Norte, Unser Boulevard, or non-mandatory 'B' Streets. No more than 50% of a lot's frontage along a 'B' Street shall be occupied by gas pumps, canopies, and/or service bays.
- 8.7.2. Any buildings associated with any automobile related use shall also have a pedestrian entrance on an 'A' Street.
- 8.7.3. Outdoor storage of vehicles or other products sold shall not be permitted along any 'A' Street. Along 'B' Street, outdoor storage of vehicles or other products sold shall not exceed 50% of a lot's frontage.
- 8.7.4. All off-street loading, unloading, and trash pick-up areas shall be located along 'B' Streets or alleys unless permitted in the specific Site Development Standards in **Section 6**.

8.8. Street Screen

- 8.8.1. Any off-street loading, unloading, or trash pick-up areas shall be screened using a Street Screen at least as tall as the trash containers and/or service equipment it is screening at the BTZ. The Street Screen shall be made up of (a) the same material as the principal building or (b) a living screen or (c) a combination of the two.

- 8.8.2. Parking visible from the public ROW along an 'A' or 'B' Street shall have a street screen of masonry, metal railing, vegetation or a combination of these. This street screen shall be a minimum of 3 feet and no more than 6 feet tall.

- 8.8.3. 'B' Street drive-through lanes shall be hidden behind the building or a Street Screen.

- 8.8.4. Utility Equipment, including electrical transformers, gas meters, etc., shall be screened with a Street Screen at least as high as the equipment being screened.

8.9. Antennas

- 8.9.1. Antennas, including cell, accessory, and mounted on top of buildings, shall be permitted on rooftops.
- 8.9.2. Antennas shall be architecturally integrated and/or screened entirely with a screen of same color as the principal building.
- 8.9.3. Antennas shall not be visible from adjacent 'A' Street.

8.10. Rainwater Harvesting Equipment

- 8.10.1. Not permitted on any 'A' Street.
- 8.10.2. On all other frontages, they shall be screened with a Street Screen at least as high as the equipment being screened.



9.0 Sign Standards

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9.0 Signage Standards

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9.1. Applicability

- 9.1.1. Except as specifically listed below, all other signage and sign standards shall comply with Section 14-16-3-5 of the City Zoning Code, as amended.
- 9.1.2. Signs along Unser Boulevard must also comply with the Design Overlay Zone for that roadway adopted in 1992.
- 9.1.3. Where conflicting, the most restrictive regulation prevails. Where the Zoning Code, Design Overlay Zone, or Plan is silent, relevant regulations in the other Plans prevail.
- 9.1.4. For new signs, the standards in **Table 8.1** shall apply and sign permits approved administratively unless specifically noted in this section.
- 9.1.5. Definitions and examples of each sign type are included in **Section 3.5** of this Plan.

9.2. **Unique Sign Applications:** An applicant has the option to establish unique sign standards including size, color, type, design, and location. Such applications shall be reviewed as “Unique Sign Plans” by the Planning Director or his/her designee and are subject to approval of the DRB. In evaluating a Unique Sign Plan, the DRB shall consider the extent to which the application meets the following:

- 9.2.1. Promotes consistency among signs within a development thus creating visual harmony between signs, buildings, and other components of the property;
- 9.2.2. Enhances the compatibility of signs with the architectural and site design features within a development;
- 9.2.3. Encourages signage that is in character with planned and existing uses thus creating a unique sense of place; or
- 9.2.4. Encourages multi-tenant commercial uses to develop a unique set of sign regulations in conjunction with development standards.

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TABLE 9.1 –SIGN TYPES BY CHARACTER ZONE

Character Zone	Town Center	Regional Center	Village Center	Mixed-Use	Neighborhood Transition	Escarpment Transition	Standard
Sign Type							
Address Signs	P	P	P	P	P	P	Per Section 14-16-3-5 of the City Zoning Code.
Attached Signs	P	P	P	P	P*	P*	<ul style="list-style-type: none"> For all ground floor commercial uses (retail, office, and restaurant): One sign per tenant space; area to be calculated at 1.5 SF per linear foot of tenant space façade along the public street frontage with a maximum of 100 SF per tenant. Second and upper floor commercial uses may also be permitted one second floor wall sign per tenant space per public street frontage; area to be calculated at 1.5 SF per linear foot of second or upper floor frontage along that public street with a maximum of 125 SF. Institutional uses (non-profits and churches): One sign per tenant space; area to be calculated at 1.5 SF per linear foot of public street frontage with a maximum of 100 SF. Live-Work and Home occupations: One sign limited to an area of 20 SF max. Building sign may encroach a maximum of 12 inches onto a sidewalk while maintaining a vertical clearance of 8 feet from the finished sidewalk if it is non-illuminated and 11 feet from the finished sidewalk if it is illuminated. Building signs may be internally or externally lit. Marquee signs as only permitted as specified below.
Banners	P	P	P	P	P	P	Per Section 14-16-3-5 of the City Zoning Code.

* Commercial and live-work uses only.



9.0 Signage Standards

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TABLE 9.1 -SIGN TYPES BY CHARACTER ZONE (Cont'd)

Character Zone	Town Center	Regional Center	Village Center	Mixed-Use	Neighborhood Transition	Escarpment Transition	Standard
Sign Type							
Blade Signs, Building	P	P	P	P	P**	P**	<ul style="list-style-type: none"> • One per building (commercial and mixed use buildings only) • Area = 30 SF maximum per sign face. • May encroach a maximum of 6 feet over a sidewalk but shall not encroach over any parking or travel lane. • Building blade signs may be attached to the building at the corners of building or along any street facing façade above the first floor facade. • Minimum vertical clearance from the finished sidewalk shall be 11 feet.
Blade Signs, Tenant	P	P	P	P	P**	P**	<ul style="list-style-type: none"> • One per commercial tenant space (retail, office, or restaurant use). • Area = 16 SF maximum per sign face. • May encroach a maximum of 4 feet over a public sidewalk, but shall not encroach over any parking or travel lane. • Tenant blade signs shall be oriented perpendicular to the building façade and hung under the soffit of an arcade or under a canopy/awning or attached to the building façade immediately over the ground floor tenant space while maintaining a vertical clearance of 8 feet from the finished sidewalk for non-illuminated signs and 11 feet from the finished sidewalk for illuminated signs.

** Commercial uses only.

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CHAPTER II: REGULATIONS
9.0 Signage Standards



TABLE 9.1 –SIGN TYPES BY CHARACTER ZONE (Cont'd)

Character Zone	Town Center	Regional Center	Village Center	Mixed-Use	Neighborhood Transition	Escarpment Transition	Standard
Sign Type							
Directory Signs	P	P	P	P	P	P	<ul style="list-style-type: none"> • Shall be allowed for all multi-tenant commercial and mixed use buildings only. • One directory sign per multi-tenant building limited to 12 SF in area. • Design of the sign shall be integral to the façade on which the sign is to be affixed.
Electronic Signs							Per Section 14-16-3-5 of the City Zoning Code.
For Sale/For Lease Signs	P	P	P	P	P	P	<ul style="list-style-type: none"> • Size is limited to 32 SF per sign face. • All other standards are per Section 14-16-3-5 of the City Zoning Code.
Interpretive Signs	P	P	P	P	P	P	Per Table 7.3.k in this Plan.
Marquee Signs	P	P	P	P	NP	NP	<ul style="list-style-type: none"> • Permitted for theaters, auditoriums, and other public gathering venues of 100 persons or more. • Marquee signs shall be attached to the building or located above or below a canopy only. • Area = 100 SF maximum. • Message board may be changeable copy (non-electronic). • Electronic message boards are regulated per City Zoning Code 14-16-3-5.
Monument Signs	P	P	NP	P	NP	P**	<ul style="list-style-type: none"> • One monument sign per lot per lot street frontage (no more than 2 per lot separated by at least 100 feet) limited to a maximum of 75 SF per sign face and 6 feet in height.

** Commercial uses only.



9.0 Signage Standards

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TABLE 9.1 –SIGN TYPES BY CHARACTER ZONE (Cont'd)

Character Zone	Town Center	Regional Center	Village Center	Mixed-Use	Neighborhood Transition	Escarpment Transition	Standard
Sign Type							
Sandwich Board Signs	P	P	P	P	P	P	<ul style="list-style-type: none"> Permitted only for retail, service, or restaurant uses. Limited to 8 SF per sign face per storefront. May not exceed 4 feet in height. A minimum of 6 feet of sidewalk shall remain clear. May use chalkboards for daily changing of messages. Shall be removed every day after the business is closed.
Window Signs	P	P	P	P	p **	p**	<ul style="list-style-type: none"> Limited to 10% of the window area. In the Transition Zones, window signs are only permitted for commercial uses (including the “work” component of live-work uses). The following shall be exempt from this limitation: Addresses, closed/open signs, hours of operation, credit card logos, real estate signs, and now hiring signs. Mannequins and storefront displays of merchandise sold. Interior directory signage identifying shopping aisles and merchandise display areas.
Temporary Construction Signs	P	P	P	P	P	P	Per Section 14-16-3-5 of the City Zoning Code.

** Commercial uses only.

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10.0 Open Space Standards

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10.0 Open Space Standards

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10.1. Purpose / Intent: Open space standards are intended to provide a balance between the built and natural environments in order to provide open space that can help relieve pressures from urban areas. Open spaces are also meant to maintain the vital, geological, and cultural link to the volcanic Escarpment and the Petroglyphs.

10.2. Applicability

10.2.1. This Plan differentiates residential, mixed-use, and non-residential developments.

10.2.2. This Plan also differentiates developments by the following sizes:

- Less than 2 acres
- 2-5 acres
- Greater than 5 acres

10.3. Detached Open Space Standards

10.3.1. **Purpose / Intent:** Detached Open Space is required to help provide relief from density to balance the urban environment of the proposed Major Activity Center. In addition, detached open space is intended to help preserve the integrity and fabric of this unique natural, cultural, geological, and volcanological area.

The Detached Open Space requirements below are calibrated to result in desired development densities with enough open space to preserve rock outcroppings (see **Exhibit 10.1**), a buffer of 100 feet, and other sensitive lands as well as to create private parks and other civic and open space amenities through optional height bonuses in the non-Transition Zones. [The total acreage of the outcroppings plus a buffer of 100 feet is 80+ acres.]

10.3.2. Detached Open Space standards are a zoning regulation separate from any subdivision regulations, such as those associated with Impact Fees. The City calculates and tracks these fees and any associated credits separately. The Detached Open Space standards shall not count toward Subdivision Impact Fee credits associated with Parks and/or Open Space.

10.3.3. Detached Open Space standards are separate from the Usable Open Space requirement.

10.3.4. Detached Open Space may be counted toward optional height bonus criteria. See **Section 7.3**.

10.3.5. **Detached Open Space** shall be provided via the alternatives listed in the City Zoning Code Section 14-16-3-8(A): (1) on-site, (2) dedication, or (3) cash-in-lieu. Dedications shall be for land within the Volcano Heights Plan area.

10.3.6. **Residential or Mixed-Use Development:** Following the intent of the RD Zoning per City Zoning Code Section 14-16-2-14(F) that these SU-2 zones have replaced in Volcano Heights, for all residential or mixed-use development, 400 SF of Detached Open Space shall be provided per residential dwelling unit until the density threshold (i.e. dwelling unit per acre) per **Table 10.1** is reached on the site.

10.3.7. **Non-Residential Development:** Following the intent to preserve open space in Developing Urban areas per City Zoning Code Section 14-16-3-18, for all exclusively non-residential developments, a minimum of 2,400 SF of open space shall be provided for every 30,000 SF of building area.

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10.0 Open Space Standards



TABLE 10.1 – DETACHED OPEN SPACE: RESIDENTIAL USES

	VHTC	VHRC	VHVC	VHMX	VHNT	VHET	Total
Available Acreage*	61.2	89.1	10.8	162	28.8	54.9	406.8
Detached OS Requirement (square feet/dwelling unit)	400	400	400	400	400	400	400
Detached OS Requirement Cap (dwelling unit/acre)	40	20	30	30	6	6	—
Intended Resulting Detached Open Space Acreage**	22	16	3	45	2	3	91

* Total acreage minus Mandatory Roads minus 10% for Usable Open Space
 ** For purposes of analysis only

TABLE 10.2 – DETACHED OPEN SPACE: NON-RESIDENTIAL USES

	VHTC	VHRC	VHVC	VHMX	VHNT	VHET	Total
Available Acreage*	61.2	89.1	10.8	162	28.8	54.9	406.8
Detached OS Requirement (square feet/30,000 SF building area)	2,400	2,400	2,400	2,400	NA	NA	400
Maximum Stories with Height Bonus	5	4	4	3	NA	NA	—
Intended Total Detached Open Space Acreage**	17	20	3	27	0	0	66

* Total acreage minus Mandatory Roads minus 10% for Usable Open Space
 ** For purposes of analysis only

10.4. Usable Open Space Standards

10.4.1. All sites in Volcano Heights shall provide a minimum of 10% of their site acreage as Usable Open Space on-site with the following exception:

- (i) Usable Open Space is not required for mixed-use or non-residential properties if located within 1,500 feet of a park, plaza or other Usable Open Space that is at least one acre in size and accessible to the public.

10.4.2. Up to 60% of the required 10% of Usable Open Space may be transferred across property lines and/or pooled to create larger open space areas within the Volcano Heights Plan area. (See Section 10.3 Detached Open Space Standards.) In this event, the development and open space improvements shall be constructed concurrently.

10.4.3. When phasing of a Site Development Plan is involved, each phase must meet the 10% Usable Open Space requirement. Subsequent phases shall not be built until the preceding phase’s Usable Open Space has been built and/or landscaped. Under no circumstances shall the Applicant be allowed to defer providing and developing the required Open Space for each phase.

10.4.4. As part or all of their required Usable Open Space, projects greater than 5 acres shall incorporate a publicly accessible plaza/patio/courtyard, amphitheater, or recreational area. This may also be counted toward an optional height bonus. (See Table 7.2 for criteria, Table 7.3 for standards, and Table 10.3 for private open space standards.)

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- 10.4.5. Projects 2-5 acres in size that are exclusively residential development shall incorporate a recreational area or playground a minimum of 2,400 SF in size as part or all of their required Usable Open Space, which can also count toward an optional height bonus. (See **Table 7.2** for criteria, **Table 7.3** for standards, and **Table 10.3** for private open space standards.)
- 10.4.6. Projects 2-5 acres in size that are non-residential or mixed use shall incorporate a publicly accessible plaza/patio/courtyard, amphitheater, or roof garden at least 1,500 SF in size, which can also count toward an optional height bonus. (See **Table 7.2** for criteria, **Table 7.3** for standards, and **Table 10.3** for private open space standards.)
- 10.4.7. Projects less than 2 acres in size may incorporate any combination of Open Space elements to satisfy the 10% Usable Open Space requirement. (See **Table 10.3**)
- 10.4.8. A minimum of 40% of the required on-site Open Space shall be permeable to rainwater. This requirement may be met through landscape area, permeable paving, trails, etc. This area may be broken up on the site but shall remain accessible and intended for public use.
- 10.4.9. Any developed Usable Open Space (i.e. Pedestrian Plaza, amphitheater, playground, etc.) shall include a landscaped portion equal to a minimum of 40% of the on-site Open Space. A minimum of 15% of all developed Open Space shall be shaded from the summer sun with trees and/or permanent or temporary shade structures.
- 10.4.10. Planting strips required as part of the streetscape standards (see **Section 4.9**) shall not count toward the on-site open space requirement.
- 10.4.11. The private open space amenities per criteria in **Table 10.3** shall satisfy the 10% Usable Open Space requirement.
- 10.4.12. Private open space amenities shall be the responsibility of property owners/developers for the cost of construction, and ongoing maintenance and liability shall also be privately funded, such as by a homeowner or merchant association. To be dedicated to the City for ongoing maintenance and liability responsibility, they must meet City standards and be acceptable to the relevant City department.
- 10.4.13. For all non-residential or mixed-use projects, on-site open space shall be open and accessible to the general public, with the exception of balconies, porches, and courtyards.
 - (i) All open space areas shall connect to pedestrian walkways. If not easily identifiable from the abutting walkway, as determined by the Planning Director or his/her designee, signage must be provided to direct the public.
 - (ii) **A public access easement shall be granted to provide public access to private open space amenities.**
 - (iii) **Private open space amenities accessible to the public shall be marked with a sign with contact information for the party responsible for maintenance and liability. See Table 9.1 for sign standards.**

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10.4.14. **Rock Outcroppings:** Significant rock outcroppings that are preserved shall count double their square footage as a landscaped area. (See **Section 3.5** for definition and **Exhibit 10.1** for locations and sizes.)

10.5. Petroglyphs: Development, trails, and recreation areas shall be set back at least 50 feet from prehistoric petroglyphs or other sites with high archaeological value as identified by City Open Space Division and/or Planning Director, unless approved by Planning Director (or his/her designee). This setback may be counted as Usable Open Space.

10.5.1. No petroglyph shall be moved, altered, or defaced without written approval of a National Park Service representative from the Petroglyph National Monument in consultation with City of Albuquerque Open Space Division staff.

10.5.2. All other archaeological sites shall be protected or mitigated per City Zoning Code §14-16-3-20. Site treatment shall include preservation, avoidance, testing, or documentation of surface and/or subsurface remains and/or artifacts.

10.5.3. All rock outcroppings containing petroglyphs shall be protected per City Zoning Code §14-16-3-20.

10.6. Public/Civic Space Standards

10.6.1. [Language pending]

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TABLE 10.3 – PRIVATE OPEN SPACE CRITERIA

Private Open Space Type	Standards and Criteria
(i) Balcony	<ul style="list-style-type: none"> Balconies that are not flush shall be a minimum of 5 feet clear in depth and a minimum of 8 feet in width. Balconies may be semi-recessed or recessed. Projecting metal or slab balconies are only permitted if they have some means of visible support.
(ii) Patio	<ul style="list-style-type: none"> Patios shall be a minimum of 150 SF. Patios shall have a clear sense of enclosure and separation from the public realm.
(iii) Courtyard	<ul style="list-style-type: none"> Courtyards shall be surrounded on all sides by buildings with at least one pedestrian connection to an adjoining building or public sidewalk. The courtyard shall be a minimum of 200 SF. Courtyard may be landscaped or a combination of landscape and hardscape. However, they shall contain amenities for residents such as seating, water features, etc.
(iv) Forecourt	<ul style="list-style-type: none"> A Forecourt shall be surrounded on at least two sides by buildings. A Forecourt shall be a minimum of 150 SF.
(v) Playground / recreational area	<ul style="list-style-type: none"> Playgrounds shall be a minimum of 400 SF. Shaded areas, landscaping, and seating shall be provided. Seating and shade covering at least 25% of the area shall be provided. Playground equipment and design shall be reviewed and approved by the City prior to installation.
(vi) Community Garden	<ul style="list-style-type: none"> Maximum size shall be 1 acre. Gardens may be enclosed by a fence on all open sides. Fences should be installed straight and plumb, with vertical supports at a minimum of 8' on center. Chicken wire may only be used in conjunction with another permitted fencing material and must be supported along all edges. <p><u>Fencing Materials:</u></p> <ul style="list-style-type: none"> Permitted: pressure treated wood (must be painted or stained medium to dark color), chicken wire, wrought iron, painted galvanized steel Not permitted: chain link, barbed or razor wire, vinyl, un-painted/stained pressure treated wood, plywood
(vii) Roof Garden or Roof Terrace	<ul style="list-style-type: none"> A Roof Garden shall at least be 50% of the building footprint area. A Roof Terrace shall provide landscaping in the form of potted plants, seating, and other amenities for the users of the building. A Roof Terrace may also include a portion of the roof as a green roof which may or may not have public access.
(viii) Private Open Space Amenities (pool, play courts, picnic area, etc.)	<ul style="list-style-type: none"> Such private open space may be incorporated with roof terraces or courtyards based on the appropriateness of the design and accommodation of privacy.
(ix) Private Walkway to Rock Outcroppings	<ul style="list-style-type: none"> Private walkways shall be asphalt, cement, or crusher fines stabilized with binder. Private walkways that connect more than one rock outcropping are eligible for additional bonus points.

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TABLE 10.3 – PRIVATE OPEN SPACE CRITERIA (Cont'd)

Private Open Space Type	Standards and Criteria
(x) Amphitheatre	<ul style="list-style-type: none"> Size, scale, and architectural style shall complement adjacent development. Noise shall be governed by the City Zoning Code Noise Ordinance [reference]. Hours of operation shall be assessed on a case-by-case basis.
(xi) Plaza	<ul style="list-style-type: none"> Plazas shall be a minimum of 0.25 acre and maximum of 1 acre. Building frontages shall define these spaces. Plazas shall front on at least one (1) street, preferably at the intersection of important streets. The landscape should consist primarily of hardscape. Casual seating, along with tables and chairs, should be provided.
(xii) Landscaped Traffic Circle	<ul style="list-style-type: none"> [Pending]
(xiii) Landscape Buffer	<ul style="list-style-type: none"> [Pending]
(xiv) Park	<ul style="list-style-type: none"> Parks shall be a minimum of 2 acres in size, with slopes no greater than 5 to 1. There must be street frontage with on-street parking on at least 2 sides. Co-location of drainage facilities shall be evaluated on a case-by-case basis. Parks shall be built to City Standards and Specifications.
(xv) Planting Strip (wider than the required streetscaping)	<ul style="list-style-type: none"> [Pending]
(xvi) Permeable Landscaped Area	<ul style="list-style-type: none"> [Pending]
(xvii) Sidewalk width beyond that required by cross sections	<ul style="list-style-type: none"> [Pending]

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10.7. Landscaping Standards

10.7.1. Walls & Fences Material Finishes & Design

- (i) **Height & Placement:** Walls and fences shall not exceed a height of 36 inches where allowed within street-facing setbacks (except for columns that support arcades or trellises). Retaining walls in all locations shall not exceed 48 inches, unless approved by the City Hydrologist. Fences and walls shall not exceed a height of 72 inches inside required setbacks along rear and interior side property lines. Height shall be measured from the lower side within the required side or rear yard.
- (ii) **Adjacency to Monument and City Open Space:** Properties abutting the Petroglyph National Monument and/or Major Public Open Space shall use coyote fencing, post and wire (not barbed), or view fencing. View fencing allows for a general sense of openness, visual transparency, and passive surveillance, while still maintaining perimeter security.
- (iii) **Design & Prohibited Materials:** The end of walls shall have a pier or pilaster at least 12 inches wide to give a substantial appearance. In order to assure durability and minimize the visual impact of development, stucco and concrete shall have an integral color with a "light reflective value" (LRV) rating within the range of 20-50 percent. Wood board, cyclone, chain link, and razor wire fencing are prohibited.

- a. *Perimeter Walls:* Exposed plain block, including all colors, is not allowed on walls visible from or adjacent to the public right-of-way, private open space, or private or public parks.
- b. *Site Walls:* Block walls not visible from or adjacent to the public right-of-way, private open space or Major Public Open Space, or private or public parks must have integral color (i.e. plain, grey cement blocks are prohibited).

10.7.2. **Private Walkways:** In surface parking lots, private walkways shall not extend more than 75 feet without one of the following features to provide shade, spatial definition, and pedestrian-friendly amenities:

- (i) arcades,
- (ii) trellises,
- (iii) awnings, and/or
- (iv) trees.

10.7.3. **Gateway Monuments:** Pillars or walls are permitted at entry points to neighborhoods and developments. Walls shall not be more than 12 feet long and conform with **Section 10.7.1** in this Plan. Pillars shall not be more than 3 feet wide and 10 feet high. Pillars and walls shall be stucco, masonry, rammed earth, adobe, native stacked stone (or synthetic equivalent), or straw bale. Exposed plain block, of any color, is not allowed. Stucco and concrete shall have an integral color.

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10.7.4. **On-Lot Trees:** Where buildings are placed more than 10 feet from a street-side property line, at least one tree shall be planted per property within the street-side setback. Properties with a street frontage over 100 feet shall have a minimum of one tree for every 40 feet. Street trees shall be maintained by the property owner.

10.7.5. **Rainwater Quality and Management**

- (i) All new developments shall incorporate water harvesting methods to supplement landscape irrigation. Water harvesting shall capture the first 1/2-inch of rainfall. Parking surfaces and other impermeable surfaces shall route rainwater through water harvesting areas on-site with 5% grade or less.
- (ii) Where appropriate, development projects shall incorporate unobtrusive rainwater features that facilitate the detention and infiltration of rainwater and the filtration of pollutants from urban run-off. At all densities and intensities, appropriate techniques include:
 - a. permeable pavers & concrete,
 - b. infiltration beds placed below paved areas,
 - c. stone-filled reservoirs and dry-wells,
 - d. roof storage systems & cisterns designed with materials allowed by this Plan,
 - e. small “rain gardens” (low-lying with moisture-tolerant grasses, wildflowers, shrubs, and trees), and
 - f. vegetated swales (in courtyards, street medians, and planting strips).

- (iii) Materials and treatments used for rainwater management shall be natural in appearance. Channels lined by concrete or rip-rap are prohibited, unless necessary for public safety.
- (iv) Fencing shall be avoided; the bottom slopes of detention basins shall be designed for safety. In addition, a hydrological study and design may be required of new development by the City of Albuquerque to identify appropriate rainwater detention and energy dissipation features.
- (v) See also **Section 11.3 Public Rainwater Design and Management.**

10.7.6. **Construction Mitigation:** Construction within the Plan area or parts of the area shall be mitigated per **Section 11.4** in this Plan.

10.7.7. **Plant Lists:** The purpose of regulating plants is to reduce water usage, maintain the character of native plants now existing in the Petroglyph National Monument, and provide a harmonious landscape image. Two plant lists shall guide landscaping within the Plan Area. Shrubs and trees shall be nursery grown. All properties in Volcano Trails more than 200 feet from public lands such as the Petroglyph National Monument, arroyos, and other Major Public Open Space shall use plants from Plant List A and/or B. Land disturbed in development shall be re-vegetated using the appropriate Plant List. (See **Section 11.2 Construction Mitigation** in this Plan.)

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- (i) **Native Plant List A:** Lots within 200 feet of public lands such as the Petroglyph National Monument, arroyos, and other Major Public Open Space areas shall follow Plant List A to limit impact of invasive and/or non-native plants on native vegetation. This Plant List includes the plant species inventoried within the Petroglyph National Monument by the National Park Service in 1994-1995, including almost 200 plants (amended). This plant list is reproduced in full in **Section 11.1.1.**
- (ii) **Xeric Plant List B:** These plant species are the official xeric or low-water use plant list of the Albuquerque Bernalillo County Water Utility Authority (ABCWUA). The majority of the list is low and medium water-use plants. Some high-water use plants are also listed in order to classify them as such in implementation of the water conservation program. This xeric plant list is extensive and updated periodically by the ABCWUA. (See contact information provided in **Section 11.1.2** to obtain the most current information.)

10.7.8. Landscaped areas shall be covered with a minimum of 75% living vegetative materials, such as trees, grasses, vines, flowers, and/or bushes/shrubs. Coverage shall be calculated from the anticipated size of mature plants.

10.7.9. [Pending]

10.8. Site Lighting Standards

10.8.1. Lighting shall have a cut-off angle that directs light downward and only toward the property on which the light source is located, per per the New Mexico Night Sky Ordinance [74-12-1 to 74-12-10 NMSA 1978]. Light fixtures shall be of a type that throws light downward and have baffles, hoods or diffusers so that no light point source shall be visible from a distance greater than 1,000 feet. On-site light poles shall not exceed a height of 16 feet. High-intensity discharge lamps and sodium lamps shall not be used.

10.8.2. All new developments shall provide pedestrian-scaled streetlights. See **Section 4.10** in this Plan.

10.8.3. Shoe box style lighting shall not be used, except in large parking areas. Metal halide lights are encouraged. High pressure sodium lights are discouraged since they visually render all colors the same.

10.8.4. VHMx & VHRC adjacent to Neighborhood Transition = special lighting? Hours of lighting? [Language pending]



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11.1. Plant Lists

11.1.1. Native Plant List A

List of Plant Species of Petroglyph National Monument - Plants found by Bleakly during survey from August 1994 through September 1995. One hundred and ninety-two (192) plants from 40 families were identified. Arrangement is alphabetical by family, genus, and species with some synonyms and common names. An asterisk (*) before the name indicates plants listed in Barlow-Irick (1993). Nomenclature according to Kartesz (1994). Common names from various sources. Number of species in each family are in parentheses after family name. A "pound sign" (#) indicates that a voucher is housed at the UNM Herbarium.

ADIANTACEAE Maidenhair Fern Family (1)
Cheilanthes feei T. Moore SLENDER LIPFERN #

AGAVACEAE Agave or Yucca Family (1)
Yucca glauca Nutt. SMALL SOAPWEED

AMARANTHACEAE Pigweed Family (3)
Amaranthus acanthochiton Sauer GREENSTRIPE #
Amaranthus wrightii S. Wats. WRIGHT'S AMARANTH #
Tidestromia lanuginosa (Nutt.) Standl. WOOLLY TIDESTROMIA

ANACARDIACEAE Sumac Family (1)
Rhus trilobata Nutt. SKUNKBUSH, SKUNKBUSH SUMAC

APIACEAE (=UMBELLIFERAE) Parsley or Carrot Family (1)
Cymopterus acaulis (Pursh) Raf. var. fendleri (Gray)
Goodrich (Cymopterus fendleri Gray) FENDLER SPRINGPARSLEY #

ASCLEPIADACEAE Milkweed Family (1)
Asclepias subverticillata (Gray) Vail WHORLED MILKWEED

ASTERACEAE (=COMPOSITAE) Sunflower Family (42)
Acourtia nana (Gray) Reveal & King (Perezia nana Gray) DWARF
DESERT HOLLY, DWARF DESERTPEONY #
Aphanostephus ramosissimus DC. PLAINS DOZEDAISY #

Artemisia bigelovii Gray BIGELOW'S SAGEBRUSH #
Artemisia filifolia Torr. SANDSAGE, SAND SAGEBRUSH
Artemisia frigida Willd. FRINGED SAGE
Artemisia ludoviciana Nutt. ssp. alba (Woot.) Keck WHITE SAGEBRUSH #
* Bahia absinthifolia Benth. #
* Bahia dissecta (Gray) Britt.
Bahia pedata Gray BLUNTSCALE BAHIA #
Baileya multiradiata Harvey & Gray ex Gray DESERT MARIGOLD #
* Berlandiera lyrata Benth.
Brickellia californica (Torr. & Gray) Gray CALIFORNIA BRICKELLBUSH #
Chaetopappa ericoides (Torr.) Nesom (Leucelene ericoides (Torr.) Greene)
WHITE ASTER

* Chrysothamnus nauseosus (Pallas ex Pursh) Britt. ssp. bigelovii (Gray)
Hall & Clements #
Chrysothamnus pulchellus (Gray) Greene ssp. pulchellus
SOUTHWESTERN RABBITBRUSH #
Conyza canadensis (L.) Cronq. CANADIAN HORSEWEED
* Gaillardia pinnatifida Torr. #
Gaillardia pulchella Foug. FIREWHEEL
Gutierrezia sarothrae (Pursh) Britt. & Rusby BROOM SNAKEWEED #
Helianthus petiolaris Nutt. PRAIRIE SUNFLOWER
Hymenopappus flavescens Gray var. canotomentosus Gray YELLOW-FLOWERED
WHITE RAGWEED, COLLEGEFLOWER #
Machaeranthera canescens (Pursh) Gray HOARY TANSYASTER #
* Machaeranthera gracilis (Nutt.) Shinnery (Haplopappus gracilis
(Nutt.) Gray) #
Machaeranthera pinnatifida (Hook.) Shinnery (Haplopappus spinulosus
(Pursh) DC.) LACY TANSYASTER
Malacothrix fendleri Gray FENDLER DESERTDANDELION #
Melampodium leucanthum Torr. & Gray PLAINS BLACKFOOT #
* Microseris sp. Palafoxia sphacelata (Nutt. ex Torr.) Cory OTHAKE #
Parthenium incanum Kunth MARIOLA #
Pectis angustifolia Torr. var. angustifolia NARROWLEAF PECTIS #
Psilostrophe tagetina (Nutt.) Greene WOOLLY PAPERFLOWER

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Sanvitalia abertii Gray ABERT'S CREEPING ZINNIA #
 Senecio flaccidus Less. var. flaccidus (Senecio douglasii DC. ssp. longilobus (Benth.) L. Benson THREADLEAF GROUNDSEL #
 Senecio multicapitatus Greenm. ex Rydb. RAGWORT GROUNDSEL #
 Senecio riddellii Torr. & Gray RIDDELL'S RAGWORT OR GROUNDSEL #
 Stephanomeria pauciflora (Torr.) A. Nels. BROWNPLUME WIRELETTUCE #
 Thelesperma megapotamicum (Spreng.) Kuntze HOPI TEA, GREENTHREAD
 Thymophylla acerosa (DC.) Strother (Dyssodia acerosa DC.) PRICKLYLEAF DOGWEEED #
 Verbesina encelioides (Cav.) Benth. & Hook. f ex Gray GOLDENCROWNBEARD, COWPEN DAISY
 Zinnia grandifolia Nutt. ROCKY MOUNTAIN ZINNIA #

BIGNONIACEAE Bignonia Family (1)
 Chilopsis linearis (Cav.) Sweet DESERT WILLOW

BORAGINACEAE Borage Family (4)
 Cryptantha cinerea (Greene) Cronq. var. cinerea (C. jamesii Payson var. multicaulis (Torr.) Payson) JAMES' CATSEYE #
 Cryptantha crassisepala (Torr. & Gray) Greene var. elachantha I.M. Johnst. THICKSEPAL CATSEYE #
 Heliotropium convolvulaceum (Nutt.) Gray PHLOX HELIOTROPE
 Lappula occidentalis (S. Wats.) Greene var. occidentalis (L. redowskii (Hornem.) Greene) FLATSPINE STICKSEED #

BRASSICACEAE (=CRUCIFERAE) Mustard Family (7)
 Descurainia pinnata (Walt.) Britt. WESTERN TANSYMUSTARD #
 Dimorphocarpa wislizenii (Dithyrea wislizenii)

SPECTACLE POD; TOURISTPLANT
 * Lepidium montanum Nutt.
 Lesquerella fendleri (Gray) S. Wats. FENDLER BLADDERPOD #

CACTACEAE Cactus Family (6)
 Echinocereus fendleri (Engelm.) F. Seitz PINKFLOWERED HEDGEHOG CACTUS
 Escobaria vivipara (Nutt.) Buxbaum (Coryphantha vivipara (Nutt.) Britt. & Rose) SPINYSTAR
 Opuntia clavata Engelm. CLUB CHOLLA
 Opuntia imbricata (Haw.) DC. TREE or WALKINGSTICK CHOLLA
 Opuntia phaeacantha Engelm. BROWNSPINE PRICKLYPEAR
 Opuntia polyacantha Haw. PLAINS PRICKLYPEAR

CAPPARACEAE Caper Family (1)
 Polanisia dodecandra (L.) DC. ssp. trachysperma (Torr. & Gray) Ilitis SANDYSEED CLAMMYWEED #

CHENOPODIACEAE Goosefoot Family (5)
 Atriplex canescens (Pursh) Nutt. FOURWING SALTBUCH
 * Chenopodium dessicatum A. Nels. #
 Chenopodium fremontii S. Wats. FREMONT'S GOOSEFOOT #
 Krascheninnikovia lanata (Pursh) Guldenstaedt (Ceratoides lanata (Pursh) J.T. Howell; Eurotia lanata (Pursh) Moq.) WINTERFAT

CUCURBITACEAE Gourd Family (1)
 Cucurbita foetidissima Kunth COYOTE or MISSOURI GOURD

CUPRESSACEAE Cypress Family (1)
 Juniperus monosperma (Engelm.) Sarg. ONESEED JUNIPER

EPHEDRACEAE Jointfir Family (1)
 Ephedra torreyana S. Wats. TORREY JOINTFIR or MORMON TEA #

EUPHORBIACEAE Spurge Family (7)
 Chamaesyce parryi (Engelm.) Rydb. PARRY'S SANDMAT or SPURGE #
 Chamaesyce serpyllifolia (Pers.) Small THYMELEAF SANDMAT or SPURGE #
 Chamaesyce serrula (Engelm.) Woot. & Standl. SAWTOOTH SANDMAT or SPURGE #
 Croton texensis (Klotzsch) Muell.-Arg. TEXAS CROTON #

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CHAPTER II: REGULATIONS

11.0 General Regulations

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Euphorbia dentata Michx. TOOTHED SPURGE #
* *Tragia ambylodonta* (Muell.-Arg.) Pax & K. Hoffmann
Tragia ramosa Torr. BRANCHED NOSEBURN

FABACEAE (=LEGUMINOSAE) Bean or Pea Family (14)
Astragalus amphioxys Gray var. *amphioxys* CRESCENT MILKVETCH #
Astragalus ceramicus Sheld. var. *ceramicus* PAINTED MILKVETCH #
Astragalus lentiginosus Dougl. var. *diphysus* (Gray) Jones SPECKLEDPOD MILKVETCH #
Astragalus nuttallianus DC. SMALLFLOWERED MILKVETCH #
Caesalpinia jamesii (Torr. & Gray) Fisher JAMES' HOLDBACK
Dalea compacta Spreng. var. *compacta* COMPACT PRAIRIECLOVER #
Dalea formosa Torr. FEATHERPLUME
Dalea lanata Spreng. var. *terminalis* (Jones) Barneby WOOLLY PRAIRIECLOVER #
Dalea nana Torr. ex Gray var. *carnescens* Kearney & Peebles DWARF PRAIRIECLOVER #
Dalea scariosa S. Wats. (*Petalostemon scariosa* (S. Wats.) Wemple) ALBUQUERQUE PRAIRIECLOVER #
Hoffmannsegia glauca (Ortega) Eifert INDIAN RUSHPEA
Pediomelum hypogaeum (Nutt.) Rydb. (*Psoralea hypogaea* Nutt.) SCURFPEA #
Psoralea scoparius (Gray) Rydb. (*Dalea scoparia* Gray) BROOM

DALEA; PURPLE SAG

FUMARIACEAE Fumitory Family (1)
Corydalis aurea Willd. GOLDEN CORYDALIS, SCRAMBLED EGGS, GOLDENSMOKE, BUTTER AND EGGS
GROSSULARIACEAE Gooseberry Family (1)
Ribes sp. GOOSEBERRY

HYDROPHYLLACEAE Waterleaf Family (4)
Nama hispidum Gray BRISTLY NAMA
Phacelia crenulata Torr. var. *crenulata* CLEFTLEAF WILDHELIOOTROPE #
Phacelia integrifolia Torr. GYPSUM SCORPIONWEED #
Phacelia ivesiana Torr. IVES PHACELIA #

LINACEAE Flax Family (2)
Linum aristatum Engelm. BRISTLE FLAX
**Linum australe* Heller #

LOASACEAE Stickleleaf Family (2)
Mentzelia albicaulis (Dougl.) Dougl. WHITESTEM BLAZINGSTAR
Mentzelia pumila (Nutt.) Torr. & Gray DWARF MENTZELIA #

MALVACEAE Mallow Family (5)
Sida abutifolia P. Mill. (*Sida filicaulis* Torr. & Gray) SPREADING FANPETALS #
* *Sida neomexicana* Gray
Spheralcea angustifolia (Cav.) G. Don ssp. *lobata* (Woot.) Kearney COPPER GLOBEMALLOW #
Spheralcea hastulata Gray (*Spheralcea subhastata* Coult.) SPEAR GLOBEMALLOW #
Spheralcea incana Torr. ex Gray GRAY GLOBEMALLOW #

NYCTAGINACEAE Four O'clock Family (7)
Abronia fragrans Nutt. ex Hook. FRAGRANT WHITE SAND VERBENA
* *Allionia choysia* Standl. #
Allionia incarnata L. TRAILING WINDMILLS #
Boerhavia spicata Choisy (*B. torreyana* (S. Wats.) Standl.) CREEPING SPIDERLING

* *Mirabilis glabra* (S. Wats.) Standl. (*Oxybaphus glaber* S. Wats.) #
Mirabilis linearis (Pursh) Heimerl NARROWLEAF FOUR O'CLOCK
Selinocarpus diffusus Gray SPREADING MOONPOD #

OLEACEAE Olive Family (1)
Menodora scabra Gray ROUGH MENODORA

ONAGRACEAE Evening Primrose Family (2)
Gaura coccinea Nutt. ex Pursh SCARLET BEEBLOSSOM
Oenothera pallida Lindl. PALE EVENINGPRIMROSE #

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11.0 General Regulations

R

OROBANCHACEAE Broomrape Family (1)
 Orobanche ludoviciana Nutt. (*O. multiflora* Nutt.) LOUISIANA
 BROOMRAPE #

PEDALIACEAE Sesame Family (1)
 Proboscidea louisianica (P. Mill.) Thelleng COMMON DEVILSCLAW,
 DEVILSHORN, RAM'S HORN

PLANTAGINACEAE Plantain Family (1)
 Plantago patagonica Jacq. (*P. purshii* Morris) WOOLLY PLANTAIN #
 Plantago lanceolata L. NARROWLEAF PLANTAIN

POACEAE (=GRAMINAE) Grass Family (42)
 Aristida adscensionis L. SIXWEEKS THREEAWN #
 * Aristida arizonica Vasey
 Aristida havardii Vasey HAVARD'S THREEAWN #
 * Aristida pansa Woot. & Standl.
 Aristida purpurea Nutt. var. fendleriana (Steud.) Vasey
 FENDLER'S THREEAWN #
 * Aristida purpurea Nutt. var. neallyi (Vasey) Allred #
 * Aristida purpurea Nutt. var. purpurea #
 * Bothriochloa barbinodis (Lag.) Herter #
 Bothriochloa laguroides (DC.) Herter ssp. torreyana (Steud.) Allred & Gould
 (*Andropogon saccharoides* Sw.) SILVER BEARDGRASS or SILVER
 BLUESTEM #
 Bouteloua aristoides (H.B.K.) Griseb. var. aristoides NEEDLE GRAMA #
 Bouteloua barbata Lag. var. barbata SIXWEEKS GRAMA #
 Bouteloua curtipendula (Michx.) Torr. SIDEOATS GRAMA
 Bouteloua eriopoda (Torr.) Torr. BLACK GRAMA #
 Bouteloua gracilis (Willd. ex Kunth) Lag. ex Griffiths BLUE GRAMA
 Bouteloua hirsuta Lag. HAIRY GRAMA
 * Cenchrus carolinianus Walt. (*Cenchrus incertus* M.A. Curtis)
 * Digitaria californica (Benth.) Henr.#
 Elymus elymoides (Raf.) Swezey (*Sitanion hystrix* (Nutt.) J.G. Sm.; *Elymus*
longifolius (J.G. Sm.) Gould) SQUIRRELTAIL #
 Enneapogon desvauxii Beauv. NINEAWN PAPPUSGRASS #
 Erioneuron pulchellum (Kunth) Tateoka (*Dasyochloa pulchella* (Kunth) Willd.
 ex Rydb.) FLUFFGRASS, LOW WOOLLYGRASS #

Hilaria jamesii (Torr.) Benth. (*Pleuraphis jamesii* Torr.) GALLETA #
 * *Koeleria macrantha* (Ledeb.) J.A. Schultes
 (*Koeleria cristata* auct. p.p. non Pers.)
 * *Lycurus phleoides* Kunth
Monroa squarrosa (Nutt.) Torr. (*Munroa squarrosa* (Nutt.) Torr.)
 FALSE BUFFALOGRASS #
 * *Muhlenbergia arenacea* (Buckl.) A.S. Hitchc.
Muhlenbergia arenicola Buckl. SAND MUHLY #
Muhlenbergia porteri Scribn. BUSH MUHLY #
Muhlenbergia pungens Thurb. SANDHILL MUHLY #
Muhlenbergia torreyi (Kunth) A.S. Hitchc. ex Bush RING MUHLY
Oryzopsis hymenoides (Roemer & J.A. Schultes)
 Ricker ex Piper INDIAN RICEGRASS
 * *Poa bigelovii* Vasey & Scribn.
Scleropogon brevifolius Phil. BURROGRASS #
Setaria leucopila (Scribn. & Merr.) K. Schum.
 STREAMBED BRISTLEGRASS #
 * *Setaria lutescens* (Weigel) F.T. Hubbard ?
Sporobolus contractus A.S. Hitchc. SPIKE DROPSEED
Sporobolus cryptandrus (Torr.) Gray SAND DROPSEED #
 * *Sporobolus flexuosus* (Thurb. ex Vasey) Rydb. #
Sporobolus giganteus Nash GIANT DROPSEED #
Stipa comata Trin & Rupr. var. *comata* NEEDLEANDTHREAD #
 * *Stipa neomexicana* (Thurb. ex Coult.) Scribn.
Stipa spartea Trin. PORCUPINEGRASS #
Vulpia octoflora (Walt.) Rydb. (*Festuca octoflora* Walt.)
 SIXWEEKS FESCUE #

POLEMONIACEAE Phlox Family (1)
Ipomopsis pumila (Nutt.) V. Grant DWARF GILIA #

POLYGONACEAE Knotweed Family (4)
Eriogonum abertianum Torr. var. *abertianum* ABERT BUCKWHEAT #
 * *Eriogonum effusum* Nutt.
Eriogonum polycladon Benth. SORREL BUCKWHEAT #
Eriogonum rotundifolium Benth. ROUNDEAF BUCKWHEAT #
Rumex hymenosepalus Torr. CANAIGRE; DOCK #

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11.0 General Regulations

R

PORTULACACEAE Purslane Family (1)
Portulaca sp. PURSLANE

RANUNCULACEAE Crowfoot Family (1)
Delphinium sp. LARKSPUR

ROSACEAE Rose Family (1)
Fallugia paradoxa (D. Don) Endl. ex Torr. APACHE PLUME

SALICACEAE Willow Family
Salix sp .WILLOW

SCROPHULARIACEAE Figwort Family (3)
Epixiphium wislizenii (Engelm. ex Gray) Munz (Maurandya wislizenii
Engelm. ex Gray) BALLOONBUSH #
Penstemon ambiguus Torr. GILIA PENSTEMON or BEARDTONGUE
* Penstemon sp.

SOLANACEAE Potato Family (6)
Chamaesaracha coronopus (Dunal) Gray GREENLEAF FIVE EYES #
Datura inoxia P. Mill. THORNAPPLE; JIMSONWEED #
Lycium pallidum Miers PALE WOLFBERRY
Nicotiana trigonophylla Dunal DESERT TOBACCO #
Physalis acutifolia (Miers) Sandw. (P. wrightii Gray) SHARPLEAF
GROUNDCHERRY #

VERBENACEAE Vervain Family (2)
Aloysia wrightii Heller ex Abrams WRIGHT'S BEEBRUSH #
* Tetraclea coulteri Gray #

ZYGOPHYLLACEAE Caltrop Family (2)
Kallstroemia sp. CALTROP

11.1.2. **Xeric Plant List B:** A list of official xeric or low-water plant species periodically updated by the Albuquerque Bernalillo County Water Utility Authority (ABCWUA).

To obtain the most current information, contact ABCWUA:

Telephone: 505-842-WATR

Website: <http://www.abcwua.org/pdfs/xeriplantlist.pdf>

For additional information, see ABCWUA's "How-To Guide to Xeriscaping":
<http://www.abcwua.org/content/view/73/63/>

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11.0 General Regulations

R

11.2. Construction Mitigation

11.2.1. **Purpose / Intent:** Any damage to the vegetation, slope, or placement of boulders due to or related to construction shall be mitigated as provided herein.

11.2.2. Grading permits for commercial lot developments in the Volcano Heights Sector Development Plan area shall only be issued concurrently with the respective commercial building permits. Grading permits for residential developments greater than 10 acres shall require soil stabilization, approved by the Environmental Health Department, which shall be applied to the disturbed area within three months after grading of the site commences. Grading within public rights-of-way or public drainage easements is exempt.

11.2.3. Prior to beginning construction, the property owner shall construct a temporary silt fence at the site boundary adjacent to sensitive lands (i.e. the Escarpment Buffer, Major Open Space Area, archeological site, or public or private conservation area to be maintained in natural desertscape) to effectively protect them from heavy equipment and vehicles. Photographs of the site in its original condition shall be submitted with the application for building permit and subdivision and/or site development plan.

11.2.4. Public- and private-sector projects facing the Escarpment and adjacent to archeological sites must include geotechnical data and analysis that demonstrate to the satisfaction of the City Engineer, in coordination with the Open Space Division and National Park Service, that proposed trenching or blasting will not affect the face of the Escarpment, whether by potentially causing erosion or caving of the slopes and boulders, threatening public safety or welfare, or otherwise damaging archeological resources.

11.2.5. In the selection of alignment and in site design, grading plans shall demonstrate that cut and fill has been kept to a minimum consistent with the standards in this Plan. Generally, the overall topography of the site is not to be substantially altered. Development must comply with the joint Albuquerque-Bernalillo County Fugitive Dust Ordinance found in the New Mexico Administrative Code 20.11.20.

11.2.6. Replacement of boulders shall approximate the original location, angle, and surface exposure. Revegetation to approximate original cover with appropriate native or naturalized plants as identified in the Plant List is required within 90 days of project completion. A deviation may be granted by the Planning Director or his/her designee if the type of vegetation or time of year make revegetation within 90 days impossible. For infrastructure projects on public lands, the construction company shall post a warranty bond effective for three years after completion of the infrastructure work to insure successful revegetation.

11

11.0 General Regulations

R

11.3. Rainwater Design and Management

11.3.1. **Purpose/Intent:** Improved naturalistic channel design shall retain as much undisturbed desert vegetation and rock formations insofar as practicable.

11.3.2. Channel treatments shall meet the following requirements:

- (i) Limited stabilization of natural channels, according to the policies contained in the City of Albuquerque Facility Plan for Arroyos, unless such treatment is determined to be infeasible by the City Hydrologist or the Albuquerque Metropolitan Area Flood Control Authority (AMAFCA) as appropriate.
- (ii) Use of many small, unobtrusive structures, such as check dams or small drop structures, rather than larger, more obtrusive structures, when structural solutions are required.
- (iii) Use of materials in treated channels that blend visually with the Escarpment and adjacent open space. Naturalistic treatments are the preferred treatment types.
- (iv) Protection of canyons from erosion through control of developed flows and through stabilization techniques that are consistent with the visual character of the open space.

11.3.3. Within large areas of open space, developed flows shall be modified through check dams or other means to approximate undeveloped flows to minimize impacts on the Escarpment and to minimize the intensity of channel treatment required. The impact of check dams as a method of controlling flows shall be thoroughly studied prior to their use.

11.3.4. Developed flows shall be managed to minimize [language pending] their impact on Major Public Open Space, Northern Geologic Window, archeological sites, and the Escarpment.

- (i) The potential impacts of water retention shall be thoroughly studied prior to use of detention areas to control flows.
- (ii) Developed flows into the Petroglyph National Monument shall not be permitted in excess of, or more concentrated than, natural flows and shall require approval by City Open Space Division in coordination with the National Park Service.
- (iii) Developments that propose more than 4 feet of fill shall be required to go through the DRB for approval and require sign-off from the City hydrologist.
- (iv) All developments within the Impact Area as defined by the NWMEP shall be required to go through the DRB for approval and require sign-off from the City hydrologist. [confirm language]

11

11.4. Infrastructure
11.4.1. Utilities

(i) Easements

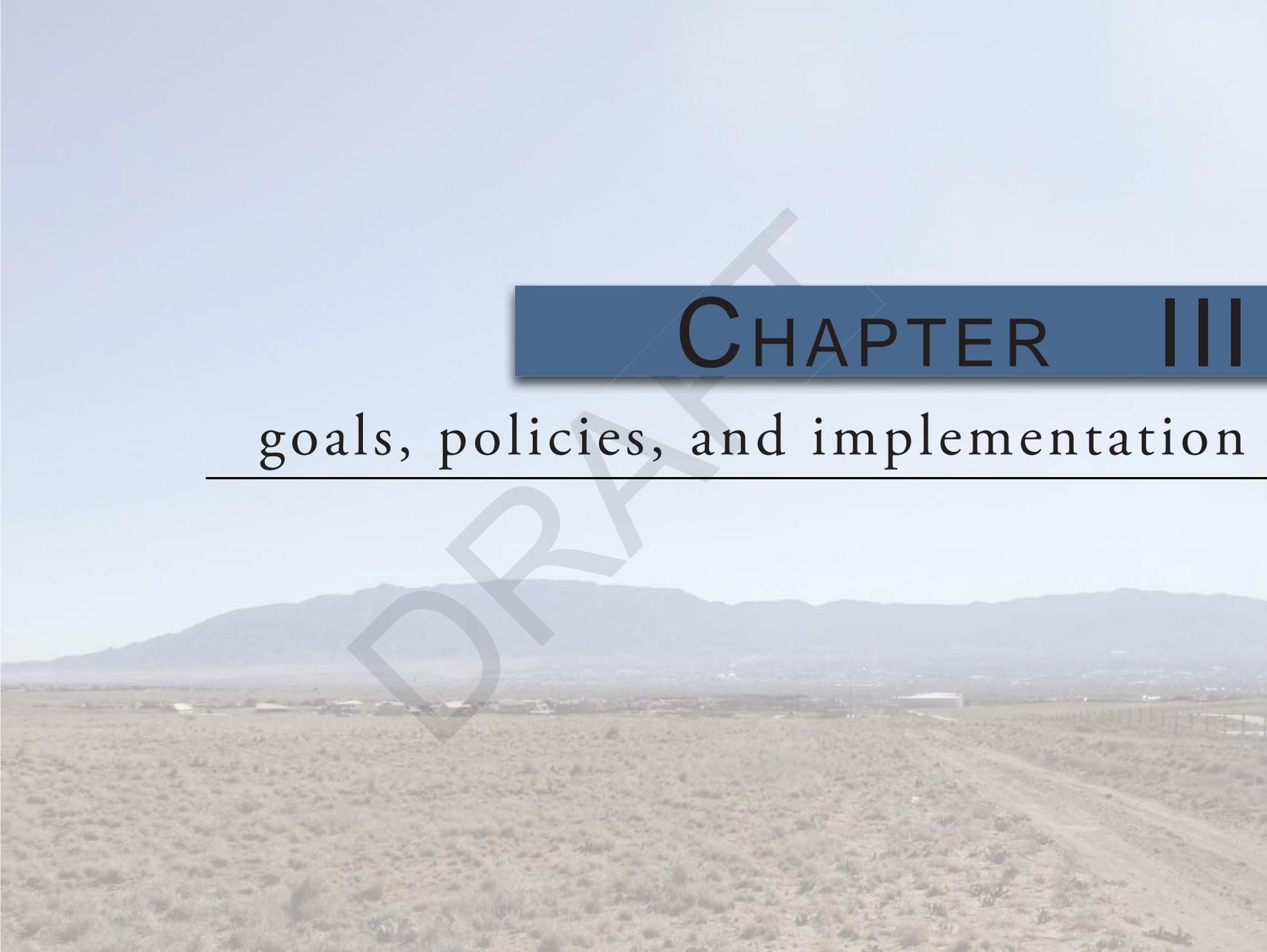
- a. In order to facilitate pedestrian movement and maintain accessibility, utility infrastructure such as poles, transformers, boxes, and access panels shall be planned for minimal visual intrusion and mobility impediment to pedestrians. Overhead and underground electric distribution lines are typically located within public utility easements (PUE) or private easements. They are compatible with other “dry” utilities such as cable, telephone, and fiber optic facilities. The width of the PUE is typically 10 feet in order to provide necessary clearances for safety.
- b. Water lines, sewer lines and storm water drainage or “wet” utilities are not compatible with “dry” utilities, and separation is required for safety purposes.
- c. In all zones, Utility Easements shall be located in alleys or rear access and parking areas, if available. Where there is no alley, utility infrastructure may be placed in a PUE or private easement in the front setback of the property, provided it does not substantially affect the pedestrian realm and is located on the edge or side of property and as far away from the main entrance and pedestrian access paths as possible.

(ii) Clearance

- a. Any screening and vegetation surrounding ground-mounted transformers and utility pads shall allow 10 feet of clearance in front of the equipment door and 5 feet of clearance on the remaining 3 sides for access and to ensure work crew and public safety during maintenance and repair.
- b. Non-permanent use of clearance, such as for parking, is permitted.
- c. Aesthetic improvements are encouraged to minimize visual impact of ground-mounted utility equipment.
- d. Trees and shrubs planted in the PUE should be planted to minimize impacts on facilities maintenance and repair.

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CHAPTER III

goals, policies, and implementation

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12.0 Goals

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12.0 Goals

The following goals are intended to support the policies set forth in the Rank I Albuquerque Bernalillo Comprehensive Plan, the Planned Growth Strategy, the Rank II West Side Strategic Plan, and the Rank III Northwest Mesa Escarpment Plan. The goals represent the overarching intent of the Plan.

12.1. Environment and Open Space

12.1.1. **Establish an interconnected open space network comprised of parks, arroyos, the Petroglyph National Monument, and other open spaces.**

An organized system of open space can help conserve the natural environment, mitigate the impacts of development, provide exceptional recreational opportunities, and increase property values and quality of life for residents, employees, and visitors in and around Volcano Heights. Opportunities to experience and enjoy nature should be plentiful, especially for children. Often, these open space features can form the boundary of neighborhoods and maintain views to the Volcanoes and Sandia Mountains.

12.1.2. **Respect Albuquerque's culture and history, including Hispanic and Native American, through contextually sensitive development of Volcano Mesa.**

Volcano Mesa provides a unique portal to understand the rich interplay of cultures that is New Mexico. The stories and meaning of this place to Native Americans can be told through living in and visiting Volcano Heights, which should influence the way this special area develops. Volcano Heights should be an entry point for Albuquerque residents into different and important perspectives on humanity's place on earth and our spiritual paths.

12.1.3. **Conserve Volcano Heights' archaeological resources and protect and emphasize views and visual connections to the Volcanoes, Sandia Mountains, and the Rio Grande.**

The Volcanoes, Petroglyph National Monument, outcrops of basalt (especially those containing petroglyphs), the Sandia Mountains, and other locations are sacred places for many Native Americans, and they still figure into their ceremonial practices. Views can be protected and enhanced through considerate site planning and by creating view corridors using streets and arroyos. Important views from locations within Volcano Heights to the Rio Grande basin, across the city of Albuquerque and the Sandia Mountains should be protected.

- 12.1.1.4. **Maintain scenic edges, protect important views, and minimize the visual impact of development that can be seen throughout the city.**

The built environment and landscape along the edge of the Petroglyph National Monument will form a pleasant transition from the natural area to the developed area. Open space constitutes an important resource that demands special landscape and architectural treatments. As development within the Volcano Heights Major Activity Center will be visible from most of the City of Albuquerque, care should be taken to achieve development that is not visually intrusive, especially in the lower-density residential areas adjacent to the Petroglyph National Monument. Appropriate building heights, sizes, and reflectivity will minimize the visual impact of development.

- 12.1.1.5. **Encourage infrastructure support practices that are economically, aesthetically and environmentally sound.**

Electrical utility distribution lines should be placed underground. Infrastructure improvements should promote and make visible an environmental ethic for the area. Infrastructure should be designed and constructed to increase and maintain property values and enhance sustainable developments. Drainage treatments should respect sensitive lands such as Piedras Marcadas Canyon, the Petroglyph National Monument, and significant rock outcroppings, as well as the unique aesthetics of the area. Transportation infrastructure should enhance the economic sustainability of the community by offering viable choices for multiple modes of travel for people of all ages and abilities.

12.2. Economic Development

- 12.2.1. **Coordinate development across property lines and among multiple property owners.**

This Plan advocates coordination across property owners, including the possibility of working with a Master Developer who can partner with owners to oversee the improvement of all properties. This strategy is particularly helpful to coordinate infrastructure planning and construction, including grading, roads, and utilities to prepare properties for development. A master developer would be instrumental in preparing an application for a TIDD, an SAD, or a PID to benefit multiple property owners, if not the whole Plan area.

12.0 Goals

An experienced, third-party master developer and marketing strategist can also help to coordinate complex projects with a variety of development types and/or projects that will overlap and/or follow one another.

12.2.2. [Pending]

12.3. Transportation

12.3.1. Provide a choice of viable transportation options for commuting and daily needs.

Walking is a safe, viable travel option, especially for children and the elderly, for recreation, commutes, and shopping. For greater distances, transit should offer a convenient alternative to single-occupancy vehicles. Reducing reliance on the automobile reduces pollution, congestion and pressures to widen roads and bridges, especially at regional “pinch points” like the Rio Grande.

12.3.2. Support an efficient and reliable transit system.

To support frequent service and maximize ridership with less public subsidy, transit stop locations should be coordinated with the development of higher density residential uses and more intense corridors should be lined with higher densities and major retail, commercial, and employment destinations. Doing so will allow the early implementation of traditional transit service, which can eventually segue to Bus Rapid Transit (BRT) systems using specialized vehicles and techniques that make transit travel times competitive with the single-occupancy vehicle. All arterial streets have cross-sections with either dedicated transit lanes on the outside edges or extra-wide medians capable of incorporating dedicated transit lanes.

- 12.3.3. Create Complete Streets for people as well as cars, by providing street trees, landscaping, wide sidewalks and active uses.**

Streets should feel safe and comfortable for pedestrians. The design of streets and the buildings that face them influences whether someone will choose to walk. Pedestrian-friendly streets have wide sidewalks, street trees and slow moving traffic. They also have pedestrian-scaled buildings with frequent entries, windows, and attractive features. Parking lots and blank walls should be minimized along pedestrian routes.

- 12.3.4. Connect different uses and areas by an efficient and convenient street network.**

Streets should create safe and direct connections between common destinations, not create barriers that separate people and neighborhoods. Streets should be designed to control auto speeds and be a comfortable place for neighbors to interact. Rather than concentrate traffic, highly connected streets will distribute traffic among roadways. A street network grid also creates redundancy for additional safety and emergency response.

- 12.3.5. Retain the primary purpose of Paseo del Norte and Unser Boulevard to serve regional traffic, while balancing the needs of the local road network to serve new development and multiple modes of traffic, including pedestrians.**

The Plan recognizes the limited-access nature and classification of Paseo del Norte and Unser Boulevard. At the same time, the Plan proposes land uses and a mandatory road network intended for safe, attractive, dense, pedestrian-friendly urban environment. As the area develops over time, the goal is to “tame” these roads to help create a gateway to a more urban, walkable environment. These regional roads will give the first “sense of place” to many potential visitors to Volcano Heights and should reflect and complement the area’s character accordingly, while still functioning as limited-access, primary regional arterials.

- 12.3.6. Remove restrictions on truck traffic on key roads to provide truck access necessary to support retail and commercial uses.**

Several roads on the West Side restrict truck traffic. The City DMD should work with the Mid-Region Council of Governments and constituent jurisdictions to remove several of these restrictions in order to facilitate truck movement to serve businesses and improve services on the West Side.

12.0 Goals

12.4. Land Use and Urban Design

- 12.4.1. **Recognize walkable neighborhoods and districts as the essential building blocks of a more sustainable city and region.**

Strong and healthy neighborhoods, because they operate at a scale where people walk and interact, are essential to successful and sustainable development. Organizing development within walkable mixed-use districts and neighborhoods supports transit, economizes on infrastructure, and respects the environment. Requiring that development comply with design standards that support the creation of safe, comfortable and visually attractive settings supports a community's long-term economic, cultural, and social viability.

- 12.4.2. **Bring homes, businesses, and daily destinations — like retail and community facilities — closer together within neighborhoods and districts.**

Homes should be within walking distance of a mixed-use center containing retail, community services, park and plazas. Studies have shown that this walkable pattern of development can reduce the number of vehicle-trips dramatically. Walkable districts and neighborhoods also have proven social and economic benefits resulting from better access to basic needs, services, and amenities; safer and more active streets; and improved health through physical activity.

- 12.4.3. **Encourage architectural and landscape treatments that are consistent with the region's traditions and climate and help to establish a unique sense of place.**

New development in Volcano Heights should continue to respect and enrich Albuquerque's design traditions that spring from its arid climate, intense sun, local materials, and the cultural background of its inhabitants. These considerations deserve continued attention to respect the past and work toward an energy- and water-efficient future. The quality of individual buildings contributes to a sense of place and permanence. High standards for architectural and landscape design for individual buildings, lighting, utilities, walls, and landscaping materials help to create a built environment with lasting character that draws on southwestern regional styles and traditions. Individual design expression within distinctive character districts contributes to an overall framework of quality. Buildings should be designed to address the unique climatic conditions of the southwest, including orientation to conserve water, protect pedestrians from intense summer sunlight and heat, provide adequate heating during cold winters, and take advantage of natural lighting.

12.4.4. Promote diverse housing options throughout Volcano Heights.

A variety of housing types—at varying densities—allows the opportunity for residents to move through all stages of life within the same neighborhood. Housing diversity will also promote and attract businesses to balance development on the West Side.

12.4.5. Support the creation of a major employment center in Volcano Heights.

Most working West Side residents commute to work on the east side of the Rio Grande (many in downtown Albuquerque or along Interstate 25), which contributes to regional traffic congestion at river crossings and on the West Side. Major job growth in Volcano Heights will provide opportunities for working closer to home, minimizing the need for river crossings or reversing commutes in a direction where roads have existing capacity. A frequent and reliable transit service for this Major Activity Center will further improve commuting times.

12.4.6. Establish a mixed-use Major Activity Center as a transit-oriented development that offers a range of retail, commercial and entertainment destinations; urban housing; and employment opportunities.

While neighborhood retail can meet most daily requirements, many needs must be met within larger centers that receive the patronage of greater populations. A mixed-use Major Activity Center will encourage opportunities for comparison shopping, entertainment, restaurants, cultural activities, and government services. Following the West Side Strategic Plan, a well-designed central plaza will serve as a focus for community life in the Major Activity Center and serve as an anchor for a sense of place. Unlike many exclusively single-family residential subdivisions, Volcano Heights will offer dense urban housing in mixed-use environments for those who prefer them, which greatly increases the likelihood of walking and transit use. Transit, amenities, and housing in the Major Activity Center will help attract employment to the West Side.

12.4.7. Incorporate street infrastructure and streetscape details that support the creation of distinct, pedestrian-friendly districts.

Where taller lights are required, for example a major thoroughfare, consideration should be given to a design appropriate to the larger scale. Reproductions of historic lamps such as the “bishops crook” poles are a more aesthetic solution than the cobra head.

12.0 Goals

12.5. Infrastructure

- 12.5.1. **Provide for the orderly expansion of infrastructure and public facilities in the area.**

Infrastructure improvements in Volcano Heights will need to be phased in a way that recognizes technical limitations and available funding and that provides infrastructure and facilities in a timely way to meet the needs of residents and local employees.

- 12.5.2. **Invest in and incorporate the most up-to-date technology and maximum capacity for infrastructure and utilities.**

The best technology and highest capacity should be planned for infrastructure and utilities, particularly telecommunications, in order to attract and support high-tech businesses as well as a diverse spectrum of desirable commercial activity.

- 12.5.3. **Coordinate among property owners to leverage investment in water source and water quality improvements.**

Volcano Heights spans two water pressure zones, one of which is smaller than the City's standards size, as it was previously owned by the New Mexico Utilities. The amount of bedrock in the area, as well as the presence of arsenic, and the need for a new water tank to support new development will pose significant front-end costs before any development can occur. These improvements could be included in a TIDD, SAD, or PID, which would provide a mechanism to share the costs across property owners based on the potential benefits to each property. See **Section 14.0 Implementation** in this Plan and **Appendix A** for more explanation of Pre-Existing Conditions, including water systems.

13.0 Policies

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13.0 Policies

13.1. Environment and Open Space

- 13.1.1. **Priorities for Open Space Purchases:** Areas identified by this Plan for acquisition should be reviewed by the Open Space Advisory Board for inclusion in the Major Public Open Space priority acquisition list. [Pending – Priority rock outcroppings/archaeological areas, playa, double-loaded Park Edge areas, etc.]]
- 13.1.2. Future acquisition needs identified by this Plan should be considered for future General Obligation Bonds.
- 13.1.3. Sensitive lands – whether rock outcrops or significant cultural, archaeological, volcanic, or geologic land – that cannot be or have not been purchased by City Open Space should be permanently protected privately through either a Transfer of Development Rights, a Conservation Easement, or replatting as private open space. See **Appendix D** for more about options for private preservation options.
- 13.1.4. Property owners should provide public access to rock outcroppings via nearby sidewalks and private walkways. Where such access is provided, the property owner should grant a public access easement that remains with the property in perpetuity. **Appendix D** offers additional options for private preservation options, including Conservations Easements, which may be eligible for tax rebates. These areas are excellent places to incorporate water harvesting, as coordinated and approved by the City Open Space Division and the City Hydrologist. (See **Table 10.3.**)

13.1.5. The City should adopt an ordinance to allow Transfer of Development Rights (TDR). This would allow property owners with rock outcroppings to “send” density to “receiving” properties where density is more appropriate. Similarly, property owners with multiple properties - some with rock outcroppings and some without - would be able to transfer development densities and height bonuses between properties.

13.1.6. Dedications of land to the City for Major Public Open Space, Detached Open Space, and/or parks are preferred abutting the Petroglyph National Monument, rock outcroppings, multi-use trails, or parks. Dedications are subject to approval by City Parks and Recreation or Open Space Division.

13.2. Economic Development

- 13.2.1. Economic development should include a range of retail, office, and light manufacturing. Non-residential development should include goods and services for regional and local residents and visitors.
- 13.2.2. Business recruitment should focus on employment opportunities that provide livable wages and high-quality work environments.
- 13.2.3. Mixed-use development should include opportunities for retail and services for local and nearby residents.

13.0 Policies

13.2.4. Innovative businesses, green businesses, and new businesses that add employment opportunities, as opposed to cannibalizing commercial uses in other areas, should receive priority consideration for public-private partnerships, matching funds, and leeway in standards within the range acceptable as Minor or Major per **Table 3.2** and **Table 3.3**.

13.2.5. City Economic Development should work with property owners, prospective businesses, and the Double Eagle Airport to recruit businesses for Volcano Heights.

13.3. Transportation

These Transportation Policies provide guidance for the development of a transportation network within the Volcano Heights Plan area. The goal is to facilitate a range of transportation options that support lifestyle choices and quality of life for people of all ages and abilities. This Plan advocates strategies to create a street network that distributes local traffic efficiently and maintains regional traffic movement through the Plan area. This street network includes bicycle facilities and pedestrian amenities to serve commuters as well as recreational users, linked to public transit nodes. The intent is to create a transportation system that provides easy access to where people live, work, and play. A transportation study conducted in 2012 analyzed the policies, standards, and regulations included in this Plan. (See **Appendix C**). [Pending]

These policies are intended to support and implement goals set by the Mid-Region Council of Governments (MRCOG) to shift the mode of travel to mitigate congestion at river crossings, promote the integration of alternative modes of transportation, and encourage higher-density land use in appropriate areas oriented to multiple modes of traffic, including pedestrians, transit, and cyclists.

The designated Major Activity Center (MAC) in the Volcano Heights incorporates the higher-densities and mixed land uses that promote walkability and support frequent high-capacity transit such as bus rapid transit (BRT). This type of development can also play a role in preserving open space and views and buffering existing lower-density areas from higher-activity areas.

Well-connected, context-sensitive local street systems work together with the form-based zoning to shorten block lengths, provide a variety of transportation options for commuters and for resident's daily needs, reduce traffic on arterial roadways, and support bicycling and walking as viable modes of travel.

Linking land use and transportation planning to investment is a key factor in managing congestion and improving the balance of housing and jobs west of the Rio Grande. More specifically, higher-density mixed use development in appropriate locations and densities is necessary to support vital local communities and a cost-effective transportation system. The Volcano Heights MAC is intended to provide a center of activity to address transportation needs throughout the metropolitan area.

13.0 Policies

Intensive uses and population density are critical to successful transit. In return, transit will play a vital role in reducing regional traffic congestion, but to be widely used it must be fast, frequent, and reliable. This Plan's emphasis on walkability and urban development will help to make more frequent transit service viable.

The BRT system proposed for the area in the future incorporates dedicated bus lanes and emerging technologies to make travel times competitive with the car. MRCOG is in the early stages of a high-capacity transit study as of 2012. Potential alignments identified for high-capacity transit routes include Unser Boulevard and Paseo del Norte in the Volcano Mesa area and extend to the Journal Center / North I-25 area. High-capacity transit would transport area residents and workers to and from the Plan area and established West Side communities, as well as provide mass transit to central Albuquerque and jobs in the I-25 corridor.

13.3.1. Transit System: The transit system in the Volcano Mesa area should serve three significant corridors:

- (i) east-west along Paseo Del Norte;
- (ii) north-south along Unser Boulevard, and
- (iii) southeast-northwest along Rainbow Boulevard.

13.3.2. Transit Network

- (i) The Mandatory Road network and non-mandatory roads should form a grid as much as possible in order to facilitate the movement of transit vehicles, pedestrian access to transit stops, and the dispersion of automobile traffic away from potential congestion points.

- (ii) Transit stops and/or stations should be located to maximize the number of residents and workers within a one-quarter mile walk to a stop or station.
- (iii) On these transit routes, crossings of a limited-access arterial will need special design treatment to ensure safe and easy pedestrian crossings. Possible treatments include pedestrian delays, raised center medians (pedestrian refuges), clearly identified pedestrian crossings, pedestrian yield signage, reduced curb radii, and pedestrian starts or leading pedestrian intervals (LPIs), which give pedestrians authority to start crossing before non-yielding, right-turning vehicles.
- (iv) Transit stops or stations should be placed on 'A' Streets adjacent to planned retail conveniences, schools, and public amenities.
- (v) Reasonably direct auto routes and acceptable system-wide travel speeds should be maintained.

13.3.3. Transit Center: A major transit center should be located in the Town Center abutting the Transit Corridor to serve the Paseo del Norte and Unser Boulevard corridors. The Transit Corridor should connect with both Unser Boulevard and Paseo del Norte at signalized intersections. A secondary transit center should be located south and west of the Neighborhood Activity Center proposed near Rainbow Boulevard and Hielo Road in the Volcano Cliffs Sector Development Plan area.

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13.3.4. **Long-Range, High-Capacity Transit Plan:** The Long-Range, High-Capacity Transit Plan should designate Unser Boulevard and Paseo del Norte as suitable for High Capacity Transit. The transit plan should be amended to be consistent with recommendations adopted with this Sector Development Plan and updated as transit plans evolve. The policies and regulations in this Plan should be updated to conform with an update to the FAABS expected in 2012.

- (i) The ultimate roadway design recommendations for Unser Boulevard and Paseo del Norte in this Plan incorporate BRT capacity in order to design them as suitable for High Capacity Transit.
- (ii) Transit improvements may be phased, and interim routing may be different from the ultimate routes in some locations.
- (iii) The Transit Corridor and the Mandatory Road Network proposed in this Plan depend on access modifications to the limited-access Paseo del Norte and Unser Boulevard. If access modification is denied or high-capacity transit is determined to be infeasible, the Mandatory cross sections should be amended to accommodate different conditions. Language directing the amendment process is incorporated in the Council legislation adopting the Plan.

13.3.5. **Transit Center and BRT/HOV Lanes:** Travel lanes dedicated solely to buses and other high-occupancy vehicles (HOVs) reduce travel times for those who carpool or use transit.

- (i) Paseo del Norte and Unser Boulevard should be designed to accommodate BRT/HOV travel lanes.
- (ii) Transfer between BRT routes and local buses should occur at a Transit Center maintained near the center of Volcano Heights to enhance its pedestrian and locational advantages.
- (iii) Lanes solely for the use of BRT should connect HOV lanes along Paseo del Norte and Unser Boulevard within the center of Volcano Heights.
- (iv) Beginning 500 feet before intersections on Paseo del Norte and Unser and any other potential station locations, a minimum of 36 feet in the rights-of-way should be provided on BRT routes for BRT lanes and station platforms.
- (v) The final location and configuration of BRT and HOV lanes will be determined during the roadway design process based on findings and recommendations from the MRCOG study expected in 2012. **[Update pending]**

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13.3.6. **Transit Center and Transit Stop Design:** The approach to the Transit Center and transit stops should offer direct pedestrian routes and be tree-lined and barrier free. The Transit Center and transit stops should be designed as prominent focal points, offering well-lit shelters that provide shade and are within or adjacent to plazas or other civic features. Shelters may be incorporated within the architecture of adjacent buildings or through the use of arcades or durable awnings. Transit route and system maps should be displayed at all stops. Bicycle storage racks should be located at major transit stops.

(i) **Transit-Oriented Development:** In deciding Transit Station and transit stop locations, preference should be given to locations with transit-supportive uses such as high-density residential and employment centers in order to attain and sustain high transit ridership. These uses should predominate within a quarter mile of transit stops. Consideration should be given to transit system policies that emphasize more frequent service along high-density corridors. [language pending]

(ii) **Convenience and Access:** Pedestrian routes to transit stops should be reasonably direct (along streets and/or off-street paths); circuitous routes should be avoided. Transit stops should be placed on 'A' Streets near retail conveniences and community amenities. Multiple stage crossings for pedestrians should be provided at transit stops for safe crossing of wide arterials by pedestrians of all abilities and ages.

(iii) **Signal Preference:** To improve travel times by transit, light signals in the Volcano Heights area should incorporate signal-preferencing technology such as "queue jumping" with a dedicated lane, or signal interruptions, to give buses priority at intersections.

(iv) **Pre-boarding Fare Systems:** Consideration should be given to technology that allows bus fares to be paid prior to boarding, thereby greatly reducing boarding and transit travel times.

13.3.7. **Transportation Demand Management (TDM) Plans:** The City should encourage applicants to prepare and implement Transportation Demand Management (TDM) plans for major employers or development projects to provide alternatives to the use of single-occupancy vehicles. TDM provisions support alternatives to the car by offering incentives for ridesharing, transit use, bicycling, and walking. Incentives may include reduced parking requirements, reduced development fees, development intensity bonuses, and/or the creation of transportation management associations to coordinate efforts among multiple users in the same area. [Who reviews? Guidelines for review? Incentives? define transportation management association]

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13.3.8. **Transportation Agency and Plan Coordination**

- (i) As development occurs, all transportation modes should be integrated, which will require coordination among property owners, City DMD, City RapidRide, MRCOG, and the MRMPO. Roadways on MRCOG’s Long Range Major Street Plan must include appropriate access management strategies, which also requires coordination across transportation agencies.
- (ii) New Mandatory Roads should be added to the functional classification system in the FAABS.

13.3.9. **Limited-access Intersections**

- (i) The City DMD should apply for Access Modifications to allow additional intersections as per the dotted circles in **Exhibit 4.3**. A traffic study conducted in 2012 confirms that traffic volumes and level of service on these roads can be protected even with the additional access points recommended in of this Plan. **[Pending]** See **Appendix A** for a summary of the process to apply for Access Modifications. See **Appendix C** for the traffic study. **[Pending]**
- (ii) Additional access points should be granted on the limited-access Paseo del Norte and Unser Boulevard as shown in **Exhibit 4.3**. These points have been selected to match the limited-access policy to the extent possible. The limited-access policy allows full intersections every 1/2 mile and a right-in/right-out (RI/RO) intersection every 1/4 mile.

- a. In order to create a loop road that allows access to new development surrounding the intersection of Paseo del Norte and Unser Boulevard while preserving the limited-access nature of these roads, this Plan recommends full intersections where the loop road intersects either Paseo del Norte or Unser Boulevard.
- b. Additional full intersections are recommended where the proposed Transit Corridor intersects Paseo del Norte or Unser Boulevard. See **Exhibit 4.3** in **Section 4** of this Plan.
- (iii) In order to protect the regional function of these roads, all access to new development in Volcano Heights should be from local roads.
- (iv) **All RI/RO intersections within Volcano Heights should allow a Left-in Only turning option.**

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13.3.10. Roadway Design

- (i) Roads in Volcano Heights should follow best practices of Context Sensitive Design and Context Sensitive Solutions, as defined by the Federal Highway Administration, the Institute of Transportation Engineer's "Designing Walkable Urban Thoroughfares: A Context Sensitive Approach, An ITE Recommended Practice" (2010), and the New Mexico Department of Transportation's "Guide to Context Sensitive Solutions" and "New Mexico Architectural and Visual Quality Design Guidelines for Context Sensitive Design and Context Sensitive Solutions," both prepared in 2006.
- (ii) Roadways should be sited and designed to minimize negative impact on views from within the Plan area to the Sandia Mountains on the east.
- (iii) The Park Edge road next to the Petroglyph National Monument should be single-loaded wherever possible. The National Park Service and the City Open Space Division prefer this edge treatment as the boundary and the transition between Open Space and development.
- (iv) Roadways designated as transit corridors should be designed to accommodate transit vehicles, pedestrians, and bicycles. See **Section 13.3.2.iii** in this Plan for more details.
- (v) Thoroughfares should be designed to provide bicycle facilities and safe multiple-stage crossings for pedestrians at transit stops and for crossing wide arterials.
- (vi) Street furniture and pedestrian amenities such as benches should be placed along 'A' Streets.
- (vii) Retail streets should be lined with a single type of tree or a coordinated palette of trees. On residential streets, street tree species should be consistent within a given street but may vary from street to street.
- (viii) Streets should be designed for all modes of transportation. On-street parking serves retail and other uses, while providing traffic calming that enhances safety and enjoyment for pedestrians and cyclists. Where permitted by the right-of-way, reverse-angle parking should be used on 'A' Streets as the preferred parking arrangement, as it provides the best sightlines for drivers to see cyclists. Reverse-angle parking generally requires 16-18 feet.

13.3.11. **Street Connectivity**

- (i) As development occurs, streets should maintain high connectivity for pedestrians, cyclists, and vehicles to provide a variety of safe, viable transportation options. A well-connected road or path network has many short links, numerous intersections, and minimal dead-ends. New roads, private walkways, and bicycle facilities added to serve new projects should improve connectivity. As connectivity increases, travel distances decrease and route options increase, allowing more direct travel between destinations.
- (ii) Property owners, developers, and the City should coordinate with MRCOG on identifying new thoroughfares in the area. Streets identified as collectors within the Volcano Heights Town Center and throughout residential subdivisions should be able to accommodate transit circulators that will connect with BRT stations and other transit infrastructure. See also **Section 13.3.8** in this Plan. [Update Pending]

13.3.12. **Street Maintenance**

- (i) New through roads should be public rights-of-way. If a property owner wants a road to remain private, and the City agrees, then the road has to be built to public standards and a public access easement needs to be granted on the private road. Public ROW is maintained by the City. Private roads, even those with public access easements, are maintained by the owner.
- (ii) Mandatory Roads should be public rights-of-way.
- (iii) Alleys that connect and serve multiple properties should be public rights-of-way. If they are internal to a site or project and are used only for deliveries, garbage pick-up, etc., then they may remain private. The City maintains alleys that are public rights-of-way, but only for drainage and filling in large holes. Weeds and any surfacing improvements are the responsibility of the abutting property owners.

13.3.13. **Bicycle and Multi-use Trail Network:** This Plan supports the implementation of existing plans for regional trails within the Volcano Mesa area. Please refer to the Rank II Trails and Bikeways Facilities Plan and the Rank II West Side Strategic Plan for more information on the area wide Trail Network. Because of the checkerboard property ownership, this Plan does not recommend any new trails within Volcano Heights. [See **Appendix A** for existing plans for regional trail locations within the Volcano Heights area.]

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13.4. Land Use and Design

- 13.4.1. Service entrances should be located to minimize visual, noise, olfactory, and air quality impacts on surrounding uses.
- 13.4.2. Pedestrian entrances for multifamily development and non-residential development should be located to provide the most direct access to nearby transit stops.
- 13.4.3. Single-family residences should be buffered to the extent possible from more intense uses.
- 13.4.4. Buildings should be sited to maximize solar gain and minimize solar impact on adjacent properties.
- 13.4.5. Buildings should be sited to minimize negative impact on views from within the Plan area to the Sandia Mountains on the east.
- 13.4.6. Buildings should be designed to minimize negative visual impact on views from across the Rio Grande west toward the Volcanoes.

13.5. Infrastructure

13.5.1. New Infrastructure

- (i) The cost of infrastructure required to service new development is the responsibility of the developer, unless coordinated with other property owners as part of a master development or other mechanism to leverage investment, such as a TIDD, SAD, or PID.
- (ii) Infrastructure planning should be coordinated along corridors and across property owners.
- (iii) The cost of backbone infrastructure that can support development throughout the Plan area should be coordinated across property owners, whether through private development agreements, a master developer, or a formal TIDD, SAD, or PID.

13.5.2. **Electrical Utilities:** A 2-acre parcel should be planned for an additional transformer to serve future development in Volcano Heights, preferably close to the Town Center.

13.5.3. **Rainwater Management:** Developments should incorporate Low Impact Design techniques wherever possible and appropriate. See **Table 13.1** and **Exhibit 13.1** in this Plan.

TABLE 13.1 – LOCATIONS APPROPRIATE FOR LOW-IMPACT DESIGN (LID) BY SCALE

Micro	
1	Tree wells
2	Medians
3	Parking lot islands
4	Pocket parks
5	Backyard/front yard ponds
6	Parkway between curb and sidewalk
7	Area behind sidewalk
8	Unused or rarely used areas of parking lots
Local	
1	Small parks
2	On-site drainage ponds retrofit for “first flush” and floatables treatment
3	Re-graded parking lot landscape areas (use speed bumps/dips as diversions)
4	Channel tributary entrances
5	Subdivision scale detention basins
6	Subdivision entry features
Regional	
1	Regional detention basins
2	Pump stations
3	Pump station discharges into Rio Grande
4	Regional parks
5	Diversion channel and arroyo outfalls to river



Streetside Raingarden



Stormwater Planter



Vegetated Swales

Exhibit 13.1 – Recommended Rainwater Design Techniques

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13.5.4. Water Improvements

- (i) Increased demand within both the Corrales and Volcano Trunks will require additional treatment capacity at the wells within both these trunks.
- (ii) Additional storage capacity (i.e. an additional water tank) may be needed to provide fire protection to new development within Volcano Heights. The Albuquerque-Bernalillo Water Utility Authority (ABCWUA) will determine future storage requirements based on details as new development is proposed.
- (iii) All necessary, required infrastructure improvements to provide water service to Volcano Heights will be constructed by the developer. This includes the internal distribution systems in the study area.
- (iv) All new infrastructure must be built to ABCWUA standards before the Water Authority accepts the new infrastructure.
- (v) As Volcano Heights is outside of the existing ABCWUA service area, all development will be assessed Water Supply Charges as well as Utility Expansion Charges. The Water Supply Charges relate to the acquisition of new water rights necessary to provide service outside of the defined ABCWUA service area.

13.5.5. Wastewater Improvements

- (i) All necessary, required infrastructure improvements to provide wastewater collection services to Volcano Heights will be constructed by the developer.
- (ii) All new infrastructure must be built to ABCWUA standards before the Water Authority accepts the new infrastructure.
- (iii) All collected waste water flows from the study area will outfall to the existing interceptor in the Paseo del Norte/ Avenida de Jaimito Corridor. Extension of this line to the west from its current terminus (near Calle Nortena) will be required.

13.5.6. ABCWUA Development Agreements

- (i) All new water services within the study area will require the execution of a Development Agreement between the owner/developers and the ABCWUA before either water or wastewater service is provided.
- (ii) The Development Agreement will detail the extent of the required water and wastewater infrastructure that must be constructed before service is provided by the ABCWUA.
- (iii) The Development Agreement does not replace or supersede the development requirements of the City of Albuquerque as detailed in the Development Process Manual.

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14.1. Priority Capital Improvement Projects

Development of the Volcano Heights Plan area and the Major Activity Center is expected to occur over a period of decades and will require a high level of infrastructure to attract high-quality development and major employers. Priority infrastructure includes, but is not limited to the following needs:

- 14.1.1. The mandatory road network, including the Transit Corridor through the Town Center;
- 14.1.2. Complete buildout of Paseo del Norte and Unser Boulevard to the City's required cross sections;
- 14.1.3. An internal, local road network to access residences and businesses and designed to meet block size requirements for each district;
- 14.1.4. Telecommunications and utility infrastructure;
- 14.1.5. Water, sewer and drainage infrastructure, including regional retention pond facilities, an additional water tank, and water source and water quality improvements;
- 14.1.6. Trails, bike facilities, and other recreational facilities;
- 14.1.7. Civic and public spaces, parks and open space; and
- 14.1.8. Transit facilities, including bus stops and a multi-modal transit center in Town Center serving proposed high-capacity transit.

14.2. Implementation Responsibilities

Though it is likely that some funding will be available for certain projects from the City, County, State and other regional agencies, the bulk of the cost for infrastructure in Volcano Heights is the responsibility of land owners and developers. Broad coordination among multiple land owners and robust financing mechanisms will be necessary for development.

14.2.1. Property Owners

- (i) Coordination – engagement of a master developer is recommended;
- (ii) Infrastructure costs;
- (iii) Development costs;
- (iv) Implementation of finance mechanisms, such as Special Assessment Districts (SADs) or Tax Increment Development Districts (TIDD).

14.2.2. City of Albuquerque

- (i) Sector Development Plan
 - Development of land use and zoning regulations
 - Development of design requirements
 - Development and implementation of streamlined review process
 - Adoption and update of the Plan
- (ii) Economic Development
 - Coordination among multiple jurisdictions;
 - Promotion of area and recruitment of business;
 - Collaboration with businesses and development of incentives, such as Industrial Revenue Bonds

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- (iii) Department of Municipal Development
 - Coordination with agencies and private utilities to develop infrastructure in a timely manner;
 - Creation and evaluation of road cross-sections;
 - Petition for access modifications to add intersections on Paseo del Norte and Unser Boulevard

14.2.3. **State, County, Mid-Region Council of Governments**

- (i) Transportation coordination
- (ii) Assistance with infrastructure costs where feasible

14.2.4. **Albuquerque-Bernalillo County Water Utility Authority (ABCWUA)**

- (i) Planning guidance to verify compliance with the ongoing ABCWUA Master Planning efforts.

14.3. Optional Financial Tools

City Ordinances allow for a variety of financial tools that enable rising property values from development to pay for infrastructure. Desired outcomes at Volcano Heights will likely require the use of one or more of the following methods:

14.3.1. **Special Assessment District (SAD):** Described in the Albuquerque Code of Ordinances, Section 6-8, Special Assessment Districts (SADs) involve an additional charge added to property taxes to fund necessary improvements in new subdivisions, such as drinking water and sewer lines, paving and other government services. SADs can be requested by a percentage of landowners in an area or imposed by a local government, and SAD revenues are used to pay back city general funds or service debts, such as bonds, incurred for infrastructure construction.

14.3.2. **Public Improvement District (PID):** Described in the Albuquerque Code of Ordinances, Section 6-9 and enabled by New Mexico Statute Section 5-11-1 to 5-11-27 NMSA 1978 (2001), Public Improvement Districts involve an additional charge added to property taxes to fund a broad array of improvements in a subdivision, ranging from roads and drainage to recreational facilities, trails, parks, public buildings, libraries and other amenities. Like SADs, PID revenues are used to pay back general funds or debts incurred for the construction of infrastructure. The City of Albuquerque currently requires unanimous vote of property owners to establish a PID, though state statutes allow PIDs to be created with $\frac{2}{3}$ of property owners in agreement.

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14.3.3. **Tax Increment Development District (TIDD):**

Described in the City of Albuquerque Code of Ordinances, Section 4-10, Tax Increment Development Districts (TIDDs) capture a portion of the increase in property and gross receipts taxes resulting from the area's development. Funds can be used to pay back debt on a range of projects similar to PIDs, including elaborate streetscapes like the urban boulevard, parks and trails, civic spaces, and other amenities. TIDDs are typically used for a large, master-planned development, rather than a single subdivision. Unlike other funding mechanisms, they do not impose new costs or taxes on property owners (except those incurred by rising property values caused by development). TIDD proceeds can also be used for ongoing maintenance and improvement of facilities. TIDDs require major coordination among property owners to apply for and maintain districts, issue bonds and manage revenues and bond payment.

14.4. Implementation Matrix: See **Table 14.1** [Pending].

14.5. Amending this Sector Development Plan: Changes to the text, graphics, or Zoning shall be per Section 14-16-4-3 of the City Zoning Code.

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TABLE 14.1 -IMPLEMENTATION MATRIX [PENDING]

Abbreviations and Acronyms used in the following table:

APD	=	Albuquerque Police Department	L RTP	=	Long-range Transportation Plan
APS	=	Albuquerque Public Schools	MRA	=	Metropolitan Redevelopment Agency (COA)
CIP	=	Capital Improvement Project	MRCOG	=	Mid-region Council of Governments
CNM	=	Central New Mexico Community College	NTMP	=	Neighborhood Traffic Management Program (COA)
COA	=	City of Albuquerque	RFP	=	Request for Pro posals
DMD	=	Department of Municipal Development (COA)	SBA	=	Small Business Administration (federal)
GABAC	=	Greater Albuquerque Bicycling Advisory Committee	SHPO	=	State Historic Preservation Office
GARTC	=	Greater Albuquerque Recreational Trails Committee	STEPS	=	Southeast Team for Entrepreneurial Success
			UNM	=	The University of New Mexico

Element	ID	Policy Implemented	Type	Action	Lead Agency	Coordination Required	Potential Funding Sources
A. Movement Systems							
Vehicles	A-1						
Bikes	A-2						
Peds	A-3						
B. Community Form							
	B-1						
C. Infrastructure							
	C-1						
D. Economic Development							
	D-1						
E. Open Space Acquisition							
	E-1						

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APPENDIX

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