

MARTINEZ TOWN / SANTA BARBARA THE FUTURE IS IN YOUR HANDS



Martineztown / Santa Barbara

Sector Development Plan

DRAFT

~~August 2010~~ February 2013



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1 Introduction

Martineztown/Santa Barbara is an historic neighborhood located within the Central Urban Area near Albuquerque’s downtown employment district and The University of New Mexico North Campus. (See figure 1.)

Martineztown/Santa Barbara is one of eleven historic and economically interrelated neighborhoods in the Central Urban Area that have suffered disinvestment and decline. Much of this central area has been redeveloped with an emphasis on cultural and historic preservation as well as commercial revitalization.

Martineztown/Santa Barbara needs redevelopment and preservation.

This Plan area is approximately 548 acres bounded by Menaul Boulevard on the north, Lomas Boulevard on the south, the New Mexico Railrunner Express Railway tracks (NMRX) on the west, and Interstate 25 on the east. (See figure 2.)

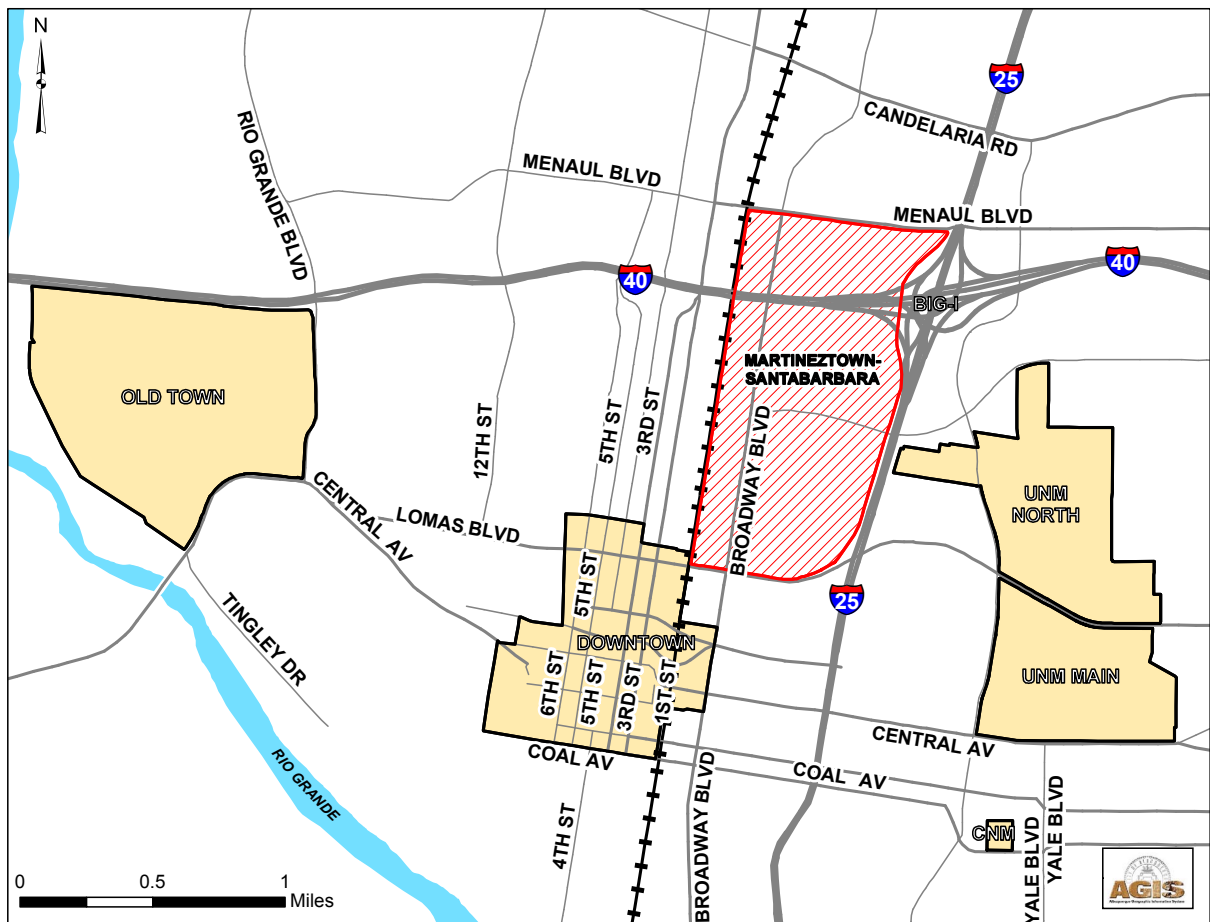
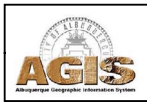
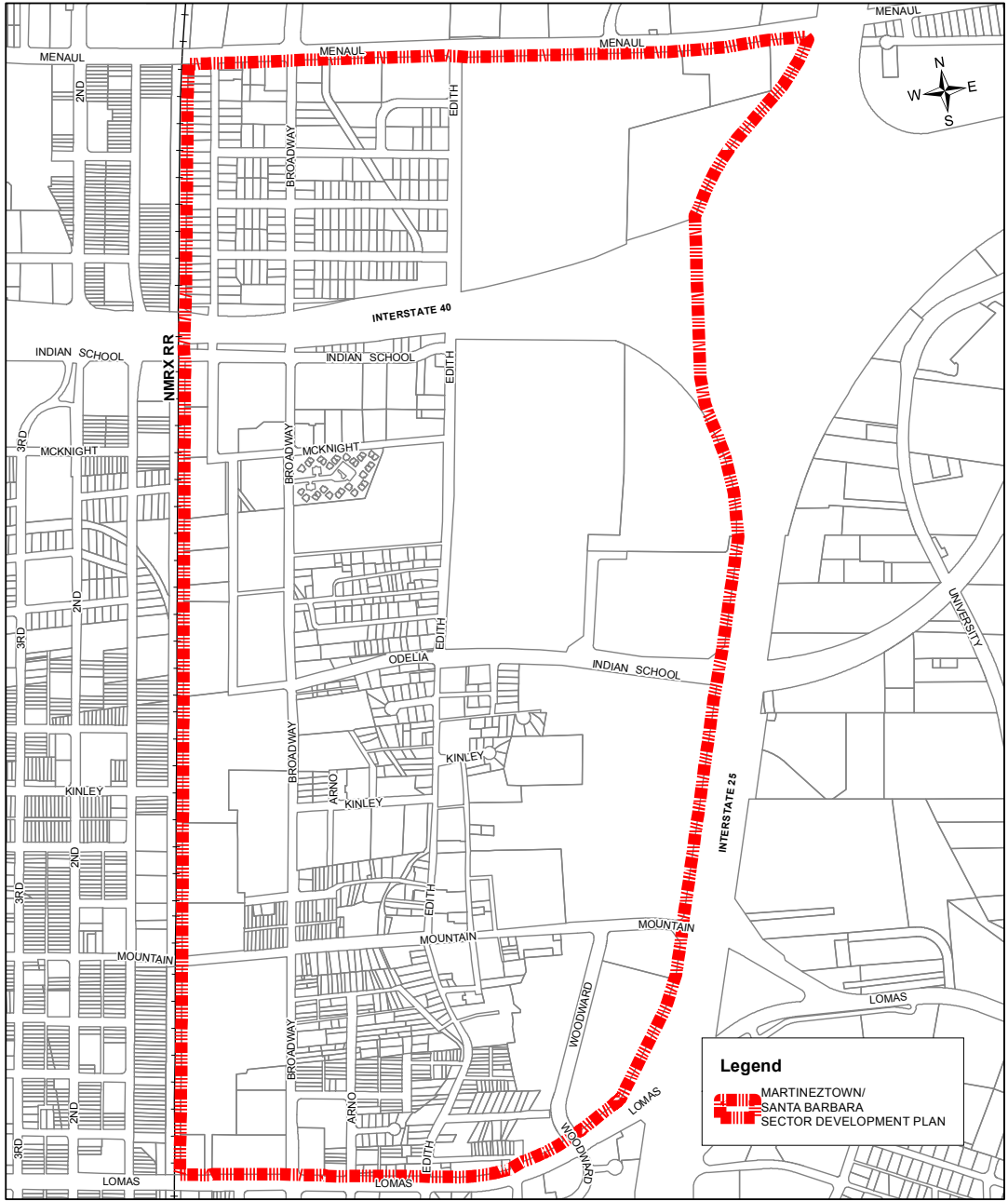


Figure 1 – Context Map

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MARTINEZTOWN/SANTA BARBARA
SECTOR DEVELOPMENT PLAN

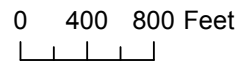


Figure 2 – Boundary Map

A. Purpose

This Sector Plan addresses issues identified by the Martineztown/Santa Barbara Neighborhood Association in their December 2006 letter to the Albuquerque Planning Department. Specific neighborhood issues were: incompatible land use patterns and existing zoning; flooding and drainage problems; inadequate sanitary sewer line capacity and condition; and, commercial and high speed traffic in the neighborhood.

This Plan establishes land development regulations and recommends capital improvements to preserve and protect the neighborhood's historic residential core and existing commercial properties. This short range action plan addresses the immediate needs of the neighborhood and serves as a guide for future development.

The purpose of this Plan is to preserve the area's history, traditional uses, and secure and maintain a balanced and stable economy for the neighborhood. The proposed programs, policies, and projects will aid in the elimination of current and prevention of future blighted conditions.

Martineztown/Santa Barbara was designated a Metropolitan Redevelopment Area in 1989. To date, no Metropolitan Redevelopment Plan (MRP) exists for this area. This Plan recommends a separate Redevelopment Plan be written and adopted using this Sector Plan for its framework. There is a section in this document that proposes specific catalytic projects for redevelopment and suggests possible funding sources.

Upon adoption of this updated plan, the former Martineztown/Santa Barbara Sector Plan adopted March 1990 is repealed.

B. What This Plan Does Not Include

This Plan does not address all area issues identified by the community. The Plan does not address social service program development nor does it emphasize the many projects that the neighborhood could initiate to improve their quality of life.

C. Process

This Plan is the cooperative work of the Martineztown/Santa Barbara community and the City of Albuquerque Planning Department with assistance by the consulting firm Sites Southwest.

In 2007, working committees were formed and composed of representatives from businesses, area property owners, and community activists. Stakeholder interviews were conducted and included one-on-one discussions and interviews. The results of the outreach are reflected in this Plan.

Advisory Committee - The Advisory Committee is a citizen's group of volunteers from the neighborhood that: assessed current needs as defined by the neighborhood association; identified additional neighborhood issues; and, assisted in the development of this update to the 1990 Martineztown-Santa Barbara Sector Development Plan. All attendees at the initial public meeting held in June 2007 were invited to join the Advisory Committee.

Community Survey - After the Advisory Committee completed their identification and evaluation of capital improvement priorities, a survey was developed and mailed to property owners, business owners, and made available to renters within the plan boundaries. The survey listed 16 capital improvement projects and requested the respondent to rank the projects in order of importance. The respondents were encouraged to add additional projects. Approximately 2,000 surveys were distributed and 93 or approximately 5 percent were returned completed.

Business Owner Involvement - The area's business owners and business property owners also provided input. They identified issues at two meetings held for the area's commercial and service community. Issues noted by the community, possible solutions, and the results of the capital project survey were presented. Members of the business community were encouraged to discuss their concerns, and a useful dialogue ensued. The inputs of the commercial and service community are also reflected in this Plan.

Small Area Meetings - Five small area meetings were held to allow residents, property owners and business people who lived, worked or owned property in a particular section of the Sector Plan boundary to further explore and comment on zoning proposals.

D. Community Vision and Goals

At the June 2007 public meeting, participants voiced their issues and concerns regarding: community identity; vehicle and pedestrian circulation; drainage; and capital improvements.

The following vision was developed by meeting attendees.

Community Vision

"Martineztown/Santa Barbara shall remain a low-density residential neighborhood that is family and child-friendly. Preservation of its historic architectural character shall be maintained through preservation of historic buildings and development of new affordable infill housing or redevelopment designed to fit that character. Its streets shall be narrow, pleasant, walkable and safe for pedestrians with good multi-modal circulation. There will be some preservation of open space, opportunities for multi-generational recreation and a local restaurant and market for residents to walk to. Stormwater drainage will be sufficient, and there will be adequate parking and paved streets."

Goals

Increase single family residential zoning

Preserve historic architectural character

Improve streetscapes and create a safe multimodal circulation network

Increase opportunity for local restaurants and markets within walking distance

Improve drainage

Increase parking opportunities and pave streets

Action Plan

In order to address the community goals, this Plan's recommendations include the following.

Increase single family residential zoning

- Change zoning to increase single family residential zoning throughout the residential district.

Preserve historic architectural character

- Create an Historic Residential Corridor along Edith Boulevard.
- Place monument identification signs at key entrances to the neighborhood.
- All amenities such as street lights, benches, signage etc., should have a consistent theme that reflects the culture and history of the neighborhood.
- Design a Plazuela (small plaza) with visual art to commemorate the crossroads of the Carnuel Trail and the Camino del Lado.

Improve streetscapes and create a safe multimodal circulation network

- Create a consistent width of Mountain Road/Streetscape from Broadway to I-25.
- Install sheltered bus stops on either side of Mountain close to the Neighborhood Activity Center.
- Add public amenities such as benches, shelters, and signage.
- Request an engineering study to identify pedestrian access needs, traffic calming and roadway needs and opportunities. The study is to include a warrant study for a traffic signal at the Woodward/Lomas intersection.
- Make streetscape improvements along Odelia to slow traffic and provide additional safety features based on the engineering study.
- Discourage non-local motorized traffic on Edith Boulevard by using decorative paving to indicate shared use and giving preference to pedestrians and cyclists.
- Implement the City's proposed bicycle facilities improvements.
- Create enhanced pedestrian routes along Mountain, Odelia, and Edith.
- Improve pedestrian street crossings by adding crosswalks based on the engineering study and consistent with city policy.
- Perform a sidewalk inventory and pedestrian circulation study.

Increase opportunity for local restaurants and markets within walking distance

- Develop a Neighborhood Activity Center at the corner of Mountain and Edith.
- Modify zoning to reflect the traditional mixed use of Mountain and Broadway.

- Modify zoning to reflect existing mixed uses along Lomas and along Broadway.

Improve drainage

- Create a green buffer between the residential and service districts by vegetating the escarpment.
- Plan and implement soil erosion control and landscaping on Odelia between Edith and High Street.
- Develop a high capacity detention basin designed as a multi-use/park facility to replace the temporary pond.

Increase parking opportunities and pave streets

- Pave unpaved roads and add curb and gutter or alternative walkway/storm drainage features.

The Plan area is divided into four districts based on their predominant land uses. The Plan describes each area's condition, analyses the issues and provides recommendations to support one or more of the goals. Vehicle, bicycle, and pedestrian circulation are examined in the same manner but for the entire plan area rather than by district.

E. Neighborhood History and Historic Character

Martineztown/Santa Barbara is a 19th-century village now grown into a 21st-century, complex neighborhood. It retains hints of its agricultural origins and is connected to its past by people, families, and organizations. Even so, some events in recent decades have changed its character and added to the challenge of planning for its future. Historic neighborhoods require ongoing stewardship and re-investment to thrive, and this one is no exception.

Despite its location in central Albuquerque, parts of the neighborhood recall rural New Mexican villages. Its hillside areas, especially, lend a picturesque quality to certain streets, buildings, and landscapes. Other parts are urban in character--industrial, institutional, even modern.

The neighborhood's significance goes far beyond its appearance, however. The diversity of land use activities, building types, and cultural history make Martineztown/Santa Barbara a place worthy of close attention. Writers such as Rudolfo Anaya, Joseph P. Sanchez, Larry D. Miller, and Susan Dewitt know this, as evidenced by their writings on the area. In his January 1992 New Mexico Magazine article, Martineztown Builds Wall of Memories, Anaya urged elders to use history to help the children of their neighborhood move into the future. Martineztown 1823-1950, by Sanchez and Miller, is subtitled Hispanics, Italians, Jesuits & Land Investors in New Town Albuquerque. It reveals much area history and some varying versions.

In Historic Albuquerque Today (1978), Dewitt describes certain segments of the neighborhood:

“But the heart of Martineztown lies in the old district that stretches along Edith between Lomas and Mountain Road. The community, which grew up after 1880, in part around the Second

Presbyterian Church, established itself in the traditional way around the Plaza Martinez, now lost under more recent streets and buildings. The predominantly Hispano neighborhood built houses in traditional fashion, using adobe rather than the frame and brick of their neighbors to the south.”

“Around the turn of the (20th) century, Santa Barbara began to grow up a little to the north, between Mountain Road and Odelia (a continuation of Indian School Road). Santa Barbara, which took its name from a local cemetery, soon focused around the building of San Ignacio, the handsome white adobe church that crowns the hill and the community. Both neighborhoods sponsored dance halls, local groceries, and small business through the years; some still persist as an essential part of the neighborhood’s character and flavor.”

ORIGINS

The Martineztown/Santa Barbara neighborhood began as an agricultural settlement sometime between 1830 and 1850. It was 1½ miles east of the plaza and San Felipe de Neri Church in the area now known as Old Town.

Don Manuel Antonio Martín founded Martineztown by leaving Old Town and building a house of terron (earthen) blocks for his family near the Acequia Madre de Los Barelás. Other families joined them in building houses near the acequia, and in time a parallel acequia was added to improve irrigation.

During the neighborhood’s settlement period the United States acquired New Mexico from Mexico as one result of the Mexican-American War of 1846-47. All the local villages comprising early Albuquerque became part of the Territory of New Mexico, and most residents were farmers.

OVERVIEW

Portions of Martineztown was annexed by the City of Albuquerque in 1898, Santa Barbara in 1948. In the late 1920s, Edith Boulevard was paved, and the section of Odelia Road from Broadway to Edith Boulevard was constructed. Water and sewer lines reached many area residents in the 1930s.

Since its beginning as an agricultural settlement, Martineztown/Santa Barbara has been somewhat isolated and independent from surrounding communities. The neighborhood was distant from established development enclaves, and over the years physical barriers have reinforced this separation. The marsh that once lay on the western edge of the neighborhood (between Mountain Road and present-day Lomas Boulevard) inhibited development and formed a natural barrier between Martineztown/Santa Barbara, Old Town, and Downtown. The railway tracks along with the interstate systems have contributed to the area’s physical separation from adjacent communities.

Today, portions of the community retain the look and feel of a traditional New Mexico village. Many families still live on properties that have been handed down through several generations. This continuity has contributed to the strong attachment residents have for the neighborhood.

Adding to the area's character is a pattern of winding streets and narrow, irregularly shaped lots, typical of many Spanish settlements. This contrasts with the block-grid style of development that dominates post-railroad Albuquerque.

The physical and economic condition of the neighborhood began declining after World War II. Population decreased and a general deterioration of housing stock took place. In the 1950s, much of Martineztown/Santa Barbara was zoned for commerce and light manufacturing, thus accelerating the transformation of the community. At the time, it was believed that downtown commercial and office uses would need room for expansion, but the downtown core did not spread as anticipated. Instead, vacant land attracted manufacturing uses, open storage and warehousing. The abundance of commercial and industrial zoned land and the resulting encroachment of commercial uses into residential areas have encouraged land speculation and discouraged residential development and reinvestment.

Since adoption of the 1976 Sector Development Plan and the 1978 and 1990 Sector Development Plan Updates, a number of physical improvements have occurred. They include housing rehabilitation and construction, street realignment and paving, development of three area parks, and conversion and rehabilitation of the Santa Barbara School into eight senior apartments, a community room and office space. More recently, the City has made flood control improvements.

AGRICULTURE

Martineztown's cultivated fields and pasture were irrigated from the Acequia Madre de los Barelas, the canal built circa 1832 by Mexican Prefect Don Antonio Sandoval. The acequia provided seasonal water in a locally governed, controllable distribution system.

Early land patterns in Martineztown were typical of irrigated Spanish settlements--narrow parcels of land running roughly perpendicular to the acequia. A few houses were scattered along Bernalillo Road (now Edith) east of the acequia. The neighborhood developed slowly and accelerated upon the railroad's arrival in 1880. About 1900 Santa Barbara began to grow north of Mountain Road, taking its name from a local cemetery.

Agricultural Character

Few historic agricultural features remain in Martineztown/Santa Barbara today, other than the narrow lots along the acequia's former course and the irregular street pattern in residential areas between Lomas Boulevard and Indian School Road. Some old farmhouses still shelter families, and enough land remains open that one can imagine parts of the area under cultivation.

TRANSPORTATION ROUTES

El Camino Real

The Martin family and others settled the area by 1850 alongside a probable track of El Camino Réal de Tierra Adentro (The Royal Road of the Interior Lands). El Camino was a 1,600-mile trade route from Mexico City to Santa Fe. It was the main north-south road linking the New World Spanish colonies. Today Edith Boulevard follows this route. Traversing the Middle Rio

Grande Valley and the site of current-day Albuquerque, El Camino Real spurred commerce and immigration, linking the remote New Mexico colony to the outside world.

The neighborhood began as an agricultural settlement at the crossroads of Mountain Road (also known as the Old Carnuel Trail that links Old Town with Carnuel) and Edith Boulevard (also known as El Camino del Lado).

The general path of El Camino Real was established in 1598 by Spanish colonists under Don Juan de Oñate. It was the first wagon road established in the province of New Mexico and followed the Rio Grande Valley in this area.

After the Pueblo Revolt of 1680, a network of roads linking valley settlements developed in the middle Rio Grande Valley. This gave travelers along El Camino Real a choice of north-south routes when they approached Albuquerque. El Camino Real, however, remained along the eastern edge of the valley floor, and its course through Albuquerque was known as El Camino del Lado or El Camino de la Ladera (the road along the edge).

El Camino del Lado was used for local and pass-through traffic when the valley floor was flooded or muddy. Historians surmise that this route followed the approximate course of what later became Bernalillo Road and is now Edith Boulevard (Cibola Research Consultants, 2001).

In 2000 the United States designated El Camino Real de Tierra Adentro as a National Historic Trail. One probable location of El Camino Real in the Albuquerque area, as mapped by the National Park Service, is the stretch along Edith Boulevard through Martineztown/Santa Barbara. The gently curving, narrow portion of Edith between Lomas and Mountain Road most evokes an old trail once plodded by draft animals and people, but the entire “corridor” of Edith in this neighborhood is rare, important, and worthy of conservation.

Martineztown/Santa Barbara is one of Albuquerque’s historic neighborhoods. Located northeast of New Town (downtown) along the Acequia Madre de los Barellos, the neighborhood began as an agricultural settlement at the crossroads of Mountain Road (also known as the Old Carnuel Trail) and Edith Boulevard (also known as El Camino del Lado).

Don Manuel Antonio Martin founded the community in about 1850 when he relocated his family from the Old Town area to the open pasture land on the edge of the east mesa sand hills. The neighborhood’s name is derived from the Martin family.

The Martin family’s settlement was intersected by the high route of El Camino Real de Tierra Adentro (The Royal Road of the Interior), a 1,600-mile trade road from Mexico City to Santa Fe. El Camino Real was the main north-south road linking the New World Spanish colonies.

Running through the Middle Rio Grande Valley and the current site of Albuquerque, there was constant activity along El Camino Real, which was New Mexico’s lifeline to the outside world. The general path of El Camino Real was established by the Spanish colonists under Juan de Oñate. The route was the first wagon road established in the province of New Mexico

and followed along the eastern edge of the Rio Grande, with the exception of the Jornada del Muerto pass (Journey of the Dead Man). The early route of El Camino Real in Albuquerque followed the eastern edge of the valley below the sand hills and above the low, marshy areas of the valley floor. After the Pueblo Revolt of 1680, a series of roads linking valley settlements developed in Albuquerque. This gave travelers along El Camino Real several choices of north south routes when they approached Albuquerque. El Camino Real, however, remained along the eastern edge of the valley floor, and its course through Albuquerque was known as El Camino del Lado or El Camino de Ladera (the road along the edge). El Camino del Lado was used for local and trade traffic during inclement weather when the valley floor was flooded and muddy.

Atchison, Topeka, and Santa Fe Railway

The railroad's arrival in 1880 created a new Albuquerque settlement near the tracks and brought change to Martineztown/Santa Barbara as well as other local villages. Many area residents found employment with the railroad and with commercial operations that developed near the rail line and spur.

Among the early businesses were two wool-scouring mills, one of which opened in 1895 at the corner of Mountain Road and the rail line. Nearby, two breweries and a flour mill opened. A number of smaller, neighborhood-oriented businesses sprang up--grocery stores, barber shops and general stores. Four dance halls operated in the area during the 1920s and 1930s.

The rail line remains, now used mainly by the New Mexico Rail Runner Express commuter train and Amtrak but owned by the BNSF Railway, successor to the Atchison, Topeka, and Santa Fe.

Interstates 40 and 25

The Interstate Highway System transformed the nation during the decades just after post-World War II. It marked Martineztown/Santa Barbara directly. First, the neighborhood's gravelly eastern hills provided right-of way for I-25. The new, limited access highway became a barrier between the neighborhood and higher ground including the Martinez Family Cemetery on Lomas east of I-25.

I-40 was built in the East Mesa's Embudo Arroyo, whose mouth lay at the northeast corner of Martineztown/Santa Barbara. The I-40 interchange with I-25, a/k/a Big I, now occupies the space. Springer Transfer Company developed Springer Industrial Center on Broadway starting in 1960, just as both interstates were under construction at the edges of town. The new highways were quick routes out of town from the Industrial Center. A subtly modern-style series of warehouses with built-in office space, the Industrial Center became an intermodal freight facility and employment site. Pending further research, the Springer Industrial Center is conceivably eligible for listing in the National Register of Historic Places.

COMMERCIAL & INDUSTRIAL

Neighborhood retail

Typical of Albuquerque neighborhoods, Martineztown/Santa Barbara had a series of grocery stores—AAA & Sons, Martinez Store, Brite Star Grocery, and FM Mercantile. All are defunct as businesses but the buildings remain. The AAA & Sons complex is probably eligible for historic registration as a small historic district, and its registration could provide tax advantages toward its rehabilitation. The F.M. Mercantile, 1522 Edith NE, is listed in the New Mexico Register of Cultural Properties. It housed a general store, blacksmith shop, wood yard, and residential space, and was headquarters of the Martineztown Bridge Club. Now in residential use, the F.M. Mercantile's rehabilitation involved a State of New Mexico income tax credit.

Construction-related

Businesses that supply materials and equipment or perform construction have long existed in the neighborhood. This employment base probably began with gravel quarrying in the area now covered by I-25, and since the railway offered a ready connection to distant suppliers, many such businesses were located near the tracks. None of the buildings appears to be historically significant.

Other longstanding business types include monument companies near the cemeteries, truck and automobile repair shops, and produce stands. More recent additions to the business mix include professional offices, art sales and spa services, restaurants and coffee shops. And large operations including a hotel, medical rehabilitation clinic, and testing laboratory add further variety. While all these lack historic significance as places, they enrich the business mix and represent opportunity for Martineztown/Santa Barbara residents.

SCHOOLS

Santa Barbara School, originally two adobe-walled rooms, was built about 1908 as part of the county school system and expanded in stages thereafter. Albuquerque Public Schools acquired it, used it as a Special Services Annex, and closed it in 1986. In 1991 the City of Albuquerque converted it to the Santa Barbara Apartments for senior citizens. It is listed in the National Register of Historic Places as the Santa Barbara School, and its current residential use shows how a historic building can be modified while protecting its historic value.



Santa Barbara School, built in stages, 1908-1930

1420 Edith NE

State Register of Cultural Properties #1510, National Register of Historic Places

Albuquerque High School, the only public school in the area, was relocated to the current site in Martineztown/Santa Barbara in 1975. The new campus accommodated growing school enrollment and car-commuting students when the historic campus at Central and Broadway had reached its limits. Even so, proximity of the campus to residential areas of Martineztown/Santa Barbara makes a short walk to school for some students.

CHURCHES

Second Presbyterian Church and San Ignacio de Loyola Church have figured prominently in the community for a century and more. Their buildings are neighborhood landmarks significant to Albuquerque as a whole, as indicated by their listings in the National Register of Historic Places.



San Ignacio Church, built 1916

1300 Walter NE

State Register of Cultural Properties #520, National Register of Historic Places

Community members did most of the adobe brick construction of San Ignacio (1916), a simple vernacular design and beautifully sited church. It was given official parish status in 1926 and has been known for elaborate fiestas, dances, and holiday celebrations. A concrete arch at Edith Boulevard fronts the church property, frames the picturesque setting, and marks the entry for processions and casual visits alike. The arch may itself be eligible for historic registration as a significant structure



Second Presbyterian Church, built 1922

812 Edith NE

State Register of Cultural Properties, National Register of Historic Places

Founded in 1889, Second Presbyterian Church was reportedly associated with the Martín family's conversion to Protestantism. The existing California Mission Style church was built in 1922, four blocks south of the original Plaza Martinez site. Because of the Protestant Hispano population in the neighborhood, it was known to some as La Placita de los Protestantes.

COMMUNITY EVENTS

In *Historic Albuquerque Today* (1978), Susan Dewitt wrote: "Fiestas and celebrations of all kinds were and are an essential part of the life of Martineztown and Santa Barbara. For fiestas—attended by relatives and friends from Old Town, Los Griegos, San Jose—streets were decorated with arches, selected yards with altars, a work of community labor and love. Dances like Los Matachines (a traditional dance-drama), velorios (wakes) for saints, high mass and processions, family feasts—all played a part on the ritual of community celebration, and while some of the old customs have fallen into disuse over the years, fiestas still bring back to the community many sons and daughters who have established households in the Heights or the West Mesa, but who still see the neighborhood as the family home."

NEIGHBORHOOD SERVICES

The Martineztown House of Neighborly Service, Casa de Amistad, occupied a building connected to Second Presbyterian Church until closing in 2011. Originally affiliated with Jane Addams' progressive social programs in Chicago, the House of Neighborly Service addressed a varying set of area residents' needs starting in 1929. Its leaders now plan to re-open as a community center with summer tutoring, a computer lab for educational and personal use, meeting space, and emergency food distribution, as needed to complement neighborhood services provided by other organizations.

Provision of municipal services and facilities began after 1900. From schools to streets to utilities to flood protection, improvements have come via local, state, and federal governments, most recently in the form of retention basins for storm water runoff.

The Santa Barbara School, parts of which are a century old, now houses seniors in bright apartments once used as classrooms. Despite two decades of use as apartments, it still serves educational functions, as the Albuquerque Community Schools Program conducts classes in the rear rooms of the old schoolhouse.

RESIDENCES

From the vernacular houses of its original farmers to the multi-family infill of recent decades, Martineztown/Santa Barbara's residential character is very mixed. Mixed in density, mixed in condition, mixed in historic value. None of the single-family houses is listed in a historic register. Pending further research a few have potential for historic registration, including the architecturally distinctive houses at 612 Page NE and 1019 Edith NE. The latter house has ornamentation that sets it apart from its simpler neighbors and much of historic Albuquerque.

Single family residential ranges from irregularly placed houses on hillside lots in the southern section to the suburban, long-block development in the Franciscan Acres Addition north of I-40.

Publicly subsidized residential projects range from single-family infill on certain cul-de-sacs to the Broadway Place complex to a planned residential development under construction on Indian School Road. Other houses built on speculation and by owner-occupants bode well for a stable residential mix in the neighborhood.

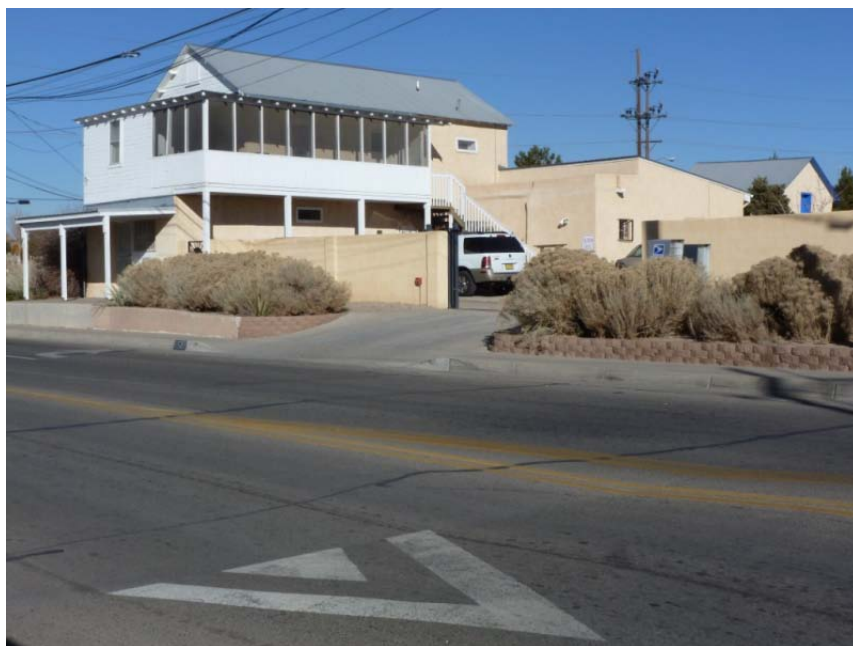
Taken together, the residential properties of Martineztown/Santa Barbara outnumber those in other land uses but occupy far less land. Their groupings are small and separated by I-25, in the worst instance. Their conservation prospects depend on more factors that can be addressed in a sector development plan. However, the public value inherent in sound residential areas, especially ones with historic and other distinctive character, is evident in this neighborhood.

CEMETERIES

Cemeteries occupy a substantial portion of Martineztown/Santa Barbara's land area, probably the highest ratio of any Albuquerque neighborhood in this regard. While cemeteries are generally not eligible for historic registration, these two exhibit historic character and serve individual and community purposes—grieving, reflection, and commemoration. They warrant care and conservation, particularly the upright burial markers, for which good technical publications are available.

The Mt. Calvary/Santa Barbara Cemetery, 1900 Edith NE, lies south of I-40 and Sunset Memorial Park north of it. Both parts are old, but Santa Barbara is the oldest part, in use since the 1870s. It has received reinterred remains from Old Town burial grounds and the site of San Ignacio Church, which had been a graveyard before its construction.

Sunset Memorial Park, 924-C Menaul NE, is a modern sort of cemetery with burial markers flush with the ground. Its frontage on Menaul and Edith makes a verdant sort of northern entry to the neighborhood.



F.M. Mercantile, built 1919
1522 Edith NE
New Mexico Register of Cultural Properties #495

Quotes from a circa 1980 conversation with Felipe M. Garcia, proprietor of the F.M. Mercantile:

“I was born in San Ysidro, close to Jemez. My father sent me here in 1907, so I could go to school. They didn’t have a school in San Ysidro, only one at the Indian Pueblo that was run by nuns. He brought me to Menaul School with some first cousins of mine – it was a Protestant school, and my mother was a Protestant, she went to a Protestant school in Santa Fe.

“Then my father, Monico, sold his 60 acres of land in San Ysidro and bought this property and the land where Santa Barbara School is now. He put a blacksmith shop in here. He struck it good when he came – he bought a lot of land, and it was cheap in those days. But I remember him telling my mother, “Here they’re very stingy with land, they sell tongues instead of acres (lenguas en vez de leguas). But I’m going to beat them.” So to beat them, he built two-story buildings, even for his horse and buggy: on top was the hay, the horse on the bottom. He built this house for me to start a store in when I came back from the war.”

“When they were ready to build a school, A.B. Montoya, who was Superintendent of (Bernalillo County) Schools, said, ‘Monico, we want 16 of these lots.’ My father got about \$2,500, and that was a lot of money then. He said, ‘I’m going to start buying land.’ But by that time, he couldn’t

buy any more. When they built the school, before the war, everyone wanted to live around it, so he could sell all the lots he had. When I came back from the war, this neighborhood (Santa Barbara) was all very well established.”

ARCHAEOLOGY

Martineztown/Santa Barbara’s origins as a 19th-century settlement, and its location on even older transportation routes, make archaeological discoveries a distinct possibility. Excavation projects, even re-grading a parcel of land, may reveal significant cultural materials and artifacts. Indeed, one family reports finding a historic branding iron in the earth on the roof of their traditional house—rooftop archaeology! Discoveries at or below ground level should be reported to the City Archaeologist for evaluation, but owners may keep any artifacts found on their land.

CONSERVING NEIGHBORHOOD HISTORIC CHARACTER

Historic neighborhood character can be conserved and promoted in many ways. Basic zoning and building regulations are essential. Public awareness, especially among neighbors, must be raised and renewed repeatedly. The Martineztown Wall of Memories exhibit at Santa Barbara School, installed in 1991, was an excellent example of on-site interpretation of neighborhood history.

Modern digital media provide new ways to share knowledge of a place both locally and broadly. However, a simple brochure for self-guided walking tours in Martineztown/Santa Barbara could be a reliable tool for neighborhood appreciation for residents and visitors alike. Combined with historic building plaques and related interpretive signs, brochures can convey the basics of a place at low cost. Such brochures have been prepared for several Albuquerque neighborhoods to promote walking as exercise for better health.

Historic registration calls public attention to an area’s historic resources and can foster their preservation. In New Mexico, taxpayers can claim a state income tax credit to defray some costs of state-approved historic rehabilitation projects on registered buildings. Registering historic resources also provides for the regulation of publicly funded projects that will affect the resources. The State Historic Preservation Division administers the registration and tax-advantaged historic rehabilitation processes.

Overlay zoning can provide legal controls specific to a place and its character. Among the City’s overlay zones, the Urban Conservation Overlay (UCO) Zone is the most applicable to the village-like areas of Martineztown/Santa Barbara. Quoting from the Comprehensive City Zoning Code, “This overlay zone may be used for areas which have distinctive characteristics that are worthy of conservation but which lack sufficient historical, architectural, or cultural significance to qualify as historic areas, and which, in addition, have recognized neighborhood identity and character; have high artistic value; have a relationship to urban centers of historic zones which makes the area’s conservation critical; or are located outside of the Redeveloping Area and are subject to blighting influences.”

An application to establish the UCO Urban Conservation Overlay Zone in an area such as

Martineztown/Santa Barbara can only be submitted by property owners. Fifty-one percent of the property owners in the area covered by the application for a UCO Urban Conservation Overlay Zone must agree in writing to the application before it is submitted. The City Council must adopt any UCO Zone, identify its significant features, and provide general preservation guidelines. The Landmarks and Urban Conservation Commission then adopts specific development guidelines for use in its decision-making on proposed alterations in the overlay-zoned area.

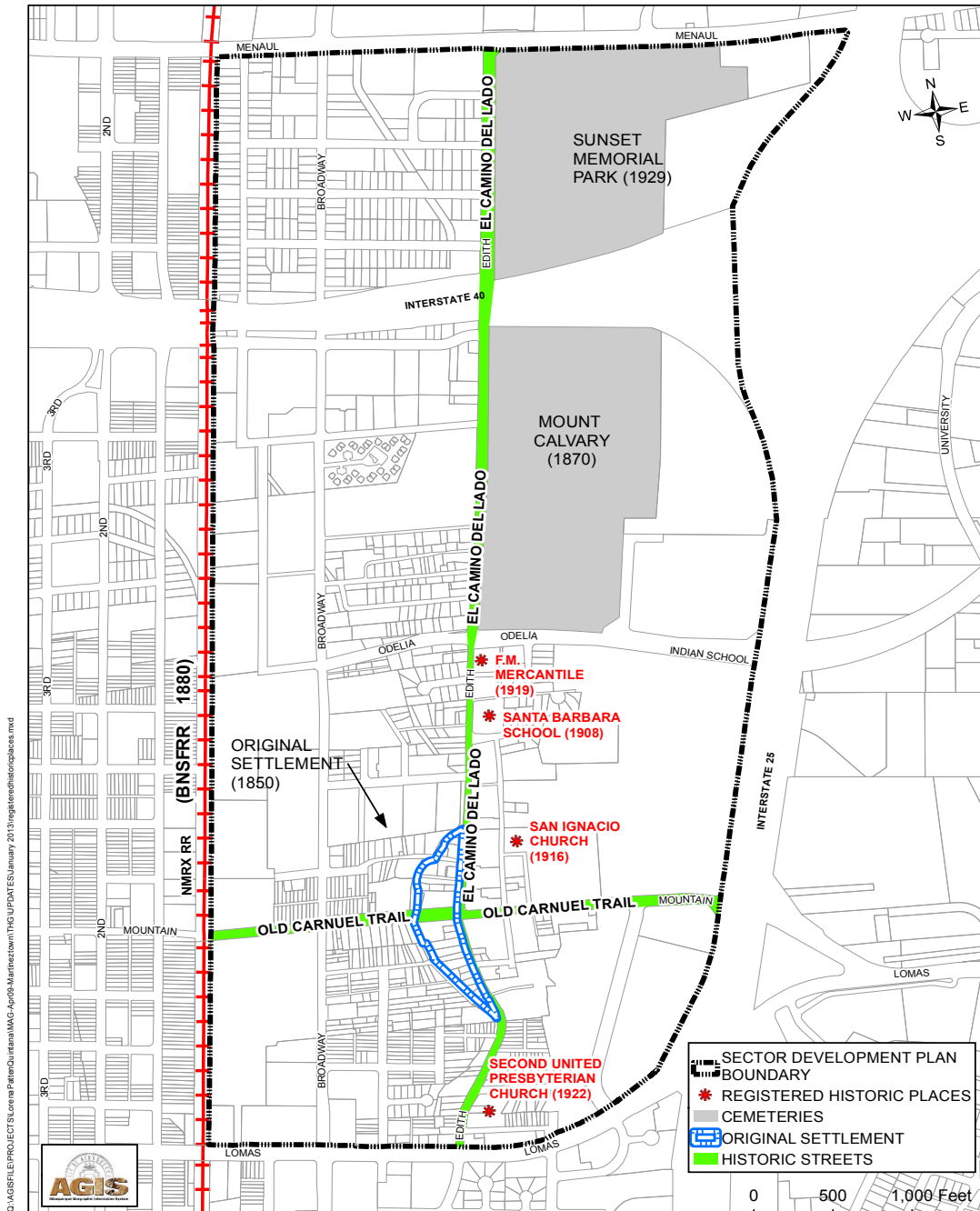


Figure 3 – Area’s Historic Relevance

F. Zoning History

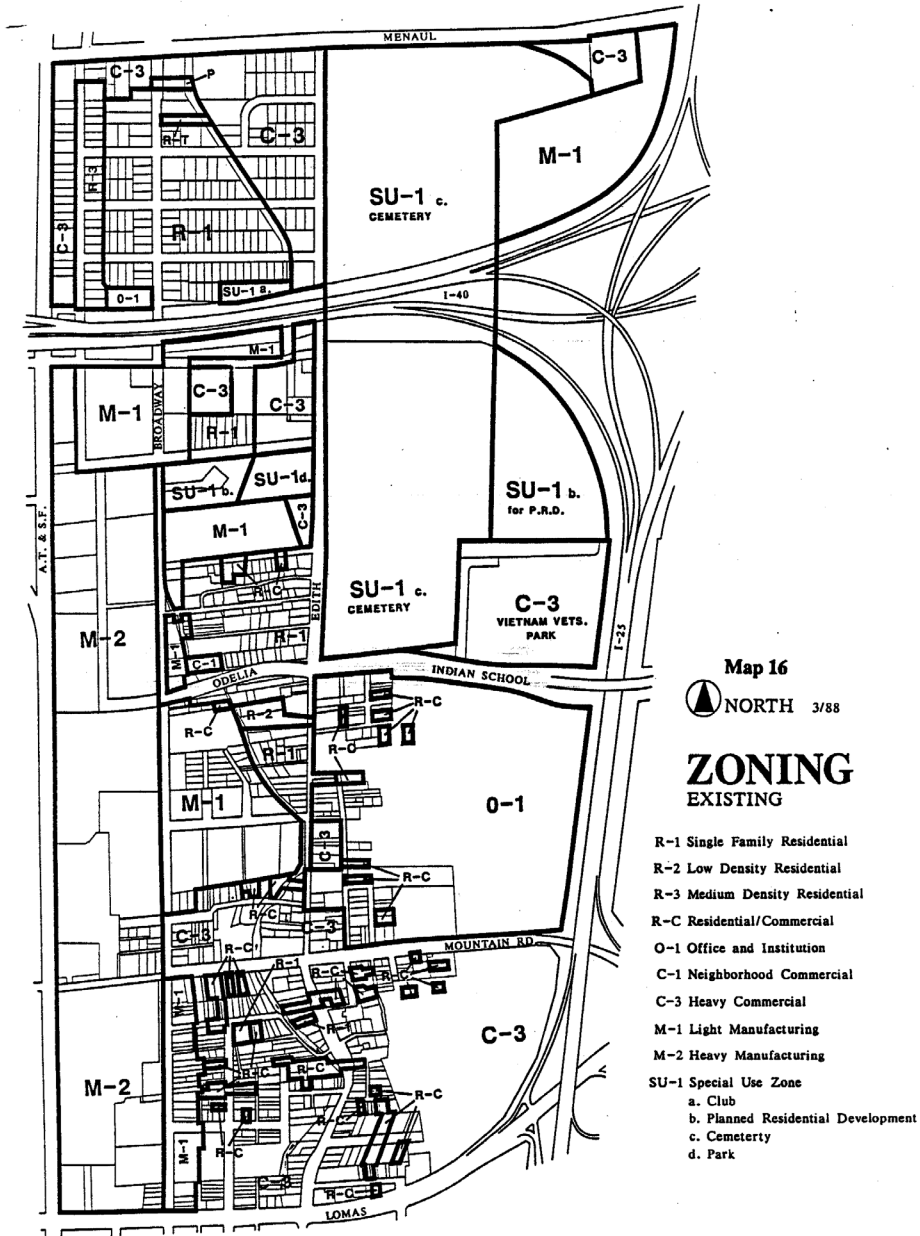
When its original zoning was established in 1959 (see figure 4), Martineztown/Santa Barbara was envisioned to be an expansion area for Downtown. Despite the existing prevalence of low density residential land use, much of the neighborhood was zoned commercial and industrial. The northern and central sections of the neighborhood developed with residential uses surrounded by commercial and light industrial uses along major arterials. By contrast, the southern area continued to develop as primarily single-family residential, despite heavy commercial and light industrial zoning.

The 1976 Martineztown/Santa Barbara Sector Plan identified the mismatch between land use and zoning in the southern area and created a non-regulatory land use plan to guide zone change requests.

The 1990 Martineztown/Santa Barbara Sector Development Plan adopted SU-2 (Special Neighborhood) zoning for the plan area to help stabilize the area in its present condition but still allow property owners flexible use of their land. It created two new mixed-use zones for the neighborhood: SU-2 RCM (Residential-Commercial Martineztown), a modification of the City's RC zoning that allowed commercial uses to occupy up to 100 percent rather than half of the gross floor area, and SU-2 NRC (Neighborhood Residential Commercial), a mixed-use zone that allowed residential and low intensity commercial uses. This zoning was considered transitional, and the plan recommended future amendments. The plan also established SU-1 Special Use zones for properties, such as the Moose Lodge, Sun Village Apartments, parks and churches that had one use but many different zones, and made some changes in the SU-2 HM (Heavy Manufacturing) zone. (See figure 5.)

Despite the changes in 1990, there remained a mismatch between existing land uses and their designated zoning categories, particularly in residential areas.

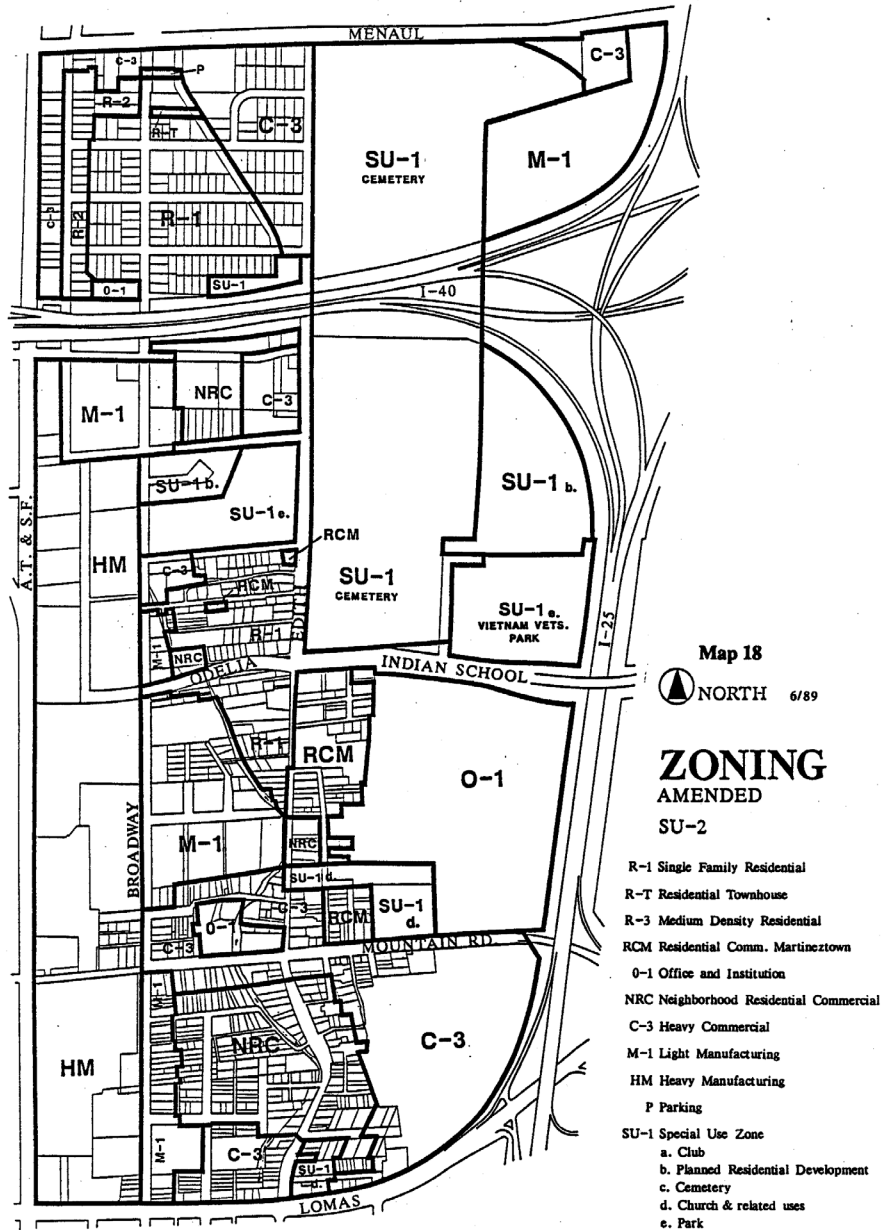
MARTINEZTOWN/SANTA BARBARA



67

Figure 4 – Zoning Established 1959

MARTINEZTOWN/SANTA BARBARA



78

Note: Zones shown on map are unique to the Martineztown/Santa Barbara Sector Plan Area.

Figure 5 – Zoning Amendments Adopted 1990

G. Demographics

Total Population: The total population of Martineztown/Santa Barbara is 2,415 compared to Albuquerque at 545,852. Compared to Albuquerque, Martineztown/Santa Barbara has a slightly larger percentage of children under 19 years old and about 13% more young adults between 20 and 39 years old. This indicates that there may be a greater percentage of children in future years.

Ethnicity: Martineztown/Santa Barbara has a higher population of Hispanic or Latino residents than the Albuquerque as a whole. 2010 Census data estimates the Hispanic or Latino population in Martineztown/Santa Barbara at 66%, while Albuquerque's is estimated at 47%.

Household Income: The median household income in Martineztown/Santa Barbara is \$20,414, which is significantly less than in Albuquerque (\$45,478). In Martineztown/Santa Barbara, 39% of households make less than \$15,000 per year and 59% make less than \$25,000.

Education: Residents in Martineztown/Santa Barbara have received slightly less formal education than their counterparts in Albuquerque as a whole. About 18% of Martineztown/Santa Barbara residents did not finish high school compared to 13% of residents in Albuquerque as a whole. However, the percent of residents with a college or advanced degree (38%) is on par with that of Albuquerque as a whole (39%).

Population Projections: Population projections performed by the Mid-Region Council of Governments indicate that Martineztown/Santa Barbara will likely lose residents in the future. The projections estimate the 2015 population at 2,197, 2025 at 2,149 and 2035 at 2,094. These are compared to projections for Albuquerque that indicate a modest increase with estimates for 2015 at 565,049, 2025 at 646,625 and 2035 at 673,983.

2 Planning Districts, Issues and Recommendations

To aid in issue analysis and solution development to accomplish the community's vision, the project team, in consultation with the neighborhood, established four character districts (see figure 6): Residential District, Railroad District, and Service District. The character, issues and recommended solutions of each district are described below.

Each section of this Plan is meant to stand alone. Issues and recommendations may appear in more than one district as well as in the transportation and drainage/sewer sections.

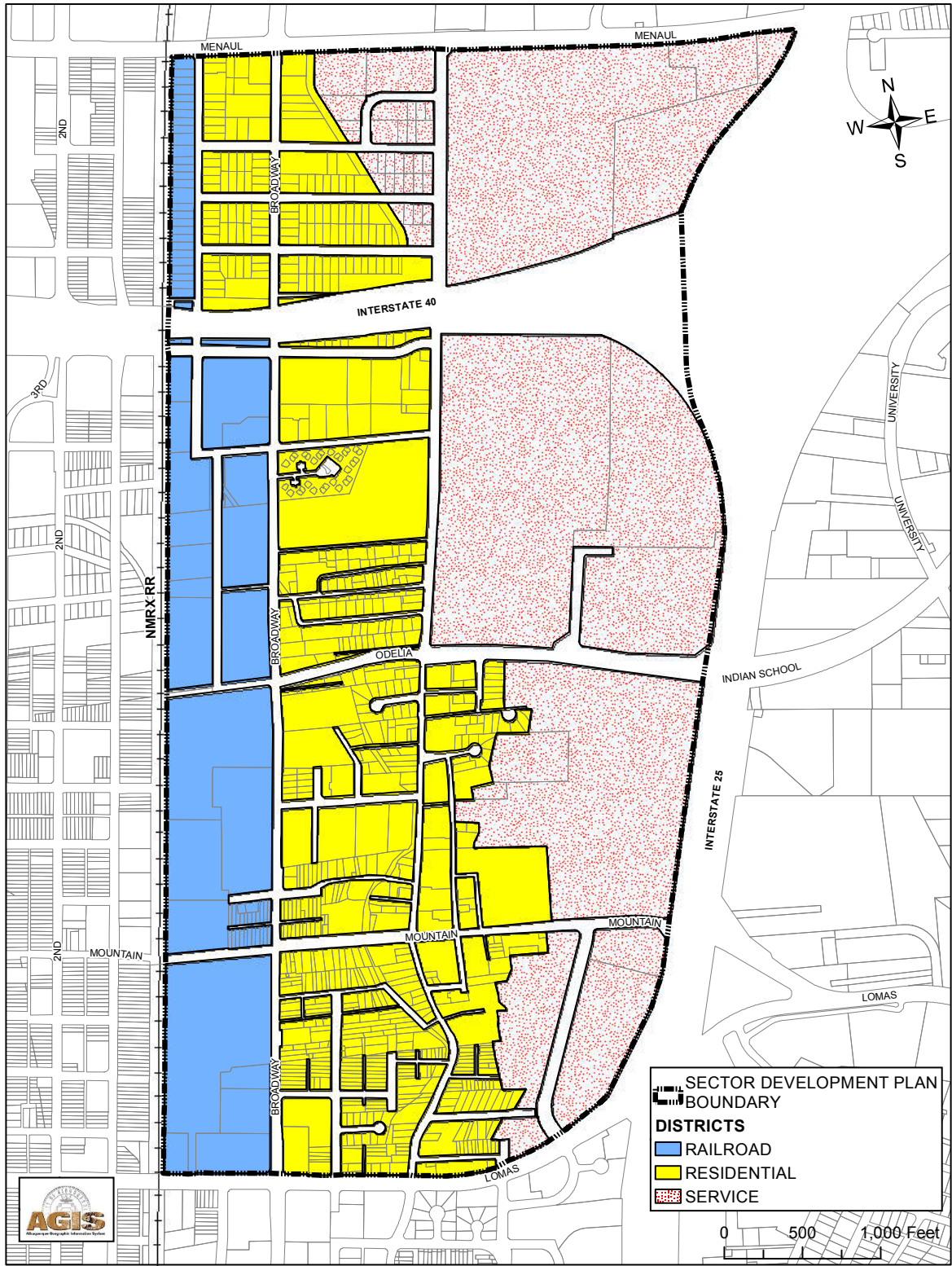
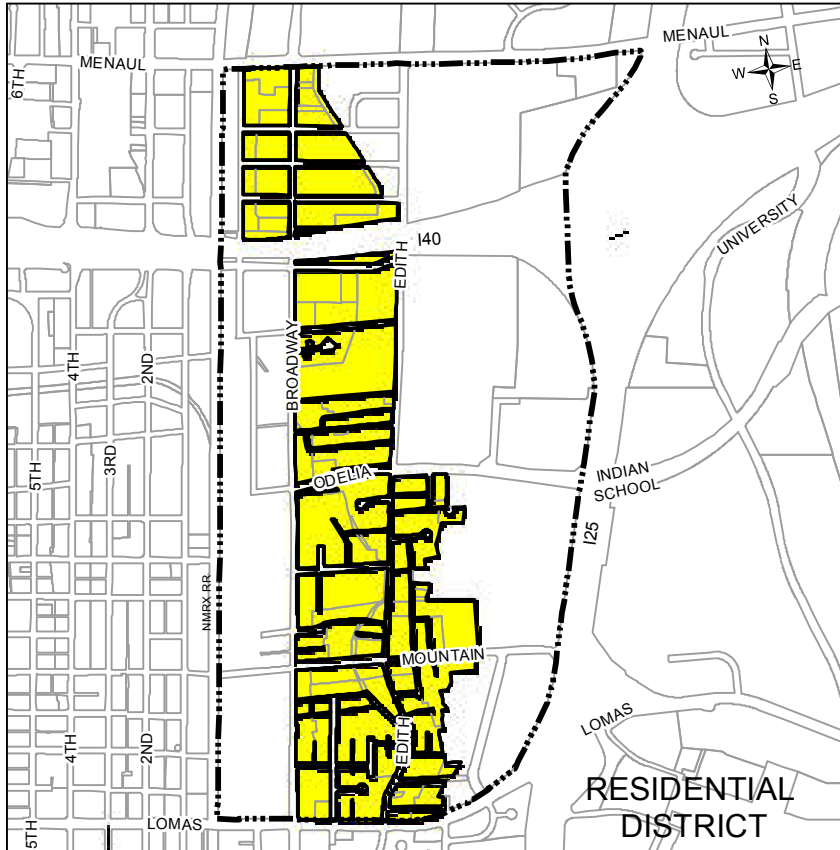


Figure 6 – Character Districts

A. Residential District



The Residential District contains the oldest part of Martineztown/Santa Barbara, including the historic core and two historic churches. San Ignacio's church spire is highly visible from most of the district and is a landmark of the community's cultural and historic roots. The Second United Presbyterian Church founded in 1889 also has historic relevance. Land uses are primarily single-family residential with pockets of commercial/industrial located mainly along Broadway, Edith and Lomas Boulevards. This district also includes two parks, a planned residential development, the Santa Barbara Community Center, and the Moose Lodge Family Center. Approximate boundaries are Menaul Boulevard to the north, Broadway Boulevard to the west, Lomas Boulevard to the south and the escarpment (south of Odelia) or Edith (north of Odelia) and the abandoned Alameda Lateral (north of I-40) to the east.

Issues and Analysis

Incompatible Zoning and Design

The zoning first established for Martineztown/Santa Barbara was predominantly commercial and industrial despite mixed uses that included residential. In 1990 zoning was changed to restrict commercial uses within residentially developed areas.

The 1990 Martineztown/Santa Barbara Sector Plan zoned the area south of Mountain Road primarily SU-2 NRC (MTSB Neighborhood Residential-Commercial). Much of the area between Mountain Road and Odelia/Indian School was zoned SU-2 RCM (MTSB Residential Commercial).

The latter is essentially the neighborhood's special live-work zoning, which allows commercial uses on 100 percent of the gross floor area. While some residents believe that this mixed-use zoning tends to destabilize rather than strengthen the area as a single-family neighborhood, others wish to preserve the ability to retain a small family business within their homes. There has not been consensus on this issue however, new zoning actions will help to preserve single-family uses and allow for mixed uses in appropriate areas.

The existing SU-2 NRC zoning allows townhouse development but provides little in the way of design guidance. Residents have voiced dissatisfaction with new infill development of townhouses along Edith, south of Mountain, which is architecturally out of scale with the rest of the neighborhood design.

It is debatable whether infill construction of single-family dwellings in the area remains financially feasible without gentrifying the neighborhood. One solution is to allow a secondary dwelling unit (for example, a mother-in-law quarters) on lots that are large enough to accommodate them.

Some residential zones abut SU-2 C-3 (commercial) and SU-2 M-1 (industrial) zones. Some of these adjacent high intensity zones are not buffered or screened from the residential zones as required by the Zoning Code.

Zoning incompatible with land use exists along Broadway from Lomas north to Rosemont. Those properties are zoned for manufacturing and heavy commercial though existing uses are a mix of single-family residential, commercial retail, commercial service and some light manufacturing. The current zoning allows far more intense future uses than now exist on those properties. Rezoning to mixed use zones would better carry out the traditional mix of residential and less intense commercial uses.

Lack of Neighborhood Goods and Services and a Central Gathering Place

Residents have expressed a need for neighborhood scale retail and services. Although there are a few restaurants, most of them border the edge of the plan boundary.

An important concept in the Albuquerque/Bernalillo County Comprehensive Plan is the development of activity centers. The Activity Centers concept concentrates land uses for greater efficiency, stability, image, diversity and control while safeguarding the city's single-family residential areas from potential intrusion by more intense land uses. Such centers are designed to connect to transit, pedestrian pathways and bikeways to encourage parking once and then walking to various destinations.

Neighborhood Activity Centers are designated to meet daily convenience goods and service

needs of residents in two or three nearby neighborhoods. The centers typically range from 5 to 15 acres in size and contain a mix of small-scale retail with expanded outdoor seating, service uses, a park or plaza and perhaps small institutional uses. They are to be located on local or collector streets, designed for walking from one side to the other, and serve as a recreational and social focal point for the neighborhood.

Mountain Road Traffic Issues

Residents called for changes to Mountain Road to create an attractive, walkable neighborhood roadway, leaving Lomas and Broadway to carry the bulk of the through traffic, including trucks. Neighbors indicated that delivery and semi trucks speed along Mountain, creating unsafe conditions along a road for pedestrians where four-foot sidewalks with no buffering from traffic are typical. Mountain Road issues are discussed further in the Transportation chapter.

Rainwater runoff

Several hill sites contribute to the flooding of Martineztown, specifically, the slopes of TriCore Medical Laboratories, and the slopes of Albuquerque High School. The property owners at the base of the escarpment owned by Albuquerque Public Schools (APS) have had rainwater runoff flood their yards, erode their patios, and enter their homes. The City of Albuquerque has no jurisdiction over APS; however, there is a site plan that requires TriCore to vegetate their slopes.

The Mid-Valley Drainage Master Plan (MVDMP) was completed in 2012 and determines the volume of runoff and ways to collect and remove this volume. A key element of this DMP is the determination of the required stormwater detention facilities needed to protect the neighborhood.

A major element of stormwater management for this neighborhood is the Broadway Pump Station located on the west side of Broadway just north of Lomas. This pumping plant is the largest in the city and pumps storm drainage uphill to the North Diversion Channel near UNM Hospital. This plant is scheduled for a major rehabilitation in 2012-2013 supported by both the City and AMAFCA. This rehabilitation will modernize the facility and improve reliability of the station. However, one drawback of this facility has been, and will continue to be, a limited capacity.

Erosion on Odelia

There is an erosion control issue in the public right of way north of the F.M Mercantile building at 1516 Edith NE. This right of way contains a sidewalk that leads east to Albuquerque High School. The space is a bare slope that sends soil over the sidewalk and into the street when it rains. The erosion poses a threat to the adobe barn behind the historic building as well as to the safety of pedestrians and bicyclists.

Recommendations (See figure 7)

Modify zoning to reflect the traditional mixed use of Mountain and Broadway

The area just north of Mountain is a good location for mixed use development as it straddles Highway 47 (Broadway) with property in both the railroad and residential district. This zone change would help buffer the residential core and provide a good transition to the railroad district. A mixed use zone includes opportunity for residential development.

Modify zoning to reflect existing mixed uses along Lomas and along Broadway

A change from commercial to mixed uses would serve to buffer the residential district from traffic on Highway 47 (Broadway) and the principle arterial Lomas while providing a smooth density transition to single-family. These are prime locations for higher density mixed use/street related development especially given the transit service on both boulevards. This change would allow for the development of new affordable infill housing.

Increase single-family zoning within the Residential District

This action supports the community's request to reclaim the historic low density residential district and will provide additional opportunity for affordable infill housing on existing vacant lots. Design regulations will ensure preservation of historic physical characteristics.

Develop a Neighborhood Activity Center at the corner of Mountain and Edith to include a Plazuela (small plaza) with visual art to commemorate the crossroads of the Carnuel Trail and the Camino del Lado

Development of an activity center at this important intersection would preserve and protect single-family residential areas while providing opportunities for neighborhood scale retail, services, and additional housing. A plazuela with art would call attention to the rich cultural history of the neighborhood by emphasizing the historic relevance of the crossroads. The plazuela would provide an open gathering space for the community and serve as a visual cue to calm vehicular traffic. Designed to reflect traditional community character, this center would serve as a recreational and social focal point for the neighborhood.

Create a consistent width of Mountain Road and Streetscape from Broadway to I-25

Narrowing the portion of Mountain Road between Edith and Broadway would protect the residential district by calming vehicular traffic. The addition of on street parking and bulb outs with landscaping on the north side of Mountain would narrow the existing lane width without reducing the number of lanes and add needed parking. This project would create a more pleasant, walkable and safe environment for pedestrians.

Install sheltered bus stops on either side of Mountain close to the Neighborhood Activity Center

During the writing of this Plan, ABQ RIDE proposed an evaluation of bus stops throughout the entire route system. The evaluation will be used to determine effective bus stop placement. Stops may be consolidated or eliminated. One of the first routes under investigation is the Montgomery/Carlisle #5, a route that has seven existing bus stops along Mountain Road within the plan area.

This route serves the Post Office, Albuquerque High School, Embassy Suites, TriCore, Career Enrichment Center, Workforce Training Center as well as Martineztown/Santa Barbara residents. At the time of this writing, ABQ RIDE is reviewing the bus stops on the #5 route from Carlisle and Menaul to the Alvarado Transit Center. As bus stops on Mountain Road are determined, shelters are recommended on either side of Mountain at the Neighborhood Activity Center.

Create a green buffer between the residential and service districts by enforcing the TriCore Reference Labs site development plan (approved October 2002).

Enforcing the planting of trees and grasses along the natural escarpment as required in TriCore's site development plan will help mediate long standing erosion issues, and protect the residential core from water run-off from the southern portion of the service district. Enhancing this natural feature will reflect the agricultural history of the neighborhood.

Place monument identification signs at key entrances to the neighborhood

This action would help to preserve and protect the unique identity of the historic residential core by alerting motorists that they are entering a low-density residential neighborhood. This measure is intended to calm traffic to increase safety of residents. Signs are recommended for southbound traffic at Odelia and Edith, northbound traffic at Lomas and Edith, westbound traffic at Woodward and Mountain, and eastbound traffic at Broadway and Mountain. Signs should be designed to complement community character.

Create an Historic Residential Corridor along Edith Boulevard

This action would preserve and protect the historic and cultural value of the "Camino del Lado" and its historically significant buildings. The land uses on either side of Edith are predominately residential and predate the zoning established in 1959. Local historians believe the area was first settled circa 1850. On Edith, from Menaul to Odelia are two cemeteries that date back to the mid 1800's. The four structures in the plan area listed on the State/National Registers are located on Edith between Odelia and Lomas. They are: F.M Mercantile built in 1938; the Santa Barbara School built between 1908 and 1930; the Catholic Church built in 1926; and, the Spanish Presbyterian Church built in 1922. The 1990 Sector Plan identified a Martineztown Plaza District that encompassed Edith from Marble to just north of the Catholic Church because of historic significance. The historic corridor should include the entire length of Edith Boulevard within the plan boundaries.

Plan and implement soil erosion control and landscaping on Odelia between Edith and High Street

Odelia provides the only east/west bicycle route in the plan area and a pedestrian route to Albuquerque High. Landscaping of the public right of way would improve the safety for bicyclist and pedestrians as well as preventing harm to an historic building.

All amenities such as street lights, benches, signage etc., should have a consistent theme that reflects the culture and history of the neighborhood

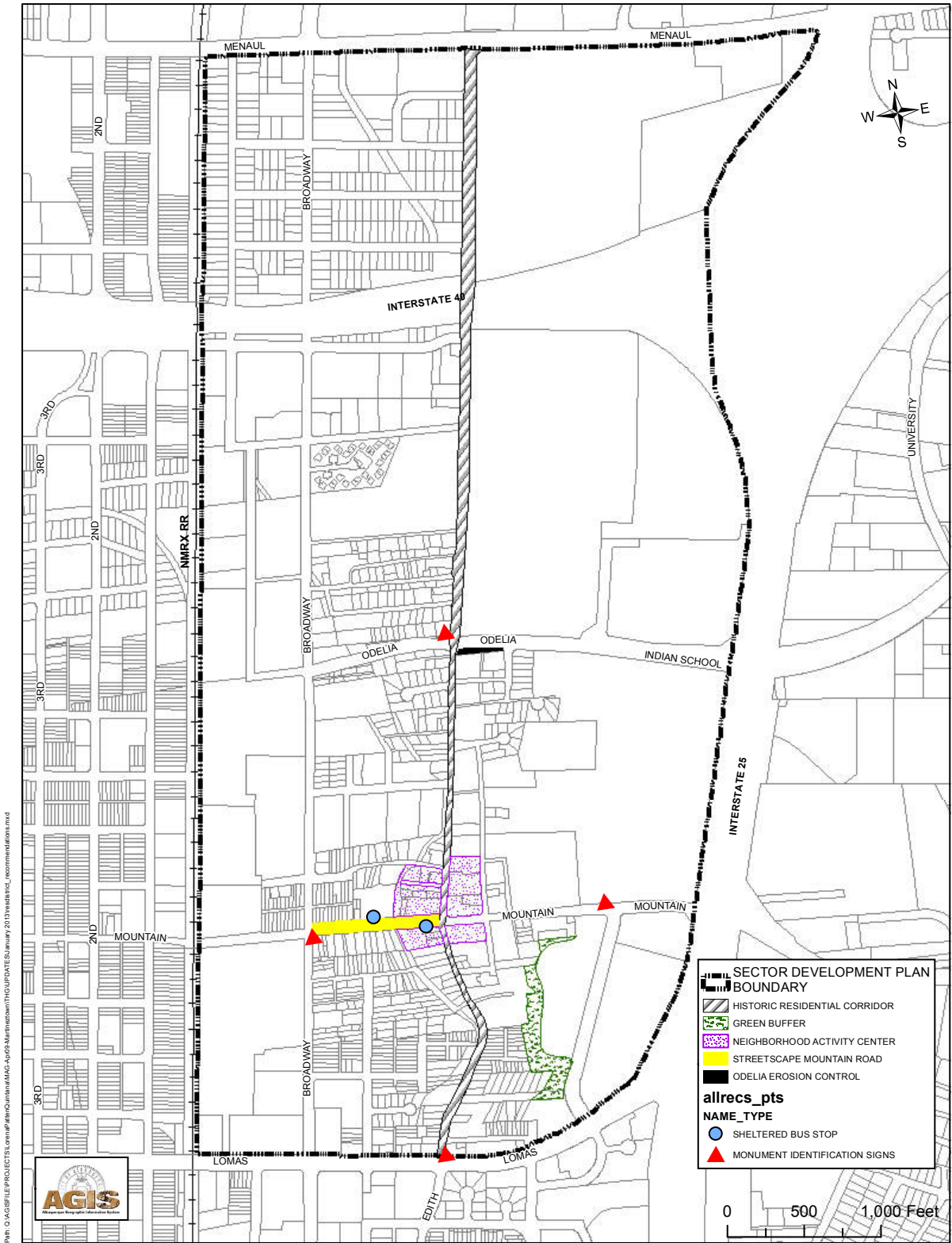
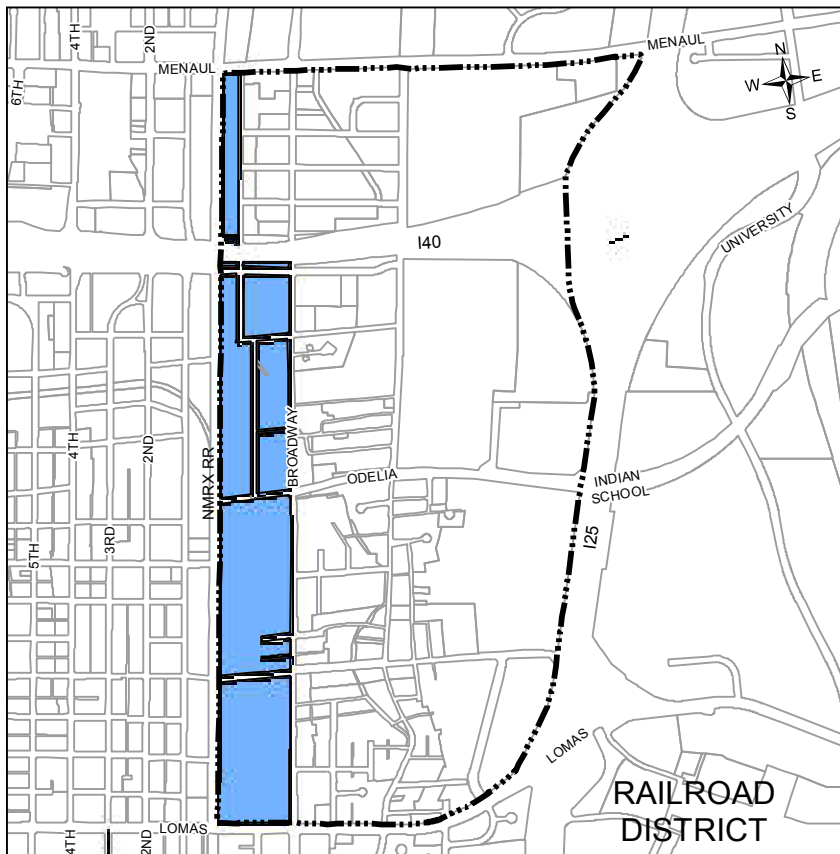


Figure 7 – Residential District Recommendations

B. The Railroad District



The Railroad District encompasses the large parcels between the railroad tracks on the west and Broadway Boulevard (south of I-40) and Commercial (north of I-40) on the east. It is bounded by Menaul Boulevard on the north and Lomas Boulevard on the south.

Most of the Railroad District is zoned SU-2 HM (MTSB Heavy Manufacturing), a special neighborhood zone which allows light manufacturing permissively and some heavy commercial conditionally. A portion near Interstate 40 is zoned SU-2 M-1 (MTSB Light Manufacturing), while a strip of parcels between I-40 and Menaul are zoned SU-2 C-3 (MTSB Heavy Commercial). The US Postal Service owns most of the property between Mountain and Lomas. Except for the public use of the post office, uses are primarily warehouse and industrial, truck terminals, and a few offices except for the northwest corner of Mountain and Broadway, which consists of retail, industrial/manufacturing and single-family residential.

Issues and Analysis

Economic Feasibility

Martineztown/Santa Barbara historically served as an employment center for the City of Albuquerque. Warehouse facilities and industrial operations have benefited from their proximity to the railroad and the interstate highway system.

Although the warehouses on these properties are generally outdated (clear heights range from 16 to 17 feet compared with 24 to 32 feet on newer structures), the price differential per square foot is large enough for businesses to lease or renovate the buildings.

Based on interviews with property owners and leasing agents, these uses will continue into the near future, and then perhaps be adapted for other uses. A film studio, for example, is leasing space in one of the facilities.

Conflicts with residential uses (Rosemont)

Within the Railroad District, near Mountain Road and Broadway Boulevard, there are a few single-family houses. Homeowners reported damaging heavy truck traffic and noise. They asked if Rosemont could be closed to trucks. The street cannot be closed as there is an existing easement for public ingress, egress and utilities that shall remain open for use at all times at the west end of Rosemont.

Landscaping and buffering of the temporary basin

Residents want the basin landscaped and buffered from the neighborhood. As a safety measure, wire fencing surrounding the area was installed. Because the pond was identified as

an emergency interim measure, no landscaping was included in the project. The pond slopes were rocked for stabilization and dust reduction. City maintenance may have to remove storm sediment therefore landscaping the bottom of the basin is not under consideration.

The City investigated costs for a landscape buffer around the pond. Estimates ranged from \$80,000 to \$150,000 for minimal landscaping. The greatest expense is due to irrigation requirements. Plants in the southwest must be irrigated (even if considered native and xeric) and the irrigation system (water connection, meter, piping, maintenance) is cost prohibitive.

Temporary detention basin lacks sufficient storage

In 2008, the City and the Albuquerque Metropolitan Flood Control Authority (AMAFCA) purchased approximately two acres on the southern portion of this district at the intersection of Broadway and Lomas. An emergency storm basin was built as a temporary facility to address immediate issues.

The interim basin along with the basin at Broadway and Odelia have a capacity of only 27 acre feet (16ac-ft at Broadway & Lomas and 11 ac-ft at Broadway & Odelia) and about 100 acre-feet of storage maybe needed to solve the drainage problem in Martineztown/Santa Barbara. The combined detention capacity of the two existing basins is approximately 27% of what may

be needed for flood relief. The City and AMAFCA are seeking additional sites to address the deficiency. This could mean one location for a larger basin or several locations for smaller basins. Because of side slopes and setback requirements, one large basin would require less land than smaller basins.

Tingley Park occupies about 12 acres or approximately 4 city blocks. Due to its shallow depth it is an inefficient drainage facility providing only about 2 acre-feet of storage per acre. In comparison the temporary basin occupies about 2 acres and provides approximately 8 acre-feet of storage per acre. Lomas & Broadway is in a FEMA flood zone because it is the low spot in the 600 acre drainage basin.

Incompatible zoning

Residents are concerned that if the Post Office relocates, the SU-2 HM zoning would allow uses that would have a negative impact on the neighborhood. The existing zoning may limit the development potential of property along the Enhanced Transit Corridor, Lomas Boulevard. There is a pump station in the district that falls under existing zoning however, the City zoning code permits public utilities in any zone provided it follows an adopted facilities plan and a site development plan for building permit has been approved by the Planning Commission.

Recommendations (See figure 8)

Modify zoning to reflect the traditional mixed use of Mountain and Broadway

The area just north of Mountain is a good location for mixed use development as it straddles Highway 47 (Broadway) with property in both the railroad and residential district. The 1990 sector plan identified the northwest corner of Broadway and Mountain Road as the AAA and Sons Grocery District, acknowledging its historical mixed use. This action would provide opportunity for affordable infill housing while protecting existing residential on Rosemont as well as existing commercial uses. The design would reflect historic architectural character providing a sense of place and a gateway feature to the residential core.

Add public amenities such as benches, shelters, and signage on Broadway

Street furniture and signs along Broadway would further define the border that separates the railroad and residential districts. Both sides of Broadway from Rosemont to Lomas are proposed mixed use zones and as such, provide an important link to the neighborhood pedestrian network. Directional signs are a practical and functional way to promote connectivity between districts and provide wayfinding to the neighborhood activity center and other points of interest.

During the writing of this Plan, ABQ RIDE proposed an evaluation of bus stop placement throughout the entire route system. The Indian School Road commuter bus (Route #6), serves riders from Tramway Boulevard to Downtown. There are currently 8 stops on Broadway between Indian School and Lomas, most are south of Rosemont. A sheltered bus stop is recommended to serve the proposed mixed use zone south of Mountain.

Provide temporary screening for the interim detention basin at Broadway and Lomas

A temporary landscape or manmade buffer would improve the aesthetics and pedestrian friendliness of both boulevards. When the interim basin is removed, the screening could be recycled and reused in another project.

Upon removal of the temporary detention pond, a redevelopment project is recommended to encourage higher density development along Lomas Boulevard

Lomas is classified as an Enhanced Transit Corridor with a goal to develop adjacent land uses and intensities that promote the use of transit. Commercial, mixed uses and residential development would serve the existing and adjacent neighborhoods and promote pedestrian opportunity. Higher density development along the Boulevard would further buffer the neighborhood, define its edges, and lessen adverse visual impacts between differing land uses. This type of development may reduce neighborhood noise levels and increase privacy while mitigating effects from dust and automobile fumes.

Change the zoning between Lomas and Mountain from Heavy Manufacturing to Mixed Use

This action would ensure that, should the Post Office relocate, manufacturing uses would not be allowed. A mixed use zone would encourage medium to high density residential, shopping, service, office, and entertainment uses along the Enhanced Transit Corridor, Lomas Boulevard. Broadway and Lomas Boulevards are prime locations for higher density mixed use street related development especially given the transit service on both Boulevards.

All amenities such as street lights, benches, signage etc., should have a consistent theme that reflect the culture and history of the neighborhood

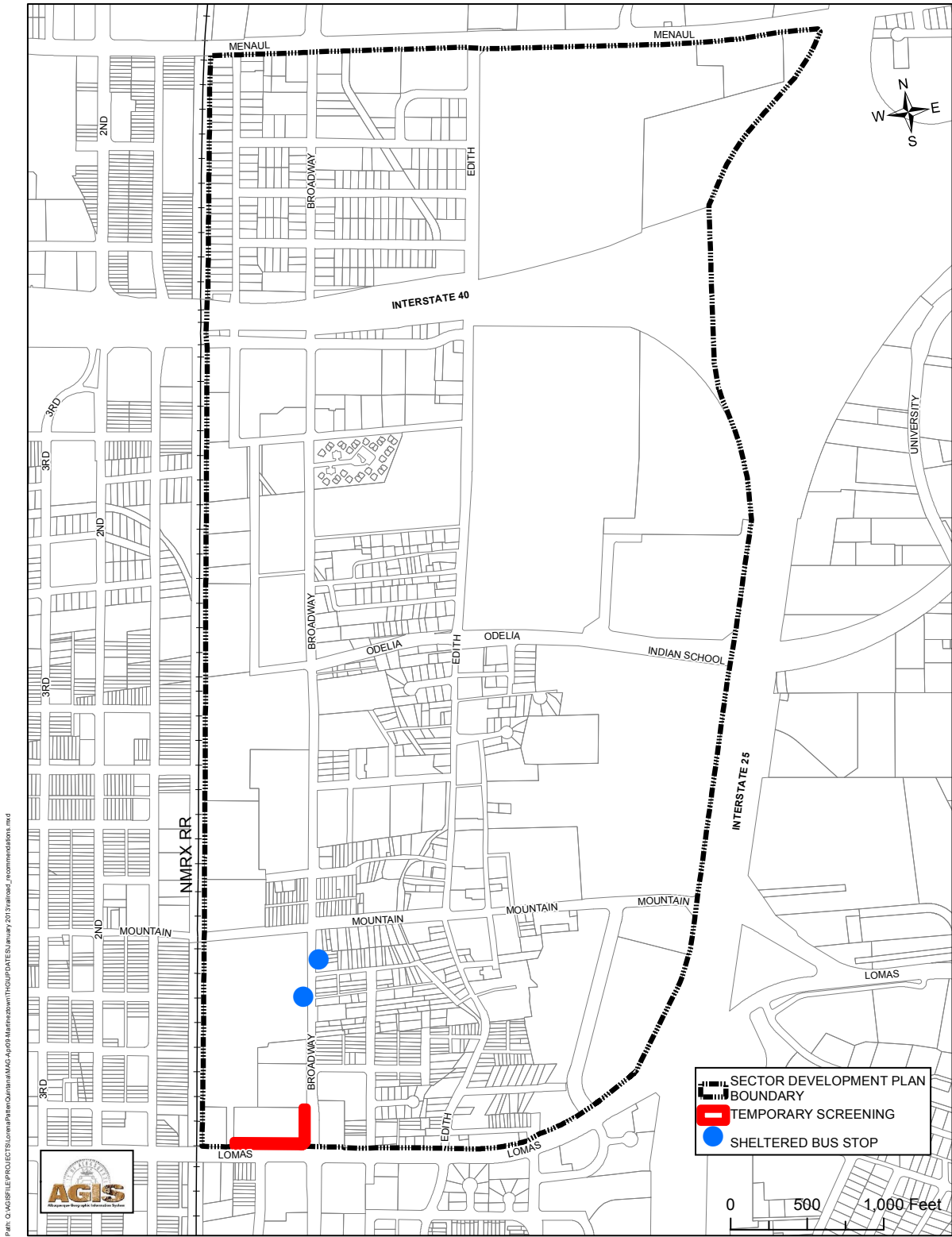
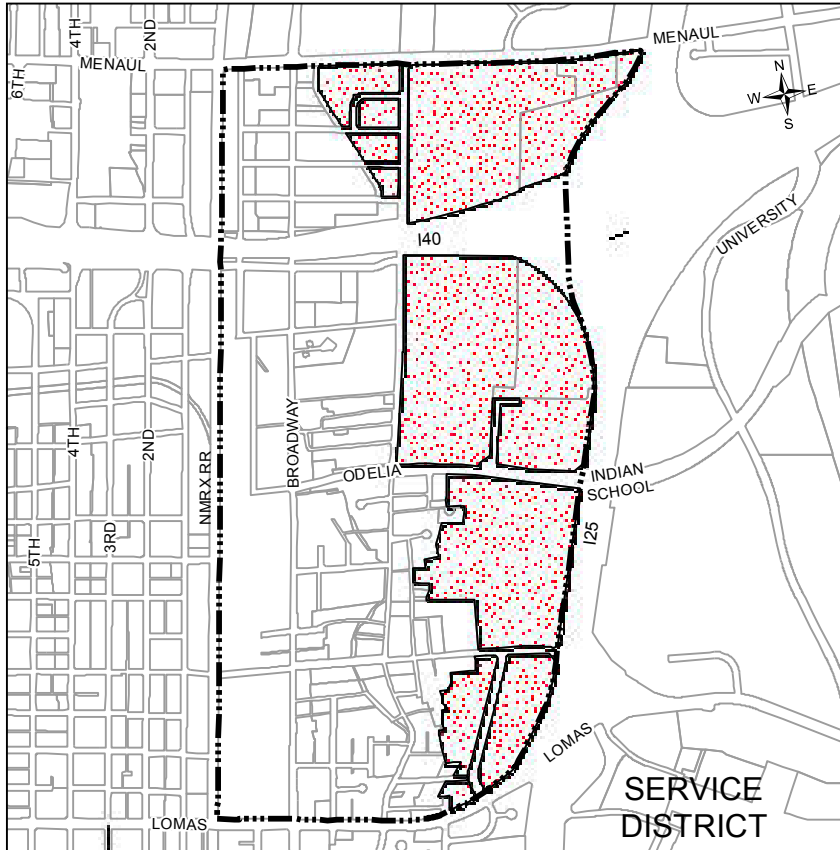


Figure 8 – Railroad District Recommendations

C. The Service District



The Service District boundaries abut the Residential District on the west and I-25 to the east. The northern boundary is Menaul and the southern boundary is Lomas. The southern part of this district consists of several large parcels with commercial establishments at the top of the old river embankment that serve to buffer the neighborhood from I-25: TriCore Reference Laboratory, Embassy Suites Hotel and the New Heart Wellness Center of New Mexico.

Albuquerque High School and the Career Enrichment Center, a science, technology and vocational APS magnet school, occupy most of the land between Mountain Road and Odelia/ Indian School. The land is owned by the Board of Education, is zoned SU-2 O-1 (Office), and permits schools. Vietnam Veteran's Park is owned by the Board of Education and the school district leases it for sports fields. The park also serves as a detention pond during storms. The City and its regulations have no jurisdiction over property owned by the Board of Education.

North of Indian School, the Archdiocese cemetery and Sunset Memorial Park lie adjacent to one another, offering visual open space and a perimeter walking route. This area also includes the Sun Village Apartments and the Moose Lodge Family Center.

Issues and Analysis

Flooding and Erosion

Rainwater run-off and resulting erosion is an issue in the Service District. Though the City has no jurisdiction over Albuquerque Public Schools, a site plan requires TriCore to vegetate their slopes.

Woodward and Lomas Intersection

There are concerns about the Woodward/Lomas intersection. The Embassy Suites and TriCore Laboratories have daily employee traffic as well as increased traffic during special events. Exiting vehicles travel south on Woodward then turn left onto Lomas enroute to the frontage road. There is no traffic light at Woodward and Lomas. This requires vehicles to enter the median halfway between the east/west traffic on a major transportation corridor and wait for the opportunity to enter the eastbound lane on Lomas. The lack of a signal and the heavy traffic at this intersection creates a hazardous condition.

Speeding traffic and unsafe conditions on Indian School/Odelia Road

Residents expressed safety concerns related to high speed traffic and pedestrian safety, particularly just north of Albuquerque High School. The Locust and Indian School/Odelia signalized intersection serves Sun Village Apartments and Veteran's Memorial Park to the north, and Albuquerque High School to the south. The pedestrian refuge east of the intersection ends mid block due to a left turn lane.

Recommendations (See figure 9)

Enforce the TriCore Reference Labs site development plan (approved October 2002).

Planting trees and grasses along the natural escarpment as required in the site development plan would help mediate long standing erosion issues.

Improve Traffic Operations for the Service District

Request a warrant study per the Federal Highway Administration's Manual on Uniform Traffic Control Devices for installation of a stoplight at the Woodward/Lomas intersection to improve safety and relieve traffic congestion.

Make streetscape improvements along Odelia to slow traffic and provide additional safety features, while maintaining the same traffic capacity

Install a raised median with low landscaping and decorative fencing on Odelia just east of Albuquerque High School to force pedestrians to cross at the traffic light in front of the high school. This amenity would signal drivers to slow down near the Albuquerque High School entrance.

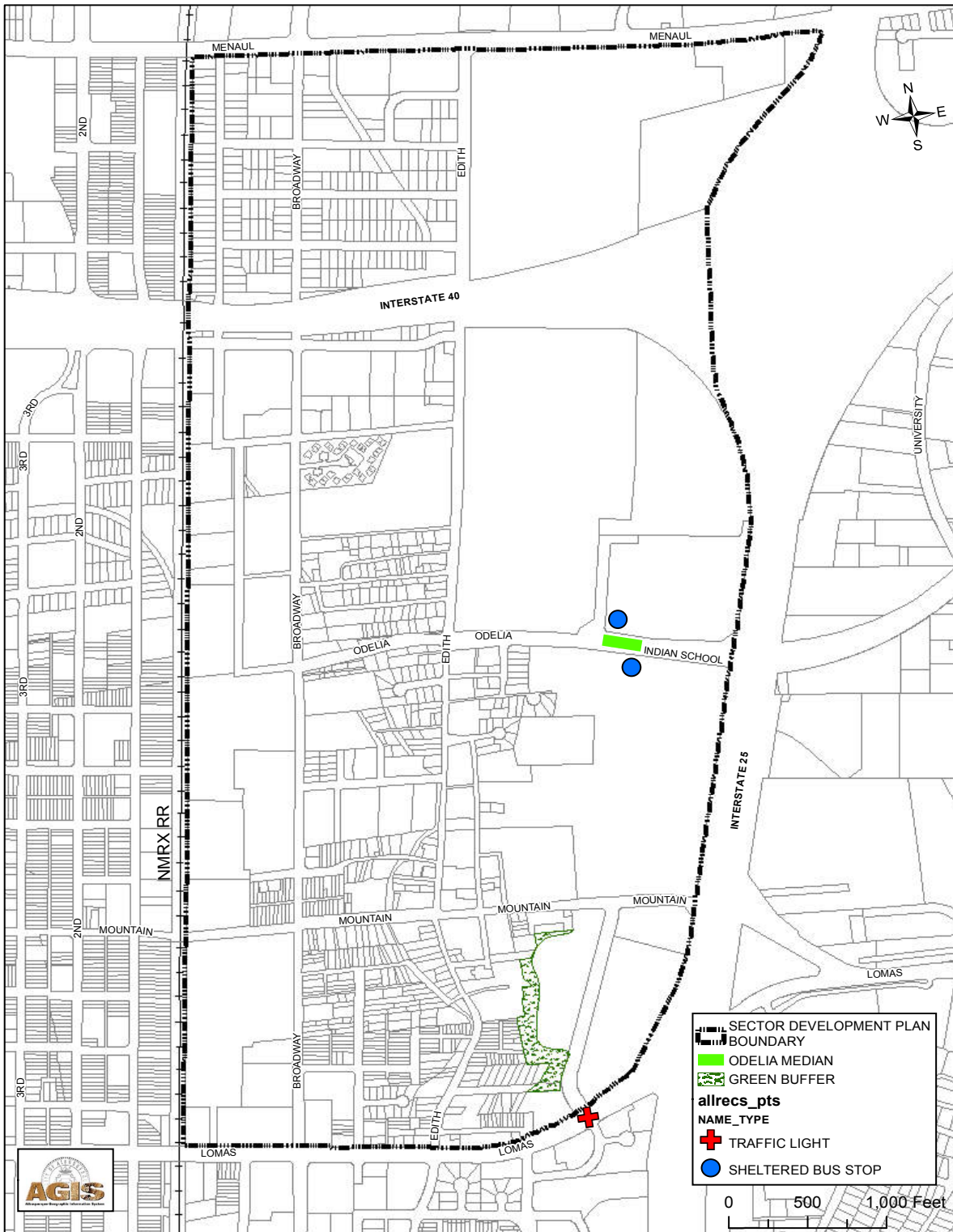


Figure 9 – Service District Recommendations

3 Transportation

A. Roadway System

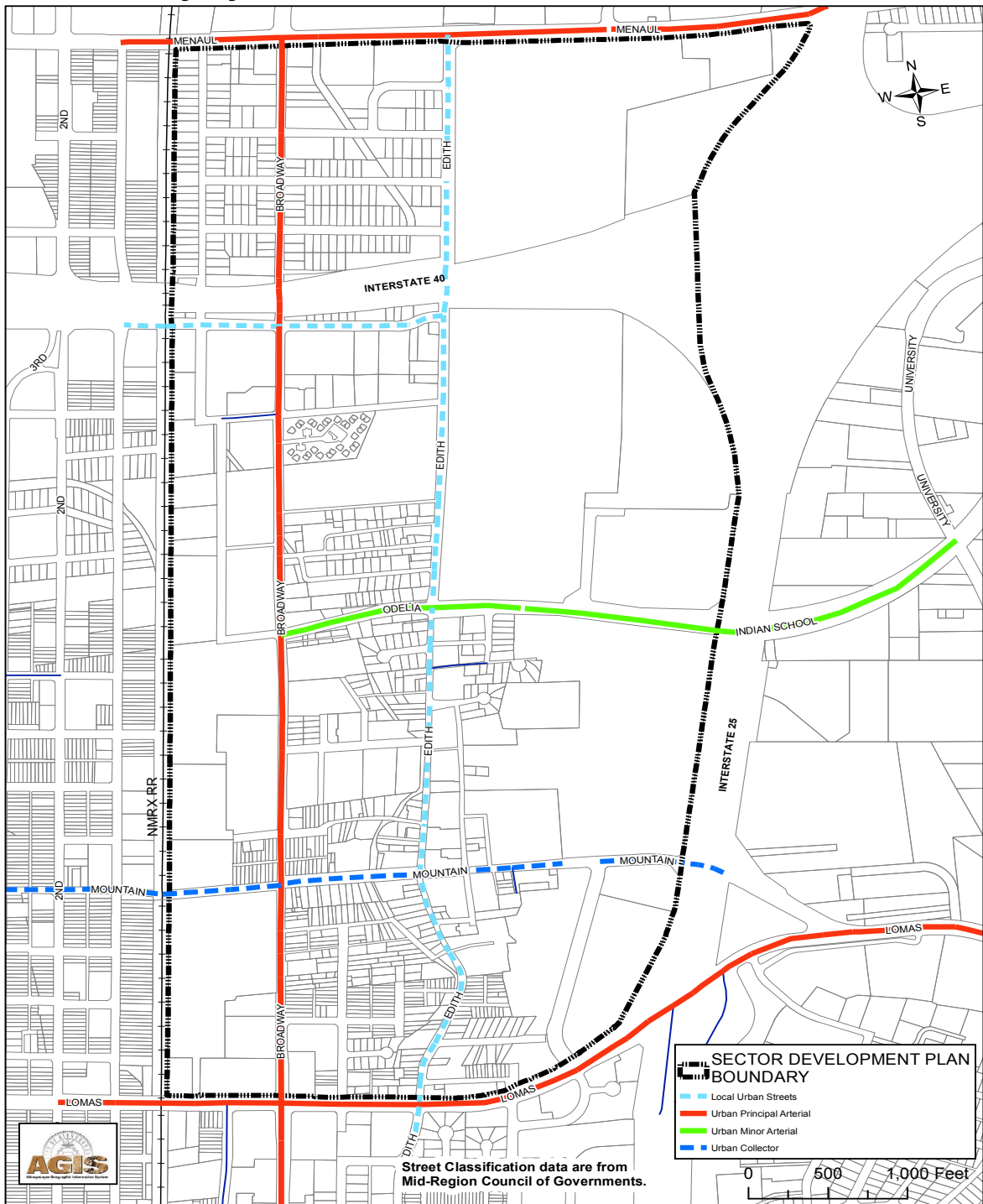


Figure 10 – Street Classifications

Lomas, Menaul and Broadway Boulevards are the areas principal arterial streets (see figure 10). Principal arterials are defined as major transportation corridors designed to carry high volumes of traffic. The Comprehensive Plan designates Menaul and Lomas as Enhanced Transit Corridors. The purpose of this designation is to develop land uses and intensities that promote the use of transit.

Odelia Road is a minor arterial street designed to accommodate heavy commercial vehicles and higher traffic volumes. Mountain Road is designated a collector street. A collector street provides land access, service, and traffic circulation within residential, commercial and industrial areas.

North of the plan area boundaries, Edith Boulevard is a minor arterial. Between Menaul and Lomas however, Edith is a local street. By definition, these streets provide the lowest level of mobility by accessing adjacent land use, serving local trip purposes, and connecting to higher order streets. This classification typically contains no bus routes and non-local traffic is most often deliberately discouraged.

A University of New Mexico Transportation Strategic Plan may impact Martineztown/Santa Barbara. The June 2009 plan recommends that Mountain Road be extended from the I-25 East Frontage Road to provide service to the proposed new hospital and expansion of the Health Sciences Center. The 2009 draft recognizes that the extension of Mountain Road has caused concern among the neighborhoods west of I-25 and south of Mountain Road that there will be more cut through traffic. The Traffic Impact Study assumes that the majority of the increase in vehicles from west of I-25 will use Broadway Blvd. to reach downtown Albuquerque and that less than 10% of the traffic will use neighborhood streets. Some of this increase on Edith Blvd. will be neighborhood traffic going to the new facilities. Speed humps installed on Edith Blvd. south of Mountain Road will continue to prevent cut through traffic. Other traffic calming measures could be added to the neighborhood streets if cut through traffic becomes a problem.

Resolution 09-326 was adopted September 9, 2009 restricting gross vehicle weight on Mountain Road between Broadway Boulevard and the I-25 Frontage Road to prohibit vehicles with a gross weight of 5 tons or more from using Mountain Road as a through street. Speed limit on this segment of Mountain Road was reduced to 25 miles per hour. October 2009 signs were added to support this resolution.

Issues and Analysis

Freeway Access

Direct access from eastbound Mountain Road was eliminated when the junction of I-25 and I-40 was reconstructed.

While traffic on the interstate highways grew substantially from 1987 to 2006 (53 percent on I-40 and 55 percent on I-25), traffic declined on major streets through Martineztown/Santa Barbara.

Traffic decreased by 27 percent on Mountain Road and 26 percent on Odelia. The exceptions were two road segments of Broadway: Lomas to Mountain (up 7 percent) and Mountain to Odelia (up 29 percent). An historical table of traffic counts provided by the Mid-Region Council of Governments can be found in the Appendix.

Mountain Road offers access, via the frontage road, off I-25 from the North. This access is of concern to the neighborhood because of heavy trucks exiting I-25 onto Mountain. The residents' reported safety issues stemming from heavy truck weights and high speeds, therefore the planning process included a traffic analysis of Mountain Road.

Truck Traffic

The residents expressed concerns about heavy truck traffic and high traffic speeds within the residential district. The "Big-I" improvements impacted traffic patterns, encouraging the use of the Mountain Road exit. The high speed of traffic, especially on Mountain, Odelia and Edith created safety issues for the neighborhood. Residents expressed additional concern regarding high truck traffic speeds on Broadway. Those living in homes on Broadway must back out onto the road to exit their driveways. Residents also report that the weight of heavier commercial trucks contribute to the deterioration of older homes lining those streets.

Semi-trucks traveling west on Mountain from I-25 have difficulty turning onto Broadway. The configuration of the intersection does not allow adequate turning radii as evidenced by building damage on the northeast corner of the intersection.

City Council Resolution 09-326 addressed most of the truck traffic concerns voiced by the neighborhood.

Traffic Crashes

Sites Southwest provided the following crash data.

Traffic crashes in the neighborhood have generally decreased since 2001 at major intersections, including Broadway/Odelia, Broadway/ Indian School, Broadway/Menaul, Lomas/Edith and

Menaul/Edith. The exceptions are Broadway/Lomas, where crashes ranged from three in 2001 to 26 in 2006 (average of 15 a year since 2000) and Broadway/Mountain, which averaged 18 crashes a year since 2000.

There were three crashes involving bicyclists during this time period: two at Broadway/Lomas in 2001 and 2004, and one at Broadway/Mountain in 2004. Similarly, three accidents involved pedestrians: one at Lomas/Edith in 2002, one at Broadway/Mountain in 2005 and one at Broadway/Lomas in 2006. There was one fatality, at Broadway/Odelia in 2001.

Woodward and Lomas Intersection

There are concerns about the intersection at Woodward and Lomas. The Embassy Suites and TriCore Laboratories have daily employee traffic as well as increased traffic during special

events. Exiting vehicles travel south on Woodward then turn left onto Lomas enroute to the frontage road. Because there is no traffic light at Woodward and Lomas, vehicles must enter the median halfway on a major transportation corridor and wait for the opportunity to enter the eastbound lane on Lomas. The lack of a signal and the heavy traffic at this intersection creates a hazardous condition.

Mountain Road Traffic Study Conclusions

Engineering sub-consultants for this sector plan conducted several studies to investigate complaints, with a particular emphasis on trucks. They performed a 24-hour traffic count on Mountain Road (prior to Resolution 09-326, an origin-destination study, and a signal and queue analysis of the Broadway and Mountain intersection, before and after proposed improvements.

Data showed that some 1,860 trucks including pick-ups, delivery trucks, and semi-truck trailers—entered Mountain Road daily from the I-25 frontage road and head west toward Broadway Boulevard. The largest percentage were light trucks (80%) followed by delivery trucks (13%), and semis (7%). There was a net decrease of about 300 trucks just east of Broadway, indicating that the rest had cut-through the neighborhood to work, home, or to other routes such as Woodward or Lomas. Approximately 990 trucks headed east on Mountain Road after the Broadway intersection. Percentages of light trucks and delivery trucks were slightly larger in this group, and semis slightly less, than in the westbound traffic group. The total count dropped off slightly to 968 after Edith Boulevard and then increased to 1,131 again between Woodward and I-25, indicating that traffic was moving onto Mountain to reach the freeway frontage road. (Traffic counts just west of Woodward dropped so severely that the data from that location is considered flawed and is not being used.)

The counts indicate that the truck traffic was dispersed throughout the day with apparent concentrations during the morning rush hour and mid-afternoon. More than 95 percent of the vehicles using Mountain Road at these times were passenger cars or pick-up trucks. Delivery trucks comprised 1.4 to 2 percent and semi-tractor trailers 0.23 to 0.45 percent of the traffic. Focusing on specific types of vehicles indicated that approximately two to four delivery trucks an hour travel in each direction while two semis an hour use Mountain Road westbound.

Narrow Residential Streets

Edith Boulevard south of Odelia is too narrow to carry high traffic volumes or heavy vehicles. In fact, a number of streets in the neighborhood are very narrow, virtually squeezed in between houses. For example, Martinez Drive, Maggies Lane and Sprunk Road have paving widths of only 12 feet; Granite, Gomez and Cordero have paving widths of only 20 feet. In contrast, the standard width of Albuquerque's residential streets is 32 feet. Many narrow residential streets have alley-type paving with center drainage, since narrow right-of-way widths cannot accommodate curb and gutter. These narrow, winding streets can handle only minimal traffic and cannot accommodate additional outside traffic without severely impacting nearby residences.

Posted signs on Edith are faded and illegible

Recommendations (See figure 11)

Create a consistent width of Mountain Road/Streetscape from Broadway to I-25 (Residential District)

Narrowing the portion of Mountain Road between Edith and Broadway would help calm vehicular traffic. The addition of on street parking and bulb outs with landscaping on the north side of Mountain would narrow the existing lane width without reducing the number of lanes and add needed parking. Streetscape amenities would be designed to reflect the historic Carnuel Trail.

Design a Plazuela (small plaza) with visual art to commemorate the crossroads of the Carnuel Trail and the Camino del Lado (Residential District)

The plazuela would provide an open gathering space for the community, increase pedestrian activity and serve as a visual cue to calm vehicular traffic.

Improve Traffic Operations for the Service District

Request an engineering study of the sector plan area to examine pedestrian access needs, roadway geometry deficiencies, bikeway needs, and traffic calming opportunities. Request a warrant study per the Federal Highway Administration's Manual on Uniform Traffic Control Devices for installation of a traffic light at the Woodward/Lomas intersection to improve safety and relieve traffic congestion.

Make streetscape improvements along Odelia to slow traffic and provide more pedestrian

These improvements can include installation of medians with decorative fencing to improve safety and force pedestrians to cross at the traffic light in front of the high school. This measure could signal drivers to slow down near the Albuquerque High School entrance. These and other improvements can be evaluated in the transportation engineering study for the plan area.

Post traffic signs to calm traffic (Residential District)

Along Mountain and along Edith, post new and replace faded signs reflecting the speed and weight limits.

Place monument identification signs at key entrances to the neighborhood

This action would help to preserve and protect the unique identity of the historic residential core by alerting motorists that they are entering a low-density residential neighborhood. This measure is intended to calm traffic to increase safety of residents. Signs are recommended for southbound traffic at Odelia and Edith, northbound traffic at Lomas and Edith, eastbound traffic at Woodward and Mountain, and westbound traffic at Broadway and Mountain. Signs should be designed to complement community character.

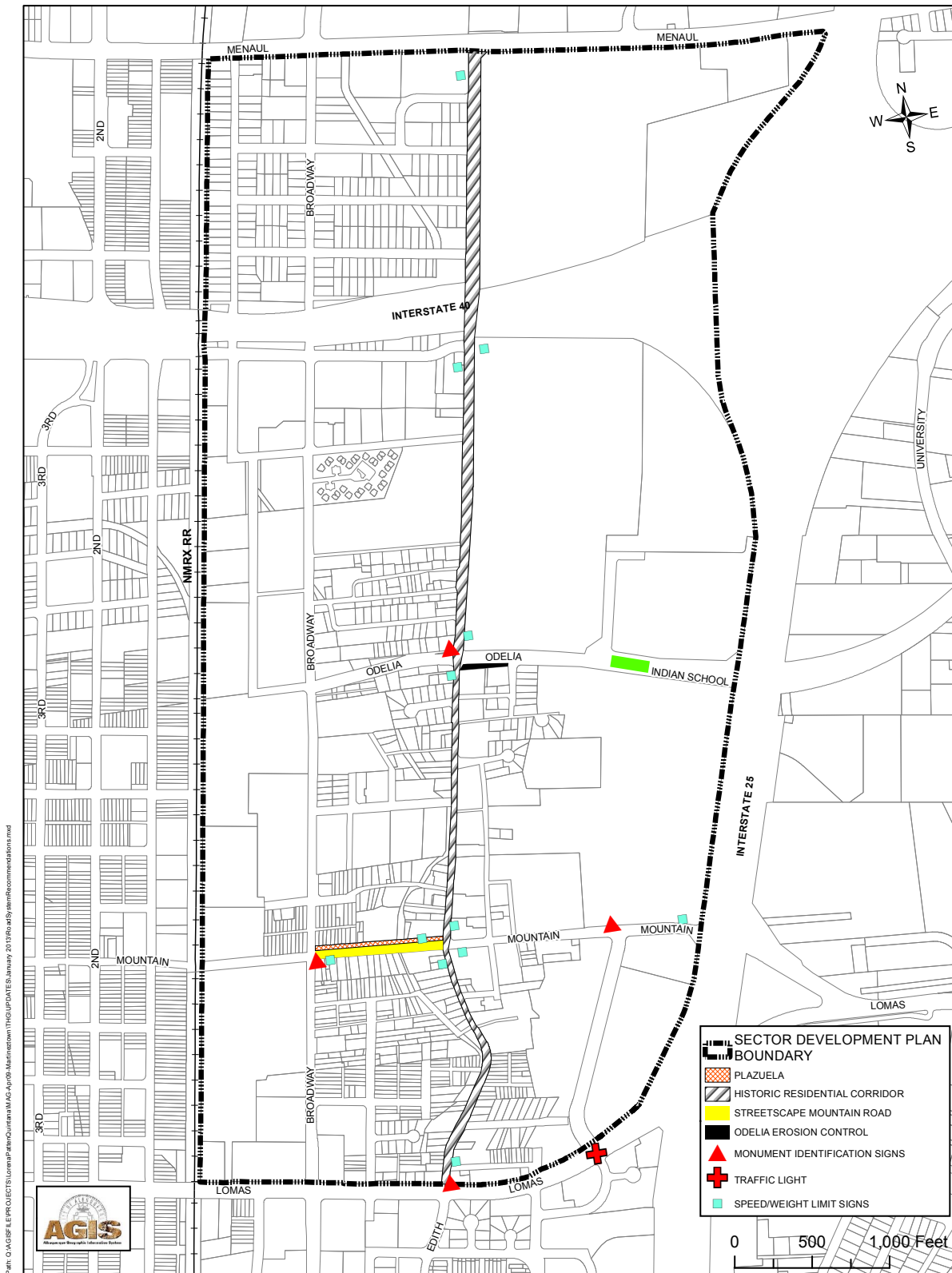


Figure 11 – Roadway System Recommendations

B. Bikeways

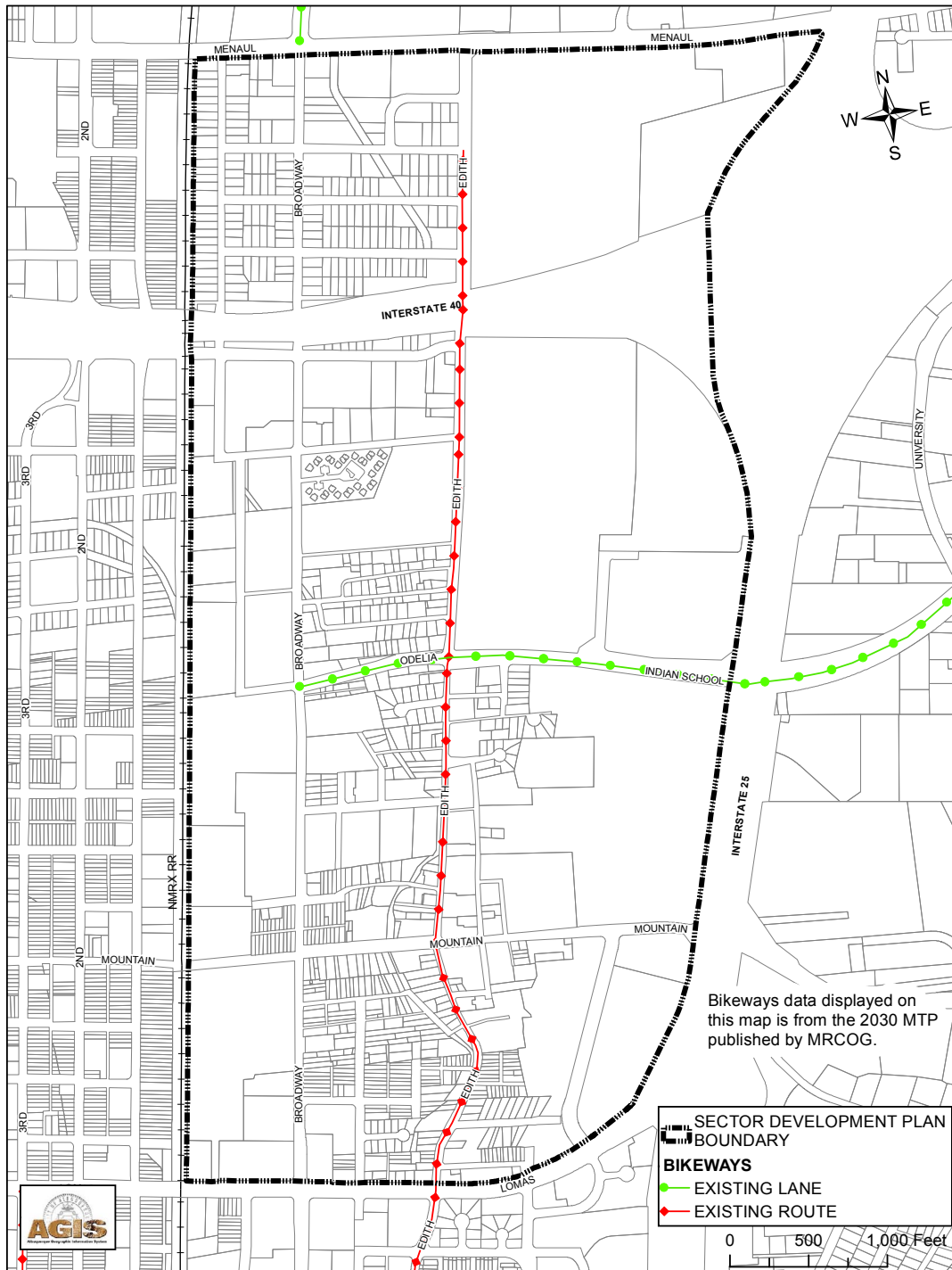


Figure 12 – Existing Bikeways

According to the Long Range Bikeway System Map, Edith Boulevard is a designated bicycle route. A route is defined as a bicycle facility located in a roadway and designated by signs as available for bicycle travel. Routes are shared with motorists and require land widths of 14 feet. No special on-pavement markings are provided.

Odelia/Indian School is designated a bicycle lane from Broadway to Tramway. A bicycle lane is a facility that is located in a portion of a roadway facility. A lane is designated by pavement markings for the exclusive or semi-exclusive use of bicycles. Travel by motor vehicles or pedestrians is prohibited, but pedestrian and motorist cross flows are permitted. Lanes are usually along the right edge of the roadway but may be designated to the left of parking or right-turn lanes.

Lomas Boulevard is a bikeway corridor. A corridor is an area where bikeway facilities are being considered but the feasibility of a facility or the specific type of facility has not yet been determined. These facilities are anticipated to be eligible for Federal transportation funding.

According to the 2030 Metropolitan Transportation Plan, the Bicycle Transportation Demand Management Program funds are used to conduct studies of arterial and collector corridors that are candidates for inclusion of bicycle facilities. The studies are to determine the potential impacts to existing traffic. The studies determine if existing street sections can accommodate bicycle lanes or if a reduction of vehicle lanes is possible while maintaining a reasonable level of service. A study of the University Boulevard corridor (a six lane section) between Lomas Boulevard and Coal Avenue is a recent example of this approach.

The City's proposed additions to the bicycle network within the plan boundary are as follows.

- Establishment of a bicycle trail adjacent to I-25 to connect the Odelia/Indian School bicycle lane to the existing segment of the I-40 Trail west of 6th street. A bicycle trail is a paved off-street facility designated by signs and pavement markings for the primary use of bicycles. Cross-flows by motor vehicles are minimized. Trails may accommodate pedestrian or other non-motorized users.
- Designate Mountain Road as a bicycle lane to connect a proposed 2nd street lane to the proposed bicycle trail adjacent to I-25
- Extend the Odelia/Indian School bicycle lane along Baca across the railroad to connect to 2nd Street (ROW needed)
- McKnight is a proposed bicycle route that would connect Edith's bicycle route to proposed routes on 1st street and another on Haines
- Just north of McKnight a bicycle lane is proposed on Indian School, adjacent to I-40 that would connect Edith to at least 1st street

The Embassy Suites hotel hosts two annual cycling events (Day of Tread & Albuquerque Century) that attract over 2000 participants. Routing for these events is westbound on Mountain directly to the Bosque Trail). Due to the number of transit stops and proximity to downtown Albuquerque, Martineztown/Santa Barbara is an ideal location for the promotion of intermodal bicycle-transit trips therefore, increase the effectiveness of both transportation modes.

Issues and Analysis

Need for additional bikeways

There are only two existing bikeways in the plan area, one north/south and one east/west. Residents and commuters expressed a need for additional facilities, as well as more connectivity with the Albuquerque bicycle network.

A north/south bicycle lane on Broadway was requested by the community however, there are safety issues due to the high crash rate at the Broadway and Mountain intersection, as well as heavy automobile traffic on Broadway. The City's proposed bike lane on 2nd street is only 4 blocks west of Broadway and runs north to south from Claremont to Lomas. The 2nd street proposal seems to be a more appropriate and visually appealing location for a facility.

The need for additional east/west facilities would be satisfied by the proposals for Mountain Road and Odelia/Indian School Road. A bicycle lane on Mountain Road would allow bicyclists to travel from the Rio Grande to I-25. Extending the Odelia/Indian School lane would allow travel from 2nd street to San Pedro.

Other proposed additions to the network under consideration would provide the neighborhood with the connectivity they desire. Bicycle commuters working in the downtown and connecting neighborhoods could easily take advantage of intermodal transit opportunities especially along Mountain Road and Odelia/Indian School Road.

Trip barriers

According to the Albuquerque Comprehensive On-Street Bicycle Plan, there are trip barriers to overcome before bicycle usage can be increased. Trip barriers that exist in Martineztown/Santa Barbara include the perception of safety, actual safety problems associated with traffic as well as a physical barrier - the Burlington Northern Santa Fe Railway track.

Perception of safety can be improved by designing road conditions which make cycling an appealing option. Calming traffic and streetscaping along Mountain Road and Odelia/Indian School Road would increase safety. Because Edith is a narrow local street, its utility as a bicycle route would be enhanced should non-local motorized traffic be discouraged. Though segregated lanes provide a better sense of safety, the reduction of Mountain Road speed limit to 25 miles per hour may validate shared transportation lanes.

Safety problems can also be addressed by reducing traffic speeds through enforcement of limits and signage. The bicycle network within the plan area could be designed to prevent bicyclists from traveling with high speed or high volume motor traffic. Junctions should be designed with the bicyclist in mind. Additional street lighting should be considered for cycling safety.

Both the proposed lane on Mountain and the proposed route on McKnight would require the acquisition of right of way to cross the railroad tracks. The 1990 Sector Development Plan recommended studying the possibility of extending Odelia/Indian School west across the railroad

tracks. The railroad, however, has been very reluctant to approve any more at-grade crossings due to safety risks.

Recommendations (See figure 13)

Implement the City's proposed improvements throughout the plan area

Priority should be given to establishment of a bicycle trail adjacent to I-25.

Create an Historic Residential Corridor along Edith Boulevard (Residential District)

This action would enhance the existing bicycle route by creating a user friendly environment with historic significance. Edith is the only north/south bikeway facility in the plan area. The Edith bicycle route runs from Prospect (just south of Menaul) to Gibson Boulevard with connections to many other bikeway facilities, both existing and proposed. This is an important cultural corridor that should emphasize the importance of Edith and its history as the Camino del Lado.

Discourage non-local motorized traffic on Edith Boulevard

Add signs that alert drivers Edith is for local traffic only. Implement traffic calming to make it unappealing for non-local traffic. This measure would further enhance Edith's utility as a bicycle route.

Add lighting to improve safety

Add lighting under I-40 overpass on Edith Boulevard and at the Santa Barbara/Martineztown Park, located at Hannett and Edith. Install additional streetlights on the northern portion of Edith Boulevard.

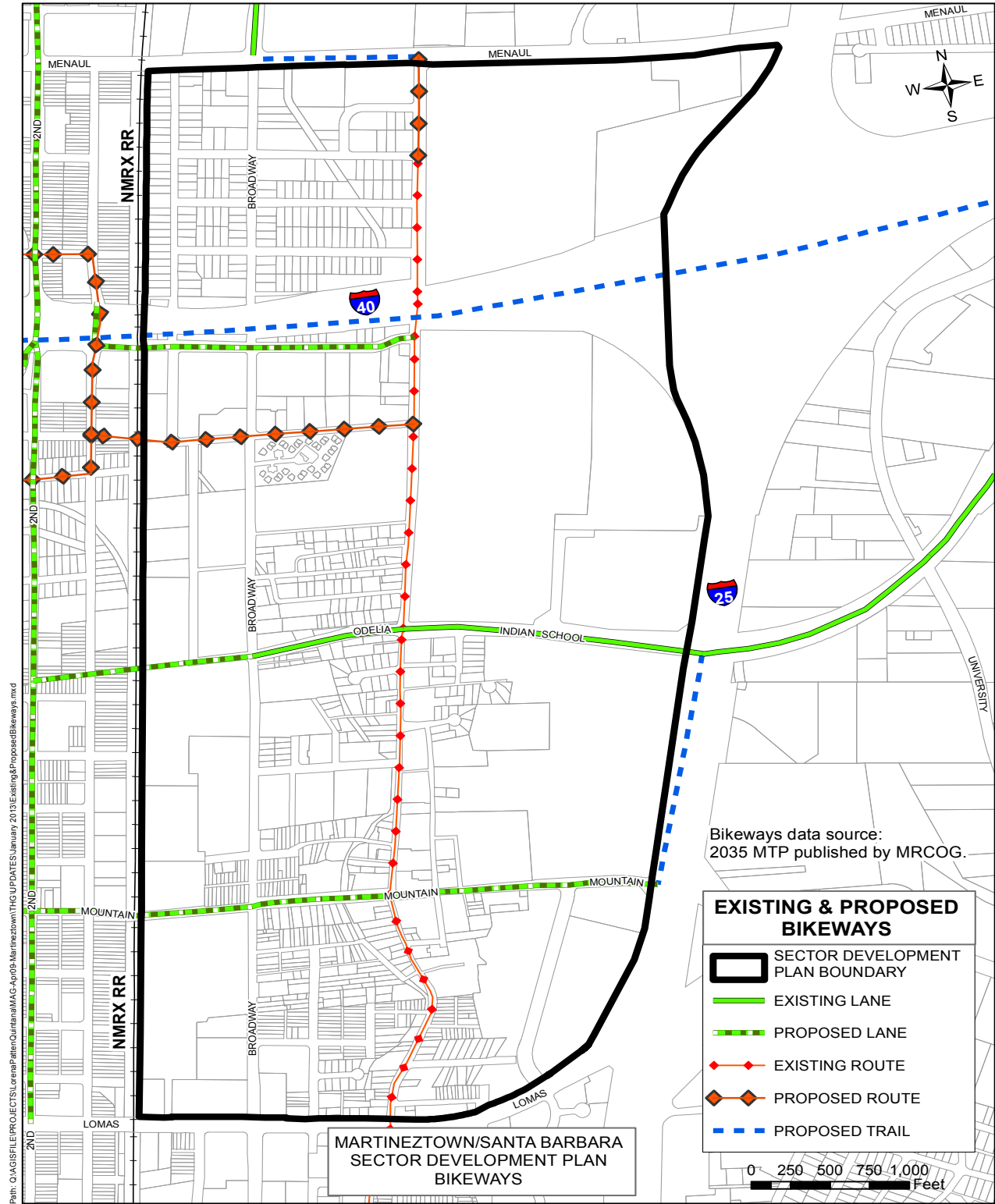


Figure 13 – Existing and Proposed Bikeways (I-40 Trail Corridor)

C. Pedestrian Circulation

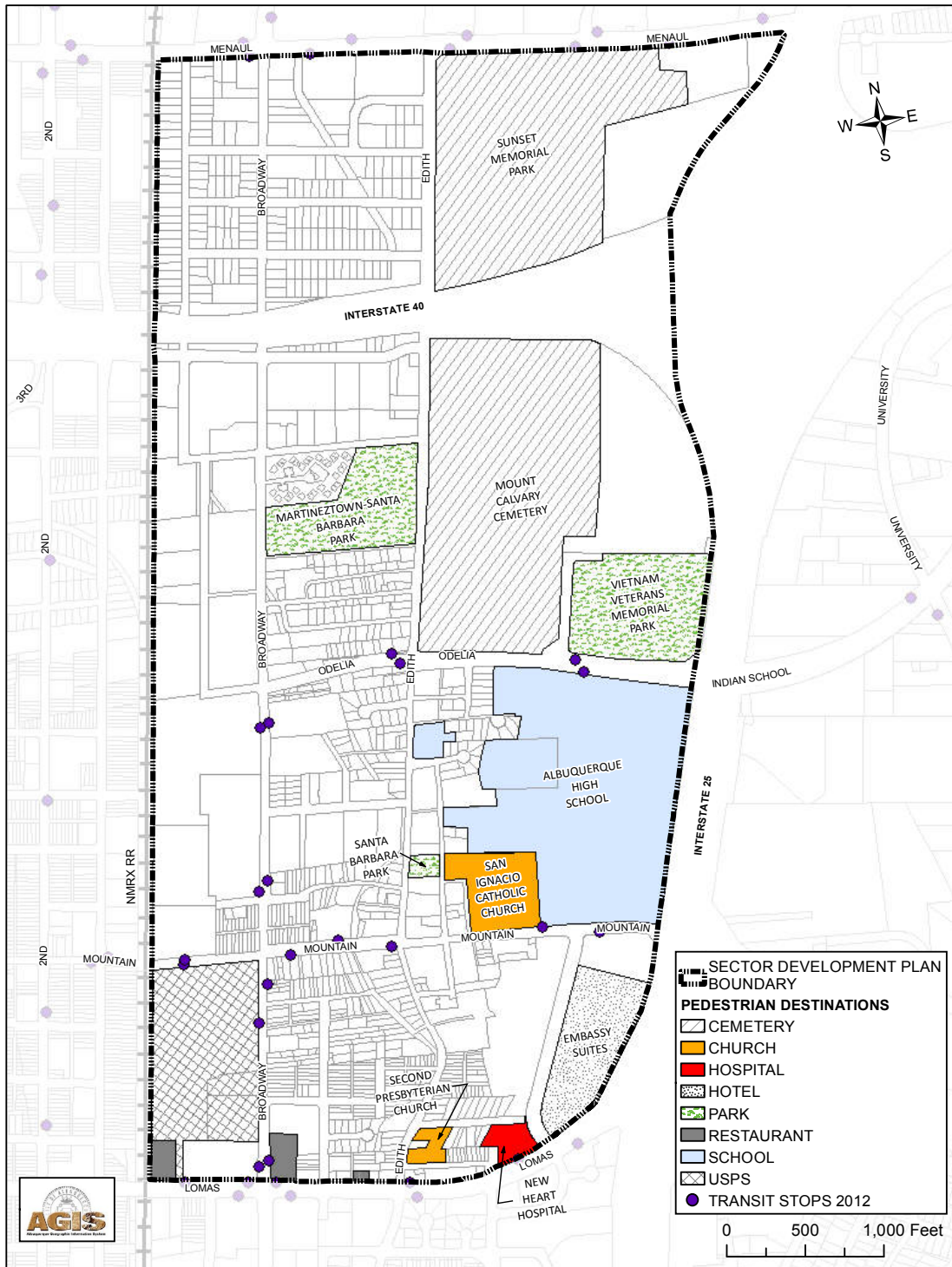


Figure 14 – Existing Pedestrian Destinations and Transit Stops

Martineztown/Santa Barbara has many pedestrian destinations within its boundaries. There are churches, schools, parks, the post office, restaurants and coffee shops in close proximity within the plan area. See figure 14.

Accessibility to transit is an important consideration when planning pedestrian networks. Lomas, Menaul and Broadway were designed to carry high volumes of traffic. Menaul and Lomas have additional designations as Enhanced Transit Corridors. The purpose of this designation is to develop land uses and intensities that promote the use of transit. Between the railroad tracks and I-25, Menaul and Lomas have eight bus stops each. Broadway from Menaul to Lomas also has eight stops. Other transit routes include Mountain Road with seven stops and Odelia/Indian School with five stops.

The 2030 Metropolitan Transportation Plan considers Martineztown/Santa Barbara a primary target area for programming federal and state resources. The plan states that the potential for pedestrian activity is present and can be enhanced with improvements to address deterrents such as crime, pedestrian crash rates, speed, and street connectivity.

The “Martineztown Stampede for Health Project” is a community health initiative led by the Santa Barbara Martineztown Neighborhood Association, the Citizens Information Committee of Martineztown Neighborhood Association, and the Medical Director of New Heart Center for Wellness. The proposed program includes almost six hundred healthy lifestyle events over the first year of implementation. Approximately five hundred of those events are neighborhood walks.

With new zoning that reflects the traditional mixed use areas within Santa Barbara/Martineztown and the development of a crossroads neighborhood activity center, pedestrian networking will become even more important to this community.

Issues and Analysis

Sidewalk infrastructure

Sidewalk infrastructure is a key component of urban design that supports walking. It separates pedestrians from vehicular traffic and contributes significantly to creating a pedestrian friendly environment.

Sidewalks and walkways are “pedestrian lanes” that provide people with space to travel within the public right-of-way that is separated from roadway vehicles. They also provide places for children to walk, run, skate, ride bikes, and play. Sidewalks are associated with significant reductions in pedestrian collisions with motor vehicles. Such facilities also improve mobility for pedestrians and provide access for all types of pedestrian travel: to and from home, work, parks, schools, shopping areas, transit stops, etc. Walkways should be part of every new and renovated facility and every effort should be made to retrofit streets that currently do not have sidewalks.

Both FHWA and the Institute of Transportation Engineers (ITE) recommend a minimum width of 5 ft for a sidewalk or walkway, which allows two people to pass comfortably or to walk side-by-side. Sidewalks should be fully accessible to all pedestrians, including those in wheelchairs.

Lack of sidewalks and poor sidewalk condition are problems within Martineztown/Santa Barbara. Key pedestrian routes such as Mountain Road, Edith and Odelia/Indian School have sidewalk sections less than the recommended width, as well as sections that are inaccessible to wheelchairs. Despite narrow widths and physical deterrents, residents prefer walking these streets to Broadway, due to traffic, lack of buffering, and adequate street lighting.

There is an issue of available right of way for sidewalk expansion throughout Martineztown/Santa Barbara.

Residents requested the addition of curbs and gutters. The City is concerned the addition may trap stormwater runoff on private property and prevent it from draining into the public right of way.

Street Crossings

Ensuring that people can cross streets safely and conveniently to access destinations is essential to creating an effective transportation network. There are several roadway intersections in Martineztown/Santa Barbara that are in need of safe crossings.

Statistics reveal that, within the plan boundaries, there were six pedestrian accidents and one fatality reported between 2000 and 2006 involving bicycles and pedestrians. These accidents occurred at: Broadway and Lomas; Broadway and Mountain; Broadway and Indian School; and, Lomas and Edith.

Residents expressed additional concern about the safety of crossing Odelia/Indian School from the Veteran's Park to Albuquerque High School. The intersection of Odelia/Indian School and Edith was also listed as a priority safety concern.

Remedies to improve pedestrian crossings include incorporation of design features, such as raised medians or traffic signal timing that helps create sufficient gaps in traffic as well as the installation of actual pedestrian crossings such as traffic signal pedestrian activation devices, marked crosswalks, and textured pavement.

Street lighting

Well lit streets and sidewalks enhance people's sense of security. Pedestrian lighting encourages walking and bicycling trips, reducing automobile congestion and emissions. A lack of street lights in Martineztown/Santa Barbara, particularly on Edith Blvd. north of Odelia, further discourages evening walking and likely contributes to the presence of crime.

The freeway underpasses at both Broadway and at Edith are insufficiently lit and according to the neighborhood, the area has a history of assaults. These underpasses are used by high school students as routes to the Albuquerque High School from the north.

A study in the Safe Routes to School Program found that “simply increasing the intensity of street lighting reduced the number of vehicle-pedestrian crashes by 59 percent”.

Buffering and street furniture

A buffer zone of 4 to 6 ft is desirable and should be provided to separate pedestrians from the street. The buffer zone will vary according to the street type. Landscaping and street furniture such as benches, streetlamps, and signage are commonly used to soften or mitigate the effects of vehicular traffic on pedestrians. While landscaped buffers are preferred, parked cars and/or bicycle lanes can also provide an acceptable buffer zone.

Mixed uses

Encouraging higher density and mixed uses ensures that the streets are not vacant after 5 p.m. on weekdays. It would help create a more comfortable and inviting environment allowing people to be out on the street all day and on weekends, thereby producing a safe environment through informal surveillance, (eyes on the street).

High speed traffic on residential streets

Speed limit enforcement is a simple, effective and easily implemented safety improvement. This can be done with the use of speed cameras or through increasing police presence and traffic citations

Infrastructure improvement projects focused on traffic calming are another means of lowering vehicle speeds. Some traffic calming features, such as road humps and rumble areas, can reduce vehicle speeds and also encourage drivers to divert to less residential streets, which reduces traffic volume.

Recommendations (See figure 15)

Create a consistent width of Mountain Road/Streetscape from Broadway to I-25 (Residential District)

Narrowing the portion of Mountain Road between Edith and Broadway could help to calm vehicular traffic. The addition of on street parking and bulb outs with landscaping on the north side of Mountain would narrow the existing lane width without reducing the number of lanes and add needed parking. Streetscape amenities would be designed to reflect the historic Carnuel Trail.

Install sheltered bus stops on either side of Mountain at the Neighborhood Activity Center (Residential District)

This action would encourage pedestrian activity, promote use of public transportation and add to the family friendly streetscape.

Create an Historic Residential Corridor along Edith Boulevard (Residential District and Bicycle Circulation)

This is an important cultural corridor that should emphasize the importance of Edith and its history as the Camino del Lado. To emphasize walkability and pedestrian friendliness would be in keeping with the Boulevard's historic use.

Add lighting to improve safety (Bicycle Circulation)

Request a study of lighting deficiencies on the collector and local streets in the plan area. Add lighting under I-40 overpass on Edith Boulevard and at the Santa Barbara/Martineztown Park, located at Hannett and Edith. Install additional streetlights on the northern portion of Edith Boulevard.

Add public amenities such as benches, shelters, and signage on Broadway (Railroad District)

Both sides of Broadway from Rosemont to Lomas are proposed mixed use zones and as such, provide an important link to the neighborhood pedestrian network.

During the writing of this Plan, ABQ RIDE proposed an evaluation of bus stop placement throughout the entire route system. The Indian School Road commuter bus (Route #6), serves riders from Tramway Boulevard to Downtown. There are currently 8 stops on Broadway between Indian School and Lomas, most are south of Rosemont. A sheltered bus stop is recommended to serve the proposed mixed use zone south of Mountain.

Make streetscape improvements along Odelia to slow traffic and provide more pedestrian amenities and safety features, while maintaining the same traffic capacity (Service District recommendations)

Request an engineering study of the sector plan area to examine pedestrian access needs, roadway geometry deficiencies, bikeway needs, and traffic calming opportunities. Install a raised median with low landscaping and decorative fencing on Odelia just east of Albuquerque High School to force pedestrians to cross at the traffic light in front of the high school.

Create enhanced pedestrian routes along Mountain, Odelia, and Edith

Widen sidewalks where feasible, constructing new sidewalks, providing landscape strips, trees, bike lanes or other buffers between pedestrians and traffic.

Improve pedestrian street crossings by adding crosswalks or other safety features at the following locations

- Across Lomas Blvd. at Edith Blvd. (elementary school route), Broadway, and
- I-40 underpass at Menaul (northern route from the high school)
- Across Odelia from the Veteran's Park to Albuquerque High School
- Intersection of Mountain and Broadway
- Intersection at Odelia and Edith

Pursue a sidewalk inventory and pedestrian circulation study

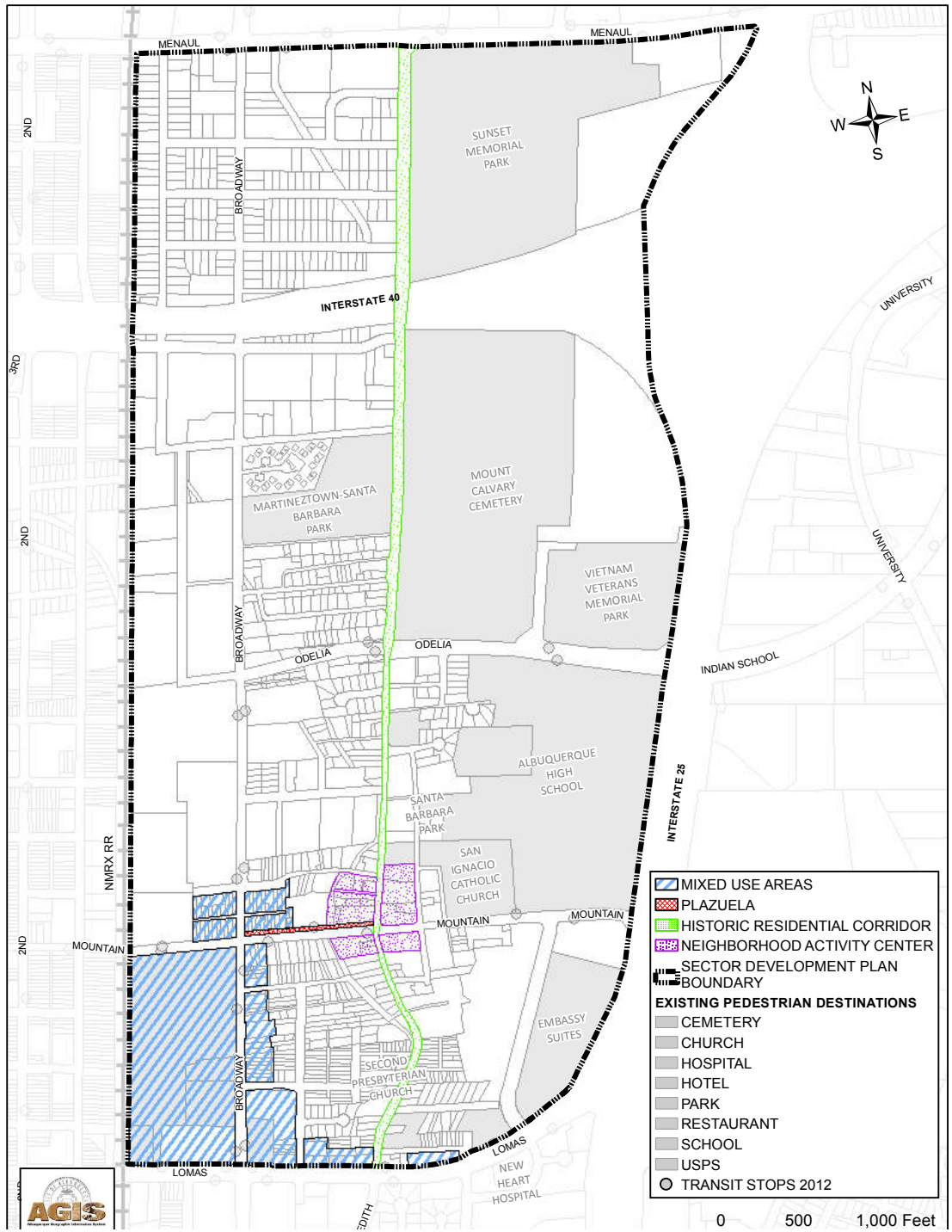


Figure 15 – Proposed Pedestrian Destinations and Recommendations

4 Drainage and Sewer

Recently completed projects in the plan area include: the Odelia Storm Drain; the Broadway and Odelia AQ Pond; Kinley, Broadway to Edith Storm Drain; the Post Office Interim Detention Pond; and the Mountain Storm Drain from Edith to I-25.

The Water Authority and the City Drainage worked together for the design and construction of storm drainage and sanitary sewer improvements at the intersection of Mountain and Walter. Construction of this storm drainage and sanitary sewer work was recently completed.

Historically, there have been drainage problems in Martineztown/Santa Barbara. The City has completed a study entitled "Mid Valley Drainage Management Plan." The study has recently been completed to determine the required stormwater detention volume and the number of stormwater detention facilities required to protect the neighborhood.

The largest pumping plant in the city that is located within the plan boundary is scheduled for a major rehabilitation in 2012-2013 supported by both the City and AMAFCA.

There are issues related to erosion rather than drainage and sewer. Those recommendations are listed in the appropriate district.

Issues and Analysis

Temporary detention basin lacks sufficient storage

In 2008, the City and the Albuquerque Metropolitan Flood Control Authority (AMAFCA) purchased approximately two acres on the southern portion of this district. An emergency storm basin was built as a temporary facility to address immediate issues.

The interim basin along with the basin at Broadway and Odelia have a capacity of only 27 acre feet (16ac-ft at Broadway & Lomas and 11 ac-ft at Broadway & Odelia) and about 100 acre-feet of storage maybe needed to solve the drainage problem in Martineztown/Santa Barbara. The combined detention capacity of the two existing basins is approximately 27% of what may be needed for flood relief. The City and AMAFCA are seeking additional sites to address the deficiency. This could mean one location for a larger basin or several locations for smaller basins. Because of side slopes and setback requirements, one large basin would require less land than smaller basins.

Tingley Park occupies about 12 acres or approximately 4 city blocks. Due to its shallow depth it is an inefficient drainage facility providing only about 2 acre-feet of storage per acre. In comparison the temporary basin occupies about 2 acres and provides approximately 8 acre-feet of storage per acre. Lomas & Broadway is in a FEMA flood zone because it is the low spot in the 600 acre drainage basin.

Landscaping and buffering of the temporary basin

Residents want the basin landscaped and buffered from the neighborhood. As a safety measure, wire fencing surrounding the area was installed. Because the pond was identified as an emergency interim measure, no landscaping was included in the project. The pond slopes were rocked for stabilization and dust reduction. City maintenance may have to remove storm sediment therefore landscaping the bottom of the basin is not under consideration.

The City investigated costs for a landscape buffer around the pond. Estimates ranged from \$80,000 to \$150,000 for minimal landscaping. The greatest expense is due to irrigation requirements. Plants in the southwest must be irrigated (even if considered native and xeric) and the irrigation system (water connection, meter, piping, maintenance) is cost prohibitive.

Outdated Sanitary Sewer Lines and Odor

There are plans to rehabilitate the sewer lines. The Albuquerque Bernalillo County Water Utility Authority operates a hotline committed to addressing sewer odors. The dispatch office number is 857-8250 and the web link is <http://www.abcwua.org/content/view/86/76/>.

Recommendations

Provide temporary screening for the interim detention basin at Broadway and Lomas (Railroad District)

A temporary landscape or manmade buffer would improve the aesthetics and pedestrian friendliness of both Boulevards. When the interim basin is removed, the screening could be recycled and reused in another project.

Develop a high capacity detention basin designed as a multi-use/park facility to replace the temporary basin

Identify and purchase property for a permanent basin with the necessary storage capacity. If a parcel of this size can be obtained, design the basin to include a multi-use/park facility. This measure would accomplish the much needed drainage improvements as well as provide additional open space and multi-generational recreational opportunities. Pedestrian circulation would be enhanced. An example of this is the multi-use park/ball fields in Barelás.

Rehabilitate Outdated Sewer Lines

During the writing of this plan, approximately 1350 feet of 8-inch sanitary sewer lines in the Martineztown area are anticipated to be rehabilitated by the Albuquerque Bernalillo County Water Utility Authority in the Small Diameter Water & Sewer Rehabilitation FY11 project.

5 Martineztown/Santa Barbara Zoning Regulations

A. Overview of Changes from 1990 Sector Development Plan

As demonstrated by the Existing Land Use/Zoning map, much of the zoning in Martineztown/Santa Barbara is inconsistent and incompatible with existing and historic land use. This incompatibility is most obvious where the land use is single-family residential but zoning is SU-2/C-3, SU-2/NRC or SU-2/RCM.

The following regulations and zone map amendments will reinforce and stabilize the single-family residential character of the Residential District, while preserving the light industrial character of the Railroad District (west of Broadway). The Plan proposes the establishment of a Neighborhood Activity Center (NAC) covering about six acres at the intersection of Mountain Road and Edith Boulevard, to be zoned SU-2/ NAC. It also proposes a new SU-2/Mixed Use (MX) Zone, to foster mixed residential/office/commercial uses along Lomas Boulevard and along Broadway Boulevard south of Mountain Road. Otherwise, the Plan retains most of the Martineztown/Santa Barbara Sector Plan (SU-2) zone categories adopted in the 1990 plan with a few modifications.

B. New Zones

SU-2/MX (Mixed Use) This zone provides a mixed-use environment with community commercial and residential uses along Lomas Boulevard, which is a designated enhanced transit corridor, and State Highway 47 (Broadway) which is classified as an Urban Principal Arterial. The intent is to provide additional service and residential opportunities that are within walking and biking distance of existing residential areas.

SU-2/ NAC (Neighborhood Activity Center): This zone encourages development of a mixed-use neighborhood activity center on about six acres of land at the intersection of Mountain Road and Edith Boulevard. Its intent is to re-establish a traditional neighborhood center as a place that provides a social setting and services for its residents. It will encourage redevelopment that fosters neighborhood activities, a pedestrian-friendly environment, and integrated land uses such as a café, small retail shops or services, housing, and small offices.

C. Modifications to Existing Zones upon Plan Adoption

The **SU-2/R-1** Single-Family Residential Zone is modified to permit secondary dwelling units (SDUs) as a conditional use. This will help maintain residential affordability and discourage gentrification of the neighborhood. Many of the single family residential properties now zoned NRC, RCM and C-3 are proposed to change to SU-2/R-1 to better reflect the actual use on the property and protect and preserve the single-family character.

The **SU-2/C-3** zone is modified to correct the language in the 1990 Plan. The 1990 Plan lists certain C-3 uses under Section A as “exceptions” when actually they are allowed permissively.

The **SU-2/M-1** Light Manufacturing Zone east of Broadway is modified to prohibit several uses that could potentially jeopardize the health, safety and welfare of residents as this zone is often adjacent to residences.

D. Zoning Conformance

Existing legal conforming uses which become non-conforming upon adoption of this plan shall be considered approved conditional uses in perpetuity.

E. Martineztown/Santa Barbara SU-2 Zoning Districts

The following SU-2 zones are established for Martineztown/Santa Barbara as shown in figure 18, (existing zoning is shown in figure 17 for reference). They are subject to General Design Regulations in Section G of this plan.

- SU-2/R-1 MTSB Single Family Residential
- SU-2/R-T Townhouse
- SU-2/R-2 Medium Density Residential
- SU-2/NRC MTSB Neighborhood Residential Commercial
- SU-2/O-1 Office and Institution
- SU-2/C-3 MTSB Heavy Commercial
- SU-2/M-I MTSB Light Manufacturing
- SU-2/HM MTSB Heavy Manufacturing
- SU-2/P Parking
- SU-2/MX MTSB Mixed Use
- SU-2/NAC MTSB Neighborhood Activity Center
- SU-2/SU-I Special Use

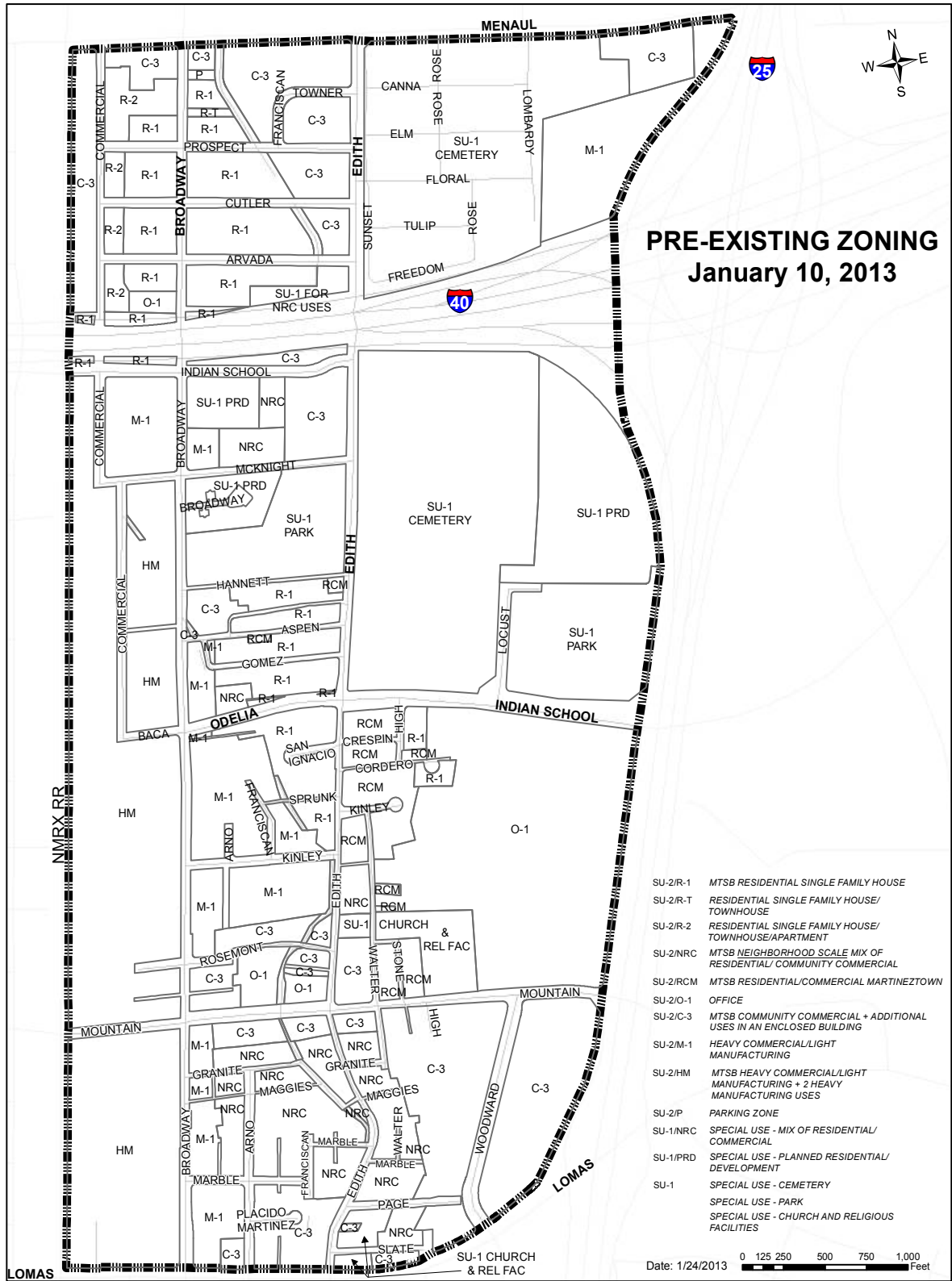


Figure 17 – Existing Zoning

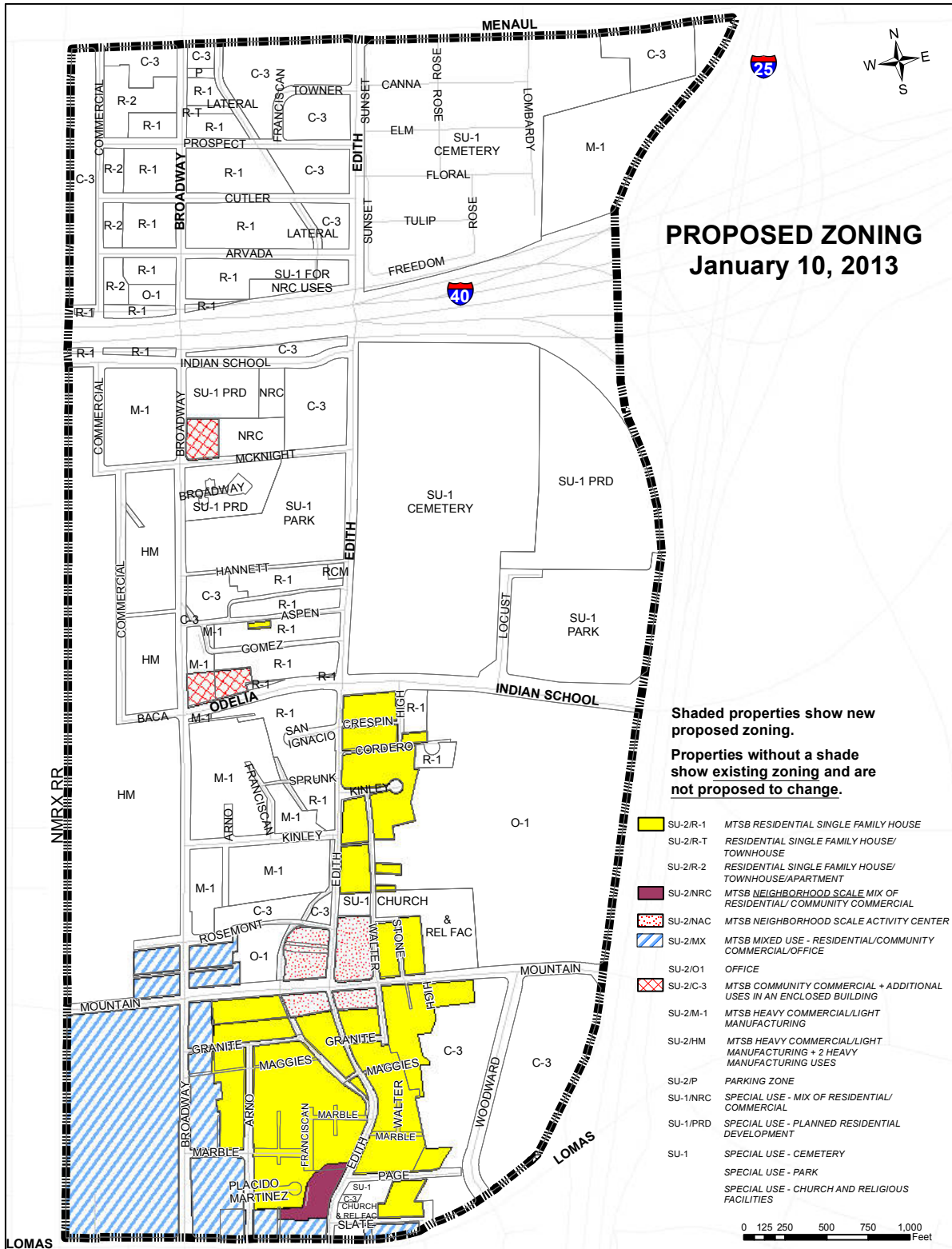


Figure 18 – Proposed Zone Changes

1. The **SU-2/R-1** (MTSB Single Family Residential) zone corresponds to the R-1 Zone in the Comprehensive City Zoning Code with the following exceptions:

- A. Setback. The R-1 setback regulations shall apply except in the area between Interstate 40 and Lomas Boulevard, there shall be a front yard setback of not less than 10 feet except setback for a garage or carport shall not be less than 20 feet.
- B. Lot Width. The R-1 regulations shall apply except in the area between I-40 and Lomas Boulevard, the minimum lot width shall be 40 feet per dwelling unit. No minimum lot size.
- C. Conditional Uses: Secondary dwelling units and NRC permissive uses.

1) A secondary dwelling unit is defined as a subordinate dwelling unit containing its own kitchen created within, added to, or detached from a single-family dwelling. Secondary dwelling units may not be subdivided from or otherwise segregated in ownership from the primary residence structure.

- a. Only one secondary unit is allowed per lot.
- b. Mobile homes and recreational vehicles are not allowed as secondary dwelling units.
- c. One off-street parking space is required for the secondary dwelling unit.
- d. The maximum floor area of a secondary dwelling unit shall not exceed 600 square feet, or 60 percent of the primary dwelling unit's floor area, whichever is less, except a secondary dwelling unit that is attached to the primary residence and is designed to look like one single family dwelling unit may have the same floor area as the primary residence.
- e. Usable open space shall be provided on-site at 800 square feet per dwelling unit or 600 square feet per dwelling unit where parking is accessed from a rear yard alley.

2) Uses permissive in the SU-2/NRC zones are conditional in this SU-2/R-1 zone only if the property abuts Edith Boulevard.

- a. Off-street parking and landscaping shall be provided as deemed appropriate by the Zoning Hearing Examiner.

2. The **SU-2/R-T** (Townhouse) zone corresponds to the R-T Residential Zone in the Comprehensive City Zoning Code.

3. The **SU-2/R-2** (Medium Density Residential) zone corresponds to the R-2 Zone in the Comprehensive City Zoning Code with the following exceptions:

- A. Setback. There shall be a front yard setback of not less than 10 feet except setback for a garage or carport shall not be less than 20 feet for lots developed with single-family units.

B. Parking. Multi-family residential development – for each dwelling unit, not less than 1 space per bath.

C. Usable Open Space. Usable open space shall be provided on-site in an amount equal to 300 square feet per one-bedroom dwelling unit, 350 square feet per two bedroom dwelling unit, 400 square feet per three bedroom dwelling unit or more.

4. The **SU-2/NRC** (MTSB Neighborhood Residential Commercial) zone corresponds to the R-2 (Low Density Apartments) and C-1 (Neighborhood Commercial) Zones in the Comprehensive City Zoning Code with the following exceptions:

- A. Lot Size. Minimum lot width for lots developed with single-family units shall be 40 feet. Minimum lot width for lots developed with multi-family units shall be 60 feet. No minimum lot size.
- B. Setback. There shall be a front yard setback of not less than 10 feet except setback for a garage or carport shall not be less than 20 feet for lots developed with single-family units.
- C. Parking. Multi-family residential development - for each dwelling unit, not less than 1 space per bath.
- D. Usable Open Space. Usable open space shall be provided on-site in an amount equal to 300 square feet per one-bedroom dwelling unit, 350 square feet per two bedroom dwelling unit, 400 square feet per three bedroom dwelling unit or more.
- E. Signage. As provided and as regulated in the RC Zone in the City Zoning code.
- F. Landscaping. All new commercial development must landscape according to requirements listed in the City Zoning Code.

5. The **SU-2/O-1** (Office and Institution) zone corresponds to the O-1 Zone in the Comprehensive City Zoning Code.

6. The **SU-2/C-3** (MTSB Heavy Commercial) zone corresponds to the C-3 Zone with the following additional uses:

A. C-3 uses which must be conducted within a completely enclosed building:

- 1) Automotive engine manufacturing, wholesale assembling or rebuilding of automotive vehicles or parts.
- 2) Dry cleaning, clothes pressing, dyeing, including rug and carpet, provided that portion of the structuring in which any dry cleaning process is done is at least 40 feet from any residential zone.
- 3) Manufacturing, assembling, treating, repairing, or rebuilding of products, as follows:

- (a) Building (structure) sub-assembly
- (b) Electrical appliances, electronic instruments and devices, radios or phonographs, including the manufacture of small parts only
- (c) Jewelry, curios
- (d) Metal stamps, tool and die making
- (e) Plumbing, assembly only
- (f) Pottery, ceramics, provided only previously pulverized clays and kilns fired by electricity or gas are used
- (g) Sewed items, including clothing
- (h) Signs, commercial advertising structures

- 4) Sheet metal working (light)
- 5) Upholstering
- 6) Welding, as a principal activity
- 7) Printing, publishing, lithographing, or blueprinting
- 8) Wholesaling

B. Uses permissive and as regulated in the R-2 Zone.

C. Properties abutting Commercial Street are allowed the following additional uses:

- 1) Cold Storage
- 2) Warehousing

7. The **SU-2/M-1** (MTSB Light Manufacturing) zone corresponds to the M-1 zone in the Comprehensive City Zoning Code with the following exceptions:

- A. Antennas are limited to 65 feet in height
- B. Products that are manufactured, compounded, processed, assembled or treated must be conducted within a completely enclosed building and must not result in detectable odors, dust, smoke, noise, vibration or other causes that will negatively impact adjacent residences.
- C. Prohibited Uses. The following uses are prohibited east of Broadway Boulevard in the Plan area:
 - 1) Adult amusement
 - 2) Auto dismantling (except in a completely enclosed building)

- 3) Commercial agriculture
- 4) Poultry storage and killing
- 5) Concrete manufacture
- 6) Gravel or sand stockpiling
- 7) Truck terminal
- 8) Fuel storage
- 9) Salvage yard

8. The **SU-2/HM** (MTSB Heavy Manufacturing) zone corresponds to the M-1 Zone in the Comprehensive City Zoning Code with the following exceptions:

A. Conditional Uses:

- 1) Uses permissive in the M-2 Zone and not permissive in this HM category.

9. The **P** (Parking Zone) corresponds to the P Zone in the Comprehensive City Zoning Code.

10. The **SU-2/MX** (Mixed Use) zone corresponds to the SU-1 Mixed Use Zone (MX) in Section 14-16-3-22 Form Based Zones of the Comprehensive City Zoning Code with the following exceptions.

A. Intent. This zone provides additional service and residential opportunities within walking distance of the neighborhood. This zone is recommended only along Lomas Boulevard which is a designated enhanced transit corridor, and State Highway 47 (Broadway) which is classified as an Urban Principal Arterial.

B. Permissive Uses:

- 1) Uses permissive and conditional in the R-2, R-C, C-1, C-2, and O-1 Zones

C. Prohibited Uses

1) The following uses are not permitted (in addition to those listed in the Section 14-16-3-22 except a church or other place of worship is allowed permissively):

- a. Adult Amusement Establishment and Adult Store
- b. Package liquor unless part of a full-service grocery store

D. Height

1) Maximum height shall be 50 feet (4 stories) along Broadway Boulevard and Lomas Boulevard.

2) Maximum height of buildings within 200 feet of a SU-2/R-1 or located north of Mountain zone shall be 30 feet.

~~E. Street Design~~

~~1) Block size and mid-block crossing requirements do not apply along Broadway Boulevard.~~

~~2) Street Types: CS-60 and CS-84, per Section 14-16-3-22(C)(4)(d) of the Zoning Code, shall apply to Broadway Boulevard, depending on the available right-of-way or other physical constraints.~~

E. Signage as regulated by the C-1 Zone.

11. **SU-2/NAC** (MTSB Neighborhood Activity Center)

A. Intent. This zone provides a medium density, mixed-use pedestrian environment inviting to neighborhood retail and office, housing, cafés, plazas and outdoor seating, and shared parking. Buildings are oriented to the street or plazas. Building uses may be mixed use, residential or commercial. This zone is designed for locations within walking or biking distance of residential areas. ~~The following zoning regulations are based on the SU-1 Infill Zone in the Zoning Code, §14-16-3-22(B)(4). The goal of the Infill Zone is to integrate infill development into the context of the built environment while protecting the neighborhood context.~~

B. Permissive Uses

- 1) Uses permissive in the R-T, R-LT, R-G, O-1, C-1 and C-2 Zones, except:
 - a. Antenna
 - b. Park and ride temporary facilities
 - c. Public Utility Structure
 - d. Retail Sales of Auto parts and supplies
 - e. Retail Sale of Gasoline, oil liquefied petroleum gas, including outside sales
 - f. Drive-in facilities
 - g. Car washing
 - h. Parking lot, freestanding
 - i. Off-premise sign

~~C. Permitted Building Types~~

- ~~1) The following Building Types are permitted (See Section 14-16-3-22(C)(1) for Building Types standards)~~
 - ~~a. Rowhouse and Courtyard Rowhouse~~
 - ~~b. Carriage House~~

- ~~e. Duplex, Triplex, and Fourplex~~
- ~~d. Stacked Flats~~
- ~~e. Terrace Apartments~~
- ~~f. Courtyard~~
- ~~g. Liner Building~~
- ~~h. Stand Alone Commercial/Office Building~~
- ~~i. Civic Institutional Building~~

C. Height

- 1) The following height restrictions apply:
 - a. Building heights shall not exceed 36 feet.
 - b. Within 75 feet of an abutting R-1 through R-T zone or corresponding SU-2 zone, building heights shall not exceed a 45-degree angle plane that begins at a height of 11 feet, measured from the residential property line.
 - c. Section 14-16-3-3(A) Height Regulations apply with the following exclusions:
 1. Section 14-16-3-3(A)(4) Walls, Fences, Retaining Walls shall not apply. Walls, Fences and Retaining Wall Heights shall be regulated by Section 14-16-3-22(C)(8).

D. Density

- 1) Maximum of 10 dwelling units per acre.

~~E. Building Frontage and Articulation.~~

- ~~1) Section 14-16-3-22 (C)(3) Articulation standards apply.~~
- ~~2) Articulation on street facades. The SU-2 General Standards for Martineztown/ Santa Barbara shall apply to all Building Types except Rowhouse, Carriage House, Duplex/Triplex/Fourplex with the following exceptions:

 - ~~a. Vertical change of color or material shall not apply~~
 - ~~b. Wall plane projection or recesses shall occur at least every 40 feet~~~~
- ~~3) Shading Elements. Facades that contain a primary customer entrance or that are adjacent to a public right-of-way shall contain shade features such as portals, awnings, canopies or shade trees along a minimum of 75% of the ground floor façade.~~

~~4) Balconies and Portals. Shall have a minimum vertical clearance of 8 (eight) feet above the public sidewalk.~~

E. Building Placement.

- 1) Buildings shall be set back a maximum of 15 feet from the front property line.
- 2) Side setback shall be a minimum of 10 feet from the property line.
- 3) Rear setback shall be a minimum of 15 feet from the property line.

F. Usable Open Space

- 1) A minimum 10 percent of the net site area shall be designated as Usable Open Space ~~in the form of patios, plazas, balconies, roof decks, courtyards, or exterior walkways.~~

~~G. Street Design:~~

~~1) Street Type CS-60, per Section 14-16-3-22(C)(4)(d) of the Zoning Code, should be used as a guideline. Its requirements may be modified based on available right-of-way and other physical constraints.~~

~~2) Alleys. See Section 14-16-3-22(C)(4)(c) for Alley standards.~~

G. Off-Street Parking. The Off-Street Parking Regulations of the Zoning Code, Section 14-16-3-1, shall apply with the following exceptions:

- 1) Required Minimum Parking.
 - a. Ground Floor Non-residential: 1 space per 1,000 square feet of net leasable area.
 - b. Residential: 1 space per unit.
 - c. On-street parking may count toward the off-street parking requirements at a one to one ratio as approved by the traffic engineer.
- 2) Shared Parking. Shared parking spaces may be located within 200 feet from the building or use that it serves.
- 3) Parking shall be located behind buildings in the rear portion of the lot or to the side of buildings that face the street.

H. Signage. Signs as regulated by the O-1 zone in the Zoning Code, with the following

exceptions:

- 1) Free-standing signs are not permitted.
- 2) No more than one wall-mounted sign per building façade.
- 3) Sign area of building-mounted signs shall not exceed 25 square feet.
- 4) Signs may project more than one foot into right-of-way per City Revocable Permit or Encroachment Agreement requirements.

12. The SU-2/SU-1/Special Use zone corresponds to the SU-1 Zone in the Comprehensive City Zone Code. SU-2/SU-1 for NRC uses and SU-2/SU-1 for Church and Religious Facilities.

F. General Design Regulations

Intent

The General Design Regulations shall apply to all properties in the Sector Plan area unless specified otherwise. The intent of the regulations is to

1. Improve compatibility among housing, institutions, commercial and industrial land uses through site design, buffering, screening, and landscaping.
2. Protect and conserve the area's distinct, historic physical characteristics by guiding the design of new construction and additions so that it blends and harmonizes with existing architectural character, sizes and massing without becoming unaffordable.
3. Improve the environment adjacent to the public right-of-way (roadways, sidewalks, landscape strips) through requirements for site parking, walls, fences, landscaping and pedestrian connections.

Required Compliance with General Design Regulations

1. In addition to complying with the provisions of the Comprehensive City Zoning Code, the following development requests shall also comply with the General Design Regulations contained in this plan:
 - New development;
 - Building additions enclosed on at least 3 sides that add 15% or more square feet to an existing building's square footage;
 - Buildings replacing existing buildings including buildings that are destroyed by flood, fire, or natural catastrophe;
 - Amendments to SU-1 site plans that include additions of 15% or more of existing building square footage.

2. The following activities are exempt from compliance with the General Design Regulations but shall adhere to pertinent regulations of the City Zoning Code:
 - Repairs, remodeling and maintenance of existing structures and/or buildings
 - Façade improvements to existing buildings
3. EXCEPTIONS TO THE GENERAL DESIGN REGULATIONS. The following two levels of modifications to the General Design Regulations are allowed:
 - A. Minor: The Planning Director or his designee may approve deviations of 10% or less from any *dimensional standard.
 - B. Major: The EPC shall review any deviation of more than 10% from any dimensional standard to determine if the request honors the intent of the regulation.

*Dimensional Standard: a standard relating to numerical measurement.

1. Preserving Residential Neighborhood Character

- A. Building additions and renovations shall blend architecturally with the style of the original building.
- B. New residential construction shall be architecturally compatible with existing adjacent buildings in height, mass, and architectural style.

2. Residential Building Design

In addition to the design regulations in the Zoning Code, residential development shall comply with the following:

- A. Building exterior materials (stucco, brick, wood, etc.), color, window and door styles, and roof slope and materials shall be the same or similar on all parts of a structure and on all detached dwelling units on one lot.
- B. Standard unstuccoed CMU block is not allowed as a finish material for buildings.
- C. Buildings shall not exceed 26 feet in height. Building portions over 10 feet high shall be set back not less than 10 feet from the front facade of the first floor.
- D. The slope of new roofs shall range from flat to not more than a 45-degree angle.
- E. New garage fronts shall be set back not less than 20 feet from the property line abutting a public right-of-way or private street.
- F. Not more than 50 percent of a building's street frontage width shall be garage front.
- G. Front doors shall face the street. In townhouse and multiple dwelling unit development, the dwelling unit(s) adjacent to the public right-of-way shall face front doors toward the street.

H. Building façades facing a public right-of-way or private street shall contain windows covering a minimum of 25% of the façade.

I. Buildings in residential zones shall comply with the Building Height Limitations to Preserve Solar Access, Section 14-16-3-3(A)(7) of the Zoning Code. The additional height limitations of this section shall apply to any lot including those platted before February 1, 1981, except that structures existing as of the date of adoption of the Plan are allowed, subject to Zoning Code regulations for buildings that are nonconforming as to height.

3. Nonresidential Building Design

In addition to the design regulations in the Zoning Code, §14-16-3-18, nonresidential development shall comply with the following:

- A. Primary entrances shall face the public right-of-way, except courtyard buildings where primary entrances may face a central courtyard.
- B. Except for buildings used only for manufacturing, assembling, treating, repairing, or rebuilding products, or for warehousing, not less than 25% of a building façade facing a public right-of-way shall be windows.
- C. Standard, unstuccoed Concrete Masonry Unit block is not allowed as a finish material for buildings.
- D. Reflective glass is not permitted.

4. Signage.

Signage shall comply with Section 14-16-3-5 of the City's Zoning Code.

5. Landscaping

The General Landscaping Regulations of the City's Zoning Code, Section 14-16-3-10 shall apply with the following exceptions:

- A. In Section 14-16-3-10(E)(4), a 6-foot high wall may be substituted for the 10 (ten) foot special landscape buffer.

6. Parking

The General Parking Regulations of the City's Zoning Code, Section 14-16-3-1 shall apply.

7. Utilities

All screening and vegetation surrounding ground-mounted transformers and utility pads are

to allow 10 feet of clearance in front of the equipment door and 5 to 6 feet of clearance on the remaining three sides for access and to ensure the safety of the work crews and public during maintenance and repair. Please refer to the PNM Electric Service Guide for specifications.

Coordination with PNM will be necessary if existing utilities are present where Form Based Zones are implemented, including:

- Extension of public utility facilities
- Projections such as canopies, portals, stoops, balconies, shop fronts and awnings in utility easements
- Parking areas and alleys
- Utility easements within rear lot lines

G. DEVELOPMENT APPROVAL PROCESS

1. The following development approval process shall apply to all properties within the Martineztown/Santa Barbara Plan boundary:

<u>Development Type</u>	<u>Approval Body</u>	<u>Notification</u>
The site is less than 5 acres; and the site is not zoned SU-1; and the proposed use is a permissive use; and the development complies with the General Design Regulations.	Building Permit Staff	No public notification required
The site is 5 acres or greater; or the site is zoned SU-1; or request for a zone change; or modification of the *dimensional standards of the General Design Regulations by more than 10%	EPC	Public notification required
Special Exceptions to the Zoning Regulations (Conditional Uses/Variances)	ZHE	Public notification required
Modification of the *dimensional standards of the General Design Regulations by 10% or less	Planning Director	No public notification required

* Dimensional Standard: a standard relating to numerical measurement.

2. Existing legal uses which are non-conforming upon adoption of the plan are APPROVED CONDITIONAL USES in perpetuity.

6 Plan Implementation - *[Implementation Workgroup]*

7 Metropolitan Redevelopment Plan Recommendation

A. Metropolitan Redevelopment Area (MRA) Designation Report

The entire Martineztown/Santa Barbara Sector Development Plan area was designated a Metropolitan Redevelopment Area (MRA) in 1989 through City Council Bill R-498. In order for the area to be designated, it had to be proven that the area was blighted. This is demonstrated by deteriorated buildings, vacant land, irregular platting and disinvestment, and by existing conditions that have “substantially impaired the sound growth and economic health and well being” of the area.

The creation of a Metropolitan Redevelopment Area should assist in achieving the following goals:

- Eliminate conditions that are detrimental to public health and welfare.
- Conserve, improve and expand housing availability to all residents.
- Improve economic conditions through coordinated public and private actions and investment.
- Specifically, the Metropolitan Redevelopment Code is the enabling legislation that enables the City of Albuquerque to work with the private sector to:
 - Assist in the establishment of new commercial ventures.
 - Assist in preserving existing businesses in the area.
 - Implement public improvements and tax increment financing (TIF) investments.

B. Benefits of Revitalization

A successful revitalization program could promote neighborhood stabilization by providing convenient services, creating new jobs, and upgrading area buildings, infrastructure and housing.

Business in the area include hotel, storage, warehousing, auto repair, monument manufacturing, as well as smaller retail uses such as Starbucks, Carl’s Jr., an American folk arts, pottery and basket store, a chile store and a taqueria. There is no grocery store, small restaurants or cafes where residents can congregate and meet their neighbors.

Redevelopment opportunities include vacant parcels in the plan area. In addition to being underutilized, these spaces are often frequented by drug users compromising the safety and stability of the neighborhood. Empty lots are often overgrown with weeds and littered with trash, including drug paraphernalia and liquor containers, giving the neighborhood a general sense of neglect. On the positive side, they represent an important opportunity for development.

Reducing the amount of land within the plan area zoned exclusively for industrial and heavy commercial, along with other vacant and under-utilized land could provide property for new

housing to support existing and future businesses. Strategies could also be developed to promote new business opportunities that could respond to the expressed needs and desires of area residents.

An economically successful Martineztown/Santa Barbara Neighborhood Activity Center will take advantage of opportunities identified by the community and the City of Albuquerque. Success will depend upon the commitment of the community and the City, as well as the private sector, and will require investments of both public and private funds in the years ahead.

Public funds could be used within the Neighborhood Activity Center public right-of-way (ROW) to improve bus shelters, construct new and widened sidewalks, plant street trees and other landscaping, and better designate street crossings with bricks or other crosswalk pavers. These improvements would embody a unique design character to help define the Martineztown/Santa Barbara Neighborhood Activity Center as a distinctive place within the City of Albuquerque.

New public improvements will help to reverse the negative economic trend, but these improvements must be combined with conscious strategies to attract profitable new businesses to the area. These strategies include neighborhood retail and service businesses but also those that thrive on a larger market area. There is strong support for local businesses that serve as “gathering places” for nearby residents.

Martineztown/Santa Barbara’s chances of revitalization and economic success will be increased if there is a significant population that takes advantage of its goods and services on a regular basis. Economic viability will intensify if there are many households within close walking distance of a distinctive place that is attractive and pedestrian-friendly.

There are select opportunity sites in the Plan area where flexibly designed buildings that accommodate mixed uses would foster limited area development to enhance the charm and small village ambiance, yet allow for the services that would strengthen the viability of Martineztown/Santa Barbara.

Section IV of this plan outlines several recommended changes from industrial and heavy commercial zoning to mixed-use zoning that permits and encourages new housing and retail opportunities.

C. Potential Catalytic Projects

- The Historic Crossroads Neighborhood Activity Center is identified as a priority catalytic project. It is envisioned as the walkable area surrounding the Broadway and Mountain intersection and is defined by a quarter-mile walking distance from the intersection. The design emphasis is to strengthen the connections between the various land uses, and to redesign the place so that it emphasizes the pedestrian’s use of the area.
- A Neighborhood Activity Strip is recommended for the area along Broadway from Mountain south to Lomas. The promotion of pedestrian activity in a medium to high density, mixed use

location is in keeping with the goals of the comprehensive plan.

- The Railroad Crossing Development could be high density mixed use on the Lomas Corridor from Broadway to the railroad tracks, including a grocery store.
- Other projects may be identified through the feasibility analysis within the Metropolitan Redevelopment Plan process

D. Recommendations

Complete a Metropolitan Redevelopment Plan for the area, using this sector plan as a framework and taking into account the catalytic projects recommended above. This plan would typically include a demographic/trade analysis, a site analysis, a location and market analysis, a financial feasibility analysis, and implementation recommendations.

E. Implementation and Funding Sources for Catalytic Projects

A number of funding sources from local, State and Federal agencies have been identified as potential opportunities to finance the implementation of the Martineztown/Santa Barbara Sector/ Metropolitan Redevelopment Plan.

- **Public/Private Partnerships**

There are a number of opportunities for partnerships to occur between these various entities. Partnerships hold the highest potential for redevelopment opportunities to occur in the Martineztown/Santa Barbara Plan area. The City can provide incentives through public financing, land holdings, or eminent domain authority, to serve as incentive/collateral for groups such as the NM Community Development Loan Fund, Accion, Wesstcorp, Small Business Association and private developers.

- **New Mexico Community Development Loan Fund**

The New Mexico Community Development Loan Fund is a private, non-profit organization that provides loans, training and technical assistance to business owners and non-profit organizations. Their services support the efforts of low-income individuals and communities to achieve self-reliance and control over their economic destiny. Loans are available to new and existing small businesses for such needs as equipment, inventory, building renovations and operating capital. They provide loans to non-profits for such needs as bridge financing against awarded private and public contracts, capital improvements and equipment, and loans to non-profits that develop affordable housing.

This program provides loans of up to \$250,000 to municipalities and counties to construct or implement projects necessary to encourage the location or expansion of industry, in order to create jobs, stimulate private investment, promote community revitalization, and expand the local tax base. Eligible uses include infrastructure improvements, rehabilitation or installation of public facilities, site improvements and utilities, and commercial or industrial buildings or structures and other commercial or industrial real property improvements.

- **Tax Increment Financing Districts**

Tax increment financing is created through a local government's property tax assessment. The incremental difference in tax is used to finance the improvement within the district. In NM, tax increment financing is enabled in forms through the Metropolitan Redevelopment Code, Enterprise Zone Act and the Urban Development Law. The City of Albuquerque uses tax increment financing within its designated Metropolitan Redevelopment Areas (MRA). Creating a TIF District of the entire Martineztown/Santa Barbara Sector/Metropolitan Redevelopment Plan Area could be beneficial, although additional research and analysis are needed.

- **Capital Improvement Plan**

The purpose of the City of Albuquerque's Capital Improvement Plan (CIP) is to enhance the physical and cultural development of the City by implementing the Albuquerque/Bernalillo County Comprehensive Plan and other adopted plans and policies. Through a multi-year schedule of public physical improvements, CIP administers approved capital expenditures for systematically acquiring, constructing, replacing, upgrading and rehabilitating Albuquerque's built environment. In practice, the CIP develops, and sometimes directly implements, diverse projects and improvements to public safety and rehabilitation of aging infrastructure such as roads, drainage systems and the water and wastewater network.

- **Industrial Revenue Bonds (IRB)**

In economic effect, an IRB is a loan by a lender/bond purchaser to a company, where the loan proceeds and the loan repayments flow through a government issuer. The tax benefits of IRBs result from the form of the loan and the involvement of a government issuer. The tax benefits of IRBs result from the form of the loan and the involvement of a government issuer. In its simplest form, an IRB structure involves a company (typically a corporation, a limited partnership or limited liability company) that wants to purchase and/or construct and/or equip a facility. Instead of purchasing, constructing or equipping directly, the company enters into an agreement (usually a lease) with a government issuer. The agreement provides that the company will lease the facility from the government issuer, construct and equip the facility and, at the end of the lease term, purchase the facility from the issuer at a nominal price. Importantly, the company constructs and equips the facility as the agent of the issuer. In order to obtain the funds to purchase, construct and equip the facility, the issuer issues bonds. Please note that the City does not finance bonds; nor does the City provide any credit enhancement. The proceeds of the bond sale are used to pay the expenses of the facility. The bonds are paid off solely with the payments made by the company to the issuer under the lease.

Only "projects" can be financed with IRBs. Projects include land, buildings, furniture, fixtures and equipment. Municipal projects (as opposed to county projects) do not include facilities used primarily for the sale of goods or commodities at retail and certain regulated utility projects.

Projects do not need to include land; they can be for equipment only. Also, any land included in a project need not be owned in fee. The costs of projects that can be financed are limited to capital costs and transaction costs. Working capital generally cannot be financed with IRBs, nor is there any benefit associated with doing so.

- **Metropolitan Redevelopment Bonds**

Metropolitan Redevelopment Bonds, while similar in some respects to Industrial Revenue Bonds, have certain differences. These projects are restricted to designated Metropolitan Redevelopment Areas, and are available to a wider variety of projects. The public purpose for these projects is to stimulate redevelopment activities in economically distressed areas.

Metropolitan Redevelopment Bonds provide a limited property tax abatement on the net improvements to the project site (i.e., current property taxes on the existing value of the property are not exempted). The maximum property tax abatement period is for seven years. Also, Metropolitan Redevelopment Bonds do not offer gross receipts or compensating tax exemptions on the purchase of equipment for the facility. However, they are a reasonable option for projects that may not generally qualify for Industrial Revenue Bonds.

As with Industrial Revenue Bonds, the City does not provide the financing or credit enhancement for the bonds; the applicant is responsible for finding their own financing, based solely on the rates they can negotiate with the purchasers of the bonds. All financing and legal costs are paid by the applicant.

- **Transportation and Equity Act for the 21st Century (TEA-21)**

Federal TEA-21 Enhancement funds, in excess of \$200 billion, is allocated to integrate transportation projects with environmental and community revitalization goals over a period of six years. TEA-21 funds are applicable beyond highways, road and transit maintenance – funds may also be used for relevant environmental restoration, pollution abatement, historic preservation, trails, bike paths and pedestrian infrastructure including aesthetic enhancements.

- **State Financing Programs**

Business Loans

The State of New Mexico has several loan programs to support business expansion and relocation to the state.

- Business Bonds
- Private Activity Bonds for Manufacturing Facilities
- Real Property Business Loan
- Severance Tax Permanent Fund/Participation Interests in Business Loans
- Severance Tax Permanent Fund/Purchases of SBA/FMHA Obligations

- **Federal Financing Programs**

Loans

1. HUD funds for local CDBG loans and “floats”

Community Development Block Grants are used to finance locally determined activities and can include coping with contamination and financing site preparation or infrastructure development.

Eligible activities include planning for redevelopment, site acquisition, environmental site assessment, site clearance, demolition, rehabilitation, contamination removal and construction. Also, when a grant recipient can show that previously awarded CDBG funds will not be needed in the near term, it may tap its block grant account on an interim basis, using a “float” to obtain short-term, low interest financing for projects that create jobs. Money borrowed from grants in this way may pay for the purchase of land, buildings and equipment, site and structural rehabilitation (including environmental remediation) or new construction.

The City of Albuquerque receives an annual Community Development Block Grant from the U.S. Department of Housing and Urban Development to fund redevelopment activities in low and moderate income communities across the City. The City’s consolidated plan, which specifies how the funds are to be spent over a five-year period, has established several programs that could support redevelopment activities in the Martineztown/Santa Barbara Sector/MR Plan: Crime Prevention through Environmental Design (CPTED), which funds neighborhood improvements designed to reduce crime and enhance security; Acquisition of Nuisance Property, which funds the purchase of property creating conditions of slum and blight; Neighborhood Business Assistance Fund, which assists businesses with low-interest loans, façade improvements and technical assistance; and Job Training for Businesses in Low/Mod Areas, which provides funding for training employees.

2. SBA Micro loans

These loans are administered through responsible nonprofit groups, such as local economic development organizations or state finance authorities that are selected and approved by the SBA. The SBA loans the money to the nonprofit organization which then pools the funds with local money and administers direct loans to small businesses.

SBA micro loans are administered much like a line of credit and are intended for the purchase of machinery and equipment, furniture and fixtures, inventory, supplies and working capital. The funds are intended to be dispersed with close monitoring of the recipient and a self-employment training program may accompany the loan. The maximum maturity for a micro loan is six years. The average loan size is \$10,000. The loan cannot be used to pay existing debts.

3. SBA’ Section 504 development company debentures

Small businesses can receive long-term capital for fixed assets from SBA-certified local development companies who issue notes backed by SBA. These resources can support up to 40 percent of a project’s total costs, up to \$750,000. A private financial institution must provide 50 percent of the project financing, but has first claim on collateral. The remaining 10 percent of funding must be obtained from the developer, a non-federal economic development program, or owner equity.

Loan Guarantees

1. HUD Section 108 loan guarantees

Under Section 108, state and local governments receiving Cobs can receive federally guaranteed loans, often at lower interest rates, to cover the cost of multi-year development projects too large for single year financing with CDBG funding. City or state applicants can pledge up to five times their annual CDBG grants as collateral. State can also pledge their own CDBG allocation on behalf of their small cities.

2. SBA's Section 7(a) and Low-Doc programs

Under Section 7(a), SBA will guarantee up to 90 percent of private loans of less than \$155,000 to small businesses and up to 85 percent of loans between \$155,000 and \$500,000. The Low-Doc Program offers SBA-backing of 90 percent and a streamlined application, review, and approval process for guarantees of loans of less than \$100,000.

Grants

1. HUD's CDBG Grants

The CDBG program, one of the nation's largest Federal grant programs, is administered by the Department of Housing and Urban Development to promote the revitalization of neighborhoods and the expansion of affordable housing and economic opportunities. This includes activities that support the redevelopment of properties in distressed areas if such activity supports the mission of the program. CDBG is a "bricks and mortar" program, with the rehabilitation of affordable housing traditionally being the largest single use of CDBG funds.

2. EDA Title I and Title IX

Grants are available to government and nonprofit organizations in distressed areas to fund improvements in infrastructure and public facilities, including industrial parks.

Equity capital

1. SBA's Small Business Investment Companies

Licensed and regulated by the SBA, SBIC's are privately owned and managed investment firms that make capital available to small businesses through investments or loans. The use of their own funds plus funds obtained at favorable rates with SBA guaranties and/or by selling their preferred stock to the SBA.

Tax incentives and tax-exempt financing

1. Historic Rehabilitation Tax Credits

Investors can receive a credit against their total income taken for the year in which a rehabilitated building is put into service. Rehabilitation of certified historic structures qualifies for a credit equal to 20 percent of the cost of the work; rehabilitation work on non-historic structures built before 1936 qualifies for ten percent.

2. New Markets Tax Credits (NMTC)

The NMTC Program permits taxpayers to receive a credit against Federal income taxes for making qualified equity investments in designated Community Development Entities (CDEs). Substantially all of the qualified equity investment must in turn be used by the CDE to provide investments in low-income communities. The credit provided to the investor totals 39% of the cost of the investment and is claimed over a seven-year credit allowance period. In each of the first three years, the investor receives a credit equal to five percent of the total amount paid for the stock or capital interest at the time of purchase. For the final four years, the value of the credit is six percent annually. Investors may not redeem their investments in CDEs prior to the conclusion of the seven year period.

3. Low-income Housing Tax Credits (LIHTC)

The LIHTC program offers a ten year credit for owners of newly constructed or renovated rental housing and sets aside a percentage of the units for low-income individuals for a minimum of 15 years. The amount of the credit varies for new construction and renovation. The project must receive allocation of New Mexico State's annual credit ceiling or use multi-family housing tax-exempt bonds that receive allocation of New Mexico State's bond volume cap. Allocations are made on the basis of the New Mexico State Qualified Allocation Plan.

8 Appendix

A. Public Involvement / Community Process

Planning Process

The process began with a Sector Development Plan kick-off meeting in June 2007. From that date until February of 2009, seven Advisory Committee meetings were conducted to assess existing conditions, further explore community issues, and develop recommendations to address those issues. Zoning issues were discussed, and zoning and design standards were developed.

During 2009, seven small group meetings were held to report the status of the plan to business owners and to the property owners within the plan boundaries. Additional input was received and recorded, resulting in further development of zoning and design standards.

In 2010, the efforts to date were incorporated into a draft plan, and an overview was presented to the Advisory Committee in August. The draft was presented at a public meeting at the Embassy Suites Hotel in September 2010.

Public Input

Community involvement throughout the planning process has been essential to the development of the Plan. The planning team organized various committees; interviewed key stakeholders; conducted various public surveys regarding land use, transportation, CIP improvements, and MRA projects; and held numerous community meetings and working sessions. The following is a summary of the community involvement opportunities.

Advisory Committee: The Advisory Committee included a citizen's group of volunteers from the neighborhood who met to assess current needs as defined by the neighborhood association; identify additional neighborhood issues; and assist in developing this update to the 1990 Martineztown/Santa Barbara Sector Development Plan. All attendees at the initial public meeting held in June 2007 were invited to join the Advisory Committee.

Community Survey: After the Advisory Committee completed its identification and evaluation of capital improvement priorities in 2008, a survey was developed and mailed to property owners, business owners, and made available to renters within the plan boundaries. The survey listed 16 capital improvement projects and requested the respondent to rank the projects in order of importance. The respondents

were encouraged to add additional projects. Approximately 2,000 surveys were distributed and 93 or approximately 5 percent were returned completed. The form and summary can be found in **Appendix D. E. Surveys.**

Focus Group Meetings: A special meeting attended by volunteers from the Advisory Committee was held in 2008 to discuss community services. Current needs were assessed, and additional issues were identified.

Business Owner Meetings: The area's business owners and business property owners also provided input and identified issues at two meetings held in 2009 for the area's commercial and service community. Issues noted by the community, possible solutions, and the results of the capital project survey were presented. Members of the business community were encouraged to discuss their concerns, and a useful dialogue ensued. The inputs of the commercial and service community are also reflected in this Plan.

Small Area Meetings: Five small area meetings were held in 2009 to allow residents, property owners and business people who lived, worked or owned property in a particular section of the Sector Plan boundary to further explore and comment on zoning proposals.

Project and Public Involvement Timeline

December 2006 letter from the neighborhood association

The planning department initiated an update to the 1990 MTSB SDP in May of 2007 with a kick-off meeting in June 2007

06/26/07 Sector Development Plan Kick off Meeting

2007/2008

Assessment of existing conditions; input gathering from community; one on one stakeholder interviews

08/21/07 Advisory Committee Meeting

10/15/07 Advisory Committee Meeting

12/10/07 Advisory Committee Meeting

01/28/08 Advisory Committee Meeting

03/03/08 Advisory Committee Meeting

04/07 08 Community Services Focus Group

05/12/08 Advisory Committee Meeting

2009

Small group meetings to report status of plan; receive and record additional input for initial development of zoning and design standards

02/09/09 Advisory Committee Meeting

03/02/09 Business Owners Meeting (present overview of AC requests)

03/23/09 Business Owners Meeting (present overview of AC requests)

04/23/09 Small Group Meeting 1 Menaul to I-40

04/23/09 Small Group Meeting 2 I-40 to Odelia

04/27/09 Small Group Meeting 3 Odelia to Mountain

04/27/09 Small Group Meeting 4 Mountain to Lomas

05/27/09 Small Group Meeting 5 Railroad District

10/03/09 Contract with Sites Southwest ends

2010

Incorporation of comments into draft plan; sent draft to appropriate city divisions for vetting/editing and project cost estimates

08/19/10 Advisory Committee Draft Overview Meeting (invitations to join implementation team)

09/02/10 Embassy Suites Draft Plan Presentation

B. June 2008 Letter and CIP Project Priorities

June 19, 2008

Dear Neighbors:

In June of 2007, a public meeting was held to obtain your input and discuss concerns regarding existing conditions and future visions for Martineztown/Santa Barbara. The issues identified of greatest concern were:

- 1) Zoning that does not reflect current land use;
- 2) Flooding and drainage problems; and,
- 3) Traffic congestion on Mountain Road.

This meeting initiated the City's process of updating the 1990 Martineztown/Santa Barbara Sector Development Plan.

We are pleased to report several major improvements soon to be completed in the plan area. These projects include: a detention pond near Odelia and Broadway; storm drain improvements along Odelia; and, improvements to the McKinley storm drain. Consultant selection for engineering analysis of drainage improvements along Mountain Road is underway.

An analysis of traffic counts along Mountain Road has been conducted by a local engineering firm and a separate report describing conditions and potential improvements of Mountain Road from Broadway to the I-25 Frontage Road has been generated by the Department of Municipal Development.

Currently the City, along with the consulting firm Sites Southwest is developing options for possible zoning modifications to present at the next public meeting.

The vision developed from comments taken at the first public meeting states:

"Martineztown-Santa Barbara will remain a low-density residential neighborhood that is family and child-friendly. Preservation of its historic architectural character will be maintained through preservation of historic buildings and development of new affordable infill housing or redevelopment designed to fit that character. Its streets will be narrow, pleasant, walkable and safe for pedestrians with good circulation for bicycles and buses. There will be a local restaurant and market for residents to walk to, some preservation of open space, and more multi-generational recreational opportunities. Storm water drainage will be sufficient and there will be adequate parking, paved streets and lighting."

In response to the above vision, we are evaluating requests received from your neighborhood regarding capital improvement projects. A list of projects is enclosed for your review and comment. Please rank the items listed in order of importance/urgency and return by June 30, 2008. If you need assistance in Spanish, please call Ramona Gabaldon at 924-3923 or Maggie Gould at 924-3910.

Thank you for your participation in this important process. The results of this survey will determine what projects are listed as plan priorities for funding and implementation.

We will send notice of the next public meeting after the projects are ranked and various zoning scenarios ready for presentation.

I look forward to meeting each of you at the upcoming public meeting. If you have questions, please call me.

Sincerely,

Lorena Patten-Quintana
Planner, Project Manager
(505) 924-3940
lpatten-quintana@cabq.gov

CIP Survey and Ranked Projects

MARTINEZTOWN/SANTA BARBARA

POTENTIAL CAPITAL IMPROVEMENT PROJECTS

Below are a number of potential capital improvement projects. Project descriptions are on the back of this survey. Please rank them according to your preference, with 1 being your highest priority and 10 your lowest, by circling the correct number. You may add and rank projects you think are missing.

1	Mountain Road: Road narrowing and streetscape improvements	1	2	3	4	5	6	7	8	9	10
2	Broadway Blvd.: Streetscape Improvements	1	2	3	4	5	6	7	8	9	10
3	Odelia Road: Streetscape Improvements	1	2	3	4	5	6	7	8	9	10
4	Lomas Blvd.: Streetscape improvements	1	2	3	4	5	6	7	8	9	10
5	Edith Blvd.: Streetscape Improvements (mostly north of Odelia)	1	2	3	4	5	6	7	8	9	10
6	Community Center	1	2	3	4	5	6	7	8	9	10
7	Community Pool (partner with APS)	1	2	3	4	5	6	7	8	9	10
8	Redesign and construction of Santa Barbara Park (next to church)	1	2	3	4	5	6	7	8	9	10
9	Redesign and construction of Santa Barbara Martineztown Park (large park)	1	2	3	4	5	6	7	8	9	10
10	Redevelopment of Mountain Rd/Broadway intersection as small activity center	1	2	3	4	5	6	7	8	9	10
11	Improve pedestrian connections at Lomas/Edith, Lomas/Broadway, Lomas/High	1	2	3	4	5	6	7	8	9	10
12	Historic interpretive signs/monument/walking tour brochure	1	2	3	4	5	6	7	8	9	10
13	Lighting improvements on Edith, north of Odelia and under freeway	1	2	3	4	5	6	7	8	9	10
14	Bus stop benches and shelters	1	2	3	4	5	6	7	8	9	10
15	Sidewalk repair/installation	1	2	3	4	5	6	7	8	9	10
16	Gateway features at Edith/Lomas and on Mountain and Odelia roads	1	2	3	4	5	6	7	8	9	10
17											
18											
19											
20											

Please return by June 30, 2008 to Lorena Patten-Quintana

By mail: Advance Planning, 600 2nd St. NW, 3rd floor, Albuquerque, NM 87102

Drop Off: 3rd floor, Plaza del Sol, 600 2nd St. NW, Albuquerque, NM 87102

Fax: (505) 924- 3339

CIP Survey and Ranked Projects

The Capital Implementation Program (CIP) is a way for the City to implement the goals of sector plans. The CIP administers approved capital expenditures for acquiring, constructing, replacing, upgrading and rehabilitating Albuquerque's built environment such as roads, drainage systems, water/wastewater systems and parks. The potential projects listed on the questionnaire emerged from the neighborhood comments during the first public meeting as well as subsequent Advisory Committee meetings and a special Community Services Focus Group.

Project List (project numbers correspond to those on the accompanying ranking survey)

- 1) Narrowing Mountain Rd may include: widening sidewalks; adding bicycle lanes, lighting and trees to help slow traffic, promote walkability and establish design identity.
- 2) Streetscape improvements along Broadway Blvd may include adding street trees and other amenities to shield residential areas from warehouses and industrial buildings.
- 3) Streetscape improvements along Odelia/Indian School may include: completing sidewalk on north side of street near the high school; constructing raised, landscaped medians west of the I-25 overpass to provide pedestrian refuge; and, slowing cars by narrowing the roadway, widening bicycle lanes, and adding street trees.
- 4) Lomas streetscape improvements may include: constructing missing sidewalk between Edith and Woodward; adding crosswalks at Arno, High and Woodward intersections; adding street trees in the landscape strip and nichos similar to those on the south side of the street.
- 5) Streetscape improvements on Edith Blvd. (mainly north of Odelia) could entail widening sidewalks and, adding street trees and streetlights.
- 6) Development of a small neighborhood community center.
- 7) Development of a Community swimming pool.
- 8) Improvements to Santa Barbara Park (next to St. Ignacio Church).
- 9) Improvements to Santa Barbara/Martineztown Park (between Hannett and McKnight).
- 10) Redevelopment of the Mountain Rd/Broadway intersection as a small Neighborhood Activity Center to encourage a mix of small retail, service and community uses.
- 11) Safety improvements to pedestrian crossings at various locations.
- 12) Signs interpreting neighborhood history, a monument and a walking tour brochure.
- 13) Lighting improvements along Edith to improve Safety of the I-40 underpass.
- 14) Additional benches and shelters at neighborhood bus stops.
- 15) Repair and install sidewalks in various neighborhood locations.
- 16) Cultural elements such as arches, sculpture, and signs at Edith/Lomas and Mountain/Odelia Roads to signal visitors entering the Santa Barbara/Martineztown Neighborhood.
- 17-20) These spaces were left blank for respondents to add and rank projects not on the list.

CIP Survey and Ranked Projects

CIP LIST PRIORITIZED BY THE NEIGHBORHOOD

1. Repair and install sidewalks in various neighborhood locations.
Mentioned widening in RR district – was removed
Under vehicular circulation issues
2. Additional benches and shelters at neighborhood bus stops.
3. Improve pedestrian connections at Lomas Boulevard/Edith, Lomas Boulevard/Broadway Boulevard, Lomas Boulevard/High.
4. Mountain Road: Road narrowing and streetscape improvements – Narrowing Mountain Road Rd may include: widening sidewalks; adding bicycle lanes, lighting and trees to help slow traffic, promote walkability and establish design identity.
5. Community Center
6. Broadway Boulevard: Streetscape Improvements - Streetscape improvements along Broadway Boulevard may include adding street trees and other amenities to shield residential areas from warehouses and industrial buildings
7. Redesign and construction of Santa Barbara Martineztown Park (large park between Hannett Avenue and McKnight Avenue).
8. Community swimming pool
9. Lighting improvements along Edith to improve Safety of the I-40 underpass.
10. Redevelopment of the Mountain Road Rd/Broadway Boulevard intersection as a small Neighborhood Activity Center to encourage a mix of small retail, service and community uses.
11. Improvements to Santa Barbara Park (next to San Ignacio Church).
12. Streetscape improvements along Odelia/Indian School Roads may include: completing sidewalk on north side of street near the high school; constructing raised, landscaped medians west of the I-25 overpass to provide pedestrian refuge; and, slowing cars by narrowing the roadway, widening bicycle lanes, and adding street trees.
13. Streetscape improvements on Edith Boulevard (mainly north of Odelia Road) could entail widening sidewalks and, adding street trees and streetlights.
14. Cultural elements such as arches, sculpture, and signs at Edith/Lomas Boulevards and Mountain/Odelia Roads to signal visitors entering the Santa Barbara/Martineztown Neighborhood.
15. Lomas Boulevard streetscape improvements may include: constructing missing sidewalk between Edith Boulevard and Woodward Place; adding crosswalks at Arno Street, High Street, and Woodward Place intersections; adding street trees in the landscape strip and nichos similar to those on the south side of the street.
16. Signs interpreting neighborhood history, a monument, and a walking tour brochure.

C. Historic Properties

Historic Landmarks Survey

The architectural resources of Martineztown/Santa Barbara were surveyed and evaluated between 1978 and 1983 as part of a City-wide Historic Landmarks Survey, which was sponsored by the State of New Mexico. The Historic Landmarks Survey identified buildings that were eligible for listing on the New Mexico State Register of Cultural Properties and the National Register of Historic Places.

The Historic Landmarks Survey identified the following three buildings as eligible for nomination to the State Register of Cultural Properties and/or the National Register of Historic Places.

1. 1431 Edith Boulevard NE, circa 1907 farmhouse, Territorial pedimented lintels.
2. 402 Aspen Street NE, circa 1910 New Mexico Vernacular style with post Railroad use of modern materials with traditional adobe walls.
3. 517 Odelia Road NE, circa 1910 New Mexico Vernacular style.

Additionally, two potential historic districts were identified.

1. Martineztown Plaza District – located along Edith Boulevard from Marble Avenue to just north of San Ignacio Church, approximately between the line of the old Barelás Ditch on the west and High Street on the east. It encompasses the part of the neighborhood which was probably the first settled circa 1850 according to local historians. It includes the most densely settled area (now thinned by demolitions), the site of the first home of Second Presbyterian Church and the 1916 San Ignacio Church.
2. The AAA and Sons Grocery District is a small area containing a wide variety of wellpreserved older homes and a store; these include an old neighborhood grocery, two rare shed-roofed cast stone-faced homes, an elaborately gabled 1920s house, and two small New Mexico Vernacular houses.

Several other buildings were deemed to have some historic or architectural interest during the Historic Landmarks Survey.

Tax Credit Programs for Historic Rehabilitations

New Mexico State Tax Credit

The State of New Mexico Investment Tax Credit program provides up to \$25,000 in income tax credit as a 50% match of approved expenses on qualified rehabilitation projects.

To be eligible for the state tax credit, a property must be listed on the State Register of Cultural Properties or listed as contributing in a registered historic district.

Contact the Historic Preservation Division in Santa Fe, (505)827-6320, to find out if your property meets this requirement.

The state income tax credit is available to owners of historic structures who accomplish qualified rehabilitation on a structure or stabilization or protection of an archaeological site. All work must conform to the Secretary of the Interior's Standards for Rehabilitation and program regulations. The property may be a personal residence, income-producing property (such as apartment building or office), or an archaeological site.

In general the program grants an amount equal to 50% of approved expenses toward income tax credit. Allowable expenses are limited to \$50,000 for any single project, limiting the credit to \$25,000, even though rehabilitation costs may exceed this figure. The credit may be used for a total of five years. Once the project is certified as a qualified rehabilitation and expenses/credit amount is established, the applicant may use the tax credit for the year in which funds were expended. Any remaining credit amount may be carried forward for four more years. If the total amount of credit cannot be utilized within five consecutive tax years, the surplus is forfeited. The Cultural Properties Review Committee must approve the proposed rehabilitation work prior to the beginning of the project.

Secretary of the Interior's Standards for Rehabilitation

The Secretary of the Interior created ten basic principles to help preserve the distinctive character of a historic building and its site, while allowing for reasonable change to meet new needs.

The Standards (36 CFR Part 67) apply to historic buildings of all periods, styles, types, materials, and sizes. They apply to both the exterior and the interior of historic buildings. The Standards also encompass related landscape features and the building's site and environment as well as attached, adjacent, or related new construction. Rehabilitation projects must meet the following Standards, as interpreted by the National Park Service, to qualify as "certified rehabilitations" eligible for the 20% rehabilitation tax credit. The Standards are applied to projects in a reasonable manner, taking into consideration economic and technical feasibility.

Source: http://www.nps.gov/hps/tps/standguide/rehab/rehab_standards.htm

1. A property shall be used for its historic purpose or be placed in a new use that requires minimal change to the defining characteristics of the building and its site and environment.
2. The historic character of a property shall be retained and preserved. The removal of historic materials or alteration of features and spaces that characterize a property shall be avoided.
3. Each property shall be recognized as a physical record of its time, place, and use. Changes that create a false sense of historical development, such as adding conjectural features or architectural elements from other buildings, shall not be undertaken.
4. Most properties change over time; those changes that have acquired historic significance in their own right shall be retained and preserved.
5. Distinctive features, finishes, and construction techniques or examples of craftsmanship that characterize a historic property shall be preserved.
6. Deteriorated historic features shall be repaired rather than replaced. Where the severity of deterioration requires replacement of a distinctive feature, the new feature shall match the old in design, color, texture, and other visual qualities and, where possible, materials. Replacement of missing features shall be substantiated by documentary, physical, or pictorial evidence.
7. Chemical or physical treatments, such as sandblasting, that cause damage to historic materials shall not be used. The surface cleaning of structures, if appropriate, shall be undertaken using the gentlest means possible.
8. Significant archeological resources affected by a project shall be protected and preserved. If such resources must be disturbed, mitigation measures shall be undertaken.
9. New additions, exterior alterations, or related new construction shall not destroy historic materials that characterize the property. The new work shall be differentiated from the old and shall be compatible with the massing, size, scale, and architectural features to protect the historic integrity of the property and its environment. New additions and adjacent or related new construction shall be undertaken in such a manner that if removed in the future, the essential form and integrity of the historic property and its environment.
10. New additions and adjacent or related new construction will be undertaken in a such a manner that, if removed in the future, the essential form and integrity of the historic property and its environment would be unimpaired.

Typical work items whose costs may be eligible for credit are:

- New roofing or repairing roofing, eaves and fascia
- Repairing deteriorated woodwork
- Stabilizing foundation and structural elements
- Repairing windows
- Restoring interior and exterior finishes
- Repairing or replacing utility services to conform to applicable code requirements

For more information contact the Department of Cultural Affairs, Historic Preservation Division:

<http://www.nmhistoricpreservation.org/PROGRAMS/creditsloans.html>

Tax Credits and Financial Incentives for Registered Properties (Statewide):

Harvey Kaplan, Tax Credit Program

harvey.kaplan@state.nm.us (505) 827-3971

Robyn Powell, Tax Credit Program

robyn.powell@state.nm.us (505) 827-4057

Federal Tax Credit

The Federal Investment Tax Credit Program allows up to 20% of qualified rehabilitation expenses to be credited against Federal Income Taxes. This program only applies to income-producing properties.

To be eligible for Federal Income Tax Incentives, a property must be listed on the National Register of Historic Properties or listed as contributing in a registered historic district.

Contact the Historic Preservation Division in Santa Fe, (505)827-6320, to find out if your property meets this requirement.

The project must be an income-producing property, not a personal residence. After rehabilitation, the historic building must be used for an income-producing purpose for at least five years. Owner-occupied residential properties do not qualify for the federal rehabilitation tax credit. Rehabilitation work must conform to the Secretary of the Interior's Standards for Rehabilitation.

The program generally allows up to 20% of the eligible costs of rehabilitation work to be credited against Federal income taxes. The credit is applied against federal income taxes owed in the year the project is completed, carried backward one year and forward for 20 years. The eligible expenses of a rehabilitation project must be at least equal to the adjusted basis of the property. The project must meet the "substantial rehabilitation test." In brief, this means that the cost of rehabilitation must exceed the pre-rehabilitation cost of the building. Generally, this test must be met within two years or within five years for a project completed in multiple phases.

Some eligible items include: Walls, ceilings, windows and doors, plumbing and fixtures, components of central air conditioning or heating systems, electrical wiring and lighting fixtures, chimneys, stairs, and other components related to the operation or maintenance of the building.

A rehabilitation project completed prior to submitting a request for "certified historic "structure" status cannot qualify for the rehabilitation tax credit. Therefore, it is important that owners apply for the rehabilitation tax credit before completing work, and preferably, well in advance of beginning work.

For more information look online at:

<http://www.cr.nps.gov/hps/tps/tax/>

<http://www.nmhistoricpreservation.org/PROGRAMS/creditsloans.html>

Or contact the Department of Cultural Affairs, Historic Preservation Division, Tax Credits and Financial Incentives for Registered Properties (Statewide):

Harvey Kaplan, Tax Credit Program

harvey.kaplan@state.nm.us (505) 827-3971

Robyn Powell, Tax Credit Program

robyn.powell@state.nm.us (505) 827-4057

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