



West Route 66

Sector Development Plan

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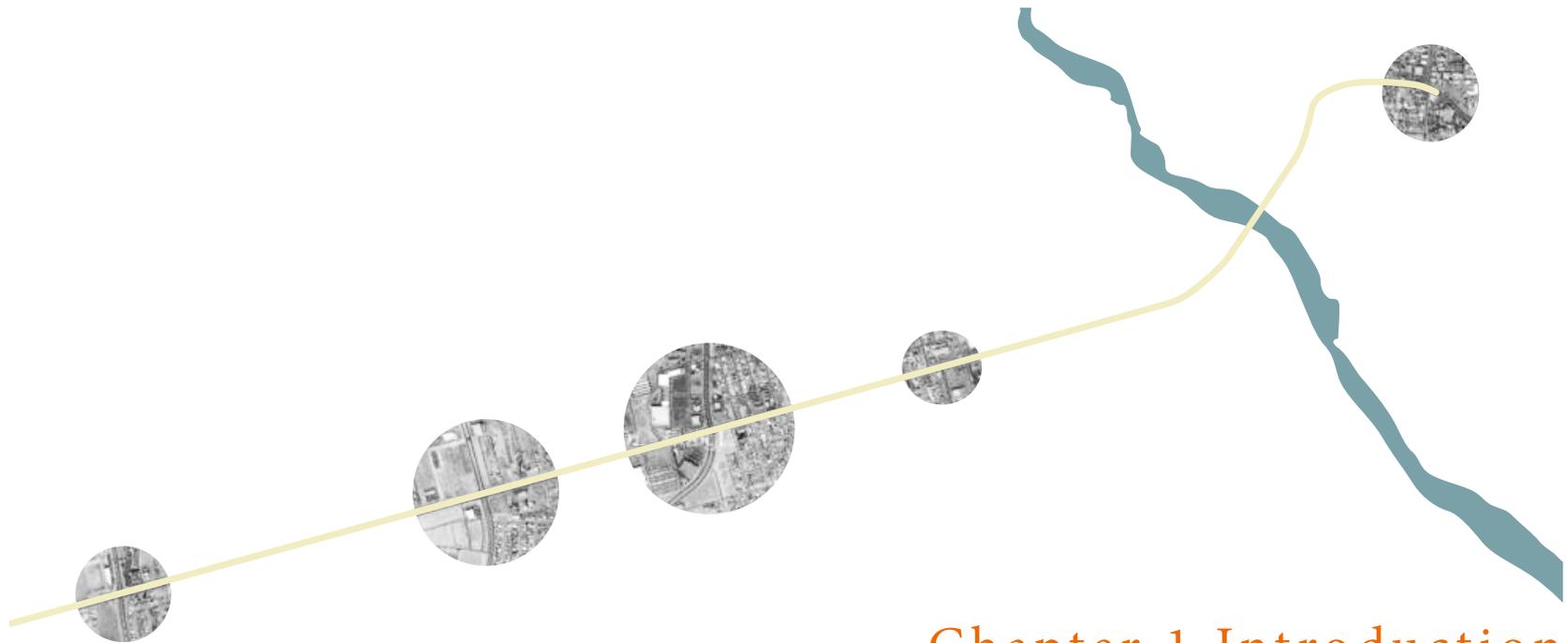
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Chapter 1 Introduction

1.0 Executive Summary

In the fall of 2010, the Albuquerque City Council and the City of Albuquerque Planning Department initiated a planning process for a new *West Route 66 Sector Development Plan* to replace the existing plan which was approved in 1987. After careful review of the existing plan, it was determined that there were enough changes to development trends, City goals and policies, and community needs to warrant a new plan for the corridor.

The goal of the planning effort was to work with the community to establish a vision for the plan area and develop the tools and strategies, such as zone changes, development and design guidelines, and streetscape and transportation improvements, to facilitate the realization of that vision.

This plan contains goals, policies, regulations, and recommendations for land use and zoning, transportation, parks, trails, open space, city services and infrastructure. These were developed based on the input that the Planning Team received from the West Route 66 community during the year and a half long planning process.

Central Avenue's designation as a major transportation corridor and its status as the City's most successful transit route played heavily in the Plan's development.

2.0 General Overview

The West Route 66 Sector Development Plan (the "Plan") encompasses roughly 6 miles of Central Ave. from the City limits at approximately 106th Street to Rio Grande Blvd. The Plan boundary generally includes properties abutting or in close proximity to Central Ave. A vast majority of these properties are commercially zoned with pockets of higher density and employment and light industrial zoning. As a designated major transit corridor, Central Ave. serves many of Albuquerque's premier destinations including the Bio Park within the Plan area and Old Town, Downtown, Presbyterian Hospital, the University of New Mexico, Nob Hill and the State Fairgrounds to the east.

The Plan area is diverse in character; the eastern portion of the Plan on both sides of the river contains denser more established development, while the area from Coors Blvd. west is characterized by larger lots, many of which are undeveloped or underutilized. The corridor has generally suffered from stagnant commercial development for the past 20 years, while single family housing development in the surrounding area has boomed, leaving a significant imbalance in the southwest area of the city between jobs and services on the one hand and housing on the other. This imbalance has forced area residents to drive long distances to meet their commercial and employment needs, causing Central Ave. in the plan area to become a commuter corridor, rather than a destination in and of itself for employment, service, retail and more diverse housing options. It is expected that if historic development trends continue, Central Avenue bridge crossings will triple from 27,500 to 90,800 by 2035.

3.0 Purpose of the Plan

The primary purpose of the plan is to provide the regulatory framework for addressing community and adopted City goals and objectives for the Central Ave. corridor. Throughout the community participation process the Planning Team heard that the cultural legacy of the corridor, including Route 66, the Hispanic agrarian traditions of the Valley, expansive views, and the eclectic and unique character of different parts of the corridor, are a source of pride and should be preserved through the planning effort. The community also stressed their need for a plan to address the significant lack of commercial uses, employment opportunities and public services. In support of adopted City policies, this Plan also seeks to ensure that development and infrastructure improvements capitalize on West Route

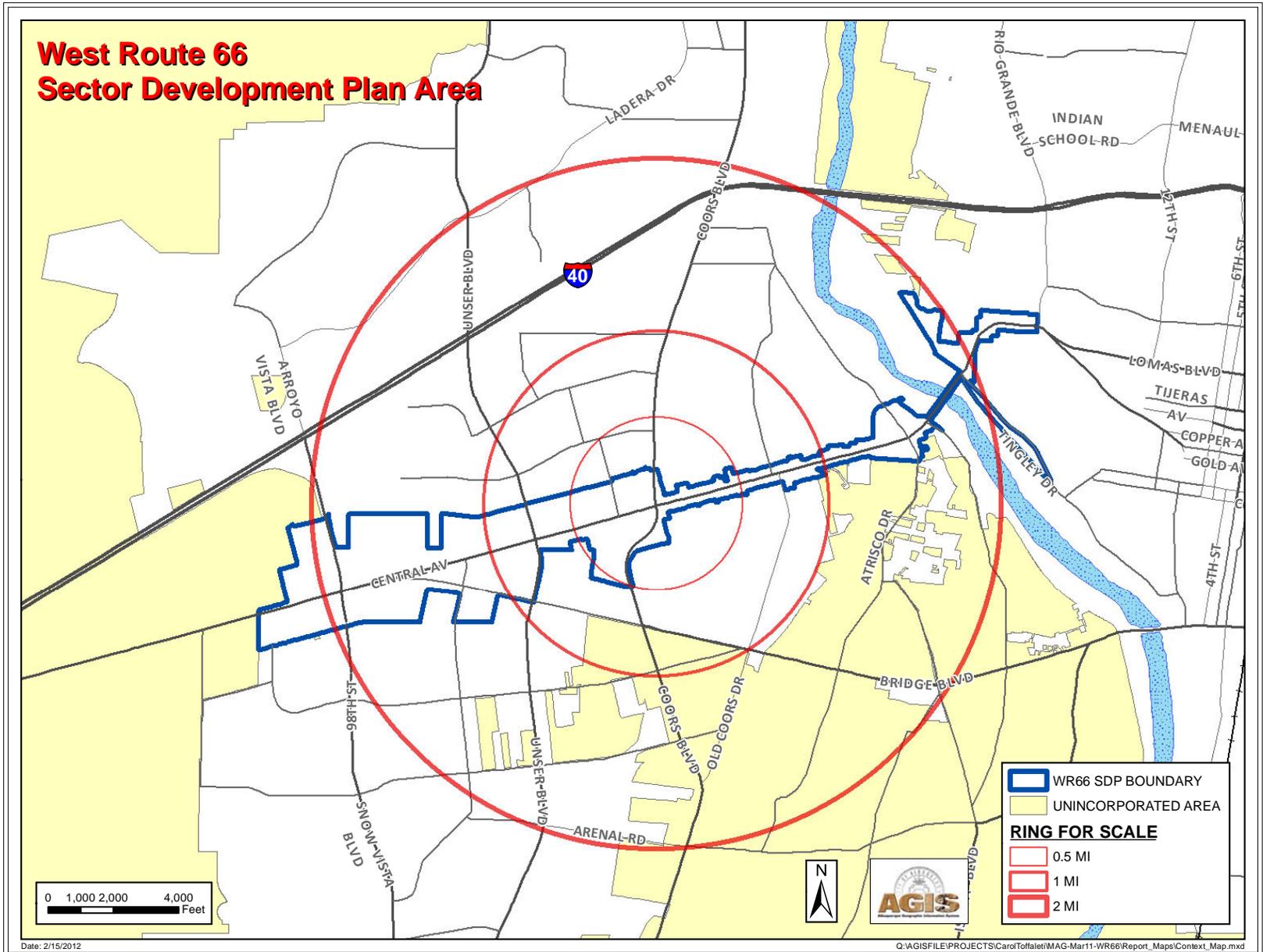


Figure 1: Sector Plan Area

Introduction

66's potential as a high capacity transit corridor, stimulating the development of needed commercial services, employment opportunities, and high quality housing options on the corridor, while recognizing and celebrating the area's cultural legacy and natural setting.

4.0 Regulatory Tool

The West Route 66 Sector Development Plan is a "Rank 3" Plan, which provides policy and regulatory guidance for development within its boundaries. Its adoption sets zoning, land use and design standards as well as policies for improvements to public services such as transportation, drainage, transit, parks and open space for the area, in order to facilitate development outcomes that meet the goals and objectives of the community. Amendments to this Plan including zone changes, text changes and clerical changes shall follow standard City process for sector development plan amendments.

5.0 Policy Guidance

In addition to the goals and objectives developed during the community visioning process, this Plan was guided by existing City policy which included the goals and policies of the Albuquerque/Bernalillo County Comprehensive Plan, the West Side Strategic Plan as well as the recommendations of other City commissioned studies and conceptual plans including the Central Avenue Streetscape Masterplan(2001), West Central Avenue Corridor Concept Plan (2010) and the Conceptual Design for Central Ave./Unser Blvd. Intersection and Adjoining Public Right of Way (2010).

6.0 Plan Layout

The West Route 66 Planning Team identified two distinct areas within the Plan Area. The western portion of the Plan Area is characterized by a high mesa landscape with expansive views of the valley below. This area has larger lots and is less developed. The eastern portion of the plan area is characterized by a valley floor and bosque landscape with more intense development on smaller parcels of land. Due to the linear nature of the Plan and the identification of these distinct districts, the Planning Team made the decision to perform analysis and display graphic information by breaking the Plan Area into two segments. Segment One starts at the western plan boundary at approximately 106th St. and ends at Coors Blvd. Segment Two starts at Coors Blvd. and ends at the eastern plan boundary at Rio Grande Blvd. In

some cases, in order to portray detailed graphic information these segments have been broken into sub-segments which are identified as Segments 1a, 1b, 2a and 2b. To accommodate the linear nature of the Plan area, the maps and written segments also move geographically from west to east through the Plan area.

7.0 Plan Contents

The Plan contains the following sections:

Chapter 2 contains the **goals and policies** for the Plan derived from both the community visioning process and adopted City policies.

Chapter 3, Context contains the **baseline information** related to transportation, infrastructure, city facilities, parks and open space, and Historic Route 66.

Chapter 4 contains the Plan's **zoning and development regulation** information including: Development Review Process, Zones, and Development Regulations Applicable to all Zones.

Chapter 5 contains **Plan Recommendations** including: Transportation Recommendations as well as recommendations for City services, parks, trails and open Space, utilities and drainage.

Chapter 6, Projects outlines projects that were identified during the Planning process. These projects further both City and community goals for the area.

Appendices contain definitions and information about the planning process. Acknowledgments

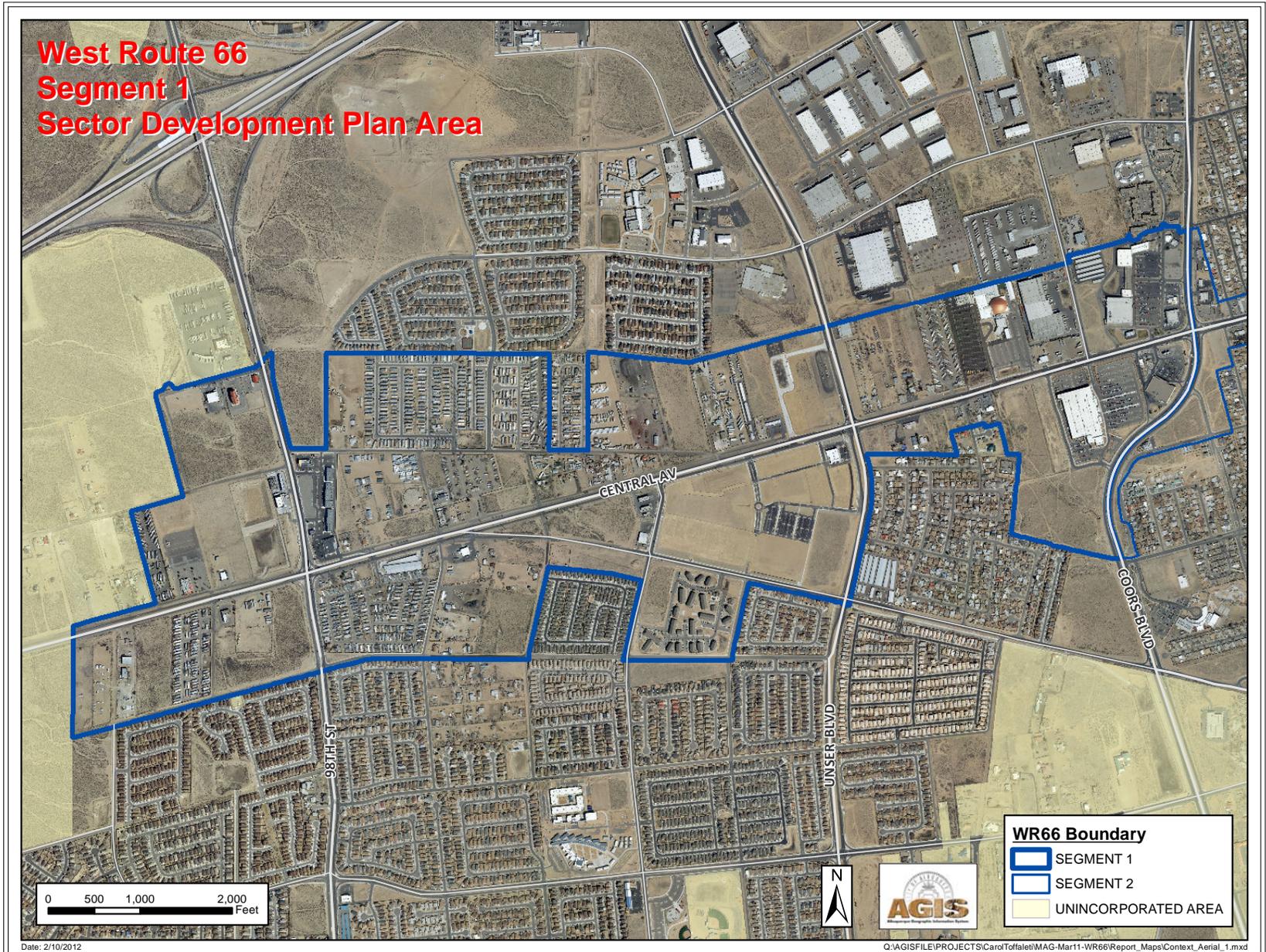


Figure 2: Segment One Aerial

Introduction

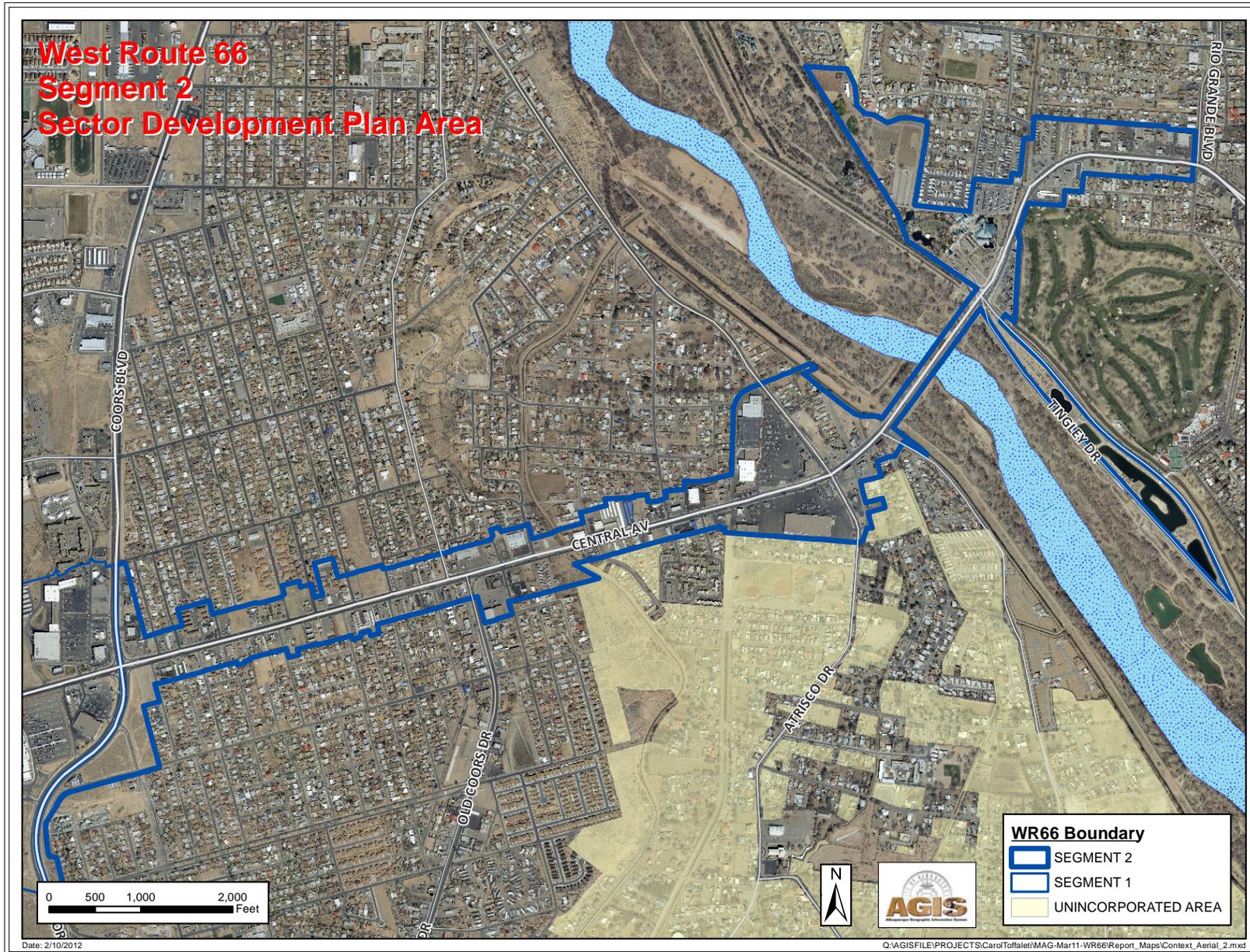
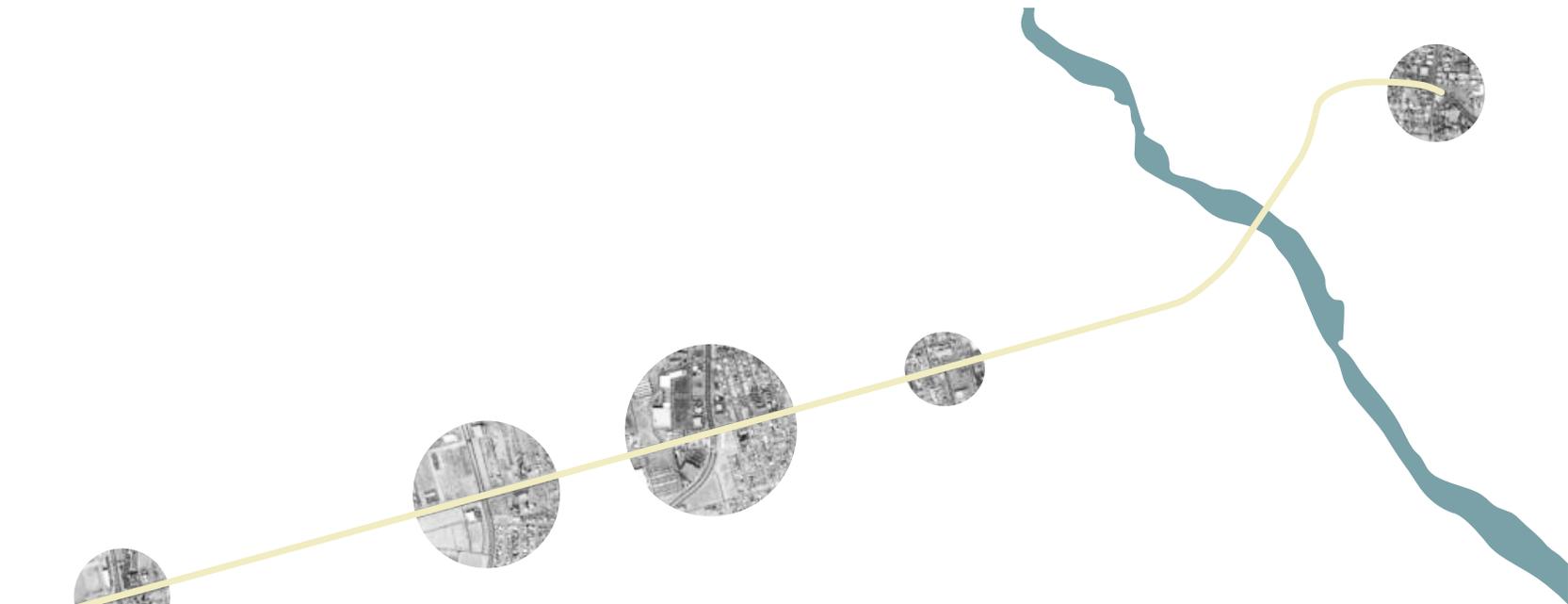


Figure 3: Segment Two Aerial



Chapter 2 Goals and Policies

Chapter Contents

This chapter contains the community goals and City policies that were used to guide the development of the Plan's regulations and recommendations.

The chapter is divided into three sections:

1.0 Community Vision, Goals and Objectives

2.0 Adopted City Policy

3.0 Conformance to City Plans

4.0 Activity Center Designations

1.0 Overview for Community Vision and Goals

Visions, goals and objectives were developed during the community visioning process for the Plan. Participants were asked to generate ideas about what they wanted West Route 66 to be like in 10 to 20 years and to consider the goals and objectives necessary to achieve their vision. The following represents the synthesis of the thoughts and ideas generated by the community on the future vision of the West Route 66 corridor.

1.1 Community Vision for West Route 66

West Route 66 will be a full service destination that celebrates its diverse cultural legacy and attracts tourists and visitors from the larger Albuquerque area while also serving the needs of local residents.

1.2 Community Goals for Land Use

1.2.1 Goal: *Improve and expand commercial, residential, employment and entertainment opportunities along the West Central Corridor to benefit neighborhood residents and to attract tourists and residents from the greater Albuquerque area.*

Objectives

- a. Attract a diversity of businesses that meet the needs of local residents as well as draw tourists and residents of the greater Albuquerque area.
- b. Identify areas that are appropriate for different types and scales of development ranging from small local businesses to large retail facilities.
- c. Encourage a diversity of housing types with a range of price values
- d. Increase employment opportunities.
- e. Attract cultural and family oriented entertainment to the area.
- f. Promote development such as recreational and cultural uses that encourage people to cross the river and bridge the historic divide of the Rio Grande.
- g. Encourage commercial activities that provide active family recreation such as gyms, batting cages, miniature golf, etc in appropriate parts of the corridor.
- h. Create incentives that encourage new development and redevelopment.
- i. Ensure that changes to land use maintain existing property rights.

1.2.2 Goal: *Promote mutually supportive land uses and minimize conflicts between less compatible land uses.*

Objectives

- a. Ensure that commercial and residential developments provide multi-modal connections to other development as well as to area destinations in order to promote mutually supportive land uses.
- b. Cluster civic uses together and make them accessible by all modes of transportation to the surrounding neighborhoods.
- c. Minimize conflicts between trucking, industrial and warehouse uses and commercial goals for the area.
- d. Encourage land use densities that support a potential Bus Rapid Transit System on the corridor.

Community Goals and Objectives

1.2.3 Goal: *Encourage land uses that support the existing open space and recreational facilities such as the Rio Grande, Bosque, the BioPark and Tingley Beach.*

Objectives

- a. Attract restaurants and services that encourage visitors to stay in the area rather than visit one destination and leave.
- b. Encourage cultural facilities, such as museums and a visitor center that celebrate Route 66 and the area's unique identity and history.
- c. Support the creation of an iconic structure, such as a tower, to overlook the Bosque.
- d. Build a Visitor Center that would draw travelers off of I-40 and encourage them to travel Old Route 66 into the City.

1.2.4 Goal: *Celebrate Historic Route 66 on the corridor.*

Objectives

- a. Encourage the adaptive reuse and preservation of registered and unregistered historic Route 66 structures and signage.
- b. Support the registration process for structures and signage through incentives such as technical assistance for preservation grants.
- c. Promote the continuance of the character of Route 66 through land use and signage regulations that allow for design elements like neon signage and iconic architecture.

1.3 Community Goals for Urban Design

1.3.1 Goal: *Preserve and enhance the unique identity of the West Central Corridor including Route 66, the New Mexican cultural legacy and the eclectic nature of existing development.*

Objectives

- a. Create a Route 66 District that features iconic structures and architecture.
- b. Incorporate wayfinding and public art elements that enhance and celebrate the existing character of the area.
- c. Create design guidelines for large and franchise retail.
- d. Encourage architectural diversity.
- e. Encourage sustainable energy use in the built environment.

1.3.2 Goal: *Protect and celebrate the distinct views from higher elevations of the Plan area to the Albuquerque Valley and the Jemez, Sandia and Manzano Mountains.*

Objectives

- a. Create height regulations that limit heights in view sensitive areas and allow greater heights in clustered and flatter areas.
- b. Promote projects that celebrate views.
- c. Acknowledge the open feeling of the West Route 66 corridor.

1.3.3 Goal: *Create pedestrian friendly activity centers and segments of commercial and residential development that encourage a "park once and walk" or "bus once and walk" strategy.*

Objectives

- a. Where appropriate, encourage development that prioritizes pedestrian use and connectivity within sites and along the corridor.
- b. Improve pedestrian connections from adjoining neighborhoods to the corridor.
- c. Improve pedestrian connectivity to and between the Bosque, the Bio Park, Old Town and other area attractions.
- d. Improve pedestrian connections to and from existing transit and future Bus Rapide Transit stops.

1.4 Community Goals for Transportation and Transit

1.4.1 Goal: *Make Central Ave. an attractive and inviting corridor that accommodates all modes of transportation including walking, biking, transit and motor vehicles.*

Objectives

- a. Improve pedestrian and bicycle safety and conditions.
- b. Identify areas along the West Central corridor that are more appropriate for certain modes of travel (pedestrian, transit and vehicular) and support these areas with design modifications and land use.
- c. Use traffic calming techniques in areas with high pedestrian volumes.
- d. Reduce noise pollution by slowing vehicular traffic.
- e. Reduce traffic congestion.
- f. Provide more street trees and landscaping.
- g. Install pedestrian oriented street lights
- h. Widen sidewalks where right-of-way (ROW) allows.
- i. Reduce traffic speeds.
- j. Limit Central to four driving lanes.
- k. Create more pedestrian friendly intersections at Activity Centers.
- l. Improve Rio Grande Bridge and/or build a parallel pedestrian and bicycle bridge to acknowledge the significance of the crossing.

1.4.2 Goal: *Improve transit service in the plan area by increasing frequency and efficiency of service and by improving the perception of transit.*

Objectives

- a. Support Bus Rapid Transit through the development of pedestrian friendly streets, a diversity of housing types and a diversity of businesses and employment opportunities.
- b. Expand Park and Ride facilities along Central.
- c. Provide benches, shelter and lighting with iconic Route 66 identity at bus stops.
- d. Provide transit service that connects commercial and residential areas, including neighborhood circulators.

1.5 Community Goals for Parks, Open Space and Trails

1.5.1 Goal: *Realize the full potential of the BioPark as an area destination and a gateway to the Bosque Open Space that serves both tourists and Albuquerque residents.*

Objectives

- a. Improve and formalize access from the BioPark to the adjacent Bosque.
- b. Encourage more BioPark interaction with the community through educational activities such as guided nature walks in the Bosque.
- c. Remove security fencing around the BioPark on Central Ave.

1.5.2 Goal: *Create more opportunities for active recreation.*

Objectives

- a. Encourage the development of neighborhood scale sports facilities.
- b. Encourage the development of neighborhood parks near higher density residential areas.

1.5.3 Goal: *Ensure that the Bosque, drains, ditches and canals are being utilized in ways that best benefit the community.*

Objectives

- a. Improve coordination between agencies: Army Corps of Engineers, Middle Rio Grande Conservancy District, and City Open Space.
- b. Improve recreation opportunities for trails adjacent to the River on land controlled by MRGCD.
- c. Protect Open Space through sustainable management techniques, including adequate fire protection.
- d. Ensure regular maintenance of canals and ditches, including trash and weed removal.
- e. Create and improve pedestrian and bike trails along canals and ditches.
- f. Ensure good connections to recreational amenities such as the Bosque, the BioPark and Tingley Beach.
- g. Improve trail linkages within the Plan area as both a means of transportation and a recreational amenity.

Community Goals and Objectives

1.6 Goals for Drainage & Utilities

1.6.1. Goal: *Improve area drainage and sewer infrastructure and on-site drainage management to prevent flooding and address environmental concerns.*

Objectives

- Encourage on-site management of stormwater through low impact design techniques.
- Where appropriate, design parks, medians, sidewalks and rights-of-way to function as both landscaping and/or recreation and stormwater drainage.
- Improve area sewer infrastructure to function at required capacity and eliminate odors

1.6.1. Goal: *Minimize visual impact of utilities.*

Objectives

- Encourage the location of utility service lines to be underground.
- Locate utilities, lighting and fire hydrants and other infrastructure outside of pedestrian realm.
- Encourage utility location to protect significant views.
- Develop appropriate locations and design standards for cell towers.

2.0 Policy Overview

The City of Albuquerque has three ranks for plans that are intended to guide development. Higher ranking plans contain general City-wide policy guidance and lower ranked plans contain more detail for specific areas. Lower-ranking plans must be consistent with higher-ranking plans. The highest-ranking plan, the Albuquerque/Bernalillo County Comprehensive Plan is designated as a Rank One plan. Facility and Area Plans are Rank Two plans. Sector Development Plans that address zoning and development concerns for individual neighborhoods and corridors are Rank Three plans.

The West Route 66 Sector Development Plan is a Rank Three Plan. The policies, regulations, and recommendations contained within are guided by and consistent with adopted higher-ranking plans including the Rank One Albuquerque/Bernalillo County Comprehensive Plan and the Rank Two West Side Strategic Plan.

2.1 Comprehensive Plan Policies

The West Route 66 Sector Development Plan falls within Established Urban and Developing Urban areas of the Comprehensive Plan. The Plan recognizes that West Route 66 is a very diverse section of Albuquerque that includes areas of high development intensity as well as areas of larger tracts of undeveloped land.

The Comprehensive Plan contains many goals and policies that are relevant to the WR66 Plan; however the goals and policies that are the most pertinent to guiding the development of this Plan are those regarding Central Ave. as an Enhanced Transit Corridor and the development of Activity Centers along this corridor. These goals and policies are summarized below.

Community Resource Management Goal 4: Transportation and Transit

Develop corridors, both streets and adjacent land uses, that provide a balanced circulation system through efficient placement of employment and services, and encouragement of bicycling, walking, and use of transit/paratransit as alternatives to automobile travel, while providing sufficient roadway capacity to meet mobility and access needs.

Policy a: Objectives for Enhanced Transit Corridors include providing an entrance from the street, setbacks should be provided only to include landscaping strips and pedestrian activity areas, parking should be located either

behind or to the side of the building and parking reductions should be encouraged, and density targets are floor area ratios of 0.5 to 1.5 and 7-30 dwelling units per acre.

Land Use Goal 7: Activity Centers and Transportation Corridors (“Centers and Corridors”)

Expand and strengthen concentrations of moderate and high-density mixed land use and social/economic activities which reduce urban sprawl, auto travel needs, and service costs, and which enhance the identity of Albuquerque and its communities.

Policy a: Existing and proposed Activity Centers are designated by the Comprehensive Plan to help shape the built environment in a sustainable development pattern, create mixed use concentrations of interrelated activities that promote transit and pedestrian access both to and within the Activity Center, and maximize cost-effectiveness of City services.

The intensity of development and types of land uses within activity centers depends on their classification as either a major activity center, a community activity center, a neighborhood activity center or a special activity center. A Major Activity Center should provide the most highly concentrated locations of commercial, service and employment uses in conjunction with area-wide needs. Community Activity Centers should provide a variety of commercial and entertainment uses in conjunction with community-wide services, civic land uses, employment, and the most intense land uses within the community sub-area. Neighborhood Activity Centers should provide for the daily service of convenience goods and personal services for the surrounding neighborhoods; they should serve as the social and recreational focal point for, and be accessible from, all surrounding residential neighborhoods. Special Activity Centers should provide locations for unique attractions serving local, regional and statewide needs. All activity centers should develop in a manner that supports a range of transportation choices including walking, biking, transit and vehicular use.

City Policy Framework

2.2 Rank 2 Plans

The following Rank 2 Plans provide guidance for the West Route 66 Plan area:

1. West Side Strategic Plan/Southwest Albuquerque Strategic Action Plan
2. Trails and Bikeways Facilities Plan
3. Bosque Action Plan
4. Facility Plan for Electric Service Transmission Facilities

All of these Rank 2 plans have provided guidance for the development of the West Route 66 Plan's goals and policies. The most relevant of these plans regarding land use strategies is the West Side Strategic Plan/Southwest Albuquerque Strategic Action Plan (WSSP/SWASAP). The following goals from the WSSP/SWASAP were specifically considered in the writing of this plan:

Goal 1. Build Complete Neighborhoods and a Network of Activity Centers to Serve Them;

Goal 4. Increase and Improve Retail and Commercial Services and,

Goal 5. Develop a Complete Multi-Modal Transportation Network

2.4 Rank 3 Plans

There are a number of Rank 3 Sector Development Plans that share a boundary or overlap with the West Route 66 Plan. These plans were reviewed and development recommendations in this Plan were developed with consideration to these shared boundaries. These plans include:

1. Coors Corridor Plan
2. Rio Grande Blvd. Corridor Plan.
3. Huning Castle and Reynolds Addition Sector Development Plan
4. Old Town Sector Development Plan

The West Central Metropolitan Redevelopment Area Plan overlaps the West Route 66 Plan area. Recommendations for redevelopment contained in that plan have been incorporated into the West Route 66 Plan.

2.5 Other Plans

In addition to the City's ranked plans, a number of other plans influenced the development of the Plan's policies and recommendations. These plans include:

1. Central Ave. Streetscape Urban Design Master Plan (2001)
2. West Central Ave. Corridor Concept Plan (2010)
3. Atrisco Business Park Master Plan
4. Conceptual Design for Central Ave/Unser Blvd. Intersection and adjoining Public Right of Way (2010)
5. 2035 Metropolitan Transportation Plan (2011)

3.0 Conformance to Applicable City Plans

The following analysis explains how the West Route 66 Sector Development Plan is both guided by and supportive of the goals and policies of the applicable higher-ranking plans. With its adoption, the West Route 66 Sector Development Plan establishes land use, design and development standards for the area to ensure development outcomes that are more predictable for the community and affected stakeholders, while at the same time are supportive of larger City and community goals and policies. The parentheses in this section reference highlighted applicable policies and regulations that are furthered by adoption of the Plan and identified on page 16.

3.1 Promoting Activity Center Development

Consistent with applicable City plans, the West Route 66 Sector Development Plan promotes complete communities and neighborhoods by encouraging the development of existing and new activity centers and transportation corridors. The Plan creates zoning which supports existing activity center designations. The Plan also designates a new Major Activity Center to support the need for regional commercial, employment and housing opportunities (1).

The Plan concentrates related service, housing, employment and commercial uses within these activity centers by allowing a higher intensity of uses within a pedestrian oriented setting, which is an efficient, cost-effective, and sustainable way of meeting the daily needs of the community (2). The Plan also promotes the development of transportation corridors to connect activity centers, both to each other and to the larger community, as an essential building block for creating sustainable, efficient and livable communities (3).

According to applicable City plans, activity centers should provide for the daily commercial, service, employment and housing needs of the community. The intensity of development and land uses within activity centers depends on their classification as either major, community or neighborhood activity center(4).

3.2 Promoting Multi Modal Development

The Plan is consistent with applicable City plans by promoting the use of various modes of transportation along transportation corridors that connect activity centers to each other and to the larger community. It does this through transportation

improvement recommendations that encourage pedestrian and bicycle use where appropriate, while also accommodating vehicular and transit use (5).

From the City limits to Atrisco Drive, Central is designated as an Enhanced Transit Corridor; from Atrisco Drive to Rio Grande Boulevard, Central Ave. is designated a Major Transit Corridor. The intensity of development, the design, and land uses along transportation corridors depend on their classification as Major Transit Corridor or Enhanced Transit Corridor (6). Major Transit Corridors generally have greater intensity of land development than Enhanced Transit Corridors; Major Transit corridors should allow for the transportation of a larger volume of people and encourage a greater variety of travel modes (vehicular, transit, pedestrian and bicycle) compared to an Enhanced Transit Corridor (7). This Plan complies with the Comprehensive Plan's policies for transit corridors through the establishment of transportation regulations that provide for multi-modal travel, as well as the land use and design regulations that promote high quality, higher density development, in order to support existing transit as well as future high capacity transit services.

According to applicable City plans, all activity centers and transportation corridors should be designed to promote environments that are safe, comfortable and visually pleasing for people walking in and around the centers and along the corridors (8). This Plan furthers these objectives through street and development design improvements.

3.3 Promoting Context Sensitive Development

In addition, applicable adopted City plan policies for activity centers and transportation corridors encourage design that encourage development that promotes community character and identity (9). This Plan encourages the preservation and creation of area character and identity through recommendations such as: creation of a frontage road multi use trail celebrating Historic Route 66, the creation of the West Route 66 Major Activity Center and its associated design standards, the preservation of historic structures and signage, land use regulations that allow for iconic signage and architecture, regulations that control building heights and setbacks in order to preserve corridor views, and recommendations that promote and protect the multiple values of the Rio Grande and Bosque, where they intersect Central Ave..

City Policy Compliance

According to applicable City plans, zoning should encourage development of desired land uses and design within designated centers and corridors (10). The zoning regulations of the Plan reflect these policies. This Plan promotes development that will further the success of designated activity centers and of Central Ave. as a designated transit corridor by providing both the land use regulations that concentrate densities and development intensities in centers and the transportation improvements that create a safe, attractive and efficient multi-modal corridor.

Policy references:

Note: Albuquerque/Bernalillo County Comprehensive Plan ("Comprehensive Plan"); West Side Strategic Plan/Southwest Albuquerque Strategic Action Plan ("WSSP/SWASAP"); 2035 Metropolitan Transportation Plan ("2035 MTP")

1	WSSP/SWASAP Goal 1: Build Complete Neighborhoods and a Network of Activity Centers to Serve Them; 2035 MTP , Chapter 3, p. 9
2	Comprehensive Plan, Land Use Goal 7: Activity Centers and Transportation Corridors policies a, c, and d. WSSP/SWASAP, Goal 1. Build Complete Neighborhoods and a Network of Activity Centers to Serve Them and Goal 2. Provide Convenient Public Services. 2035 MTP, Chapter 5, p. 14
3	Comprehensive Plan, Environmental Protection and Heritage Conservation Goal 9: Community Identity and Urban Design, policy e. WSSP/SWASAP, Goal 3. Plan and Build a Complete Interconnected System of Public Parks, Trails, and Major Public Open Space and Goal 5. Develop a Complete Multi-Modal Transportation Network. 2035 Metropolitan Transportation Plan, Chapter 3, p. 9
4	Comprehensive Plan, Environmental Protection and Heritage Conservation Goal 9: Community Identity and Urban Design, policy c. WSSP/SWASAP, Goal 1. Build Complete Neighborhoods and a Network of Activity Centers to Serve Them and Goal 2. Provide Convenient Public Services
5	Comprehensive Plan, Community Resource Management Goal 4: Transportation and Transit Policy a. WSSP/SWASAP, Goal 5. Develop a Complete Multi-Modal Transportation Network and Goal 3. Plan and Build a Complete Interconnected System of Public Parks, Trails and Major Public Open Space. 2035 MTP, Chapter 5, p. 14 and Chapter 5, p. 10
6	Comprehensive Plan, Community Resource Management Goal 4: Transportation and Transit Policies a, b and c
7	Comprehensive Plan, Community Resource Management Goal 4: Transportation and Transit Policy a. 2035 MTP, Chapter 3, p. 40; Executive Summary, p.2; Chapter 2, p. 7; Chapter 3, p. 5; Chapter 5, p. 11; and Chapter 2, p. 27
8	Comprehensive Plan, Community Resource Management Goal 4: Transportation and Transit Policy a; Land Use Goal 7: Activity Centers and Transportation Corridors policy a; and Environmental Protection and Heritage Conservation Goal 9: Community Identity and Urban Design, policies d and e. WSSP/SWASAP, Goal 5. Develop a Complete Multi-Modal Transportation Network; Goal 1. Build Complete Neighborhoods and a Network of Activity Centers to Serve Them. Goal 4; Increase and Improve Retail and Commercial Services; and Goal 3. Plan and Build a Complete Interconnected System of Public parks, Trails and Major Public Open Space. 2035 MTP, Chapter 3, p. 18; Chapter 3, p. 54; and Chapter 2, p. 26
9	Comprehensive Plan Land Use Goal 7: Activity Centers and Transportation Corridors, policy a; Environmental Protection and Heritage Conservation Goal 9: Community identity and urban design, policies c, d, and e; Land Use Goal 1: Open Space Network, policies b and c; 2035 MTP, Chapter 5, p. 19
10	Comprehensive Plan Land Use Goal 7: Activity Centers and Transportation Corridors, policy a; Environmental Protection and Heritage Conservation Goal 9: Community identity and urban design, policies c and d; and Community Resource Management Goal 4: Transportation and Transit, policies b and c. WSSP/SWASAP Goal 1. Build Complete Neighborhoods and a Network of Activity Centers to Serve Them; Goal 2. Provide Convenient Public Services; and Goal 4. Increase and Improve Retail and Commercial Services

4.0 Activity Center Overview

The Activity Center designations are intended to drive land use and design policies in order to create regional, community or neighborhood wide destinations that are accessed by a range of transportation modes including vehicular, transit, biking and walking. Often this means locating residential and commercial uses in close proximity to each other and with an urban design that supports real street life. As stated by the Comprehensive Plan, the goal for activity centers is

“... to expand and strengthen concentrations of moderate and high-density mixed land use and social/economic activities which reduce urban sprawl, auto travel needs, and service costs, and which enhance the identity of Albuquerque and its communities.”

The intent of the Major Activity Center (MAC) designation is to concentrate community service and employment uses in a center with a regional draw. Special Activity Centers (SAC) provide locations for unique attractions that serve a regional area. Community Activity Centers (CAC) contain entertainment, commercial and service uses for a community area. Neighborhood Activity Centers (NAC) provide daily services for surrounding neighborhoods. For more information on the characteristics and criteria for different types of Activity Centers, see the Comprehensive Plan.

In the case of the activity centers located within the Plan area, the designations have not yet achieved the larger community vision for these areas, and in some cases the existing zoning makes it very difficult to achieve.

The Comprehensive Plan (Comp Plan) designates Community Activity Centers at Unser Blvd., Coors Blvd. and Atrisco Dr., and, a Major Activity Center at the Atrisco Business Park, of which only the southernmost portion lies within the plan boundary. (Note that the MAC and the business park have the same name, but not the same boundaries, see Appendices page xx.)The West Side Strategic Plan (WSSP) designates a Community Activity Center at Unser Blvd. and Coors Blvd., and Neighborhood Activity Center. Neighborhood Activity Centers at Central and 98th St., Coors Blvd., and Atrisco Dr.

The following are the activity centers, proposed and existing, in the Plan area:

Activity Center Name	Acreage
Central / 98th NAC (WSSP)	23
Central/Unser CAC (Comp Plan and WSSP)	71
Proposed West Route 66 MAC	350 (28.5 previously in ABP)
Atrisco Business Park(ABP) MAC (Comp Plan)	545 (employment center)
Central/Coors CAC (WSSP)	48
Central/Coors NAC (WSSP)	xx
Central & Atrisco CAC (Comp Plan)	54
Proposed SAC	42

4.1 New Activity Center Designations

During the planning process, activity center designations were analyzed in relation to land use and transit objectives for the corridor, resulting in the following proposals:

4.1.1 West Route 66 Major Activity Center

There is currently only one Major Activity Center in the SW quadrant of the City, the Atrisco Business Park MAC. The Atrisco Business Park however, does not function as a Major Activity Center. A typical MAC should function as a regional destination for retail, employment and housing opportunities within the context of a multi-modal setting. The Atrisco Business Park MAC, instead, contains a checkerboard of light industrial uses, punctuated by some housing and limited commercial uses. Much of it remains vacant.

This Plan is recommending that a new Major Activity Center designation be placed on the West Route 66 corridor between approximately 86th St. and Coors Blvd. An analysis performed by the Planning Team found that the two Community Activity Centers located at Central Ave. and Unser Blvd. and at Central Ave. and Coors Blvd. were in such close proximity that it made more sense to combine them into a single, larger Major Activity Center. The portion of the Central/Coors NAC near the corridor would also be incorporated in the new MAC. The remainder of the NAC is oriented toward the residential neighborhood to the south. In addition, the analysis performed in the retail market study commissioned for the Plan, found that this area needs more residential and employment development in order to support a wide range of commercial development. This new Major Activ-

Activity Center Designations

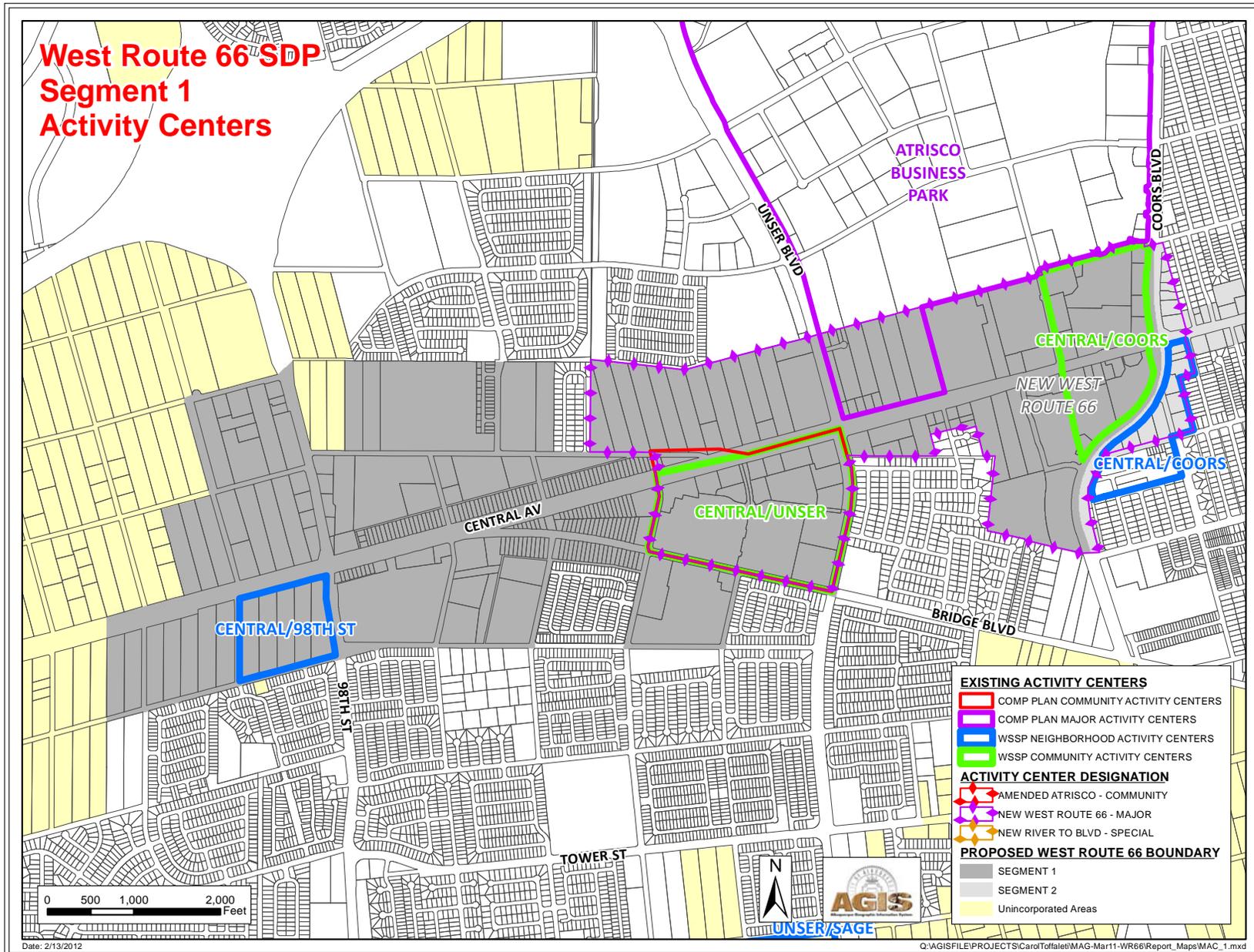


Figure 4: Activity Centers, Segment One

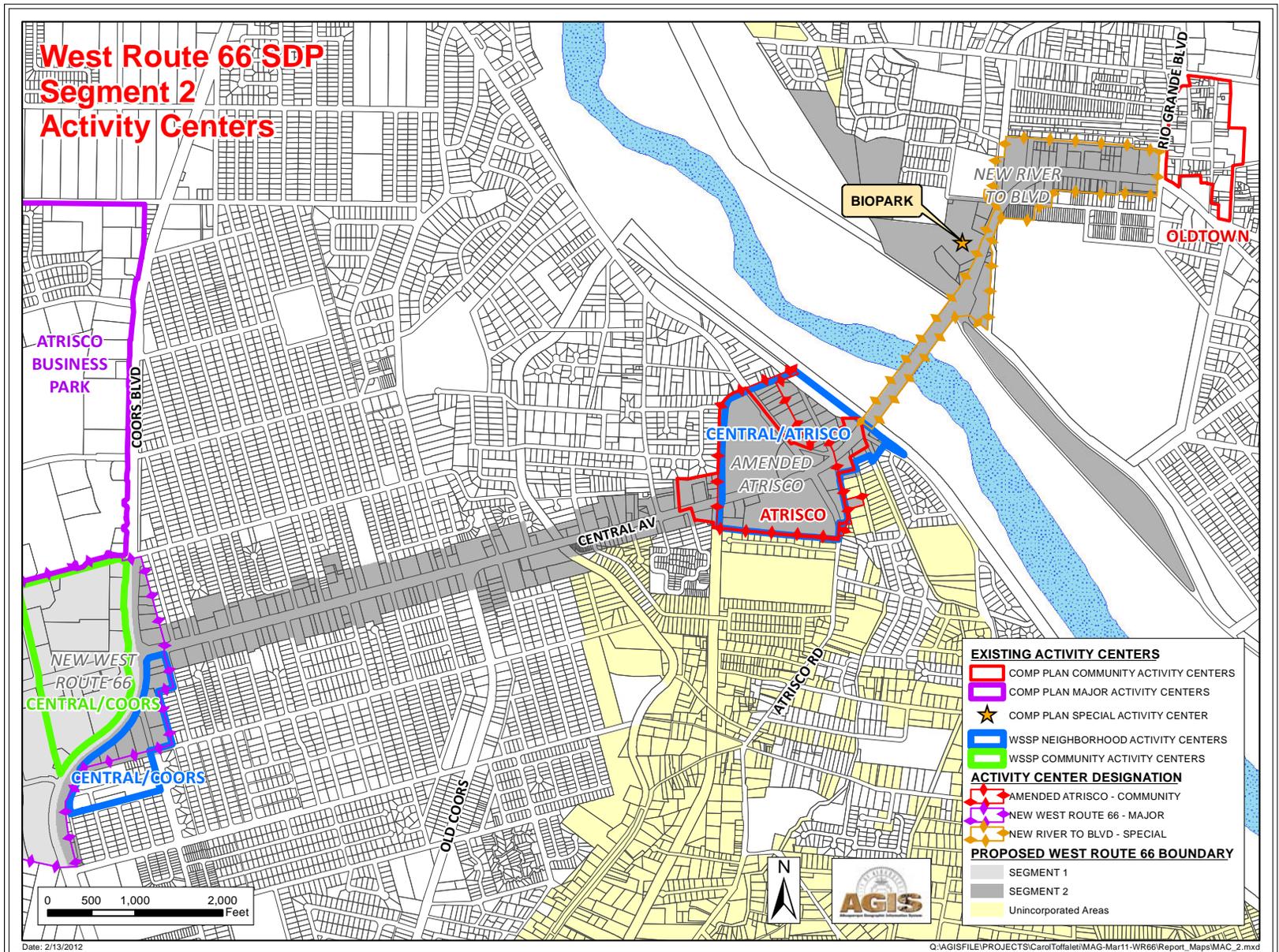


Figure 5: Activity Centers, Segment Two

Activity Center Designations

ity Center would provide a regional draw to the area and create the opportunity for a high quality mixed use center that draws on the unique Route 66 identity of the area through a “live, work, play” approach to development.

4.1.2 Special Activity Center

A new Special Activity Center is proposed by the Plan between the river and Rio Grande Blvd. This new designation is an opportunity to create a distinctive district of land uses that support the existing amenities in the area such as Old Town, the BioPark, Tingley Beach and the Bosque. The Center is envisioned as a pedestrian oriented environment with supporting retail and service uses such as restaurants, gift shops and hotels. Providing these types of services in close proximity to the amenities in the area will encourage visitors to walk between facilities and perhaps spend the day in the area rather than at just one location. It is important in the development of this district to embrace a unifying identity for the area. This can be accomplished through unified streetscape improvements and the consistent use of urban design elements such as adobe street walls and pocket gardens.

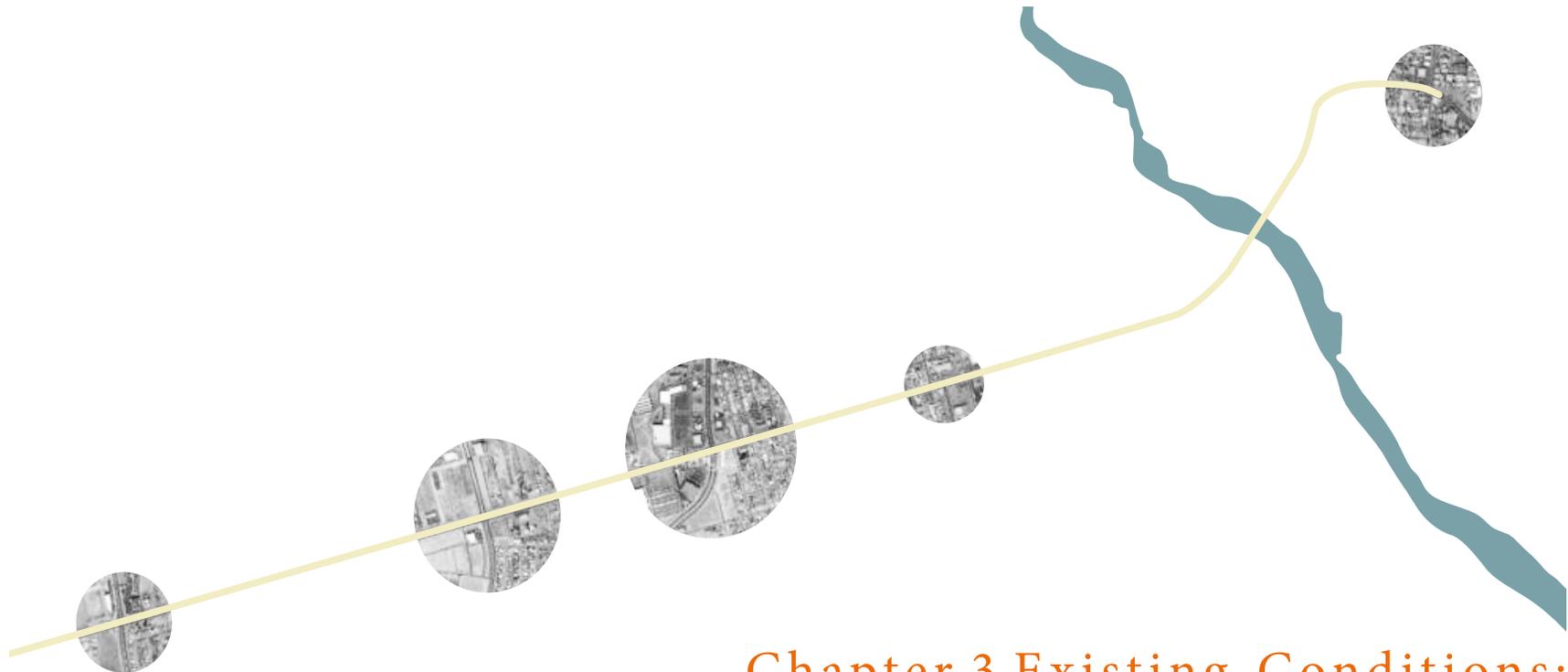
4.1.3 Atrisco Community Activity Center

The Plan proposes to concentrate this center at the crossroads of Central and Atrisco and to acknowledge its function as a community activity center, per policy in the Comprehensive Plan. Areas have been removed because the corridor west of the Arenal Canal is more characteristic of a “main street,” while the area east of 40th street should be allowed to capitalize on its proximity to acequias, river and Bosque.

The Activity Center map shows the designated activity centers adopted by the Comprehensive Plan and the West Side Strategic Plan as well as activity centers which are proposed by this plan.

4.2 Activity Center Designation Process

The Activity Center designations entail changes to the West Side Strategic Plan, which were addressed in conjunction with adoption of this Plan. The designations for the Major and Special Activity Centers are also reflected in a wider revision to the Albuquerque/Bernalillo County Comprehensive Plan being undertaken by the City and County.



Chapter 3 Existing Conditions: Issues and Opportunities

This chapter contains all the baseline information for the Plan including analysis of existing conditions related to the following areas:

- 1.0 Land Use and Zoning
- 2.0 West Route 66 Market Study
- 3.0 Demographic Study
- 4.0 Metropolitan Redevelopment Areas
- 5.0 Historic Route 66
- 6.0 Transportation
- 7.0 City Facilities and Services (Fire, Police, Libraries and Community Centers)
- 8.0 Water and Sewer
- 9.0 Drainage
- 10.0 Utilities
- 11.0 Parks, Trails and Open Space

Existing Conditions: Land Use and Zoning, Segment One

1.0 Land Use Overview

The West Route 66 Sector Development Plan includes approximately 1,000 acres and six miles of Central Ave. The Plan area generally includes parcels north and south of Central Avenue between 106th Street and Rio Grande Boulevard, with the main exceptions at the intersections with Bridge Boulevard and Volcano Road where the boundary expands to follow those roads. West Central Avenue, formerly Route 66, has historic significance to area development, and is the western gateway to Albuquerque.

Development patterns along Central Ave. in the Plan area range from older, fine-grained development between Old Coors to Rio Grande Blvd., to vast areas of undeveloped land toward the western plan boundaries. New commercial development has recently sprung up adjacent to major intersections west of Old Coors Rd. in response to a housing boom on the Southwest Mesa in the last decade.

During the community participation process, specific issues and challenges related to existing land uses were raised, including: a predominance of auto repair, tire shops and other auto-related businesses located on the corridor, an emerging conflict between trucking uses and area commercial goals, and a lack of retail services that are easily accessible by multiple modes of transportation in the Plan area.

1.1 Segment One Land Use Analysis

Segment One includes parcels primarily fronting Central Ave. from the City limits at 106th street to Coors Boulevard. Lot sizes tend to be large (at least 4 acres) and deep (1,000 feet). With the exception of small lots fronting Central Ave. and Bridge St. between 98th and 86th St. Access to these deep lots is limited since they generally front on Volcano or Central Ave. only. Development on the western fringe of the City is sparse and consists of mobile homes, a dog kennel, a few fast food restaurants and vacant land. There is new commercial development at Central Ave. and 98th St. to serve the area's housing boom. Two neighborhood shopping centers, constructed within the past five years, are located on the north side of Central Ave. and offer neighborhood goods and services. There are some vacant spaces in these centers. Development continues to be sparse between 98th St. and Unser Boulevard, with large tracts of undeveloped land. Land uses in this stretch of Central Ave. consist primarily of mobile homes, self-storage, a campground and a few motels and miscellaneous businesses.

Exiting Conditions: Land Use and Zoning, Segment One



Figure 6: Newer strip commercial at 98th



Figure 7: Unser Crossing

The intersection of Unser Blvd. and Central Ave. has seen recent development attention. Unser Crossing is an approximately 50 acre planned commercial center located on the southwest corner of Central Avenue and Unser Boulevard. In 2008, the developer obtained a zone change from SU-1 for C-2 (10 acres), O-1, and PRD 2-DU/acre to C-2. The applicant applied for and obtained a zone change because the SU-1 zoning established by the former West Route 66 Sector Development Plan limited the commercial on the entire 50 acre site to 24 acres of commercial. The site development plan for Unser Crossing includes two large anchor retail facilities, a health club and several smaller retail shops. The infrastructure for the center, including internal roads, curb and gutter, storm drains, sidewalks and lighting, has been built. However the center is largely vacant; only a CVS Pharmacy has been built.

As noted in the market analysis performed for this Plan, it is likely that once the effects of the national housing crisis subside and funding for commercial development becomes more accessible, this center will be one of the first in the area to develop.

On the north west corner of Unser and Central, the City has begun development of a 13 acre tract, which is home to a transit park and ride facility. The remainder of the property is undeveloped. City plans for the rest of the site include a library and commercial development that meets the requirements. Northeast of the park



Figure 8: Transit park and ride site

and ride, on a separate lot, is the recently opened University of New Mexico health clinic. As a testament to the need for health care services on Albuquerque's West Side, the clinic was at capacity and no longer accepting new patients soon after opening.

From Unser Boulevard to Coors Boulevard, development is again sparse and limited to mobile home parks, storage facilities, a large church campus, the Verizon call center, a small range of auto related shops and services and large parcels of vacant

land. The development at the intersection of Coors Blvd. and Central Ave. provides goods and services for nearby residential areas with two supermarkets, and several banks and fast food restaurants. These shopping facilities provide the only opportunities for the purchase of daily goods and services within a 1.5 mile area.

Summary of large parcel land use in Segment 1 (over 5 acres)

- Shopping centers at 98th and Central (north side)
- Unser Crossing shopping center (pad ready, one pad developed)
- Legacy Church site
- Shopping centers at Coors and Central

1.2 Segment One Zoning Analysis

Prior to the adoption of the 1987 West Route 66 Sector Development Plan, the zoning for Segment One along Central Avenue included residential, commercial, industrial and mobile home. The zoning on these lots changed parcel by parcel, creating a discontinuous land use pattern. At the time of the adoption of the 1987 plan, zone changes had been granted in this area for C-2 zoning. The 1987 plan removed mobile home and residential zoning from the properties fronting Central Ave., replacing it with IP and C-2, reflecting several zone change requests.

In 2009, an amendment to the 1987 plan was adopted, creating two new optional zoning designations, Community Activity Center and Neighborhood Activity Center, to “concentrate moderate density mixed land use and social and economic activities to reduce urban sprawl, auto travel needs, and service costs and enhance the identity of Albuquerque and the southwest Albuquerque Community.” The new zones encourage mixed land use, create minimum density requirements, and established site requirements for areas designated Community or Neighborhood Activity Centers within the plan boundary by the Comprehensive Plan.

Since 2000, several zone change requests have been made to either add commercial uses to the IP zone or to change zoning from IP to C-2, whereas only one request was made to change a C-2 zone to an IP zone to accommodate a warehouse and an office. Based on the recent development in Segment One, the requests for zone changes and community concerns regarding the lack of commercial services, the 1987 zoning does not appear to be fulfilling the land use demands of the current market.

Existing Conditions: Land Use and Zoning, Segment One

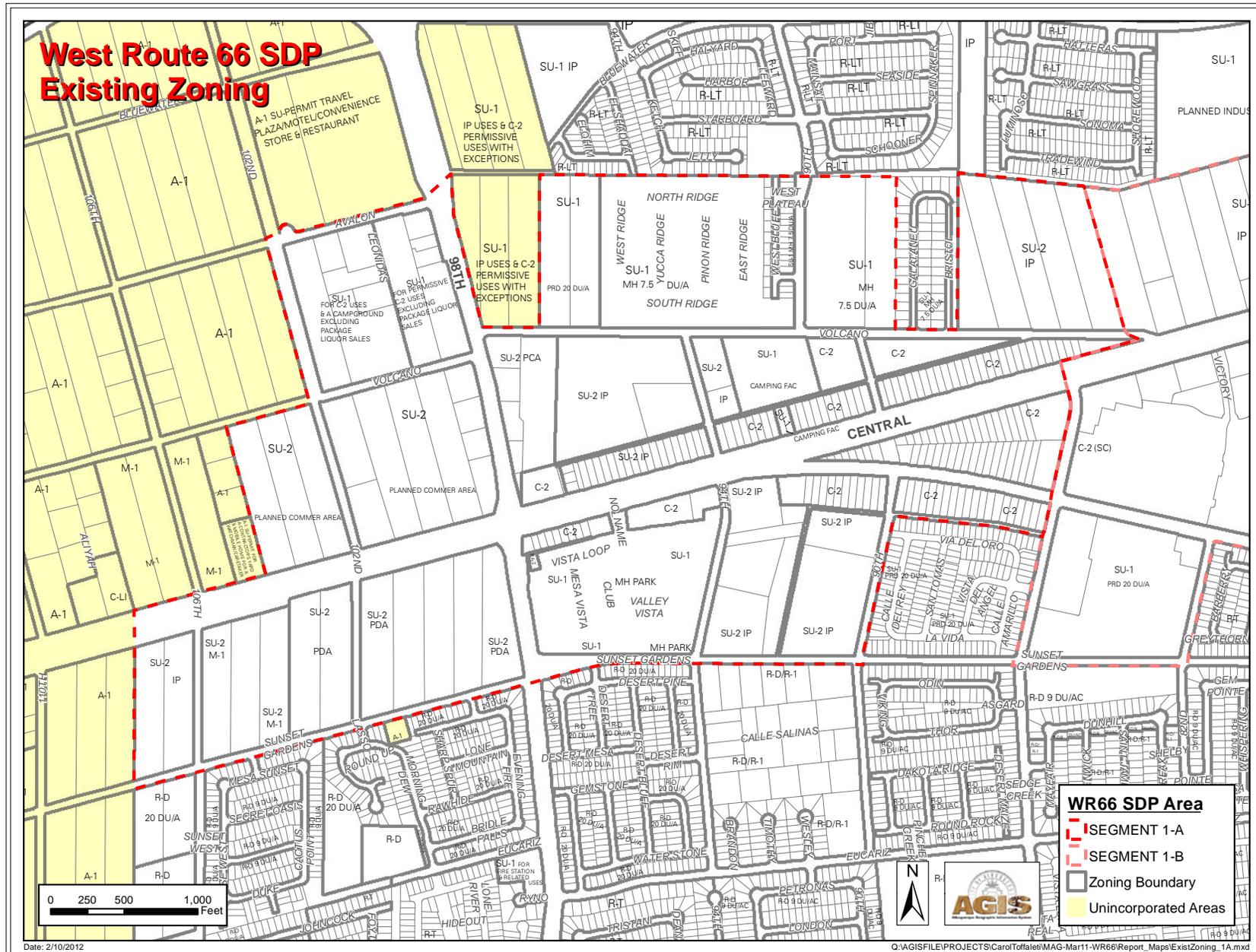


Figure 9: Existing Zoning, Segment One

Existing Conditions: Land Use and Zoning, Segment One

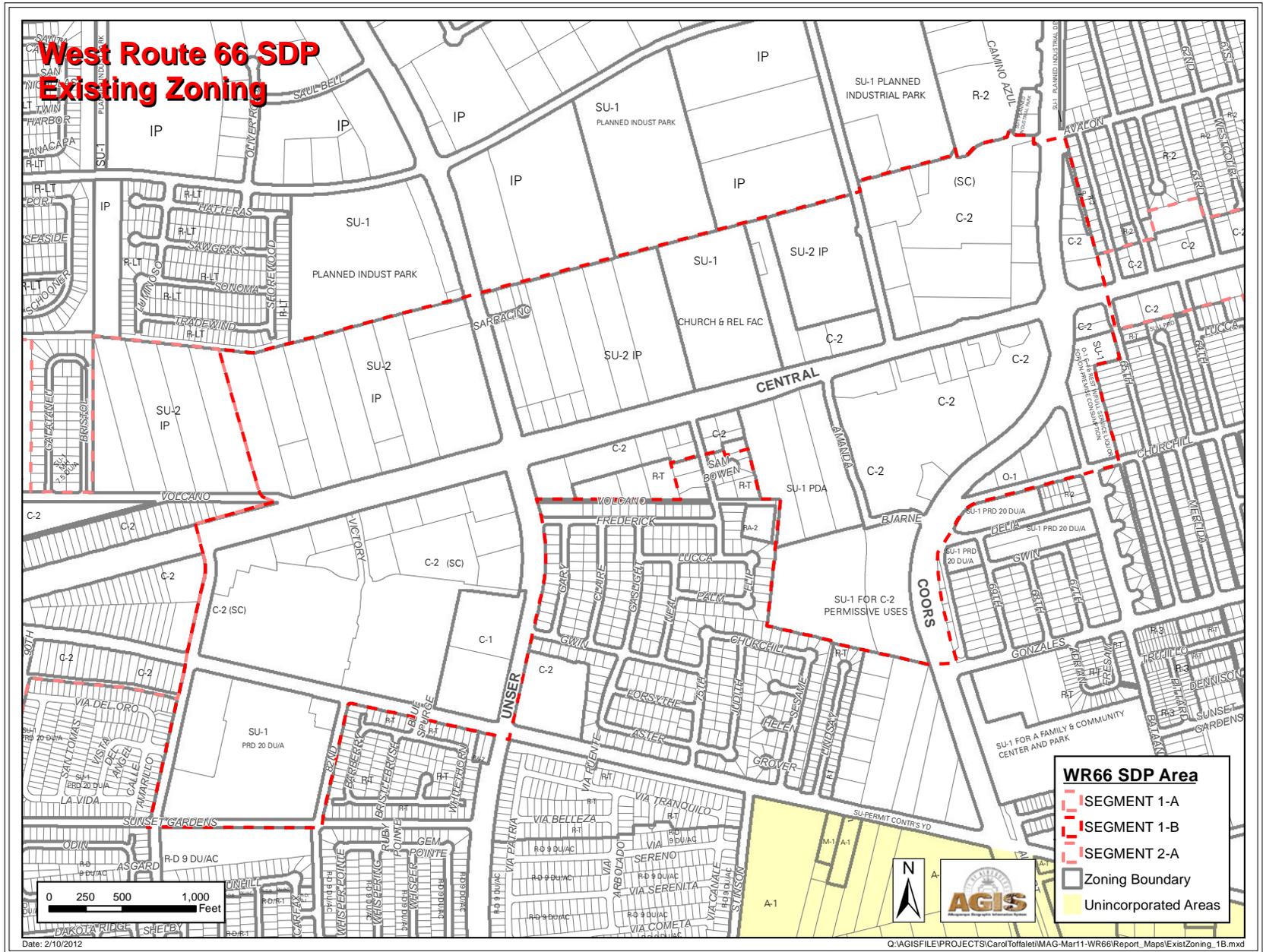


Figure 10: Existing Zoning, Segment One

Existing Conditions: Land Use and Zoning, Segment Two

1.3 Segment Two Land Use Analysis

Segment Two has a long history of corridor commercial activity, with initial businesses supporting automobile traffic to and from Albuquerque. Segment Two primarily consists of commercially zoned properties along the north and south sides of Central Avenue from Coors Boulevard to Rio Grande Boulevard. The segment is bisected by the Rio Grande, which historically has acted as a barrier, dividing the neighborhoods on either side of the Bosque into two unassociated areas. The eastern side of the river has a historical and land use relationship to Old Town, Downtown and the Country Club neighborhood while the western side of the river relates to the West Mesa and the South Valley neighborhoods.

Development patterns in the western portion of Segment Two tend to relate to the period in which the buildings were constructed. Older structures are built adjacent to the road, in many cases directly adjacent to the sidewalk with no parking, or parking in the rear. Development within the last 30 years tends to be set back from the street with parking in the front. Platting and lot sizes in this area vary, although lots are primarily narrow with an average depth of 150 feet. Block lengths are inconsistent, with small blocks of less than 300 feet alternating with blocks larger than 600 feet. Cross streets do not align north and south of Central. No alleys are present in this portion of the plan area.

There are two shopping center sites located at the intersection of Atrisco Dr. and Central Ave. The southwest corner includes a Kmart and several fast food restau-



Figure 11: Pro's Ranch Market

rants and has seen little site improvements in the past 20 years. The northwest corner has recently been redeveloped and hosts two large anchor stores, Ross Dress for Less and Pro's Ranch Market, a private vocational college, as well as several smaller retailers offering neighborhood goods and services. The two shopping center sites, Atrisco Plaza and Kmart Plaza, create a super block west of Atrisco Dr. Other development along this portion of Central tends to be strip development or stand alone commercial. These uses include automobile services, restaurants and fast food, several Route 66 era motels and self-storage facilities, a charter school, Samon's lighting and some neighborhood goods and services. Several large tracts of vacant land exist throughout the segment.

Residential uses on Central Ave. west of the river are limited to some short term housing provided by former motel properties and a small lot zoned SU-1 for apartments on 59th street. Some multi-family residential zoning is located off the Central Ave. corridor in this portion of the Plan area behind commercial properties. Some of this property remains vacant with the exception of a small area of single family development located north of Central Ave. and east of Atrisco Dr. on property zoned R-2.

East of the river, uses include the Albuquerque BioPark, an apartment complex, Route 66 era motels, fast food restaurants, auto repair, Walgreens, and other neighborhood related goods and services. Development in the area ranges from pre-war to the more recent developments constructed during the housing boom of the past



Figure 12: BioPark Entrance

Existing Conditions: Land Use and Zoning, Segment Two

20 years. Several large land holdings are located east of the river. The Albuquerque BioPark occupies the largest parcel in the area, followed by The Beach, a multi-family housing development, the Old Town Shopping Center at the corner of Rio Grande and Central Ave., and several motels located on the north and south sides of the street. As a result of the large parcels, block sizes along this portion of Central tend to be long with limited connectivity to adjacent neighborhoods to the north and the south. Platting is irregular and many other lots are small, narrow and shallow. An alley exists on the south side of the street behind Central Ave., and a small road, Soto, serves as an alley for a portion of the properties on the north side of Central Ave.

Land uses east of the river have historically served tourist traffic along Route 66 and Old Town, as well as providing neighborhood services for area residents. Two of the larger parcels, the El Vado motel site, a historic Route 66 motorcourt on the National Register of Historic Places, and the former Casa Grande restaurant have recently come under City control and are anticipated to be catalytic revitalization projects for the area.

Many parcels along this section of Central are underutilized. Early motels are minimally maintained and function as short term housing. Older fast food restaurants are surrounded by parking, and some vacant parcels are located within walking distance of several of Albuquerque's most desirable attractions - Old Town, the BioPark and Tingley Beach - but fail to draw area tourists. The Old Town Shop-



Figure 13: Commercial development at Central and Rio Grande

ping Center provides one of the few locations for limited neighborhood goods and services for the Downtown, Old Town and Country Club neighborhoods.

In general, development patterns east of the river tend to have minimal setbacks with buildings located at the street; however, large portions of most sites in the plan area are devoted to parking which abuts the street edge. Frequent curb cuts, narrow sidewalks and limited pedestrian amenities make pedestrian access difficult and unpleasant.

Large parcel land use (over 2 acres)

- Samon's site
- Kmart Plaza
- Atrisco Plaza (Ross, Pro's Ranch Market)
- Albuquerque BioPark
- El Vado Motel, other motels
- Old Town Shopping Center

1.4 Segment Two Zoning Analysis

Prior to the adoption of the 1987 plan, the zoning in Segment Two along Central Avenue consisted of primarily C-3 zoning between Coors Blvd. and Old Coors Rd. with a small portion of land on the southeast side of Coors Blvd. zoned residential. From Old Coors Rd. to the river, the zoning was C-2. With the adoption of the 1987 SDP, the C-3 zoning between Coors Blvd. and Old Coors Rd. became C-2 to match existing land uses.

There are some issues resulting from C-2 zoning requirements, given existing site constraints in Segment Two. Lot sizes and existing building configurations make meeting parking requirements on site difficult for most uses. Landscaping and buffering requirements are lacking on most sites, especially in parking areas. Zoning regulations for C-2 signage does not permit the historic use of "traveling" and building mounted signage which is commonly found in older signs along Central. In addition, current zoning requires that copy on a sign must be removed no more than 30 days from the date of vacancy of a business, which may conflict with any desire to preserve historic signs in this area.

Existing Conditions: Land Use and Zoning, Segment Two

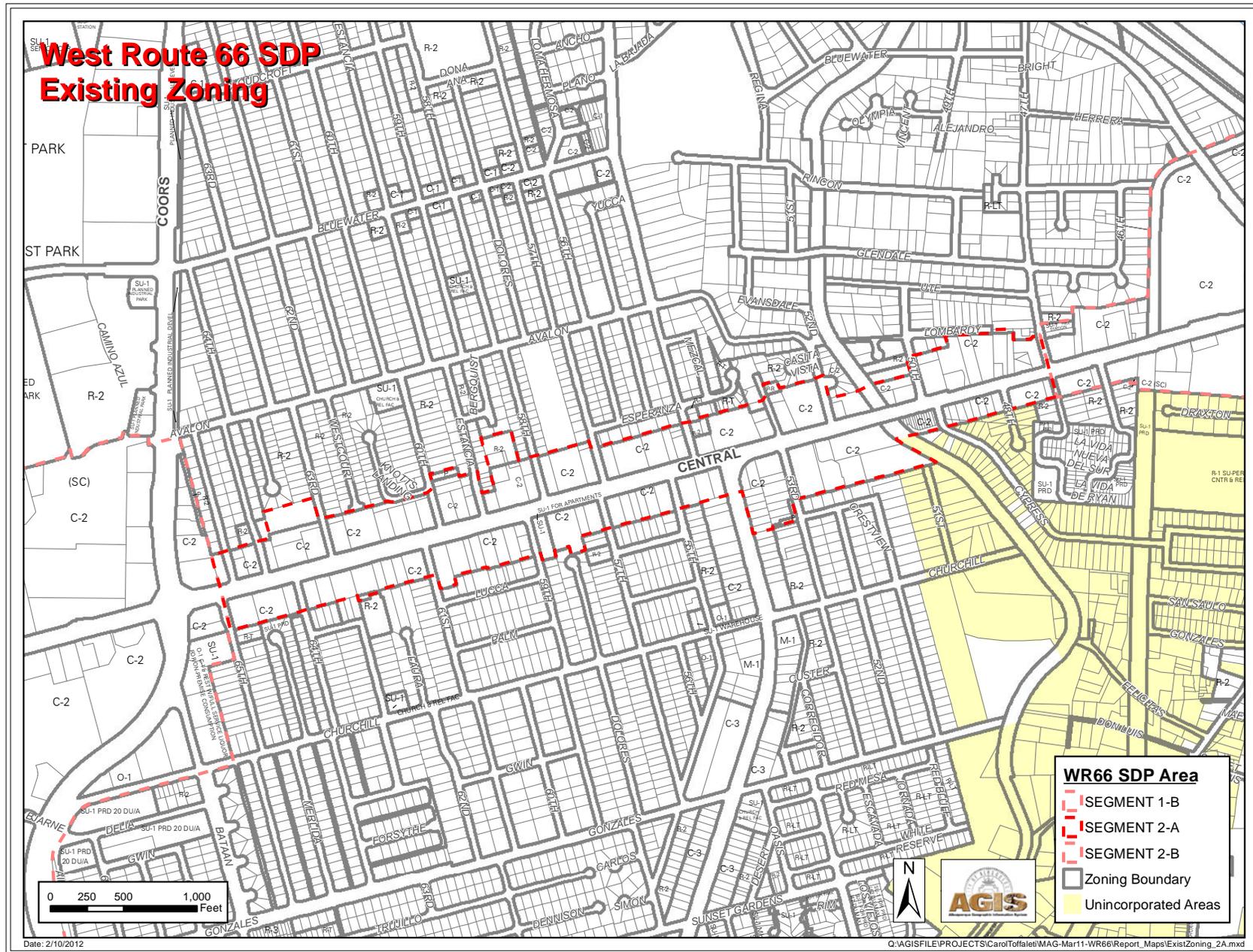


Figure14: Existing Zoning, Segment Two

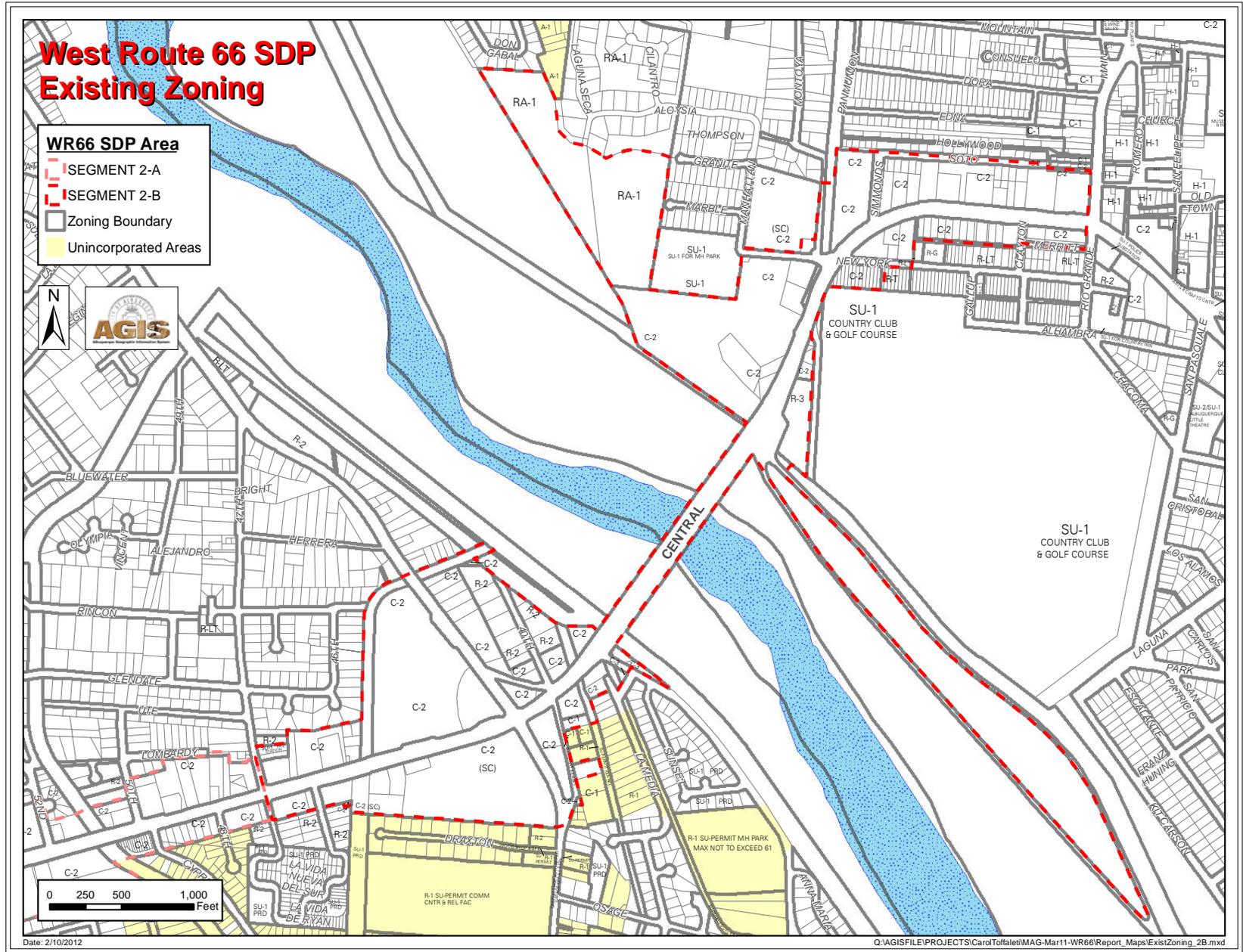


Figure 15: Existing Zoning, Segment Two

Existing Conditions: West Route 66 Retail Market Study

2.0 West Route 66 Retail Market Study

In January 2011 a Retail Market Analysis was completed by Gibbs Planning Group, Inc. (GPG) for the West Route 66 Sector Development Plan. The objective of the analysis was to evaluate the potential for retail development in the Plan Area in order to guide the creation of land use and zoning regulations in the Plan.

The study found that Albuquerque's West Central neighborhoods can presently support an additional 161,700 square feet of retail and restaurant development. By 2015 the study area can support an expansion to 185,800 square feet of new commercial growth.

2.1 Trade Area

Based on GPG's field evaluation, their analysis of the existing retail hubs, population clusters, highway access, and the retail gravitation in the market, as well as on their experience defining trade areas for similar communities throughout the United States, GPG determined that the West Central study area is a growing community which is generating demand to support a wide variety of additional neighborhood and community scale retailers. This potential will continue to grow over

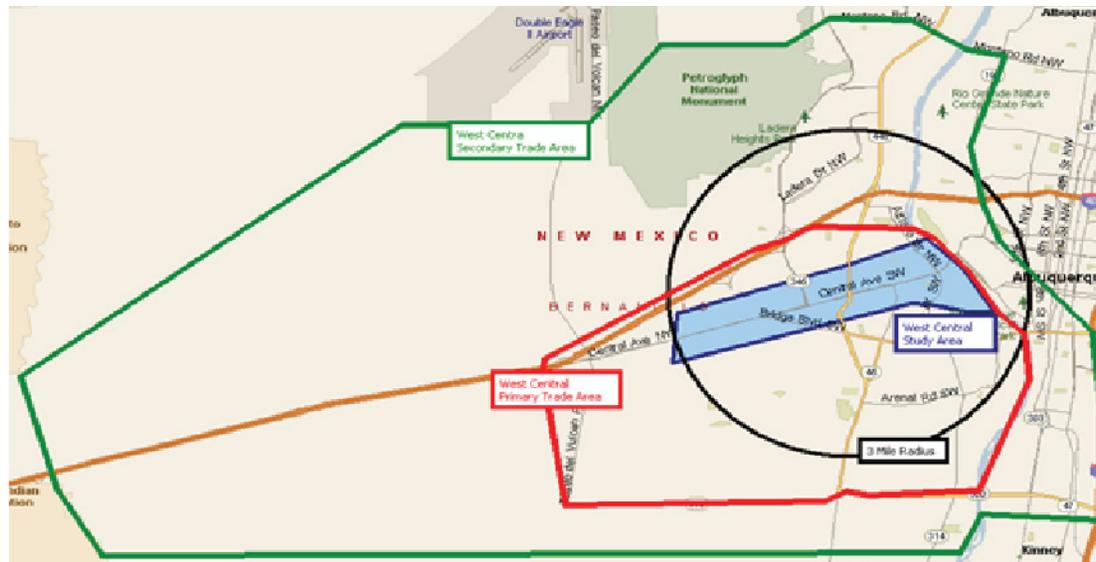


Figure 16: Trade Area. The West Central primary trade area is shown above inside the red line, and the total trade area is delineated by the green line.

the next five years, sustained by an annual population growth rate of 2.68 percent. The primary trade area is the consumer market where the West Central study area has a significant competitive advantage because of access, design, lack of competition, and traffic and commute patterns.

2.2 Demographic Characteristics

The West Central primary trade area has an estimated 2010 population of 98,575 persons, which is projected to grow to 112,500 by 2015, a 14.1 percent projected increase over the total five-year period (2010-2015). This annual growth rate is much greater than the Albuquerque, New Mexico or National levels.

2.2 Access

Regional linkage to the West Central study area is excellent, with multiple interchanges (Coors Blvd., Unser Blvd., 98th St., Atrisco Vista, and Central Ave.) along Interstate 40. The main north-south connector is Coors Blvd., which parallels the Rio Grande throughout Albuquerque. Eastern traffic flows are somewhat restricted by the limited number of local bridges crossing the Rio Grande.

High traffic volumes are largely the result of the limited employment base inside of the West Central study area. Morning traffic flows out of the district, due to the imbalance of jobs in the study area versus residents who commute outside of the study area.

2.3 Retail Competition

The strongest direct competition to the West Central Albuquerque study area are the community and neighborhood retail concentrations found along Coors Blvd., just north of the junction with Interstate 40, reaching past St. Joseph's Dr. This sprawling assemblage of neighborhood and community centers is anchored by a Walmart Supercenter and a Home Depot store. Additional retail includes a dense collection of full and limited service restaurants. This retail cluster is disadvantaged by being spread out over a two-plus mile stretch of busy local highway (Coors Blvd.). Consumers who need to shop at multiple stores are hampered by having to make multiple trips onto and off of busy Coors Blvd. while traveling from store to store.

Existing Conditions: West Route 66 Retail Market Study

2.4 Potential Types of Supportable Retail

- 66,500 square feet of Department and Discount Department Stores
 - 16,300 square feet of Electronics, Appliances and Computer retail
 - 15,300 square feet of Food & Restaurant
 - 12,900 square feet of Sporting Goods, Hobby, Books and Music Stores
 - 12,200 square feet of Health Care & Personal Services
 - 11,700 square feet of Apparel, Shoes and Accessories
 - 11,300 square feet of Hardware, Lawn and Garden Stores
 - 7,700 square feet of Miscellaneous Retail, including: office supplies, gift shops, pet supplies, florists, and tobacco supplies.
 - 6,200 square feet of Home Furnishings
 - 1,600 square feet of Food and Beverage Stores: All of this supportable square footage can be concentrated into the convenience food category.
- (Total : 161,700)**

2.5 Rationale for Study Findings

Strong demographic growth: Although average and median household incomes are modest, the annual growth rate of 2.68 percent reflects a total growth rate of 14.1 percent by 2015. Regardless of disposable income levels, the growing base of households in the primary study area need daily goods.

Existing retail infrastructure development: The suddenness of the 2008 housing crisis and the 2009 financial crisis stopped residential and retail development in its tracks in the West Central District. This study area can fill first generation tenant space overnight at the retail centers on Unser Boulevard and Central Avenue, or begin construction of warehouse retailers without the delay of acquisition, approval, demolition, and infrastructure development.

Single site critical mass development: The West Central District's potential availability of a large, single site development, such as Unser Crossing, will likely attract new retailers to the market. In addition, existing retailers presently located along Coors Boulevard seeking to expand may consider relocating into new or infill West Central District shopping centers.

Access to Surrounding Neighborhoods: Although the West Central District is not located directly along Interstate 40, its convenience to surrounding neighborhoods will appeal to numerous modern retailers.

Impacts on Other Business Districts: The implementation of new shopping centers and the opening of new types of retailers that are not presently located in the West Central District area will capture projected population growth and household spending that is presently occurring elsewhere in the region, or not at all. This shift in consumer habits will impact existing retailers to various levels, depending on how they respond to the opening of new businesses. (The complete West Route 66 Retail Market Study is available online from the Planning Department.)

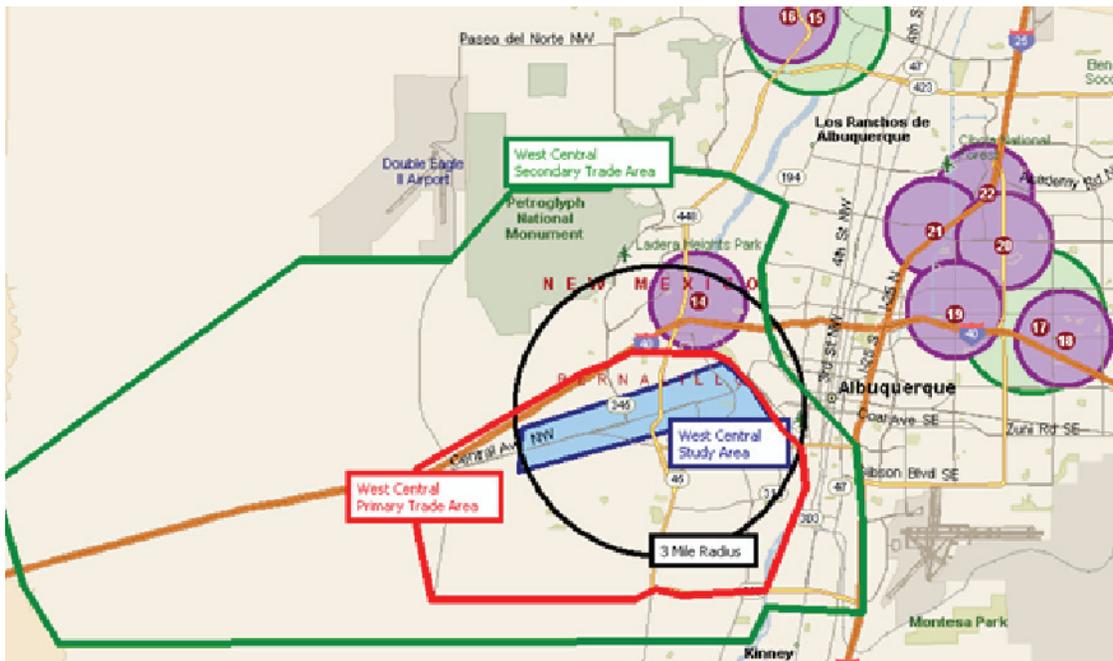


Figure17: Regional and Community Retail Location Map. Community Retail Centers are marked with purple rings; Regional Retail Centers are indicated with green rings.

Existing Conditions: Demographic Summary

3.0 Demographic Summary

Pending

4.0 Metropolitan Redevelopment Areas

In 2001 the Albuquerque City Council approved a Metropolitan Redevelopment Area (MRA) Designation for the West Central Area. Three years later, in 2004 the Council approved the West Central Metropolitan Redevelopment Area Plan. The Metropolitan Redevelopment Plan provides important guidance for overall redevelopment in the MRA area and identifies specific redevelopment strategies for opportunity sites. The West Route 66 Sector Development Plan includes a large portion of the MRA. The overlapping boundaries are along the Central Ave. corridor from west of Unser Blvd. to the west side of the Rio Grande. The MRA also extends beyond the Sector Plan boundary, to the south along Old Coors Blvd. and to the north, incorporating the Atrisco Business Park.

The City of Albuquerque Metropolitan Redevelopment Agency and the City Council are currently considering the creation of a new Historic Central MRA that would extend from the Central Bridge to Laguna Blvd. Also under consideration is the expansion of the West Central MRA to the western plan boundary of the new West Route 66 Sector Plan.

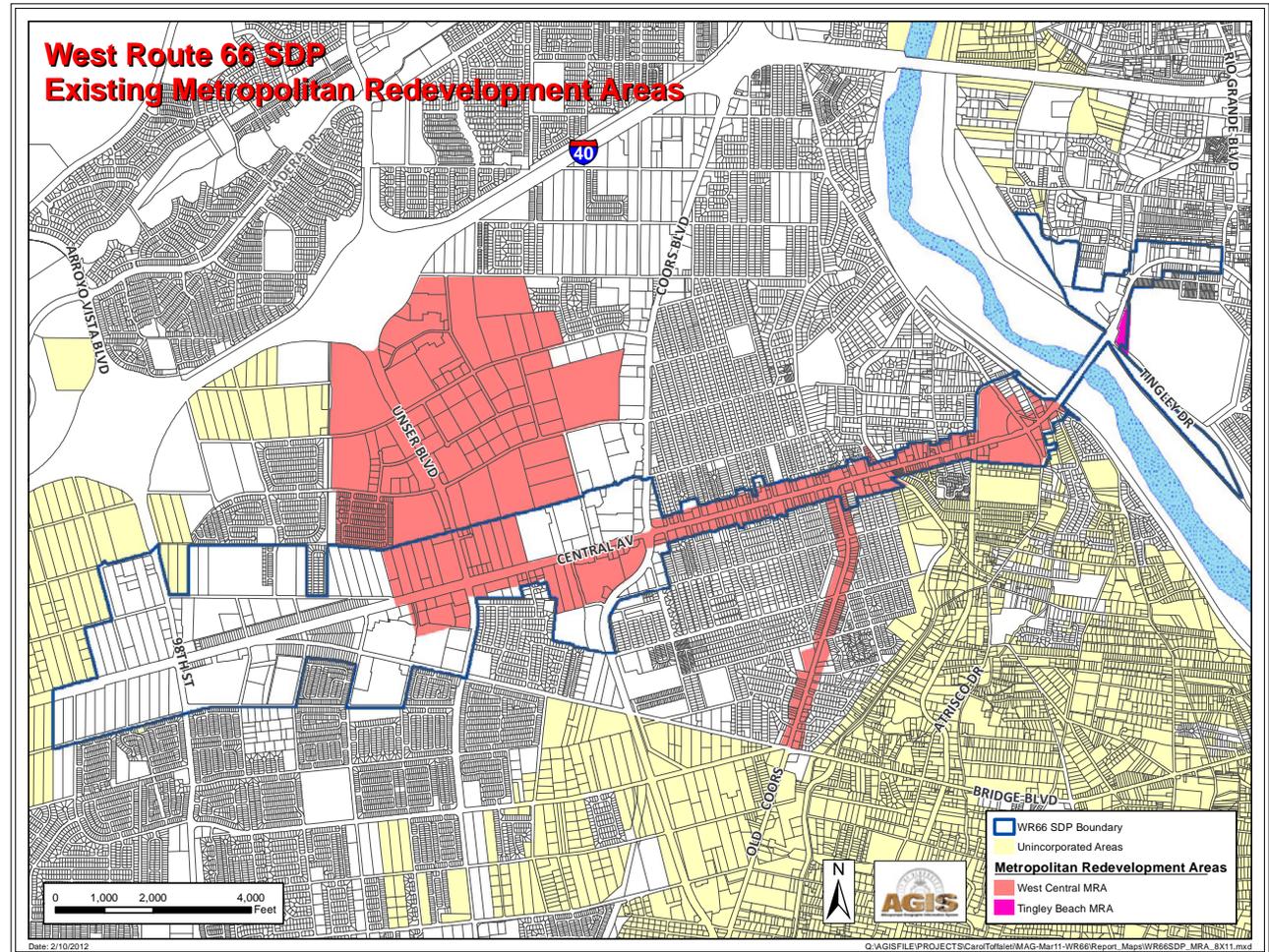


Figure 18: MRA map

Existing Conditions: Historic Route 66

5.0 Historic Route 66 Overview

Historic US Route 66 played a key role in the development of West Central Ave. In 1937, the east/west realignment of Route 66 created the first paved road across New Mexico. Prior to the paving of Route 66, El Camino Real had provided the main route to and through Albuquerque, setting the pattern for development to extend in a north/south manner following the Rio Grande and the AT & SF Railroad tracks. However, with the 1937 east/west alignment of Route 66 through Albuquerque, development began to spread along the highway that is now known as Central Avenue. Businesses arose to meet the needs of the travelers along the highway. Travelers stopped at cafes, trading posts and New Mexico-themed motels in order to experience a taste of the local culture.

From 1933 to 1941, during the Great Depression, refugees fleeing the Dust Bowl traveled Route 66 on their way to work the agricultural fields of California. Less-troubled automobile owners also took to the highways, improved by New Deal-funded construction programs, to visit, recreate, or pursue opportunity.

During WW II, from 1942 to 1945, Route 66 fell into decline. Heavy trucks transporting war machinery were the primary users of the road and this led to disrepair. Businesses that catered to tourists struggled as they lost a large portion of their market.

The post-war years from 1946 to 1956 are known as the “Golden Age” of Route 66. The country entered a renewed period of prosperity after the war and more of the population was able to afford cars and vacations. The road once again began to serve visitors in search of adventure. It was at this time that the portion of Route 66 that runs through Albuquerque was widened from two lanes to four lanes. Financing for the construction of tourist-oriented businesses was easily available and roadside architecture along the route flourished. During the “Golden Age” of Route 66, the West Central corridor was populated with small mom and pop businesses such as service stations, restaurants and motor courts all intended to serve the passing motorists. In Albuquerque, trading posts along the corridor capitalized on the tourism potential of Native American culture by selling artifacts and souvenirs to Route 66 travelers.

In 1956, the Interstate Highway Act provided the funds to build a new highway system that would bypass cities and towns and provide an efficient, although ster-

ile, way of traveling across the country. Corporate franchises became the dominant business model in Interstate-proximate development. Route 66 began to decline as people opted to take the Interstate and no longer had convenient access to the small businesses that lined the highway and provided livelihoods for so many.

When Albuquerque’s Interstate 40 segment was completed in the 1960’s, I-40 joined Central Ave. at the two current interchanges near Tijeras Canyon and atop Nine Mile Hill. Central became an I-40 “business route” while most new highway oriented businesses went up near other I-40 interchanges. Route 66 was de-certified nationally in 1985, by which time Central Avenue’s travel based commerce had fallen far and not recovered.

Although most of the roadside buildings and signs built to entice travelers on Route 66 have been removed, remodeled or abandoned, the history and architecture associated with US 66 through Albuquerque has not been forgotten. In recent decades there has been renewed interest in preserving the remaining elements of Route 66 as a part of our cultural heritage. In 1991-1992, an inventory of highway segments and tourist-related properties was commissioned by the New Mexico State Historic Preservation Division and the Tourism Department. That inventory provided the foundation for further historic preservation initiatives. In 1993, it resulted in approval of the documentation of the Historic and Architectural Resources of Route 66 through New Mexico by the National Register of Historic Places, along with the listing of a number of local buildings, some on West Central Avenue in Albuquerque. The documentation provides the historic and architectural context for resources associated with Route 66 and describes the property types relevant to Route 66 history: cafes, gas stations, tourist courts and motels, trading posts/curio shops and municipal roadside attractions. In 1994, the New Mexico Legislature designated Route 66 as a State Scenic Byway, and in 1999 Route 66 was designated as a National Scenic Byway.

Today, Albuquerque retains enough original architecture of the period to make it a Route 66 heritage tourist destination. With the renewed interest in preserving the Route 66 history, the Plan has the opportunity to direct development in a way that celebrates and enhances its unique Route 66 history, as well as builds upon that identity to encourage business and high quality development to better serve residents and visitors.

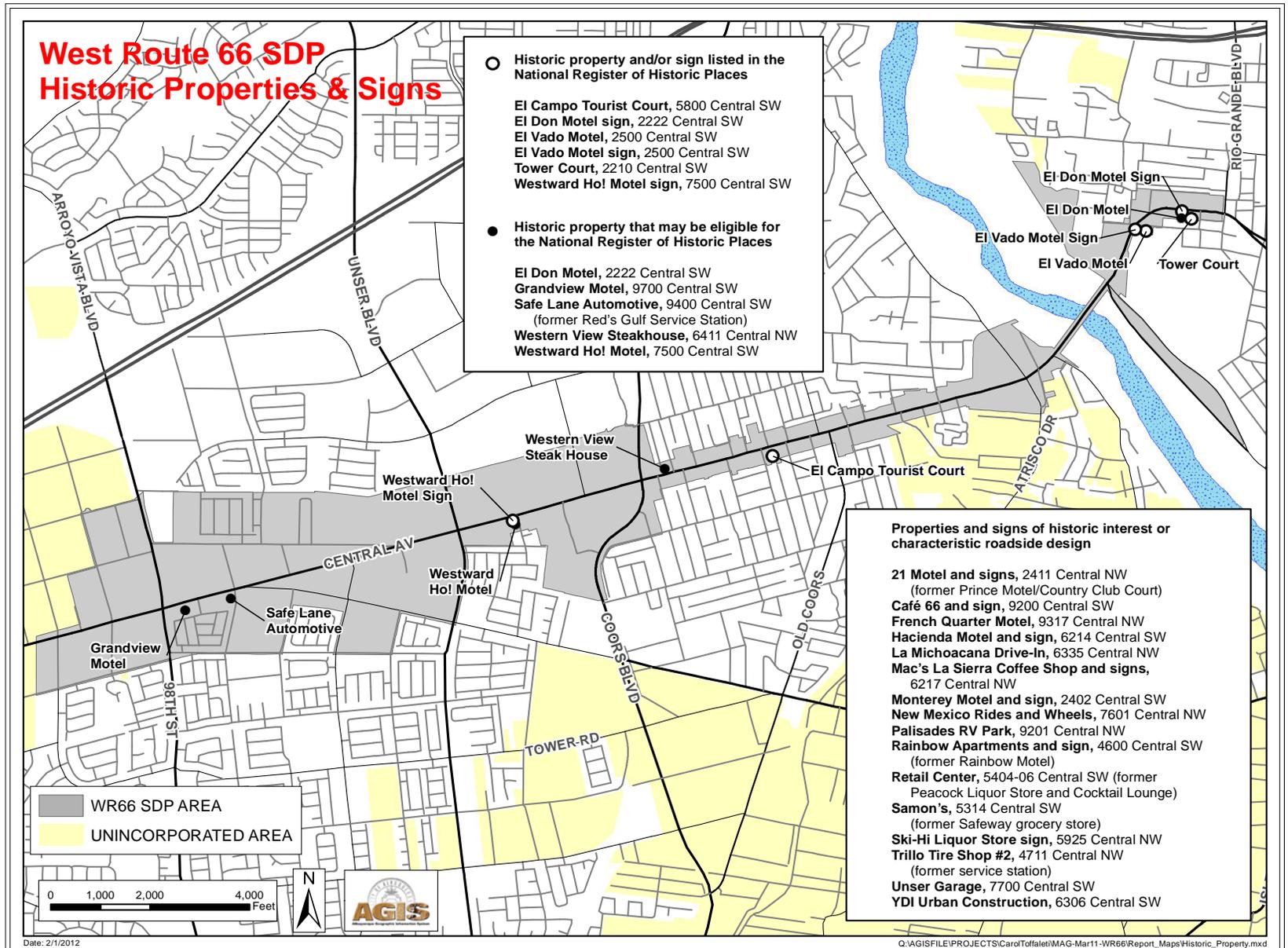


Figure 19: Historic Properties and Signs

Existing Conditions: Historic Route 66

Officially, historic properties are those properties that are either listed on the National Register of Historic Places and/or the State Register of Cultural Properties, or have been determined by the appropriate agencies to be eligible for listing. Several properties in the Plan area are listed on the historic registers as shown on map on page 96. The Hilltop Lodge at 5410 Central, a Route 66 motel listed on the historic registers, was demolished in 2002.

5.1 Historic Buildings

Currently, there are three Route 66 buildings listed on the National Register of Historic Places in the plan area. These are some of the earliest tourist courts that developed on West Central Ave. **El Vado Motel**, an example of Pueblo Revival style architecture opened in 1937. This iconic tourist court is a City Landmark. Once proposed for demolition, it was acquired by the City of Albuquerque and will be redeveloped with City assistance. **El Campo Tourist Court**, built in 1939 on West Route 66 approaching the escarpment, employed a regional Southwest Vernacular architectural style and once included a filling station and store. The **Tower Courts**, another little-altered pre WW II tourist court, in the Streamline Moderne style, also opened in 1939. A stepped, thirty-foot tower once accentuated the building's Moderne architectural style. Both the Towers Courts and El Campo Tourist Court have since been converted to apartment use.

Almost all of the remaining structures of historic significance were built in the post WW II Route 66 era. Notable buildings that may be eligible for the historic registers include the **Western View Diner and Steak House** and **Safe Lane Automotive**. Other motels that are potentially eligible for the historic registers as Route 66 resources include **El Don Motel**, the **Westward Ho Motel** and the **Grandview Motel**. To be considered eligible, a property must retain sufficient elements of location, design, and material, convey a strong association with automobile tourism and have been built between 1926 and 1956, the official period of significance for historic Route 66.

There are other buildings of historic interest in the plan area, such as the **Samon's** building (an early Safeway store), the **Monterey Motel** and its sign, the **Prince** and **Hacienda Motels**, **La Michoacana Drive-In** and the **Unser garage**. Some of these properties might be eligible for the historic registers under another registered historic context: Automobile Oriented Commercial Development.



West Central Ave. in the plan area retains a notable collection of early tourist courts built between the 1930's and 1950's. These properties are listed on the National Register of Historic Places.

As shown on page xx, Tower Court (top photo) once featured a Streamline Moderne style tower.

These motels have been adapted for apartment use. Registered properties are eligible for state and federal tax incentives for rehabilitation.

Figure 20: Historic Properties in Plan Area

5.2 Historic Signs

Roadside commercial signs in Albuquerque have ranged from the simple to the extreme. Painted on rocks or mounted on poles, walls and roofs, they were placed and sized for readability from a moving vehicle. When illuminated by electric light it was better yet, and here in Albuquerque, lighted signs transformed the urban landscape during the 20th century. First, incandescent bulbs were used to form words on, or outline, a sign. Bulbs flashing on and off sequentially imparted “motion” to a sign.

Neon signs first appeared in the United States in the 1920’s and reached their height of popularity in the 1950’s. Glass tubes bent into myriad shapes glowed on most commercial sites and even some churches. As with incandescent light, neon tubes could be switched for sequential illumination and movement. Neon tubes along the eaves or parts of a building, along with an illuminated sign, could transform even the simplest architecture into a dazzling nighttime spectacle.

In addition to buildings, three neon signs in the plan area are listed on the National Register of Historic Places (see page 96). The figurative signs of the **Westward Ho**, **El Don** and **El Vado Motels** represent some of the best examples of neon sign art. Restoration of the saguaro cactus and the lasso swinging cowboy was made possible with a grant from the National Park Service’s Route 66 Corridor Preservation Program. This program makes grants to selected property owners who are willing to match restoration funds with those of the National Park Service. These distinctive signs evoke the Route 66 heyday and serve as minor landmarks in the miles-long corridor of West Central, even when they are not lighted. At night their glow warms the streetscape and marks the businesses even better.

Sign as Icon

“Signs often become so important to a community that they are valued long after their role as commercial markers has ceased. They become landmarks, loved because they have been visible at certain street corners-or from many vantage points across the City-for a long time. Such signs are valued for their familiarity, their beauty, their humor, their size, or even their grotesqueness. In these cases, signs transcend their conventional role as vehicles of information, as identifiers of something. When signs reach this stage, they accumulate rich layer of meaning. They no longer merely advertise, but are valued in and of themselves. They become icons.” - - Preservation Brief #2, National Park Service



Figure 21: Historic Signs in Plan Area

Existing Conditions: Historic Route 66

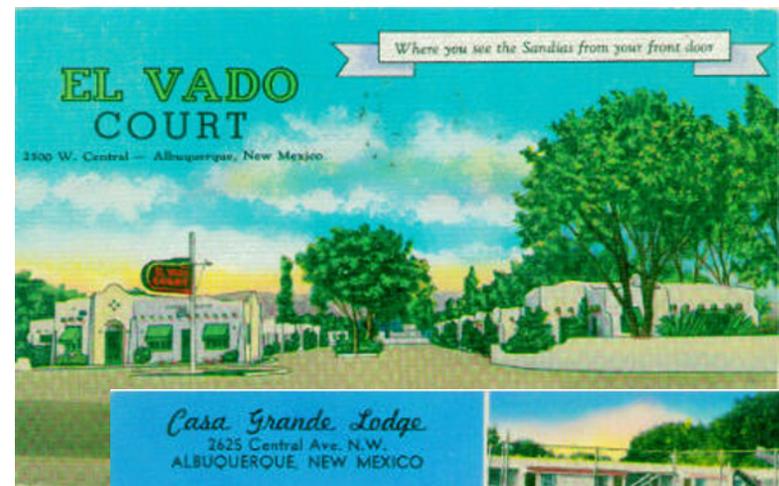


Figure 22: Historic Postcards of Plan Area Buildings

Many early automobile travelers on U.S. 66 preferred camping along the roadside to the more formal hotel accommodations downtown. In the 1920's, campgrounds, and then small cabins for daily rental began to appear along the road. Soon these entrepreneur business owners began to attach the cabins, often alternating units with carports. By the mid-1920's a new building form had emerged, with the lodging units unified under a single roof. Buildings were organized in two rows, "U" or "L" shaped and sometimes crescent-shaped. These building forms naturally defined a courtyard space, typically enhanced with landscaping, patios, picnic tables and the like, where travellers could socialize. These new businesses were most often family-run, and the owners lived on-site. Small curio or sundry stores and gasoline pumps were often included. These new "Tourist Courts" were most often located on the outskirts of town where land was less expensive, and there were many on West Central Ave. It was not until the early 1950's that the term "motel" first appeared in Albuquerque City Directories. By that time, the pre-dominant lodging type had become a larger two-story building.

5.3 Issues and Opportunities

The Plan area has strong associations with historic US 66. Central Avenue is part of the Route 66 State and National Scenic Byway. Although much of the early roadside architecture has been lost, the plan area retains many of the “intrinsic qualities” of the historic roadway. These qualities include geography – sweeping vistas both east and west as one travels Central Avenue in the plan area and a picturesque river crossing. Cultural diversity is another quality that contributes to one’s experience of the road and the plan area has small, diverse, and distinctive roadside businesses. Despite some losses, the plan area retains a number of registered or eligible historic buildings and signs associated with Route 66.

It is important to consider the conservation of the remaining resources not only from the perspective of the plan area, but as a part of the whole Route 66 experience through Albuquerque. Each remaining resource is one vital link in a chain of historic properties that is sufficient to keep Albuquerque “on the map” in this heritage tourism niche.

5.3.1. Conserving Historic Buildings

El Vado is an iconic Route 66 property. It is owned by the City of Albuquerque and was acquired because of its historic significance to the city. As a designated City Landmark, the property benefits from local controls over inappropriate alterations and demolition. Redevelopment can capitalize on El Vado’s proximity to other local cultural destinations including Old Town, the BioPark and Tingley Beach. Re-use that includes a strong interpretative component and public access will help to anchor an important cluster of cultural attractions on this portion of Central Avenue, a destination for locals and visitors alike.

Properties listed on the State and National Registers of Historic Places can benefit from state and federal tax credit programs that support rehabilitation and preservation of historic properties. Property owners can utilize the existing Route 66 Multiple Property Documentation to register eligible Route 66 resources and take advantage of rehabilitation tax credits. Consult City and State historic preservation staff for direction and information.

Except for El Vado Motor Court, the historic properties in the plan area are not



Several properties in the plan area may be eligible for the State and National Registers of Historic Places (see map on page 96). A registered historic context, “Historic and Architectural Resources of Route 66 through New Mexico” can be used for building registrations.

Figure 23: Eligible Properties for Historic Registration

Existing Conditions: Historic Route 66

protected from demolition and alterations that affect their historic integrity. Although properties that are listed on the State and National Registers are eligible for federal and state financial incentives for rehabilitation, listing does not include any development controls. A demolition review by-law can provide an opportunity for public and private sector dialogue when a historic property is proposed for demolition. Demolition review provides for a waiting period, under stated parameters, during which alternatives to demolition can be explored and implemented.

The City can also facilitate the preservation of historic buildings by purchasing conservation easements. A conservation easement is a voluntary, legally binding agreement between a landowner and a qualified land trust or government entity that permanently limits uses of the land in order to protect ecological, historic, or scenic resources. It restricts real estate development, commercial and industrial uses, and other activities on a property to a mutually agreed upon level. The property remains the private property of the landowner. The decision to place a conservation easement on a property is strictly a voluntary one, where the easement is sold or donated. The restrictions of the easement are negotiable and, once set in place, are binding on all future owners of the property. For example, a conservation easement could provide a cash payment to the property owner by the City in return for a commitment to retain a historic façade or sign. Funding for such an easement program might utilize Scenic Byway or National Park Service grants, Metropolitan Redevelopment funds, or other combination of City resources including Lodger's Tax and Urban Enhancement funds.

5.3.2 Characteristic buildings

Other buildings in the plan area may not be eligible for the historic registers, but some display physical characteristics that are similar to designated historic properties and contribute to the visual character of the area. The examples opposite are a drive-in, a café, and a curio shop now in a different use. Their distinctive shapes and signs embody the exuberance of roadside commercial architecture. Improvements to these properties should respect their characteristic architecture and retain those elements that define the building's form. In these auto-oriented, stand-alone buildings, a key issue is strengthening the relationship of the building with the street and surrounding buildings and inviting pedestrian access. Landscaping and low walls at the property line can reinforce a connection with the street. Curb cuts should be limited to a maximum of 24 feet and clearly delineated.



These buildings are characteristic of early roadside architecture and they compliment the collection of historic Route 66 properties in the plan area.

Figure 24: Route 66 Roadside Architecture

5.3.3 Iconic Signs

In 2002, the City's Urban Enhancement Trust Fund in the Public Art Program funded a survey and management plan for signs and neon enhancements along Route 66 through Albuquerque. The report concluded that all surveyed signs were "threatened", that is, that none of these important resources have any degree of protection from loss. Key recommendations of the plan were that the City act to implement a comprehensive management plan to include conservation easements as discussed above, and make changes to regulatory codes to provide for the conservation of existing signs. The report emphasized the potential of these signs to contribute to economic development through tourism.

In 2002, the National Park Service Route 66 Corridor Preservation Program provided a grant to restore historic neon signs, where the owners and sign makers contributed 50% of cash or in-kind costs. Such incentives can still be utilized for further restoration activities, and the City can participate. A similar program specifically for Albuquerque might be developed with cooperation between various City agencies to fund financial incentives for both new and restored neon signs. The City can also set an example for the promotion of neon signage by installing such neon on City-owned properties along West Route 66 as a catalytic strategy.

Iconic signs in the Plan area, whether listed or eligible for the historic registers or not, can be promoted and preserved through zoning regulations that provide for their continuous display and through a conservation easement program as discussed above.



Figure 24: Route 66 Roadside Signage

Existing Conditions: Transportation

6.0 Transportation Overview

The portion of the West Central corridor located within the Plan is approximately 6 miles long. It is designated by the Long Range Roadway System Map (2004) as an existing urban principal arterial with a right-of-way (ROW) varying from approximately 200 feet at the plan's western boundary to 100 feet at its eastern boundary. This classification is based on the primary function of the roadway, not the volume of traffic it carries. The classification criteria for principal arterials include: routes connecting sub-areas within an urbanized region, routes to high density activity centers, and routes characterized by long distances.

The lack of neighborhood and community services along West Route 66 has had a significant effect on how it functions as a roadway. Rather than serving as an area destination for jobs, services, and commercial and entertainment needs, West Route 66 has, over the last 20 years, become a commuter route characterized by peak hour traffic congestion, fast moving traffic, few if any pedestrian friendly features and poor neighborhood connectivity. As a commuter corridor, it currently handles an average of 21,444 vehicle trips per day, bringing area residents east over the bridge to jobs and services. Based on past trends, it is estimated that the number of vehicle trips will double in the next 25 years along with a host of negative impacts on quality of life, the economy and the environment (source: 2035 MTP).

Despite current challenges, there are opportunities for a different outcome: vacant land ready for development, older sites ready for redevelopment and reinvestment, large area populations eager for neighborhood services, historic and cultural identities on which to build, and West Central as the main access to Old Town and the BioPark. Key to making the most of these opportunities is to create a more multi-modal transportation environment that encourages redevelopment by addressing problems such as wide streets, narrow sidewalks, poor pedestrian connectivity, high traffic speeds, traffic congestion, lack of shade and vegetation, and an absence of pedestrian destinations which serve area residents and visitors. Transportation and transit improvements, coordinated with land use and urban design changes in the Plan, can help create more employment opportunities, quality higher density housing, and retail and service development to better serve residents' needs and attract visitors to the corridor.

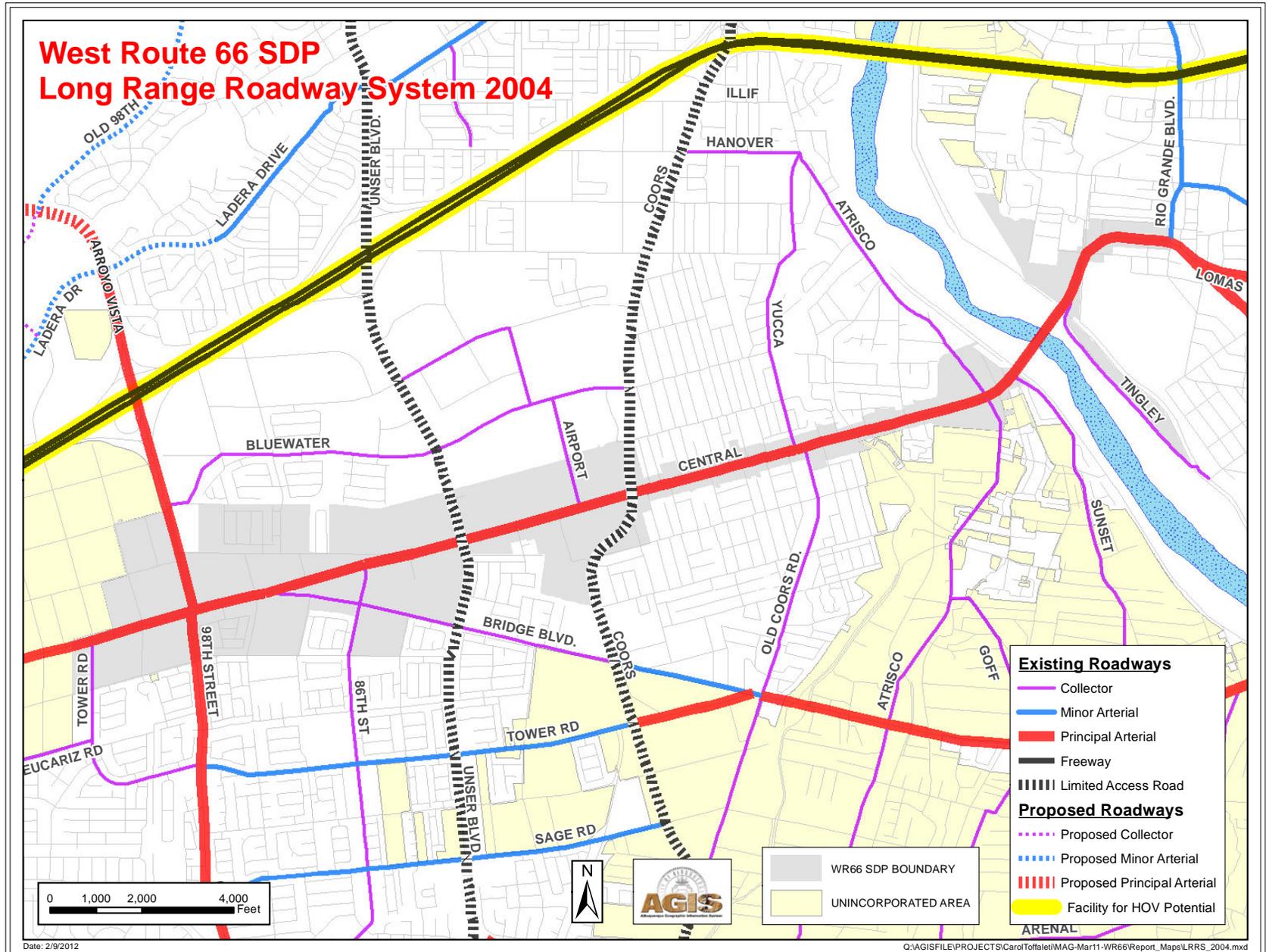


Figure 25: Roadway Classification Map

Transportation Conditions: Pedestrian Composite Index Analysis

6.1 West Central Pedestrian Composite Index Analysis

In the summer of 2011, the MRCOG performed a Pedestrian Composite Index Analysis for the plan area in order to determine locations that would most benefit from pedestrian improvements. The Pedestrian Composite Index (PCI) is a tool to prioritize pedestrian improvements by comparing pedestrian “generators” and “deterrents.” Generators are data that show pedestrian activity or draws for pedestrian activity. Generator data includes proximity to schools, parks, and community and cultural centers, as well as other factors such as percent of people walking or taking transit to work, street connectivity, etc. Deterrent data indicate the area is an uncomfortable or unsafe walking environment. Deterrent data include traffic volumes and speeds and pedestrian crash rates. Locations that have high pedestrian generator and high pedestrian deterrent scores are rated as high priority areas for pedestrian improvements. According to the MRCOG study, the locations with particularly high pedestrian composite index scores are:

1. At Rio Grande Blvd. and the area immediately to the west.
2. Area around New York Ave.
3. West of Old Coors Rd.
4. Area around Coors Blvd.
5. Area around Unser Blvd.
6. Area around Atrisco Dr.

6.1.1. Route 66 Pedestrian Generators and Deterrents

a. Primary Generators

The most significant pedestrian generators in the area are the high volume bus stops. On weekdays, transit riders have up to 400 opportunities to catch a bus at bus stops near either end of this corridor. For the Central NM region, the eastern end of this corridor has high percentages of households without motor vehicles (15%) and high percentages of people walking or taking transit to work (10%). Areas in western segments of this corridor have grocery stores and other retail that generate pedestrian activity.

High Volume Bus Stops (number in parenthesis is the number of weekday bus visits to the stop for 2010 schedule):

- Rio Grande Blvd & Central Ave (406)
- Tingley Dr & Central Ave (303)
- Atrisco Dr & Central Ave (341)

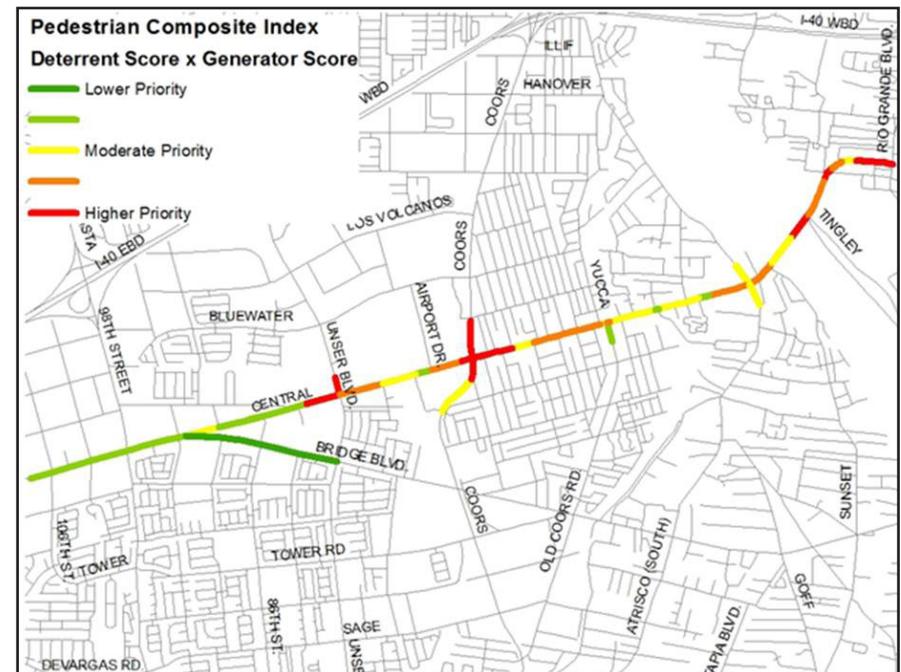


Figure 26: PCI Map

Coors Blvd & Central Ave (299)

Unser Blvd & Central Ave (408)

Presence of Educational/Cultural/Community/Park/Recreational Centers:

Albuquerque BioPark (New York Ave & Central Ave)

Tingley Beach (Tingley Dr & Central Ave)

Bosque Trail Entrance (Tingley Dr & Central Ave)

Areas with high proportions of people walking & taking transit to work and high proportions of households with no motor vehicles available:

In neighborhoods surrounding Central Ave. between Rio Grande Blvd. and Tingley Dr., nearly 10% of workers 16 years and older walk or take transit to work (Census 2010).

Transportation Conditions: Pedestrian Composite Index Analysis

In neighborhoods surrounding Central Ave between Rio Grande Blvd and New York St, 15% of households have no motor vehicle. A little further west around Tingley, 13% of households have no motor vehicle (Census 2010).

Roadway Connectivity

Areas around Central Ave between Old Coors and 65th St have good roadway connectivity, providing good multi-modal access to Central Ave in this area.

Destinations: Grocery, Restaurants, Some Retail

Areas near Central Ave and Coors Blvd

b. Primary Deterrents

The significant pedestrian deterrents for the area are the high number of pedestrian crashes: 34 pedestrian crashes for the 6.2 mile from 2004 to 2008. The highest number of pedestrian crashes occurs at Coors Blvd and Central Ave. This area also has high motor vehicle speeds for the corridor. Other areas with high numbers of pedestrian crashes are at Rio Grande Blvd and Atrisco Dr. These areas have high volumes of motor vehicles for the corridor.

High Numbers of Pedestrian Involved Crashes (2004-2008):

- Coors Blvd & Central Ave area (9 pedestrian crashes)
- Rio Grande Blvd & Central Ave area (6 pedestrian crashes)
- Atrisco Dr & Central Ave area (6 pedestrian crashes)
- Unser Blvd & Central Ave area (3 pedestrian crashes)
- Total Pedestrian crashes from Rio Grande Blvd to Bridge Blvd: 34

Areas with high speed traffic (50 percentile or higher, 2010 Travel Survey, Off-Peak Speeds):

- New York Ave through bridge deck over river
- 55th St to Airport Dr.
- Unser Blvd. to 106th St.

Areas with high traffic volumes (40 percentile or higher, 2010 Traffic Counts):

- Rio Grande Blvd to Tingley Dr
- Area around Atrisco Dr.

Transportation Conditions: Segment One

6.2 Issues and Opportunities Inventory

The following sections outline the specific transportation issues and opportunities by corridor segment. From these issues and opportunities, strategies and recommendations were developed for roadway improvements which support the Plan's goal to:

Make Central Ave. an attractive and inviting corridor that accommodates all modes of transportation including walking, biking, transit use and automobile travel.

6.2.1 General Overview Segment One, 106th St. to Coors Blvd.:

Throughout this segment, Central Avenue is a four lane roadway divided by 36' wide medians. The road is improved with curb and gutter from Victory (in Unser Crossing) to Coors; however sidewalks are intermittent. This section of West Central is characterized by large areas of undeveloped or underutilized land. The vacant land, in conjunction with the large ROW and street width, as much as 200 feet, creates an impression of openness with often unobstructed views to the Sandia, Jemez and Manzano mountain ranges as well as views of the City of Albuquerque in the valley below. A frontage road, provides access to businesses along Central's north side.

a. Missing sidewalks, curbs and street trees: As much of the land adjoining West Central is undeveloped, standard improvements to the roadway have not yet been made. Generally, City policy is that these improvements are made by the adjoining parcel owner upon development of their parcel. There is missing curb and gutter along sections of the frontage road on the north side of Central Ave., especially in areas where the street is separated by unimproved vegetated areas.

b. Missing sections of bike lanes: Bike lane striping is intermittent in this section. From 98th to 86th St., there are no bike lanes. From 86th to Unser 4 foot wide bike lanes (five feet including gutter) are in place on the south side of Central and intermittently on the north side. Between Unser and Coors bike lanes are in place on both sides of Central; however at both the Unser and Coors intersections they are not striped, making bicycle travel through the intersection difficult, particularly along the free right hand turn lanes.

c. Medians: Between 98th St. and Unser there are 36 foot wide unimproved medians. Between Unser and Coors the medians are improved with landscaping installed in 2005. The width of the medians gives the roadway a sense of great

openness in this segment, a visual experience that many in the community want to protect.

d. Central Ave. frontage road/drainage area: There is a frontage road on the north side of Central which extends just west of Unser Blvd. to past the City limits. It is lower than the grade of Central and serves as a separate access road. The road is owned and maintained by the City of Albuquerque. City records indicated the street condition between 90th St. and Unser Blvd. was poor to very poor (2011). According to property owners in the area, the road floods periodically, preventing or complicating access to parcels. There is an informal, naturally vegetated area between the frontage road and Central, which varies in width. Some portions of it have been depressed to capture surface runoff.

e. 98th Street: 98th north of Central is a busy street as it carries local traffic as well as eastbound motorists exiting I-40 at what is the first major exit with travel-related services. As a potential first experience of Albuquerque, the roadway along 98th lacks wayfinding elements that could connect travelers to Route 66. Where new development has occurred, the street has been improved with sidewalks, street trees and landscaping. While the number of street trees is meeting code requirements, they are located far from the sidewalks and are not providing pedestrians with shade. The speed limit on Central Ave. from 98th St. to Unser Blvd. is 55 miles per hour. There is excess right of way on both the north and south side of Central Ave.

f. 98th and Central intersection: There is considerable pedestrian use of this intersection by area residents walking to and from stores and services at the strip center located at the intersection's northeast corner. There are some pedestrian safety issues at this intersection related to the north/south crossings and high traffic speeds, wide street width, wide right turning lanes, and lack of developed pedestrian refuges, but as the traffic volumes on Central Ave. are still relatively low, those conflicts only occur at peak hours.

g. Volcano Road and Bridge Boulevard: The portions of these roads within the Plan area are missing curb, gutter and sidewalks, as much of the land remains vacant or underutilized. The diagonal alignments of Volcano Road and Bridge Boulevard where they intersect with Central may become problematic if the properties along those roads develop more intensively, which may trigger a reconfiguration of the intersections.

h. Unser and Central intersection: There are pedestrian and bike safety issues at this high volume intersection related to wide street widths, free rights, lack of adequately sized pedestrian refuges and high traffic speeds. The speed limit is 55 miles per hour. This section of Central Ave. has curb and gutter with intermittent sidewalks. Under an agreement between the City and State, Unser is owned and maintained by the City. It is a limited access roadway.

This intersection was identified as a high priority for pedestrian improvements in the MRCOG PCI (see MRCOG Pedestrian Analysis section). A conceptual roadway improvement plan for this intersection was accepted in 2009 and is moving forward. The goal of this plan was to address pedestrian and cyclist needs at this intersection in order to facilitate the multi-modal development of the Activity Center in this area, in addition to accommodating forecast vehicular traffic on Unser Blvd. and Central Ave. The key components of the intersection designs that were selected as the preferred alternatives include:

1. Two through lanes on Central Ave. in each direction (traffic forecast in three per 2035 MTP).
2. Three through lanes on Unser Blvd. in each direction with no extended storage length required.
3. Dual left turn lanes on Central Ave. in each direction with extended storage length to accommodate expected queuing for projected 2035 traffic.
4. Dual left turn lanes on Unser Blvd. in each direction.
5. Pedestrian and cyclist improvement features that include:
 - a. Right-turn speed table with rumble strips on the approach.
 - b. Pedestrian countdown signals.
 - c. High visibility crosswalks.
 - d. 10 foot sidewalks with landscape buffer zone.
 - e. 10 foot wide medians to provide pedestrian refuge areas and bull-noses on the intersection side to separate refuge areas from vehicular traffic.
 - f. 6' wide bicycle lanes with colored treatment.
 - g. Extended timing push buttons at channelized islands.
 - h. Reduced speed limit on Central Avenue to 40 or 45 MPH.

i. Coors and Central intersection: There are pedestrian and bike safety issues related to free rights in all directions and wide street widths, which make crossing this intersection difficult. There is significant usage at this intersection by transit riders making the Central/Coors transfer. The speed limit is 45 miles per hour

from Coors Blvd. to Airport Drive, and 50 miles per hour west of Airport Drive. This area was identified as a high priority for pedestrian improvements in the MRCOG PCI. The City and State have entered an agreement to transfer Coors Boulevard from City to State jurisdiction, which would mean future improvement projects would be developed and constructed by the NMDOT. Coors is a limited access roadway.

j. Coors Blvd. and Airport Drive: East-west pedestrian and bicycle connectivity to the Alamosa community center is limited to Bridge Blvd. and Central Ave. Access could be improved in the future if a signal was located at Airport Drive, which is located roughly halfway between those intersections. As development occurs in this area, a future signal at this intersection may also help alleviate congestion at the Coors and Central Ave. by allowing a bypass to that intersection.

k. General Segment One Transportation Issues:

Distance between intersections: Currently there are signalized intersections at 98th, 86th, Unser and Coors. A non-operational signal exists at Victory to provide signalized access into Unser Crossing upon its development. The large distances between intersections, in combination with the wide ROW, may inhibit pedestrian connectivity and use across Central and maintain more auto oriented land uses in this segment.

1. Views: It is in this segment that views feel the most open and expansive. Any streetscape plans should preserve vistas.
2. Traffic accidents: According to MRCOG data, the intersections with the highest number of accidents are Central and Coors, Central and Unser Blvd.
3. Traffic speeds: Speed limits in this segment range from 45 miles per hour at Coors Blvd. to 55 miles per hour at 106th St. These high speed limits are not conducive to pedestrian activity. According to MRCOG data (on speed differentials) actual speeds are 10-30% also higher than the speed limit throughout this segment.
4. Signal timing: According to MRCOG data, the current timing of signals appears to hinder vehicular traffic flow in the corridor.
5. Improved Connectivity: Generally, the corridor in this segment would benefit from improved connectivity particularly in the area between Coors and Unser Blvds. Improved connectivity would help alleviate congestion at these intersections and also provide additional access opportunities for many of the deep, front loaded lots in the area.

Transportation Conditions: Segment Two

6.2.2 General Overview Segment Two, Coors to Rio Grande Boulevard:

Segment 2 is generally characterized by smaller lots accessed by individual curb cuts. The most significant grade change on the corridor occurs in this section around 59th St., where there is a grade difference of 70 feet. In addition, this section is marked by a significant narrowing of the ROW. This narrowing combined with the grade change creates unique transitions for the roadway which should be recognized by streetscape improvements. From Atrisco Dr. east, the street changes from a 4 lane, median divided roadway to a 6 lane, median divided roadway as it crosses the river and continues to Rio Grande Boulevard.

a. The West Central Corridor Concept Plan: A concept plan for the segment of Central Avenue that extends from 8th St. to 47th St. was produced in the summer of 2010. The goal of this plan was to make recommendations and present preliminary design concepts that would improve the pedestrian and cyclist environment along Central Avenue. The plan overlaps West Route 66 Sector Development Plan area, and makes the following observations and recommendations concerning this segment. There is an abundance of curb cuts along Central Avenue between 47th St. and Rio Grande Blvd.; driveways should be narrowed and curb cuts reduced where possible in order to create a more comfortable pedestrian environment. The plan also recommends pedestrian improvements for the Central Avenue/Rio Grande Blvd. intersection that include: providing a two-to-five second leading pedestrian signal in order to give pedestrians a head start when crossing Central Avenue; extending the median on the west side of the intersection in order to provide pedestrians a small refuge; eliminating the westbound turn lane in order to provide a larger median refuge and landscaping; decreasing the turn radii at the northeast corner of the intersection as means of slowing right-turning vehicles; and restriping the crosswalks.

b. Median improvements: Median improvements were installed in the period between 1997 and 2002. Much of the vegetation in the medians located between Old Coors and Coors Blvd. appears to have died.

c. Sidewalks, street trees, street lights and curb cuts: Sidewalks exist along both sides of the road throughout this segment, but are narrow, at the curb, and interrupted by frequent curb cuts. From Old Coors to Atrisco and from the river to Rio Grande Boulevard, the sidewalk is frequently obstructed by utility poles. In

other areas the sidewalks are in disrepair. The sidewalk is missing on the east side of Atrisco Dr. in front of the New Mexico Gas Company site. There is no pedestrian street lighting along this segment of Central which undermines safety at night.

d. Distance between intersections: Signalized intersections generally occur every $\frac{3}{4}$ to $\frac{1}{2}$ mile between Coors Blvd. and 47th St. and every $\frac{1}{4}$ mile between 47th St. and Rio Grande Blvd. The limited number of signalized intersections make pedestrian accessibility across Central difficult and unsafe, particularly in areas that are planned for more pedestrian oriented development.

e. Views: The higher elevations and steeper grades of this segment, generally between 59th St. and the Arenal Canal, boast significant views, which should be preserved in streetscape design.

f. Varying street widths and excess ROW: According to City ROW data, there is an excess of 5 to 8 feet along both sides of Central from Coors Blvd. to Atrisco Drive. Marking the edge of this ROW are power and phone lines, which run the length of this section. This excess ROW should be considered for streetscape improvements.

g. Pedestrian improvements: From Atrisco Drive to Rio Grande Boulevard is the only six lane section of Central, with traffic volumes on the road that make pedestrian experience very unpleasant in terms of noise and vehicular activity. Any plans to create a new pedestrian oriented land use environment must be coordinated with transportation improvements that mitigate the effects of this 6 lane section, in order to ensure their mutual success.

h. Vehicular traffic speeds: This section is characterized by high traffic speeds, often in excess of 15 miles per hour higher than posted speeds. Much of the speeding occurs as vehicles move up and down the hill and over the bridge. Speed limits in this segment are 40 miles per hour between Coors and Old Coors, and 35 miles per hour between Old Coors and Rio Grande Blvd.

i. Off-set intersection at Old Coors and Yucca: The offset intersection creates problems for vehicular movement through the intersection, as well as confusion for pedestrians about when and how to cross. Crosswalks at Yucca and Old Coors

are not well defined. There is no crosswalk on the east side of the intersection at Yucca or on the west side of the intersection at Old Coors.

j. Atrisco and Central Ave intersection: This intersection is difficult to cross on foot due to free rights, wide street widths, and a lack of pedestrian refuges. The forced right onto northbound Atrisco is often a surprise to westbound drivers, who must either dart back into the west bound Central Ave traffic lane or take the right onto Atrisco and circle back to Central Ave. This area was identified as a high priority for pedestrian improvements in the MRCOG PCI.

k. Atrisco Drive: The primary entrance for the Atrisco Plaza is located at the northwest corner of Atrisco and Central Ave., near the western edge of the site. With a free right into the parking lot from westbound Central Ave. As a result, it is very difficult to cross this drive pad/curb cut into the Atrisco Plaza on foot. There is currently no pedestrian access to and through the site from the eastern edge of the site or from the Rapid Ride stop on Central Ave. A high volume of traffic also accesses Atrisco Plaza from Atrisco Drive, especially from the eastbound left turn lane on Central Ave.

l. Atrisco to Central Bridge: The sidewalk along the north side of Central in this section is less than 6 feet wide and meanders along the street edge, with some portions separated by minimally planted landscape buffers. Even in portions with a landscape buffer, vehicular noise is high and speeds exceeding 50 mph make walking unpleasant for the pedestrian. The sidewalk along Central's south side is less than 6 feet wide and is located at the curb. A small landscaped strip is located on the southern edge of the sidewalk with street trees. This landscaped strip provides some shade, but no buffer between fast moving vehicular traffic and the pedestrian realm.

m. Sunset Drive and Central intersection: Crosswalks are not well defined and there is no pedestrian refuge.

n. Bridge Crossing: Despite improvements, which include artwork and balconies which open up views onto the River, the lack of buffering from the high traffic speeds still creates an unpleasant walking experience, potentially discouraging pedestrian crossings of the bridge to area destinations on both sides of the River.

o. Tingley Drive: Pedestrian crossings are not well defined at this wide intersection. Vehicles exit onto Central from multiple locations--the Bosque parking lot, BioPark parking lot, and Tingley Drive. Improvements have been made for vehicular access to the BioPark, including a right turn slip lane and wide drives, which may lead to the perception that this entrance is the primary entrance into the BioPark. An insectarium is being built on the west side of the property and the parking lot accessed from Tingley Drive will be reconfigured as a part of this project. However, the entrance to the east, at New York Ave., will continue to be the primary entrance to the Bio Park.

p. Central along the BioPark: There is a wide sidewalk in front of the BioPark, but it is at the curb, with landscaping on the parcel side, so that pedestrians are not buffered from fast moving traffic. On the south side of Central, a narrow sidewalk runs along the curb. There is a significant problem of speeding during non-peak hours in this section of Central Ave.

q. New York Ave. and Central Ave. intersection: Pedestrian crossing of this six lane intersection is difficult and uncomfortable. This intersection was identified as a high priority for pedestrian improvements in the MRCOG PCI. The entrance to the BioPark is often unclear to first time visitors and, with no streetscape improvements in place at this intersection, gives a rundown image of Central Ave. The Alameda Lateral runs underground at this intersection leaving an unimproved area which could become a pocket park, with landscaping that enhances the appearance of the intersection. A bike route along this lateral could be signed to indicate a connection via Panmunjon Road to the Mountain Road Bike Boulevard.

r. Rio Grande Boulevard and Central intersection: This is a heavily used intersection which is unsafe and difficult for pedestrians to cross. Traffic congestion, high traffic speeds, multiple turn lanes, and wide street sections discourage pedestrian travel from Old Town to the BioPark. The pedestrian signal at this intersection does not allow enough time for pedestrians to cross Central Ave. and vehicles making right turns from Central Ave. on to Rio Grande Blvd. are not yielding to pedestrians who have the right of way. This intersection and the area immediately west were identified as a high priority for pedestrian improvements in the MRCOG PCI.

Transportation Conditions: Segment Two

- s. Soto Ave. from Simmonds St. to Rio Grande Blvd.:** Soto Ave. is paved but looks and functions more like an alley than a street. It currently has issues with drainage when it rains, and does not connect to Rio Grande Blvd due to a vacation that blocked its connection with Rio Grande Blvd. However, there maybe an opportunity to reconnect Soto Ave. with Rio Grande Blvd. via Hollywood Ave. for pedestrians and cyclists.
- t. Flooding in streets (Rio Grande Blvd. to river):** Flat topography and too much impervious surface area create drainage issues in this area (see also Drainage Conditions).
- u. Traffic accidents:** According to MRCOG data, the highest numbers of vehicle/pedestrian accidents along Central Ave. in Segment Two are at Rio Grande Blvd. and Atrisco Dr.
- v. Arenal Canal, Isleta Drain, Atrisco Lateral/Drain/Ditch and Alameda Lateral.** These MRGCD facilities cross Central and may offer opportunities for trail connections from surrounding residential neighborhoods to the corridor. Under current MRGCD policy, trails would remain informal unless another entity is prepared to obtain a permit to improve and maintain them (See Trails section page xx for more information).

6.3 Transit Overview

Public transit plays a key role in the transportation system of the West Route 66 plan area. West Central Avenue has been identified as a Priority Transit Improvement Corridor by the 2035 Metropolitan Transportation Plan (MTP). This designation identifies the West Central corridor as “well suited for further evaluation and development of potential high frequency and high volume transit service over the coming decades”. Portions of the Corridor are also designated as either an “Enhanced Transit Corridor” or “Major Transit Corridor” by the Albuquerque/Bernalillo County Comprehensive Plan. The segment of Central Avenue within the plan area is also of particular regional interest because of the Rio Grande bridge crossing which will be facing increased congestion based on projected growth trends. To reduce projected vehicular crossings, the 2035 MTP has identified a mode share goal of 10% of all river crossing trips to be taken by transit by 2025 and 20% of all trips by 2035.

6.3.1 City Transit Policy

Albuquerque/Bernalillo County Comprehensive Plan has designated Central Avenue with two separate transit designations within the plan area: Enhanced and Major Transit Corridor. From the City limit to Atrisco Blvd, Central is an Enhanced Transit Corridor, and from Central Bridge to Rio Grande Boulevard, it is designated a Major Transit Corridor. The designations of the major roadways intersecting Central are:

- a. Unser Blvd. as an Express Corridor on the north side of Central Avenue and as an Enhanced Transit Corridor on the south side of Central Avenue.
- b. Coors Blvd as an Enhanced Corridor on the north side of Central Avenue and as an Express Corridor on the south side of Central Avenue.
- c. Rio Grande Blvd. as an Enhanced Transit Corridor.

The purpose of an Express Corridor is to move passengers quickly from one destination to another, in a manner that supports commuter travel. It is often characterized by limited access restrictions and a recreational and commuter bicycle and trail network which emphasizes connections among Activity Centers.

An Enhanced Corridor is characterized by some access control and pedestrian improvements that facilitate transit. Other identifying elements include:

- speed limits 35-45 mph

- travel lanes shared between transit and autos
- on street parking on a case by case basis
- maximized pedestrian connections to transit stops and between adjacent developments
- 6-8 foot sidewalks
- 4 foot sidewalk setback
- bicycle circulation based on bike plan
- Modal Hierarchy: Transit, Auto, Pedestrian, Bikes

6.3.2 Issues and Opportunities Inventory:

There are three existing bus routes along Central Avenue within the plan area. Route 198 serves Central Avenue from 98th St. to the Central and Unser Transit Center. Route 66 is a local line that has frequent stops and serves the plan area from the Central and Unser Transit Center to Tramway. The 766 Rapid Ride route also serves Central Ave. from the Central and Unser Transit Center to Rio Grande Blvd. The Rapid Ride is a minimal form of Bus Rapid Transit (BRT). ABQRide is beginning an “Alternatives Analysis” to decide how to enhance transit service significantly in the Central Avenue corridor. Potential improvements include dedicated lanes for buses, raised platform stops, transit signal priority, queue jumpers, and off-board fare collection. ABQRide does not have fixed route service from 106th St. to 98th St. This area is currently only covered by paratransit.

a. Central and Unser Transit Center/Park and Ride: This facility was opened in 2009 and serves about 2% of 66 Central westbound and eastbound boardings and about 5% of all Rapid Ride 766 boardings. At this time the Central and Unser Transit Center is the only Park and Ride located or planned in the area. However, ABQRide has suggested that additional locations west of the Central Bridge could increase transit ridership and reduce vehicular bridge crossings.

b. Red Line Rapid Ride: Rapid Ride stops are located along Central at the Unser Transit Center, Coors Blvd, Atrisco Drive and Rio Grande. The Red Line Rapid Ride connects with the Railrunner Commuter Train in downtown Albuquerque at the Alvarado Transportation Center.

The intersections with Rapid Ride stops should be enhanced to improve pedestrian safety and accessibility. Passengers are currently exiting the buses and crossing

Transportation Conditions: Transit

Central Ave. outside of crosswalks in order to make their connecting bus. Rapid Ride stops provide easy regional access to existing and proposed shopping centers. However, difficult pedestrian access along and across Central often impedes pedestrian movement making public transportation difficult, especially when carrying goods or travelling with young children.

c. Route 66: extends within the plan area from the Central and Unser Transit Center to Rio Grande Blvd., stops are placed less than ¼ mile apart for both east and west bound lines. Route 66 connects with the Railrunner Commuter Train in downtown Albuquerque at the Alvarado Transportation Center. Bus stop facilities are missing or inadequate in many locations.

d. Route 198: serves Central Ave. from 98th St. to the Central and Unser Transit Center. From the Transit Center, passengers can connect to either the Red Line Rapid Ride or the Route 66 to continue travelling east on Central Ave.

e. Condition of Bus Stops: Bus Stops located at major intersections within the plan area all have shelters and benches. For the most part, other stops for the Route 66 have benches, but no shelter. Some notable locations without benches include the stop just west of Coors in front of Verizon Wireless, Legacy Church, 64th St, 52nd St., Cypress St., 48th St., and the Beach Apartments. The stop just east of Unser, on the south side of Central, is located in the dirt ROW with no improvements. ABQ Ride is working on a City-wide project to install shelters at higher ridership stops that have sufficient right-of-way. That project will include adding shelters at some locations identified here.

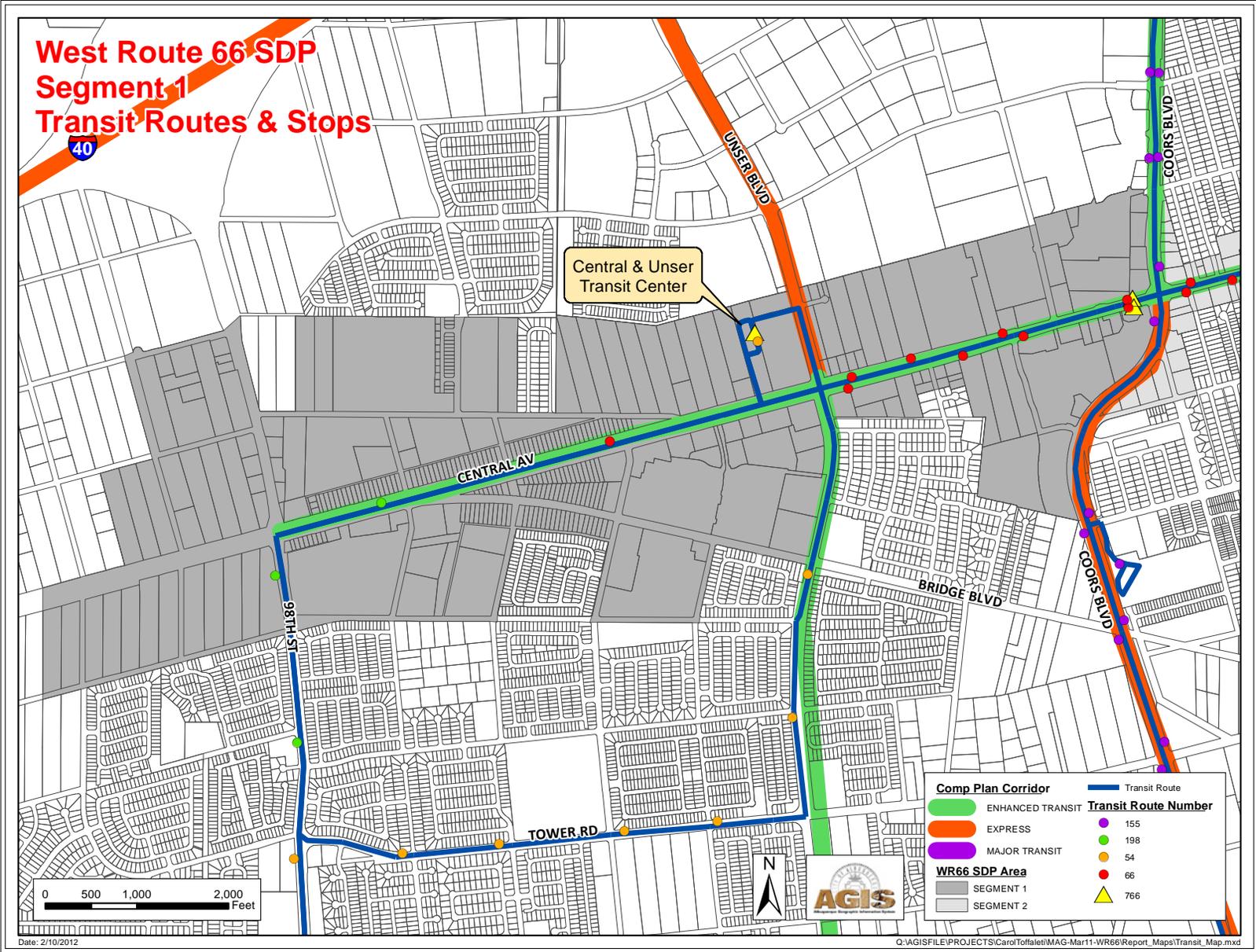


Figure 27: Transit, Segment One

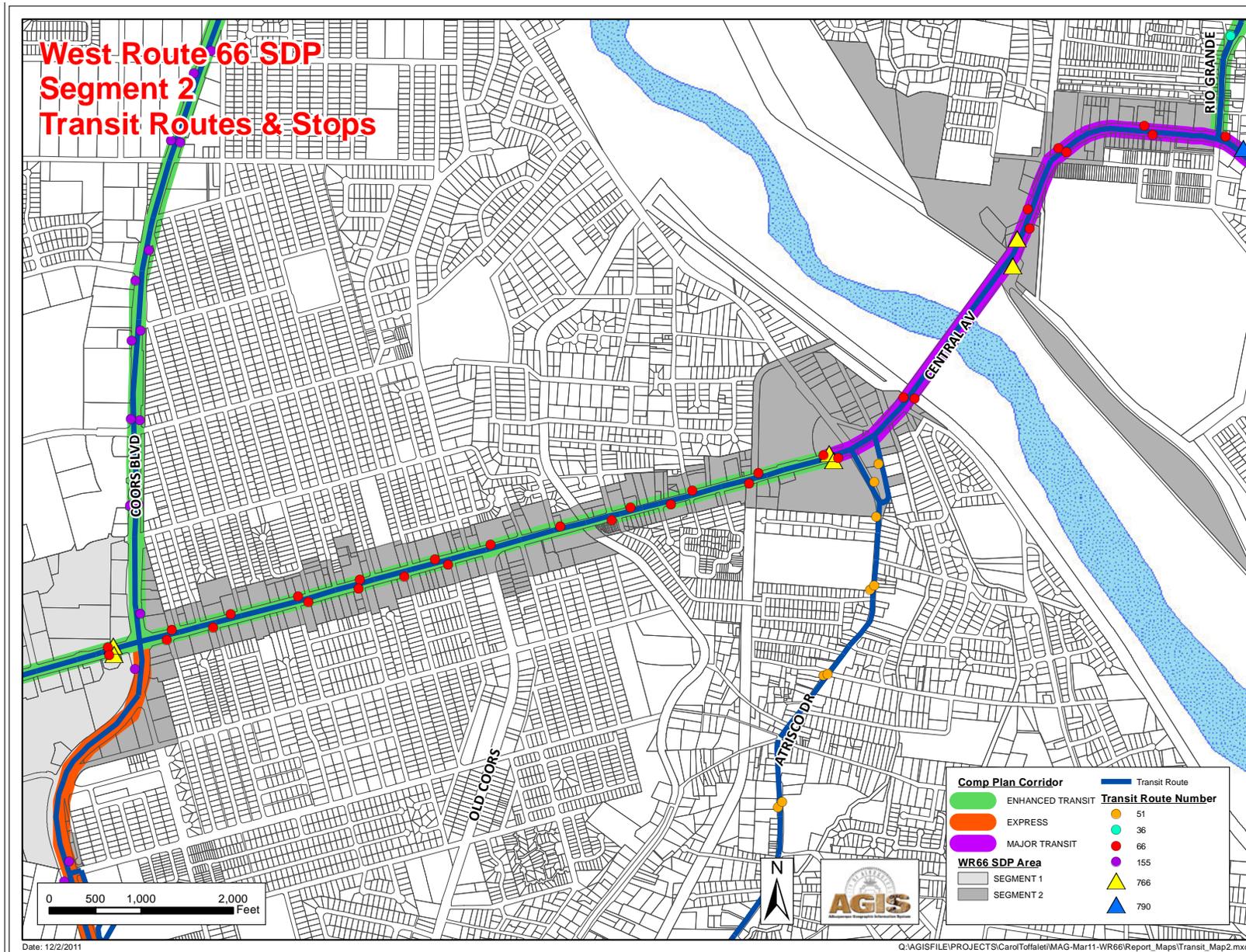


Figure 28: Transit, Segment Two

Transportation Conditions: Bikeways and Multi-Use Trails

6.4 Bikeways and Multi-Use Trails

There are three types of bicycle facilities within the plan area: bike lanes and routes maintained by the Department of Municipal Development, multi-use trails maintained by the Parks Department and multi-use trails maintained by the Open Space Division. Existing conditions for bike lanes and multi-use trails are outlined below. Please see the Parks, Open Space and Trails Section for information on Open Space and MRGCD trails.

6.4.1 Issues and Opportunities Inventory

Bike lanes are complete from Unser to Tingley Drive. From 106th St. to Unser Blvd. they are intermittent and from Tingley Ave. to Rio Grande Blvd. they are missing.

The multi-use trails that intersect the plan area are: the Paseo del Bosque (Open Space), the north side of Coors Blvd., the south side of Unser Blvd., and the south side of 98th St. (Parks). The gap in the multi-use trail on the north side of Unser Blvd. would be completed as part of the Central Ave./Unser Blvd. intersection project.

a. Missing sections of bike lanes:

Segment 1: Bike lane striping is intermittent in this section. From 98th to 86th St., there are no bike lanes. From 86th to Unser 4 foot wide bike lanes (5 feet including gutter) are in place on the south side of Central and intermittently on the north side. Between Unser and Coors bike lanes are in place on both sides of Central. However at both the Unser and Coors intersections they are not striped, making bicycle travel through the intersection difficult, particularly along the free right hand turn lanes. The gap in the multi-use trail on the northside of Unser Blvd. would be completed as part of the Central Ave./Unser Blvd. intersection project.

Segment 2: Bike Route between New York and Rio Grande. The preferred alternative for the reconfiguration of the Central Avenue/Lomas Boulevard intersection in the West Central Avenue Corridor Concept Plan proposes that rather than travel west on Central Avenue, bicyclists should be directed to turn left on San Pasquale Avenue, just east of the plan area, in order to connect to the bike route on Alhambra and New York Avenues. Bicycle safety would be improved on this route by the indication of shared use between bicycles and vehicles through the use of

sharrows (special arrows that indicate the lane is to be shared by bicycles and vehicles) and colored pavement.

b. Other identified issues:

In Segment 2, excessive vehicular speeds and frequent curb cuts create an unsafe biking experience.

- Bicycle crash data from 1995 to 2005 (2006-2009 data was unavailable) indicates that Central Ave. is a high crash corridor. Statistics for Central Ave. west of the river show that there were 33 bicycle crashes in this area. The intersection at Central Ave. and Sunset Blvd. was identified as a high crash intersection, with 6 crashes occurring in the 1995-2005 period.
- Data obtained through community surveys for the update of the Bicycle Facilities Masterplan, indicate that Central Ave. between Rio Grande Blvd. and Coors Blvd. has a high bicycle ridership with riders performing errands and travel to other destinations. According to the surveys, these are trips that are being performed by bicycle that would otherwise likely be taken by automobile.

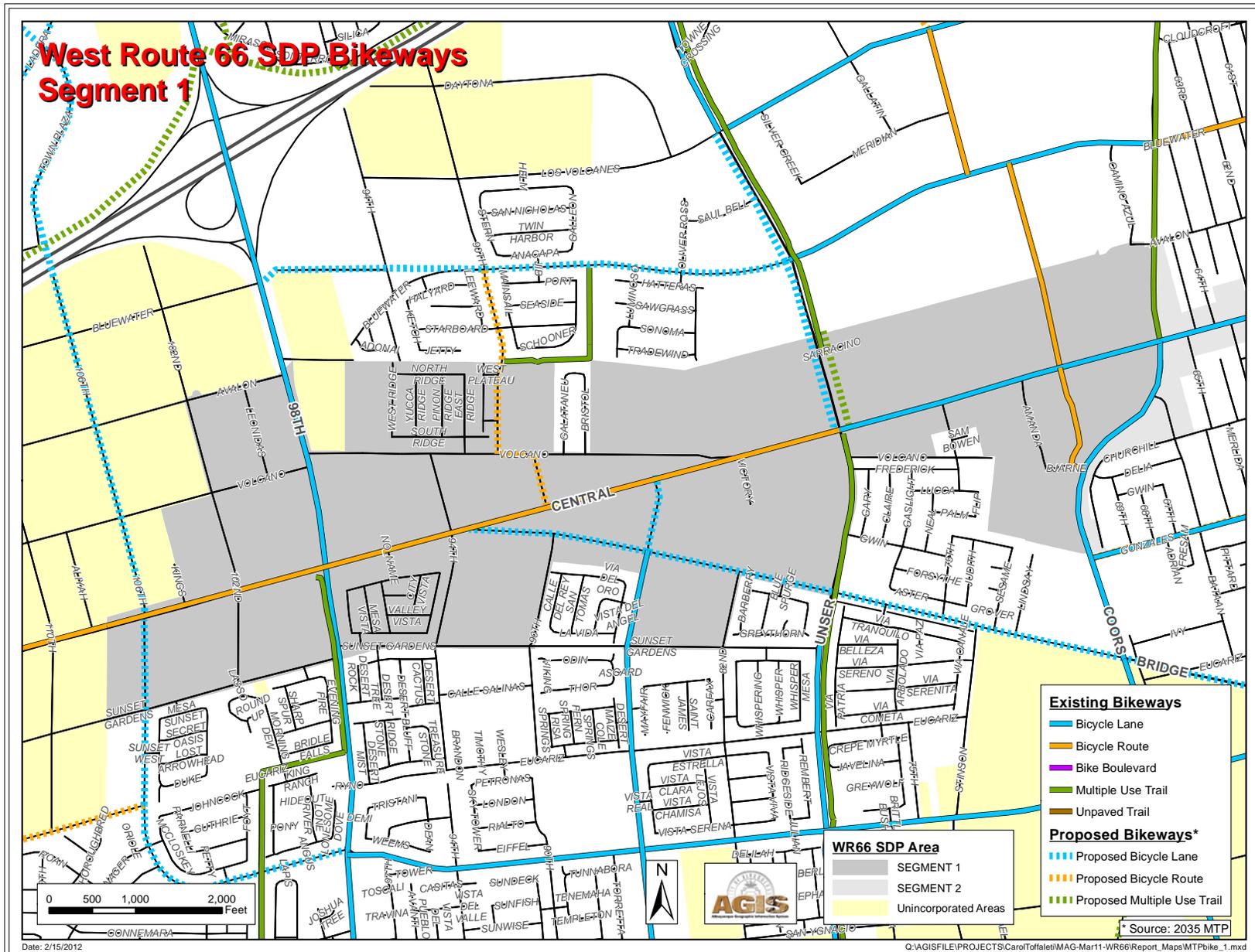


Figure 29: Bikeways, Segment One

Transportation Conditions: Bikeways and Multi-Use Trails

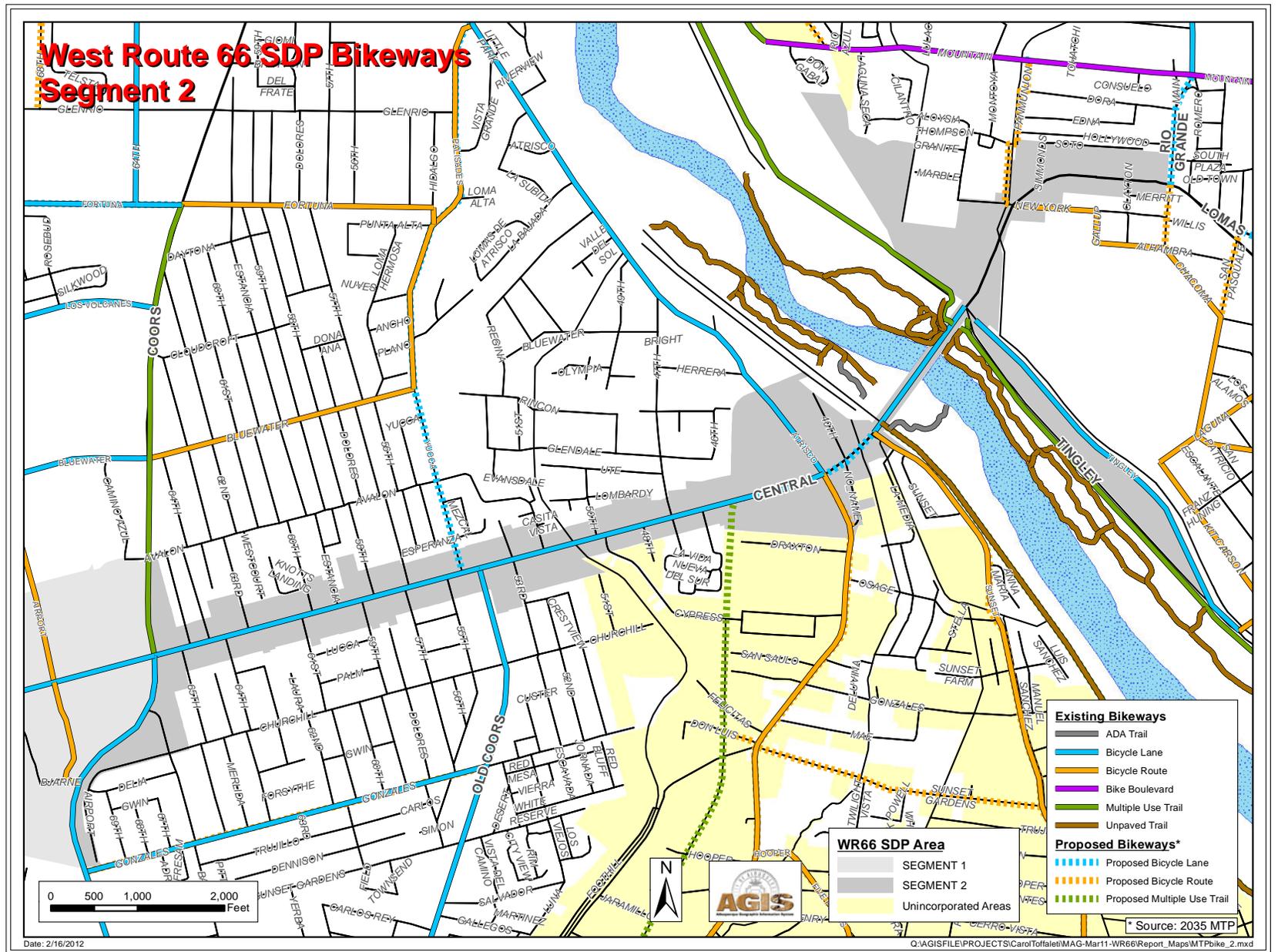


Figure 30: Bikeways, Segment Two

Existing Conditions: City Facilities and Services

7.0 City Facilities and Services Overview

City Facilities within and near the Plan area include Fire and Police Stations, Libraries, Community Centers and the Bio Park. The map in Figure 27 indicates the location of City Facilities serving the Plan area.

7.1 Fire Stations and Police Sub Stations

Fire Station 7 is currently located just north of Central on 47th St., however it is being replaced by a new station located at Central and 57th St. The existing Fire Station 7 serves a 4.1 square mile area. Fire station 14 lies just south of the plan area off 98th street, but with a coverage of 17.5 square miles serves the plan area as well. East of the river is covered by Fire Station 1, located downtown at 724 Silver SW. The response zone is 2.1 square miles.

The Plan area west of the river is served by the Shawn McNethy Police Substation west of Coors. The Old Town Substation located just outside the Plan area no longer serves as a police substation.

7.2 Libraries

There are currently no libraries located within the plan area. The Robert L. Murphy library, located inside the Alamosa Community Center, serves the middle and western portion of the plan area. The Westgate Library is located south of Central and east of 114th St. and serves the southwestern edge of the plan area. A new library project is underway, to be located on the City-owned Metropolitan Redevelopment site at Central and Unser.

7.3 Community Centers

7.3.1 Alamosa Multi-Service Center. This center, just south of the plan boundary off Coors, serves the western portion of the plan area. It shares a building with a City-operated Health and Social Service Center, a Child and Family Development Services Program and the Alamosa/Robert L. Murphy Library. The Center has a gymnasium, game room, outdoor basketball courts, arts and crafts room, meeting rooms, fitness center and an outdoor amphitheater/performance space. An early childhood development center is also located at this facility. Adjacent to the Center is the Alamosa Skate Park, managed by Parks, which was designed for BMX bikes, skateboards, and in-line skates. The Health and Social Services Center provides emergency food, clothing, utility assistance, as well as community meeting rooms

and seasonal services and activities to residents of the Southwest Mesa. The following health care and social service organizations have facilities inside the Alamosa Center: Community Dental Services, First Choice Community Healthcare, First Choice Women, Infant, and Children, Public Health Division, UNM Hospital Clinic, UNM Hospital-Maternity and Family Planning Clinic, Ser de New Mexico, Youth Development Inc., and the Alamosa Child Development Center.

7.3.2 West Mesa Community Center. This center, located off Glenrio Rd. NW, just north of the plan boundary serves the eastern portion of the plan area. The facility has two large meeting rooms, a small kitchen, 2 classrooms, a crafts room, fitness center, lockers and showers, a children's library, game room, computer lab and a mini gym. Outside there is a park facility managed by Parks, (see Parks Context), which includes: 1 playground, 1 softball field, 1 indoor pool, in outdoor pool, 1 volleyball court, and 2 picnic tables.

7.3.2 Pat Hurley Center. This center is a small community center located at 3928 Rincon Drive NW, adjacent to Pat Hurley Park, and just north of the plan boundary. The Center has before and after school youth programs, provides transportation to other community centers for sports activities and provides meeting space for neighborhood associations.

7.4 Issues and Opportunities

The community is generally well served by community services; however pedestrian and bicycle connectivity should be improved between the services and adjacent neighborhoods. There are areas in and around the western portion of the plan area that are not as well served by community centers. West of Coors there are no community centers to serve the large residential population north and south of the Central area. Improved connections between Alamosa Community Center across Coors could help alleviate some of this need. Currently there is no east west pedestrian access across Coors to the Alamosa Center. Future plans for Coors should consider improving the connectivity across this limited access roadway.

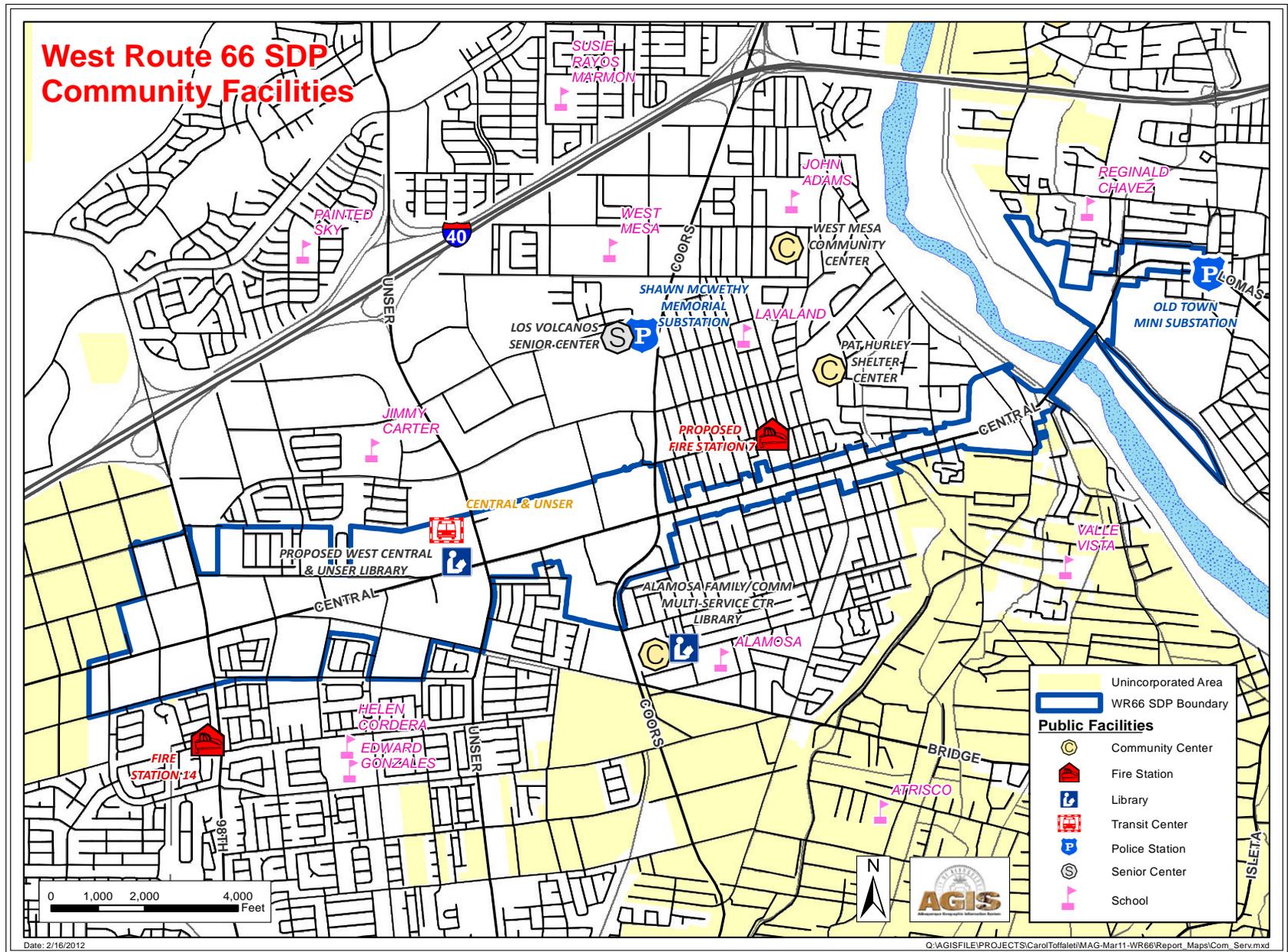


Figure 31: Community Facilities

Existing Conditions: City Facilities and Services

7.7 Albuquerque Bio Park. The Plan area contains the following facilities of the Albuquerque Bio Park: the Albuquerque Aquarium, the Rio Grande Botanic Garden and Tingley Beach. The Zoo is also part of the BioPark, but is located south of the Plan area. A small trail links it to the other BioPark facilities. These facilities are a premier destination within the City of Albuquerque for residents and visitors alike, attracting over 400,000 visitors a year. The Albuquerque Biological Park operates as a division of the Department of Cultural Affairs under policy direction set by the Mayor, City Council, the Director of the Department of Cultural Affairs and the Albuquerque Biological Park Advisory Board.

7.8 Issues and Opportunities pending

8.0 Water and Sewer Overview

The Albuquerque Bernalillo County Water Utility Authority (ABCWUA) is the government entity in charge of water and sewer service for the City Albuquerque. One issue related to the service was identified during the community planning process: an ongoing odor problem at the gravity interceptor sewer line located at Central and Yucca.

8.1 Issues and Opportunities

The source of the sewerage problem is a gravity-fed sewer line that runs from the northwest mesa to the ABCWUA wastewater treatment plant in Mountain View. There is a dramatic grade change around the Central and Yucca intersection. When the sewage travels through this grade change, it creates a lot of turbulence and excess gases are released at manholes and other openings in the system.

In order to remedy this problem, the city installed a biofilter at the site in 2007. The biofilter seemed to solve the problem in part by reducing the area where the smell was an issue. However the city continued to receive complaints from residents immediately adjacent to the site of the biofilter.

An outside engineering firm was hired to perform tests on the effectiveness of the biofilter, measure scent levels and analyze neighborhood complaints. Their 2010 report indicates that while the biofilter was performing to expectations, some smell did remain in the area.

Based on this finding, the Authority re-designed the filter; this new system was installed in May 2010. The Authority believes that this new system is taking care of the problem; however they continue to monitor the issue. According to the community, the smell in the general area, if not on this particular site on Yucca, continues to be a problem.

Existing Conditions: Drainage

9.0 Drainage Overview

The City of Albuquerque is currently working on their application to the Environmental Protection Agency (EPA) for a new stormwater permit. The EPA has tightened their regulations regarding stormwater pollution and on site management of stormwater since the last permit was issued in XX. In response to these new EPA regulations, the City is also strengthening its requirements. The City is drafting a new Stormwater Ordinance that may be approved in 2012. In anticipation of the ordinance approval, the City's Hydrology and Stormwater Management Divisions are very supportive of innovative on-site stormwater management techniques that help reduce pollution, volume and velocity of stormwater. Once the Ordinance is approved these types of techniques will become a mandatory element of development.

The following is a list of drainage studies and projects that affect the plan area:

Plans and Studies:

- Alameda and Riverside Drains (Leedshill-Herkenhoff, 1991): Projects ARD-7A and ARD-8A Albuquerque Master Drainage Study, Volume 1 (Bohannan-Huston Inc, 1981): no projects in area
- Amole Watershed Drainage Master Plan (Holmes & Narver Inc, 1986): no projects in area
- Amole-Hubbell Drainage Master Plan (Leedshill-Herkenhoff, 1999): no projects in area
- Isleta Watershed Study (CH2M HILL, 1986): Projects CE2-513B, CE3-515C, and VA1-630C
- Southwest Valley Flood Damage Reduction Study (Resource Technology Inc, 2004): Project K-12D
- West Bluff Drainage Plan (Andrews, Asbury & Robert Inc, 1987): no projects in area

Projects:

- ARD-7A: Reach No. 7 from Mountain to Central: concrete line the Alameda Drain - Construct an additional 6'x10' box culvert at Hollywood
- ARD-8A: Reach No. 8 from Central to the Riverside Drain: concrete line the Alameda Drain
- CE2-513B: Replace 2,000 feet of 12" and 15" RCP with a 24" RCP along 57th St from Bluewater Rd. to Central
- CE3-515C: Replace approx 1,100 feet of 15" RCP with 24" RCP along Yucca

Dr from Avalon Rd. to Central Ave.

- VA1-630C: Construct 2 inlets and 400 feet of storm drain with a flap gate from Sunset Place to the Riverside Drain.
- K-12D: Approximately 900 feet of storm drain in Sunset Road north of La Media Rd.

Additional studies and projects are available from the Hydrology section of the Planning Department.

9.1 Issues and Opportunities

- a. There are two drainage ponds/jurisdictional dams that are owned by the City and managed by the Office of the State Engineer located at the northwest corner of the Central Avenue and 98th St. intersection. These ponds are mostly handling stormwater drainage from undeveloped properties in the area. These ponds are located on land that is more ideally suited for commercial and/or higher density commercial development, as the rest of the intersection has already begun to develop in this way;. However this drainage facility will likely not change unless there is significant development in the area which would require property owners to manage more of their stormwater on-site.
- b. There is a significant issue with poor drainage and flooding along the frontage road which extends from Unser Boulevard to the western plan boundary. The most significant flooding issues appear to be in the portion from Unser Boulevard to 98th St. The flooding has been reported to create access issues for properties located along this road. Opportunities exist to redirect stormwater from the road into street side swales, particularly in the vegetated area between the frontage road and Central Ave.
- c. There is limited stormwater capacity in the storm drain between Unser Blvd. and Coors Blvd. This constraint requires development in this area to pond on site. Opportunities exist in this area to employ Low Impact Development techniques to reduce stormwater runoff and to treat required ponding as landscape amenities.
- d. There are some issues with flooding in the area between the river and Rio Grande Boulevard. This is largely due to the flat topography and the abundance of impervious surfaces, but also to an insufficient electricity supply in the area. The Alcalde pump station is not operating at optimal capacity due to

electricity voltage fluctuations. This would be a good location within the Plan area to manage the excess stormwater through increased landscaping and Low Impact Development techniques.

- e. The portion of the Plan area along Central Ave. from the river to Rio Grande Blvd. falls within the Alcalde Drainage Basin. This is a closed basin that is lower in elevation than the River and has minimal opportunities for storage of excess water can be conveyed to the river by the pump station. This causes the excess water to be stored in the roadways and areas around the lowest point in the drainage basin.

Existing Conditions: Drainage



Figure 32: Drainage Facilities, Segment 1- A

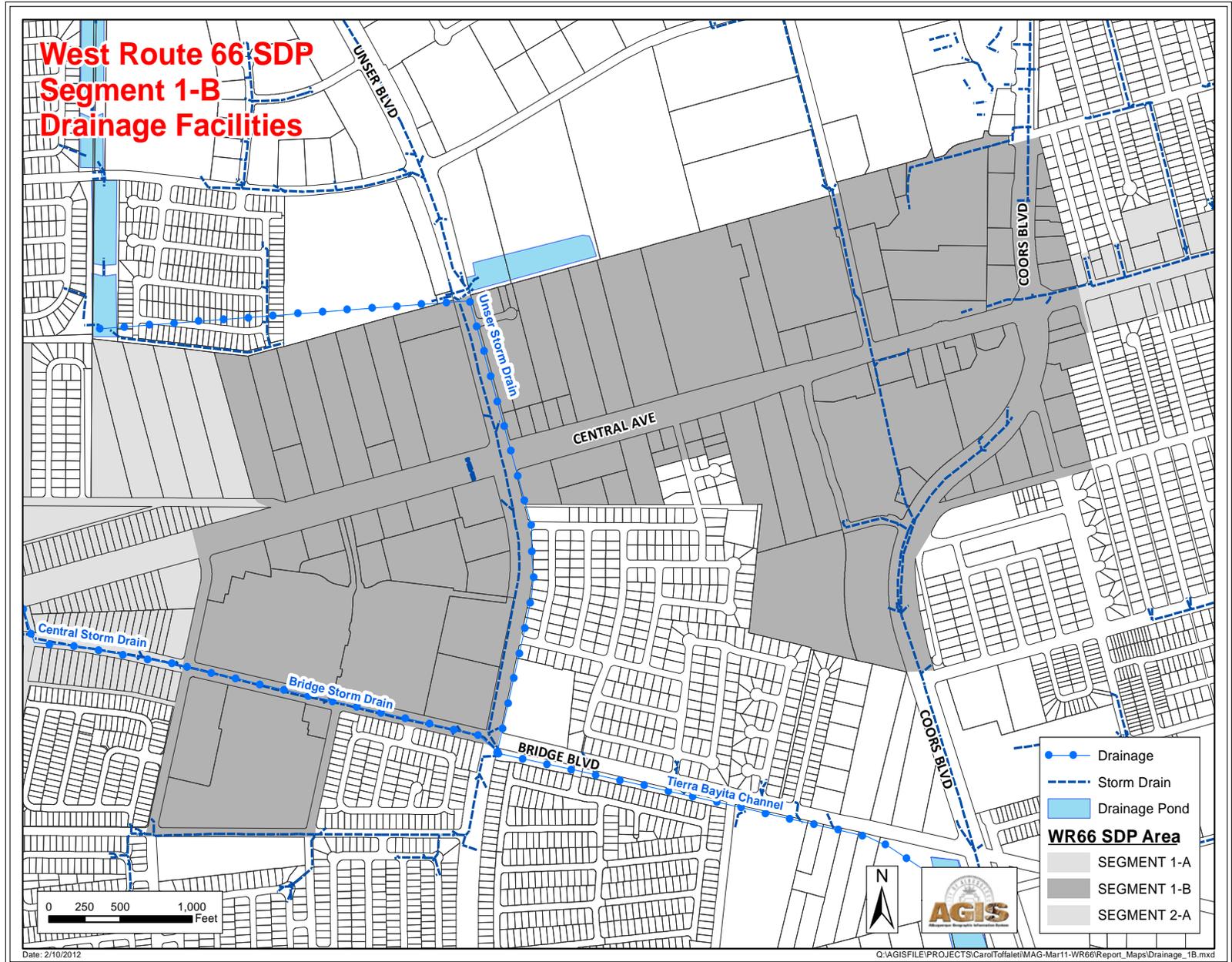


Figure 33: Drainage Facilities, Segment 1-B

Existing Conditions: Drainage

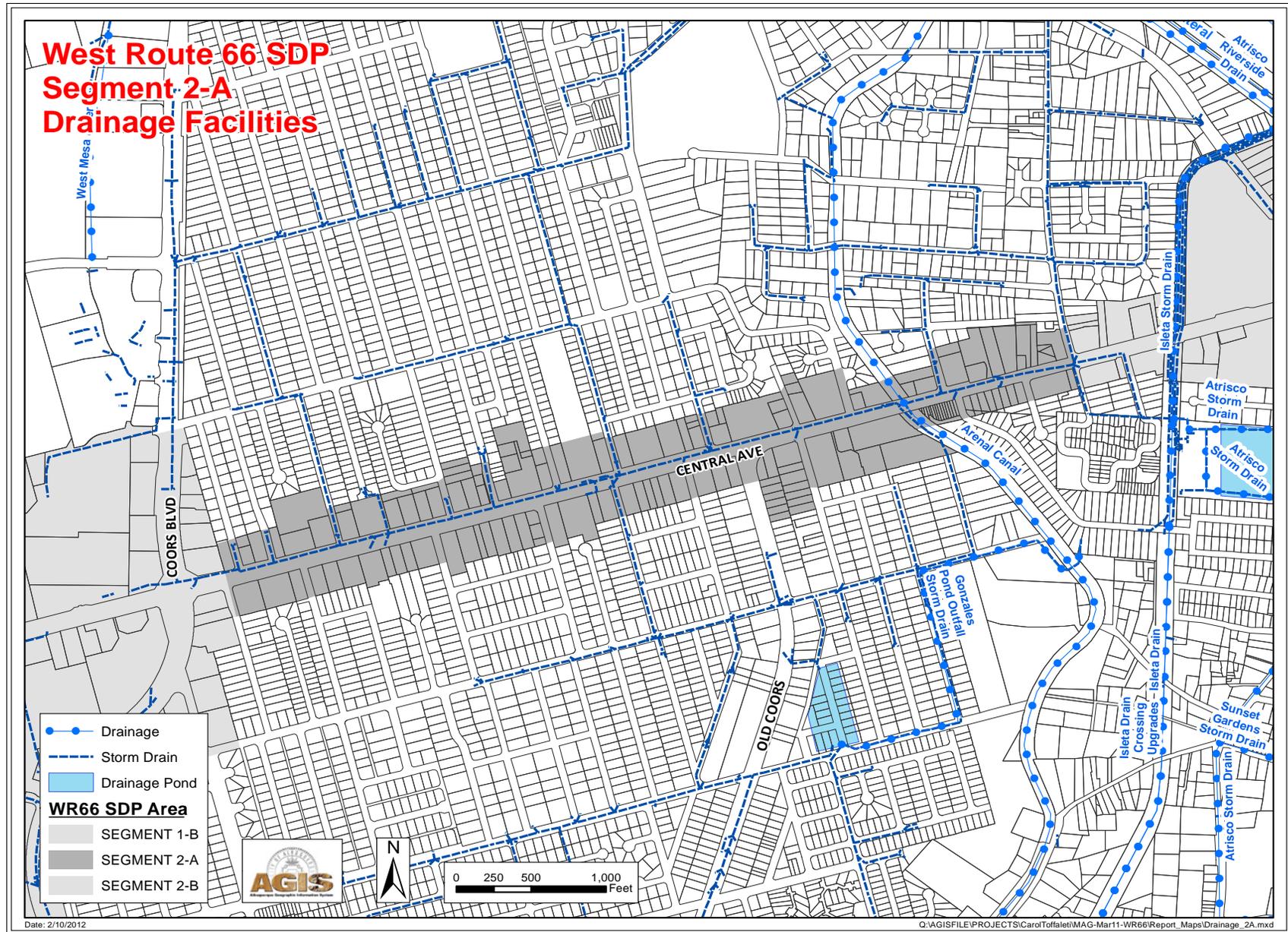


Figure 34: Drainage Facilities, Segment 2-A

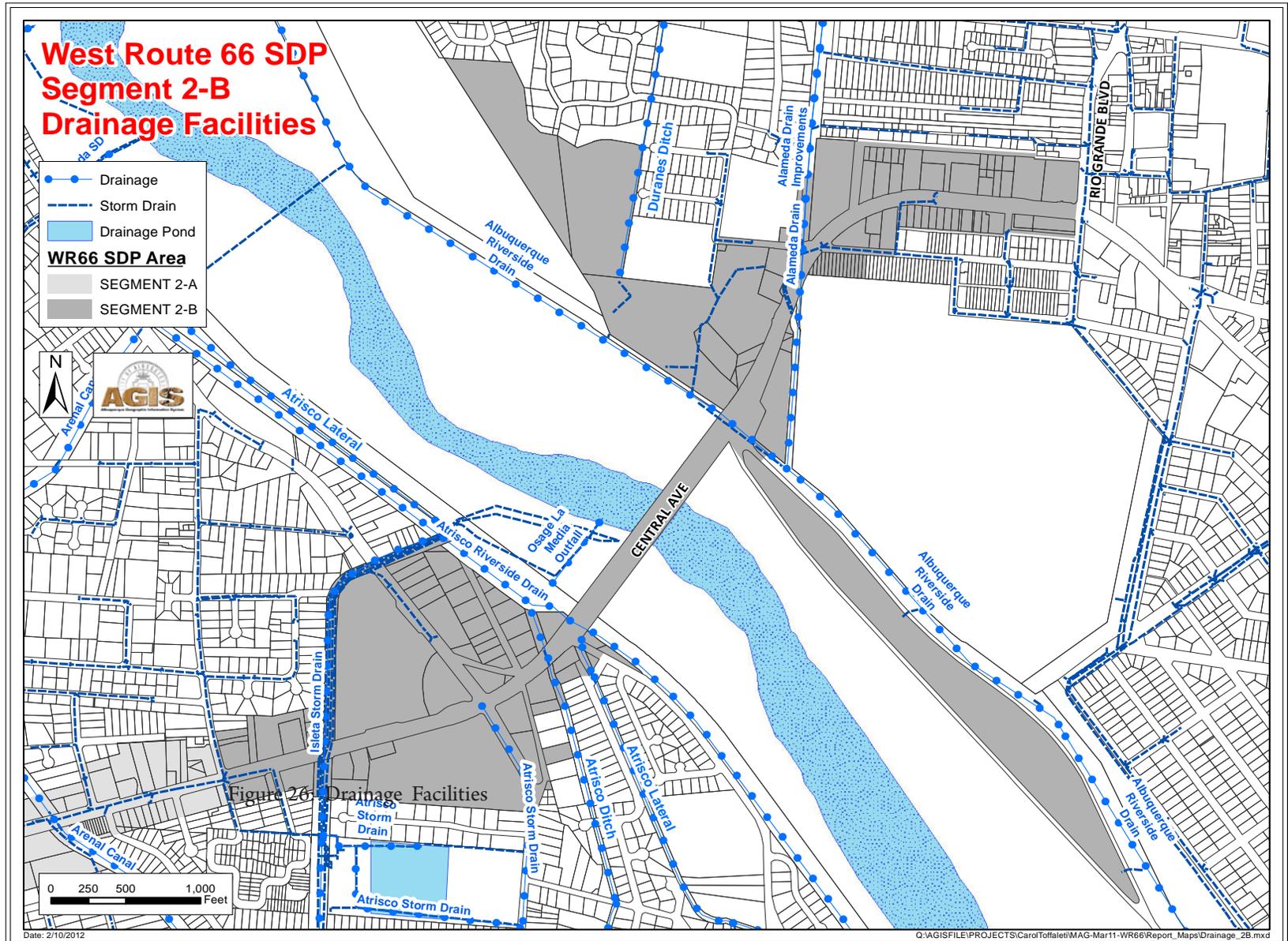


Figure 35: Drainage Facilities, Segment 2-B

Existing Conditions: Utilities

10. Utilities

10.1 Gas

There is a New Mexico Gas Company border station which is used to reduce pressure from distribution levels to transmission levels just north of Central Ave. on Atrisco Dr. The New Mexico Gas Company has no plans to develop or change the site in the near future.

10.1.1 Issues and Observations

The border station site is fenced by a chainlink fence with no landscaping to screen the facility and sidewalks are missing along the site. The lack of improvements gives the site and adjacent streetscape a aura of neglect. There are no special landscaping requirements for public utility facilities located in C-2 zones, the existing zoning of the property.

10.2 Electricity

Two double-circuit 115 kV transmission lines, the PM line and the PW line, are located within the West Route 66 Sector Development Plan area (see Figure xxx). The line is located on 80 to 100 foot high poles which run at the edge of the ROW. The transmission voltage is “stepped down” to lower voltages at distribution substations and distribution lines, called feeders, to provide electric service to residential and business customers. The distribution lines are located throughout the Plan area. Smaller power lines and telephone lines run intermittently along the corridor.

Public utility easements exist within the Plan area. Overhead and underground electric distribution lines are typically located within PUEs. They are compatible with other “dry” utilities such as cable, telephone and fiber optic facilities. The width of the PUE is typically 10 feet in order to provide necessary clearances for safety. Water lines, sewer lines and storm water drainage or “wet” utilities are not compatible with “dry” utilities and separation is required for safety purposes.

10.2.1 Issues and Opportunities

- a. The power and telephone transmission poles and lines can create visual clutter along Central Avenue, but are generally located at the edge of the ROW, outside the walking zone, for most of the corridor.

- b. From Cypress Dr. to Atrisco Dr., on the south side of Central Ave., and from New York Ave. to Rio Grande Blvd., on both sides of Central, utility poles are located in the sidewalk, intermittently blocking the sidewalks, creating unsafe conditions, and rendering them inaccessible to wheelchairs users.
- c. In the section between New York Ave. and Rio Grande Blvd. there have been reports of excessive power outages due to inadequate power transmission. This issue also affects the drainage of stormwater, as the Alcalde pump station is not functioning at optimal capacity due to fluctuations in voltage.

10.3 A 2010 Facility Plan: Electric Service Transmission and Subtransmission Facilities, is currently undergoing the standard review and approval processes of the City and County.

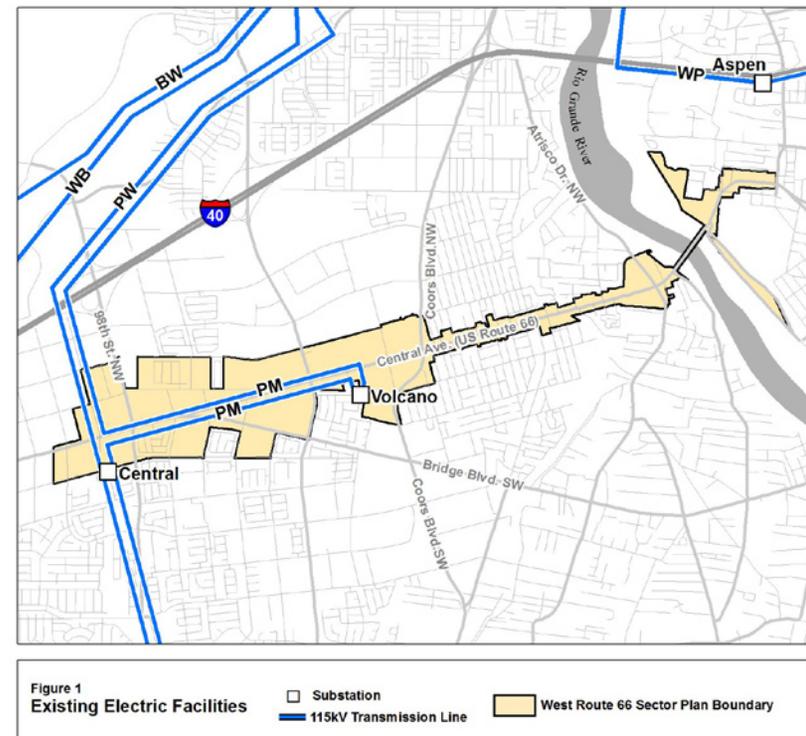


Figure 36: PNM Facilities

Existing Conditions: Parks, Trails and Open Space

11.0 Parks, Trails and Open Space Overview

The Plan area is characterized by two very distinct levels of service for Parks, Trails and Open Space. The western portion of the plan area, between 98th and Coors, including the communities living in proximity to it, is significantly underserved in comparison to the area east of Coors to Rio Grande Blvd. There is a need in the western portion of the plan area to introduce new opportunities for outdoor recreation, including trails and parks.

11.1 Parks Overview

The following parks serve the neighborhoods adjoining the Plan area:

Name	Location	Facilities	Size
Alamosa	Sunset Garden Rd. & Bataan Dr. SW	4 tennis courts, skate park, 1 playground, 2 half basketball courts and 10 picnic tables.	5 acres
Lavaland	64 th St. & Avalon Rd. NW	2 playgrounds.	1 acre
Pat Hurley	Yucca Dr. & Bluewater Rd. NW	2 playgrounds, 4 tennis courts, 4 half basketball courts, 1 full basketball court, 3 picnic tables.	19 acres
West Mesa Community Center	5300 Glenrio Rd. NW	1 playground, 1 soccer field.	3 acres
West Mesa	6705 Fortuna Rd. NW	1 playground, 1 softball field, 1 indoor pool, in outdoor pool, 1 volleyball court, and 2 picnic tables.	9 acres
Tom Cooper (Osage)	Osage Ave. & Atrisco Blvd. SW	1 playground and 1 picnic table.	.22 acres
Atrisco Park & Little League Fields (County)	229 Atrisco Blvd. SW	1 horseshoe pit, 1 basketball court, 1 playground, 5 youth ball fields.	16 acres

The current level of service standard used by the City of Albuquerque Parks Department is 2.6 acres per 1,000 population. Based on this level of service, the Plan area is adequately served by parks under existing development conditions. In the past, funding for parks has been obtained through impact fees; however there is currently a moratorium on impact fees while they are being studied to determine their efficacy. The only funding that is currently available for parks is General Obligation (GO) bond money. The priority for this money is to construct parks on land that has already been obtained by the Parks Department.

11.1.1 Issues and Opportunities

- a. There is a 2.2 acre City owned vacant parcel of land located at 90th St. and Volcano Rd. This is a potential joint use site for Parks and Family and Community Services.
- b. East-west pedestrian and bicycle access across Coors Blvd. to the park facilities at Alamosa should be improved; that access could be coordinated with a future signal at Airport Drive south of the Verizon site.
- c. Plan a linear park within the R.O.W. of the Central Ave. frontage road which runs from Unser Blvd. to the City limits. See page xxx for more details.

Existing Conditions: Parks, Trails and Open Space

11.2 Trails Overview

Many trails in the Albuquerque area are located along waterways, ranging from natural waterways like the Rio Grande, to agricultural irrigation ditches and waterways used for drainage and flood control. These trails and waterways are an important part of the history of Albuquerque and often serve triple functions as recreational pathways, commuter connections for pedestrians and cyclists, as well as conveying water.

Two types of trails that are separate from the road network exist in the plan area: the formal trails, either paved or made of compacted crusher fines, located in the Bosque, which are managed and maintained by City of Albuquerque Open Space or the Middle Rio Grande Conservancy District (MRGCD); and informal dirt trails in the Bosque or along MRGCD waterways that have been created through community use. During the planning process, residents expressed a strong desire to maintain and enhance the existing paths in the plan area, as well as to formalize trails along the canals, ditches and drains in the area.

The named irrigation ditches, drains and laterals may not be owned in fee simple by MRGCD. However, MRGCD may have easements, or at minimum an agreement with the land-owners, for maintenance and access to their facilities. MRGCD is undertaking a comprehensive mapping project of its facilities, many of which go back to the early 20th century. It reviews development proposals affecting its facilities on a case-by-case basis. The agency currently allows informal use of paths along its open waterways at the user's risk and does not have policies to surface paths or clear vegetation alongside them. Paths may be converted to official trails through a licensing procedure that requires evaluation by MRGCD staff and review and approval by the elected MRGCD Board. A trail then becomes the responsibility of the licensee. An example is the Paseo del Bosque trail where the licensee is the City. Under current MRGCD policy, a separate entity would need to take the initiative to implement any new official trail along MRGCD waterways and take on the responsibility for maintaining them.

11.3 Issues and Opportunities

a. Arenal Canal. There is an informal trail on the east side of the canal on both the north and south sides of Central. This canal begins at the Rio Grande in the Atrisco area and extends south all the way to Isleta Pueblo.

- b. Isleta Drain. The Isleta Drain goes underground where it intersects with Central, flowing under the alley behind Pro's Ranch Market and an informal access road to residential properties on the north side of Central, before daylighting just west of the river. The alley is owned by Pro's Ranch Market and may not be the most desired trail link for users due to aesthetic, safety, ownership and maintenance issues. On the southside of Central Ave. the drain runs under an informal access drive to the Kmart site. The drain on the southside may be widened in the future to handle increased stormwater flows.
- c. Atrisco Lateral and Ditch. The Atrisco Lateral splits into the Atrisco Lateral and the Atrisco Ditch near Central Ave. and continues south. On the north side of Central Ave., the Atrisco Ditch runs along the vacant site owned by the City's Metropolitan Redevelopment Agency. The Lateral goes underground under Central Ave. and resurfaces just south of Central Ave and west of Sunset Rd. There is an unimproved trail along the Lateral that could be developed into a multi-use trail that would connect the plan area with Albuquerque's South Valley.
- d. ADA Accessible Trail. There is a short MRGCD Americans with Disabilities Act (ADA) accessible trail west of the river on the north and south sides of Central Ave. While the MRGCD trail has been designed to be accessible, there is currently no ADA access or ADA parking at the trailhead.
- e. Alameda Lateral. The Alameda Lateral intersects Central Ave. east of the BioPark at New York Ave. On the north side of Central Ave. there is a small parcel of undeveloped land, owned by the MRGCD, just before the lateral daylight. This small parcel currently appears neglected and adds to the run-down appearance of the area. There is an opportunity at this site to design a mini park or plazuela (with sculptural amenities rather than plants) which would enhance the appearance of the corridor at this gateway location to the BioPark. A component of the site's design could be to provide information about the acequia system.

The lateral provides informal trail access between Central Ave. and the adjacent neighborhoods to the north. The lateral is partially fenced with chainlink and the section visible from Central appears rundown. There is significant community interest in removing the fencing along the Lateral and improving

Existing Conditions: Parks, Trails and Open Space

its appearance so that it could once again become an asset to area neighborhoods and the corridor. On the south side of Central Ave. the lateral runs behind buildings and along the Albuquerque Country Club property in a tight configuration that is not conducive to trail access.

11.3 Open Space

The Rio Grande State Park is the only Open Space in close proximity to the Plan area. It is located in the Bosque on both banks of the Rio Grande and is managed by the City in collaboration with MRGCD and the Army Corps of Engineers. The formal Open Space access point in the plan area is located on the northeast corner of the Central Avenue Bridge, and provides parking, a picnic area and access to the river and the Paseo del Bosque Trail. Access to the river and the Bosque is also available from the south side of Central along the Paseo del Bosque Trail. There are additional access points on the west side of the river, north and south of Central. However the north parking area is unimproved and no parking is available on the south side, and trailheads are informal and not well marked.

11.3.1 Issues and Opportunities

- a. There are numerous illegal homeless campsites in and around the Central Avenue area of the Bosque. In the spring of 2011, Albuquerque Police Department officers joined forces with the Crisis Outreach and Support Team and the Crisis Intervention Team to remove twenty homeless camp sites and provide assistance to homeless citizens living in the area of the Central Ave. Bridge. Reducing the number of illegal homeless campsites in the area will require this kind of on-going effort and collaboration.
- b. Parking on the west side of river is deficient and trailheads are unclear.
- c. There are concerns regarding fires in the Bosque.
- d. There is no ADA access to the ADA trails located on the west side of the river in the Bosque.

Existing Conditions: Parks, Trails and Open Space

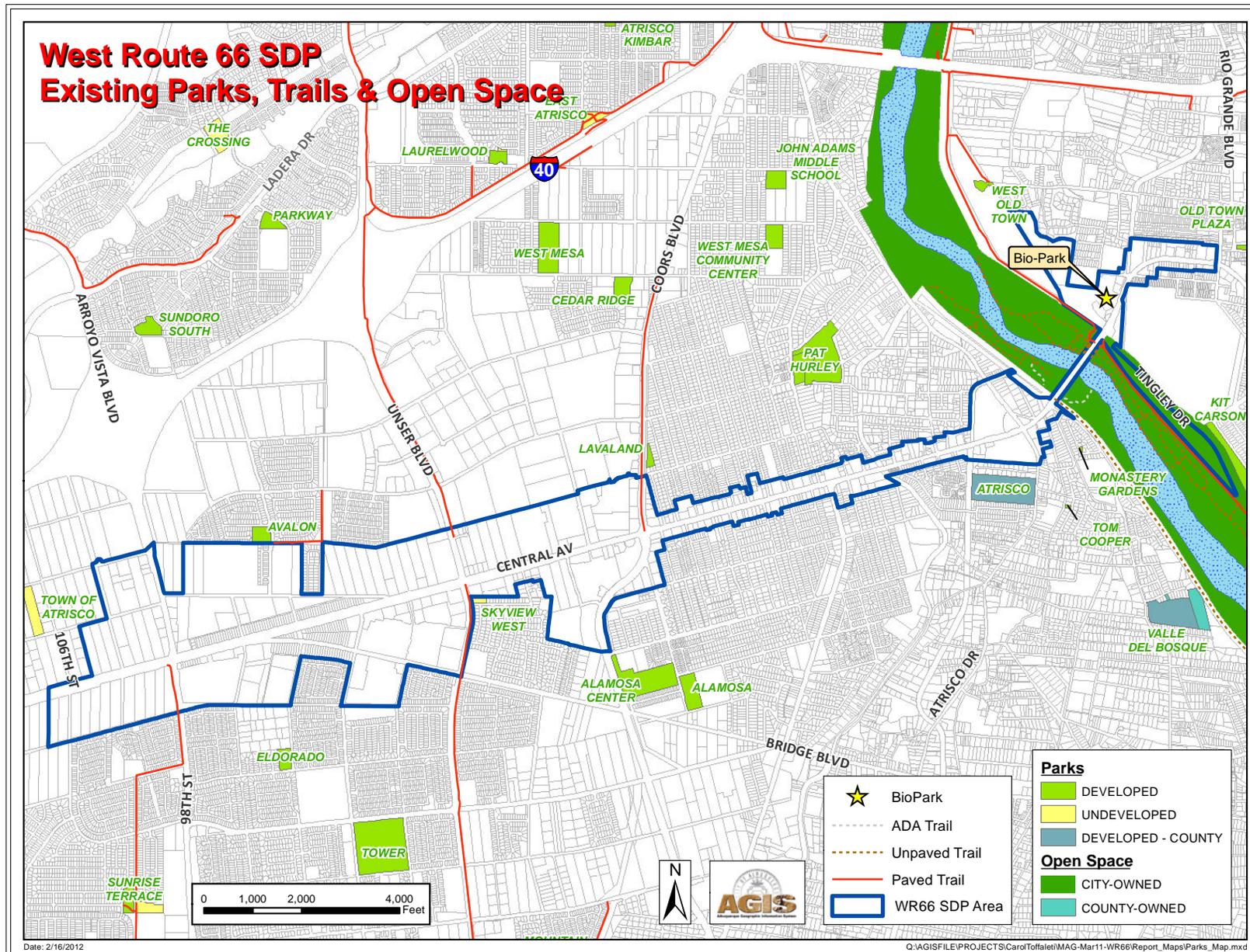
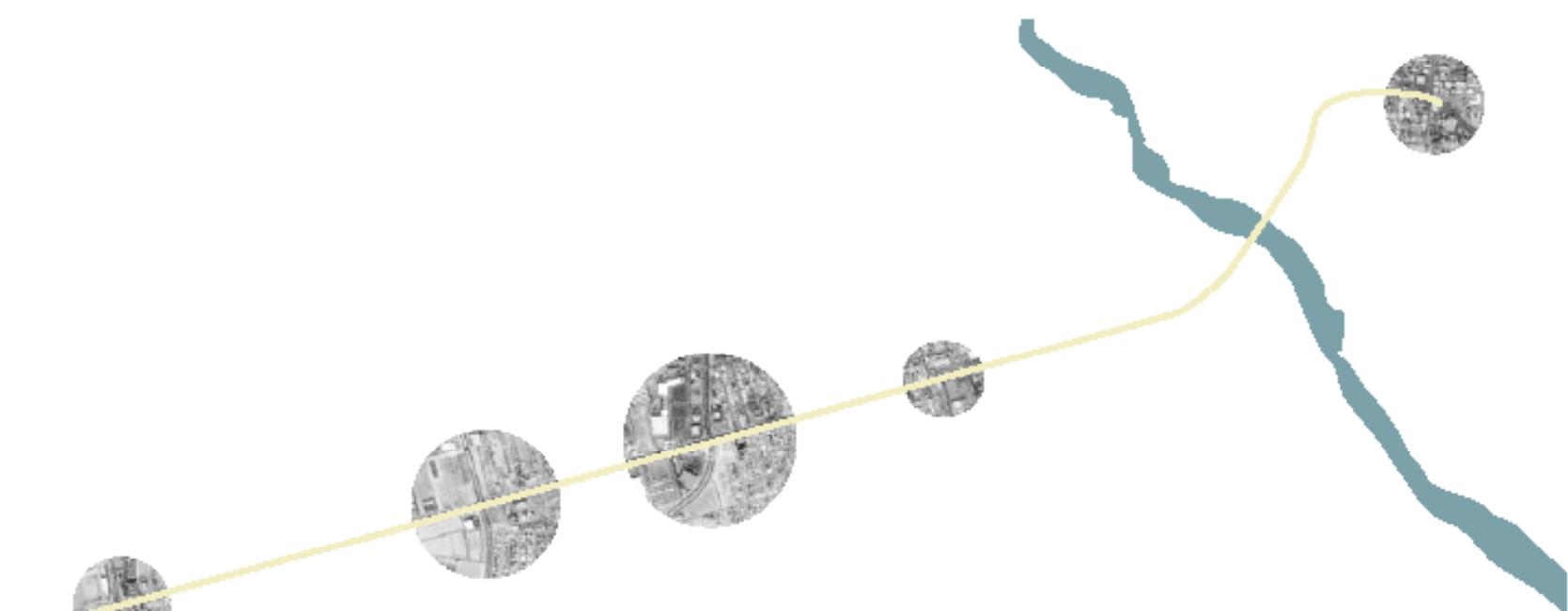


Figure 37: Parks, Trails and Open Space



Chapter 4 Zoning and Development Regulations

Zoning and Development Regulations

This chapter establishes the regulatory framework to realize City and Community vision for the plan area. The zoning is prescriptive so that development outcomes are more predictable for property owners, the City Review authorities, and community stakeholders, while ensuring flexibility to encourage development.

This chapter contains the following sections:

- 1.0 Zone Descriptions
- 2.0 Development Compliance
- 3.0 Development Approval Process
- 4.0 Zone Map
- 5.0 Useful Definitions
- 6.0 Zones
- 7.0 General Development Standards

1.0 Zone Descriptions

The zoning developed for the Plan consists of both conventional zones and form based zones. For the purpose of this Plan, a conventional zone is defined as “use based” zone that is typical of Comprehensive City Zoning Code prior to the adoption of the Form based zones. Whereas, in this Plan, form based zones prescribe what the development of a zone shall look like and may or may not specify what uses are permitted. Form based zones require that a use be accommodated within specific building types. All zones included in this Plan are subject to the General Development Standards of this Plan.

Modified conventional zones: These zones consist of modifications to existing Comprehensive Zoning Code zones. The SU- 2 zone within the Plan includes new regulations and references the Comprehensive Zoning Code for all other requirements. The conventional zones included in this Plan are as follows and are articulated beginning on p. X.

SU-2/W66 R-2 - W66 Moderate Density Residential Zone. The W66 Moderate Density Residential zone corresponds to the R-2 zone of the Comprehensive zoning code with the addition of the General Development Standards of this Plan.

SU-2/W66 C-2 – W66 Community Commercial Zone. The W66 Community Commercial zone creates a mixed commercial and residential zone, based on City C-2 zoning, with added residential opportunities to increase development flexibility while providing community commercial goods and services. The residential component only is form based. The W66 Community Commercial zone is subject to the General Development Standards of this Plan.

SU-2/W66 O-1 – W66 Office Zone. The W66 Office zone corresponds to the O-1 zone in the Comprehensive Zoning Code with the addition of the General Development Standards of this Plan.

SU-2/W66 IP - W66 Industrial Park Zone. The W66 Industrial Park zone corresponds to the IP zone in the Comprehensive Zoning Code with the addition of the General Development Standards of this Plan.

SU-2/W66 SU-1 - W66 Special Use Zone. The W66 Special Use zone corresponds to the SU-1 zone in the Comprehensive Zone Code. Sites within SU-1 zones without approved, current site development plans are also regulated by the General Development Standards of this Plan and applicable regulations of the Zoning Code.

Zoning and Development Regulations

Form based zones: These zones have been developed for the Plan by tailoring the form based zones in the Comprehensive Zoning Code. The SU-2 form based zones within the plan include new regulations and reference the Comprehensive Zoning Code for all other requirements including references to Building and Frontage Types and the General Standards sections. The form based zones developed for this Plan are as follows and are articulated beginning on p. X

SU-2/W66 CAC – W66 Community Activity Center Zone. The W66 Community Activity Center zone is based on the Zoning Code’s Community Activity Center Zone (FBZ TOD-COM) in order to develop a mixed use center in a Comprehensive Plan designated Community Activity Center. A Community Activity Center is envisioned as a mixed use center with attractive, high quality development that meets the commercial and service needs of a community wide area in an environment that is comfortable and inviting for pedestrians and bicyclists.

SU-2/W66 EPR – W66 Employment Park Residential Zone. The W66 Employment Park Residential zone creates a new zone which allows for employment and residential opportunities. This zone creates the opportunity to mix employment and residential in a campus environment where residents have the option to live close enough to their place of employment to be able to walk to work or to have the option of living within a live/work development.

SU-2/W66 MAC – W66 Major Activity Center Zone. The W66 Major Activity Center zone is based on the Zoning Code’s Major Activity Center zone (FBZ TOD-MAC) in order to develop a commercial, service and employment center in designated Major Activity Centers. A Major Activity Center is envisioned as a mixture of uses at a higher intensity of development that serves a regional population. The W66 MAC zoning is intended to create a center with commercial and employment development to serve all of southwest Albuquerque.

SU-2/W66 MX – W66 Mixed Use Zone. The W66 Mixed Use zone is a form based zone based on the Zoning Code’s Mixed Use zone (FBZ MX) and is sensitive to the existing built environment of the West Central corridor. The W66 Mixed Use zone is located in a part of the corridor that is envisioned as a vibrant pedestrian zone that includes a mix of commercial and residential activity and is easily accessed by pedestrians, bicyclists and automobiles alike. The zone is intended to enhance a fine grained, existing development character of minimal setbacks and orientation toward the street.

SU-2/W66 RA – W66 River Activity Zone. The W66 River Activity zone is a form based zone developed to support and facilitate limited residential, commercial development and public facilities that capitalize on proximity to the Rio Grande and serve cultural, retail and recreational needs of residents and visitors. The proposed zone provides for multi-modal access and lower density commercial uses such as restaurants, recreational goods and services, community garden, museum, educational facilities and low impact outdoor recreational uses that are appropriate in locations adjacent to the river, the acequia system and the Bosque trails.

SU-2/W66 SAC – W66 Special Activity Center Zone. The W66 Special Activity Center zone is a mixed use, form based zone developed to support and facilitate development which serves neighborhood and tourism needs while remaining sensitive to the character of the Old Town and BioPark areas. The Special Activity Center is envisioned as a distinctive district of land uses that support the existing amenities in the area. Amenities such as the BioPark and Tingley Beach are currently location specific attractions. People visit these sites then leave the area. The intent of SAC zone is to create a special activity district where, for example, people can visit the BioPark and then walk down the street to have lunch at a café with an outdoor patio before crossing Central Ave. to take in some afternoon fishing at Tingley Lakes.

Development Compliance

2.0 Development Compliance

Development shall comply with the goals, intent and design regulations of the West Route 66 Sector Development Plan as follows:

- A. For undeveloped sites: all new development.
- B. For sites with existing structures: when there is an increase of 25% or more of a building's existing square footage.
- C. Exemptions to compliance with the zoning regulations of this Plan:
 1. Repairs, facade improvements and maintenance of existing structures and/or buildings.
 2. Construction of a replacement building after involuntary damage to or destruction of existing building (such as fire damage).
 3. Change in ownership of a parcel.

2.1 Existing Entitlements

All properties with existing approved site development plans, including SU-1 sites that have been rezoned by the Sector Development Plan, will retain their existing zoning entitlements. Site with approved, current site development plans are regulated by the General Development Standards of this Plan and applicable regulations of the Zoning Code for additions of 25% or more of building area.

	W66 C2	W66 CAC	W66 EPR	W66 MAC	W66 MX	W66 RA	W66 SAC
Establish compliance and process *	Pre-Application Review	Pre-Application Review	Pre-Application Review	Pre-Application Review	Pre-Application Review	Pre-Application Review	Pre-Application Review
Compliant on Use and Form	< 5 acres: Building Permit 5 acres or more: SDP by EPC	Building Permit or SDP by DRB**					
Compliant on Use and Minor Modifications to Form	Form modifications: Planning Director then as above	Form modifications: Planning Director then as above	Form modifications: Planning Director then as above	Form modifications: Planning Director then as above	Form modifications: Planning Director then as above	Form modifications: Planning Director then as above	Form modifications: Planning Director then as above
Compliant on Use and Major Modifications to Form	SDP by EPC	SDP by EPC	SDP by EPC	SDP by EPC	SDP by EPC	SDP by EPC	SDP by EPC

Table 1: Development Approval Matrix.

* Developments with a single land use and on sites of under 1 acre are exempt from Pre-Application Review.

** DRB approval is required if development includes phasing or platting, or requires infrastructure. DRB provides sketch plat review prior to submittal.

Notes:

Conditional uses: Conditional use approval by the Zoning Hearing Examiner (ZHE) is required prior to building permit or site development plan approval.

Conventional zones: Building permit or approval of site development plan by DRB.

Modifications to DPM and subdivision standards that are not described by the W66 form based zones: EPC approval is required.

Public notification: All cases heard by the EPC or ZHE, and all site development plans heard by the DRB are publicly notified.

SDP: Site Development Plan. The amount of detail on the SDP shall be sufficient to demonstrate compliance with applicable requirements of the Plan.

3.0 Development Approval Process

All development occurring in the following SU-2 zones requires review and approval as outlined in the matrix below.

3.1 Modifications to Sector Plan Zoning Regulations

The zoning regulations in this Plan attain a level of detail in order to provide certainty for applicants, neighborhoods and zoning staff. However, it is not the intent of the regulations to limit design creativity or ignore unique site conditions. As such, two levels of modifications to the zoning regulations are permitted as follows:

A. Minor: The Planning Director or his/her designee may approve deviations from any dimensional standards by no more than 10%.

B. Major: Any modification of dimensional standard that is greater than 10% shall be reviewed by EPC.

In order for the EPC to grant the deviation(s) and approve the site development plan, the applicant must demonstrate that 1) the original standard(s) cannot be reasonably met without substantial hardship, due to the uniqueness of the site, and 2) applicable goals, policies and intents of the Plan are still met, even with the proposed deviation(s).

3.2 Non-conforming uses

Any existing, legal use that becomes a non-conforming use upon the adoption of the Plan shall not require a public hearing to become a conditional use. An application for Conditional Use shall be filed with the Planning Department within 6 months of the adoption of this plan.

Existing uses that are non-conforming upon adoption of the plan are APPROVED CONDITIONAL USES per the requirements in §14-16-4-2(D) of the Zoning Code. An approved conditional use shall be void/expired if the use ceases for a continuous period of one year or more. Upon expiration of the approved conditional use, the property owner is required to comply with the regulations of the adopted zone.

3.3 Relationship with other City Codes

Where a conflict arises between zoning regulations and general development standards in this Plan and the Zoning Code, this Plan shall prevail. When the Plan is silent on an issue that would otherwise be governed by the Zoning Code,

or other applicable City codes and Rank III plans, including the Rio Grande Boulevard Corridor Plan and the H-1 Historic Old Town zone, those codes and plans shall prevail.

Modifications/flexibility from DPM standards. The regulations set forth by the form based zones allow variation from standard DPM practices. Deviation from the DPM standards not described by the form based zones, but which support the greater intent of the form based zones, may be granted by the Environmental Planning Commission (EPC) upon reasonable justification by the applicant and only in conjunction with a site development plan for form based zones. Deviations granted by the EPC shall be acknowledged and implemented by the Development Review Board.

3.4 Subdivision and Public Right-of-Way Regulations

Subdivision and Public Right-of-Way Regulations were added to the 1987 plan in 2009 for the purposes of creating more pedestrian oriented subdivision and roadway development standards and improving connectivity between residential areas and commercial and community services. Applicable standards related to land use have been incorporated in the zoning regulations and general development standards of this Plan.

Modifications/flexibility from subdivision standards. In order to create a more compact urban form and allow for more efficient infrastructure design, the Environmental Planning Commission may grant deviations from the city's subdivision standards for form based zones upon reasonable justification by the applicant. Modifications granted by the EPC shall be acknowledged and implemented by the Development Review Board.

4.0 Zone Map

The following maps establish the SU-2 zoning for the plan area including new zones indicated by the prefix W66.

Zone Map

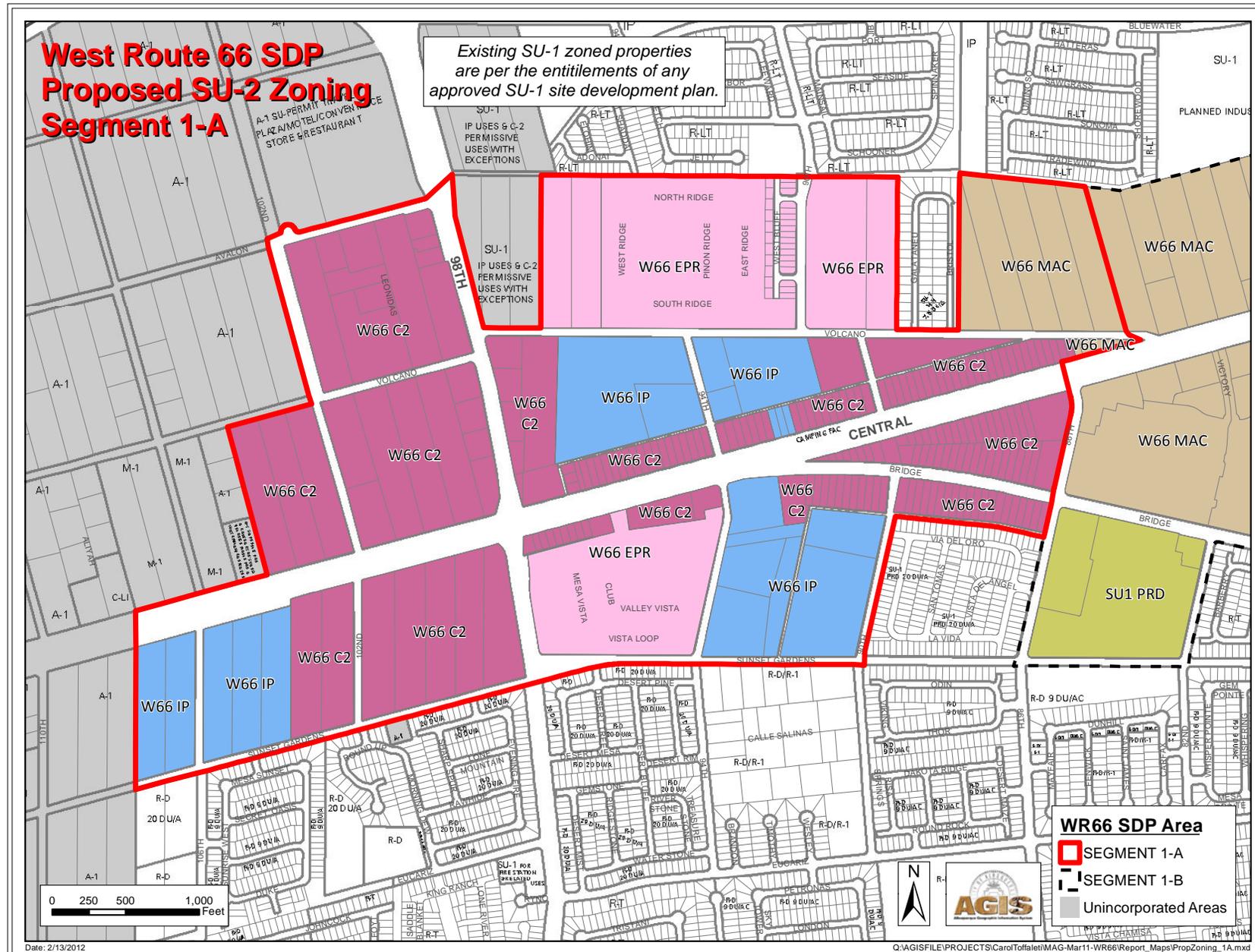


Figure: 38 Zoning Segment 1-A

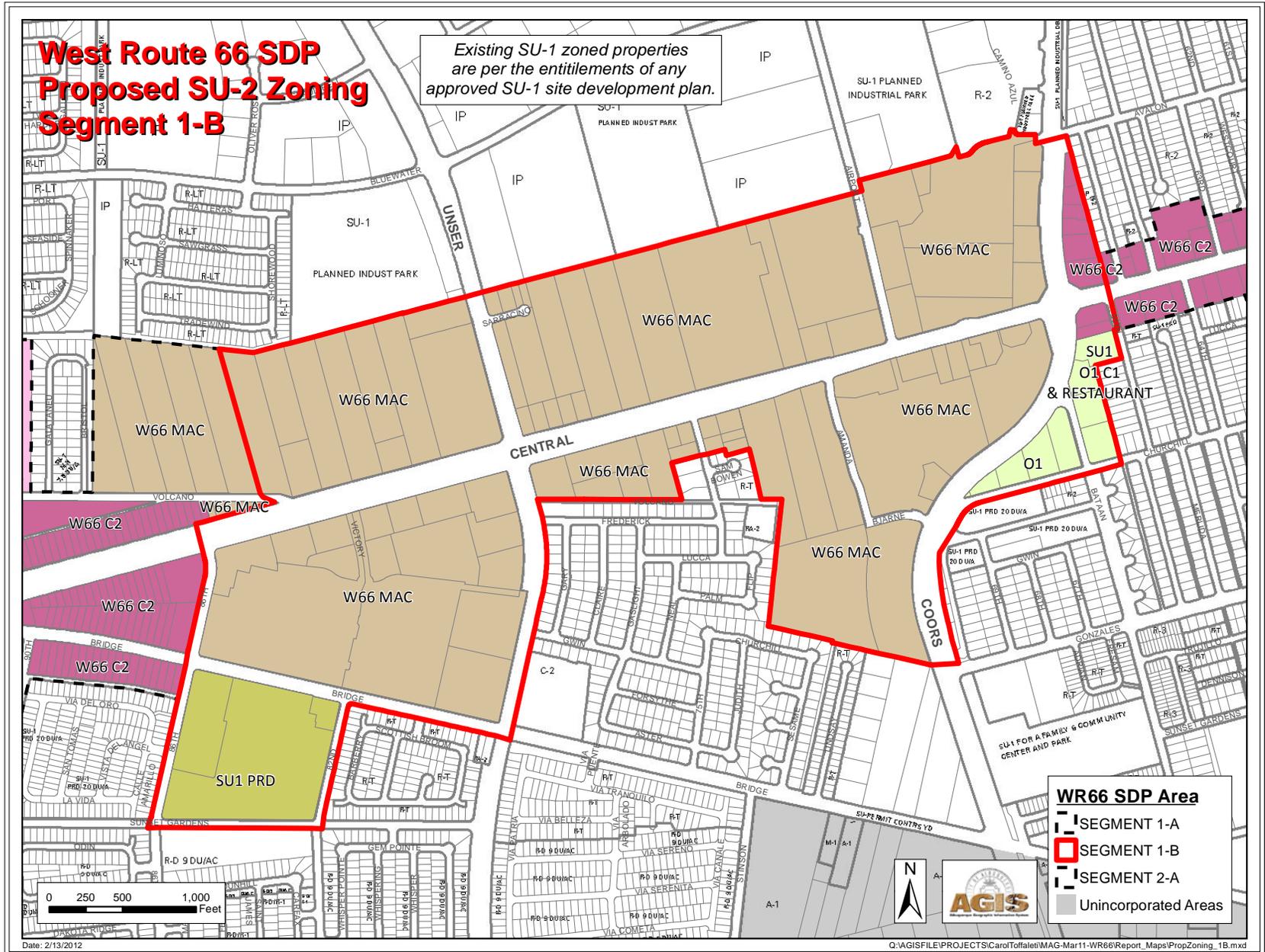


Figure: 39 Zoning Segment 1-B

Zone Map

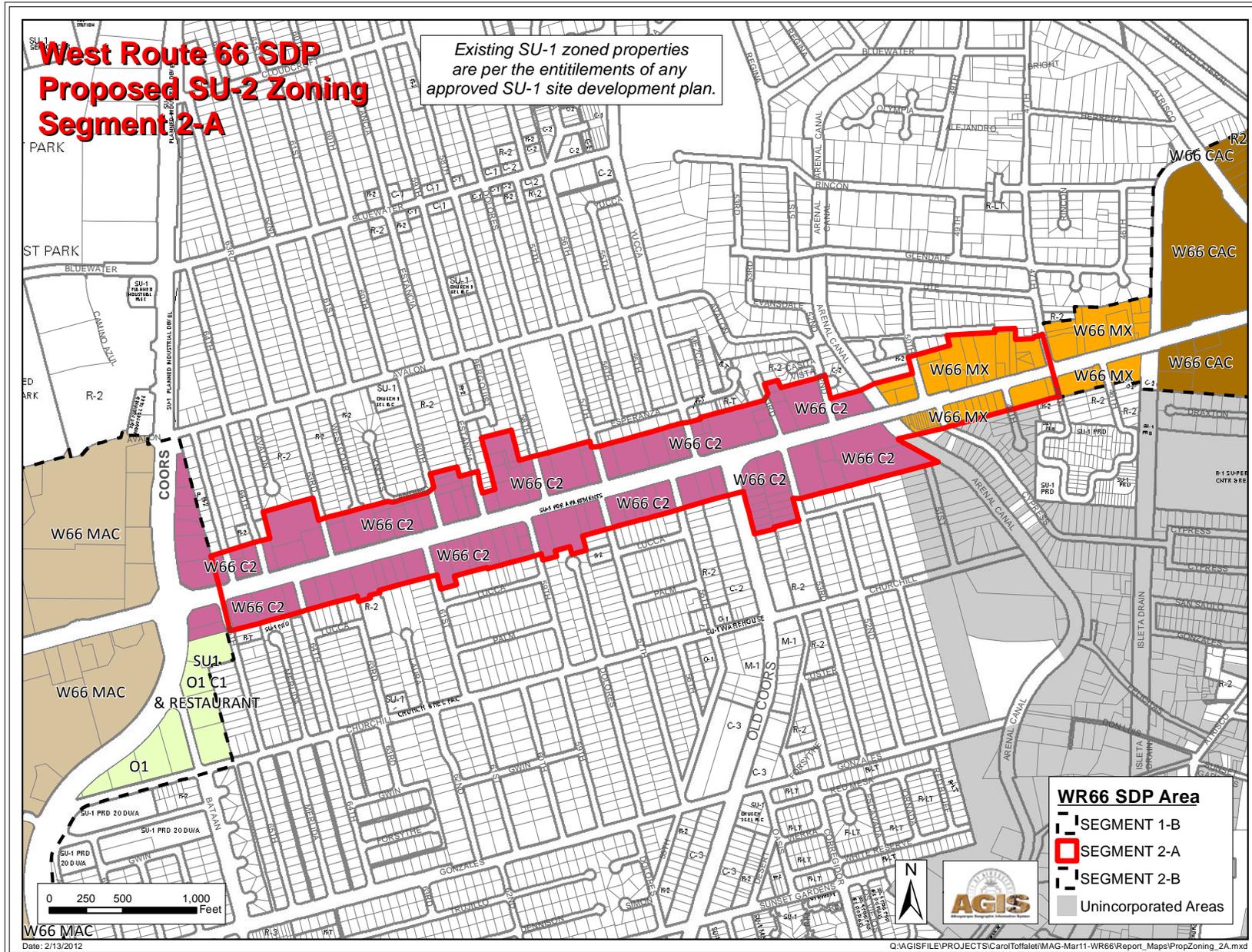


Figure 40: Zoning Segment 2-A

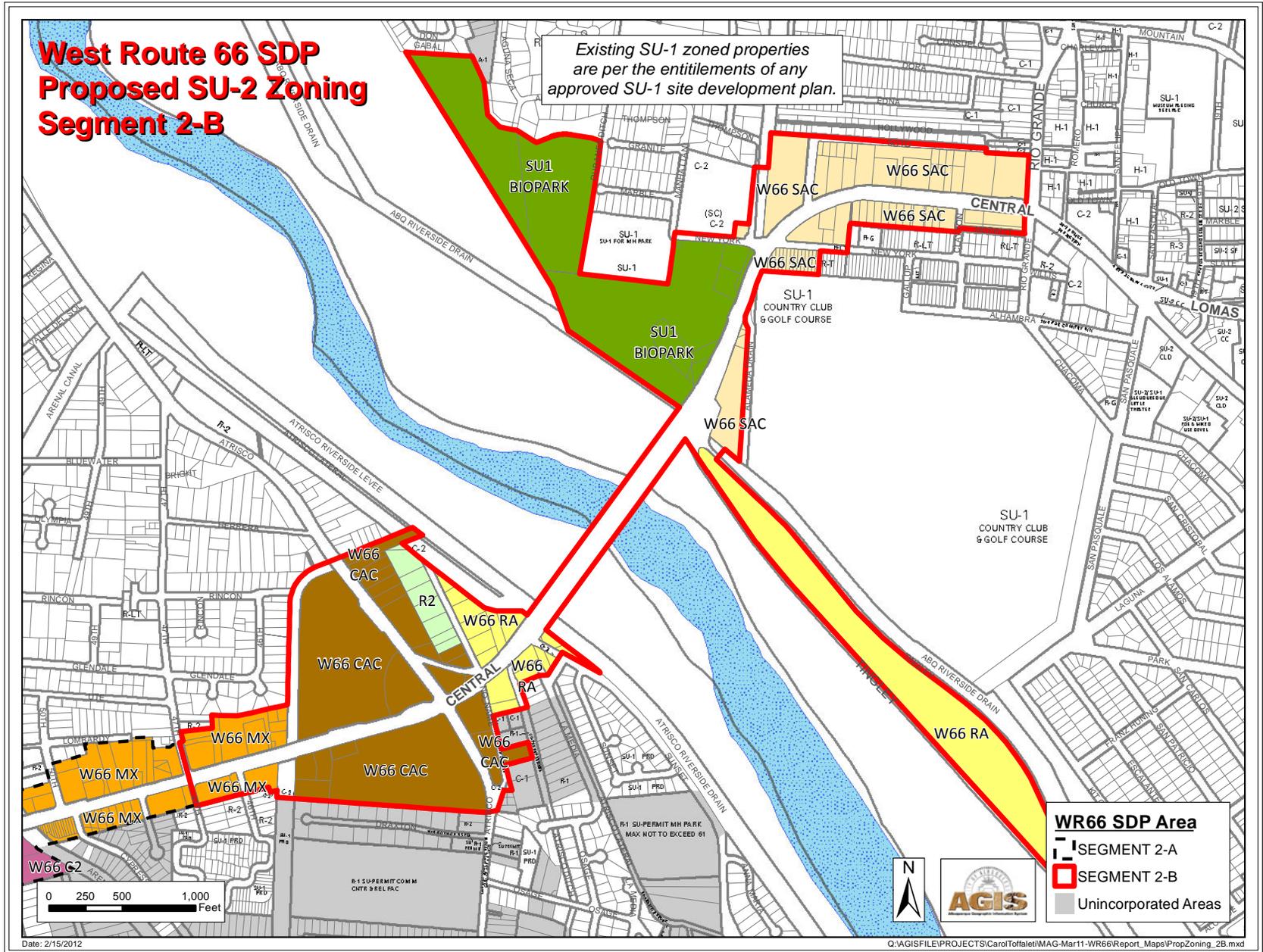


Figure: 41 Zoning Segment 2 - B

Zones

5.0 Useful Definitions.

The following terms are used throughout this section.

Allee. A walkway lined with trees.

Articulation. Off-sets, projections, recessed walls, windows, doors, etc. that provide variation to a building façade.

Building mass. The aggregate size of a building, or the total height, width, and depth of all its parts.

Building orientation to the street. Primary facade of building fronting/facing street.

Building Type. The category of a building, as described in the General Development standards.

Clear Height. Unobstructed vertical distance between two objects.

DMD. Department of Municipal Development.

DRB. Design Review Board

Drive aisle/Driveway. An improved (e.g., paved) driving surface for one line of vehicles.

EPC. Environmental Planning Commission.

Façade. The face or front of a building.

Fenestration. The openings which form a part of a building façade.

Frontage Types. The category of building facades, as described on pages X.

Form Based Zones. Zones which use physical form, rather than separation of land uses, as their organizing principle.

Grove. A group of trees.

LID. Low Impact Development.

Mixed use development. Development in which multiple land uses are permitted such as retail and residential.

Pad Site. An individual freestanding site for a retailer, often adjacent to a larger shopping center .

Paseo. A street or path for pedestrians only. Usually wider than a sidewalk and landscaped.

Pedestrian-friendly/pedestrian-oriented. Development which is designed with an emphasis primarily on the street sidewalk and on pedestrian access to the site and building, rather than on auto access and parking areas. The building is generally placed close to the street and the main entrance is oriented to the street sidewalk. There are generally windows or display cases along building façades which face the street. Typically, buildings cover a large portion of the site. When parking areas are provided, they are generally limited in size and they are not emphasized by the design of the site.

Plaza. A public square or extra-wide sidewalk (e.g., as on a street corner) that allows for special events, outdoor seating, sidewalk sales and similar pedestrian activity.

Primary entrance. The entrance to a building that most pedestrians are expected to use. Generally, each building has one primary entrance. It is the widest entrance of those provided for use by pedestrians. In multi-tenant buildings, primary entrances open directly into the building's lobby or principal interior ground level circulation space. When a multi-tenant building does not have a lobby or common interior circulation space, each tenant's outside entrance is a primary entrance. In single-tenant buildings, primary entrances open directly into lobby, reception, or sales areas.

Right-of-way (ROW). Land that is owned in fee simple by the public, usually for transportation facilities.

(ZHE). Zoning Hearing Examiner

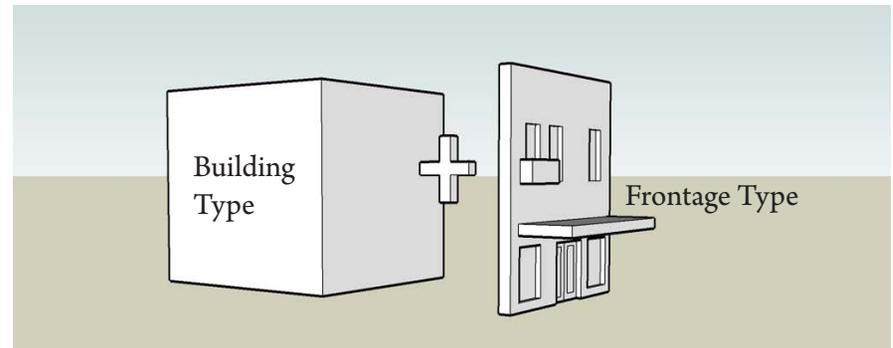
Definition Sources:

CABQ Form Based Code, Form Based Codes Institute, Model Development Code and User's Guide for Small Cities, 1999 (Oregon TGM Program), Lexicon of the New Urbanism, Duany Plater-Zyberk & Company, Version 2.1 (1999), Webster's New Collage Dictionary (1995)

6.0 Zones

This section contains the zones for the West Route 66 Sector Development Plan. Each zone contains specific requirements for development including height, building placement, parking, open space and additional development requirements specific to the zone. In addition, the General Development Standards are applicable to all zones.

The Form base zones are included in the Plan rely on using the allowable building form to control the density, intensity and use of development. The building form, known as Building Type, prescribes the building envelope, building access and orientation, access to light and air and general site requirements. All activity in a Form based zone shall be conducted within the prescribed form or Building Type. In addition to the prescribed form. In addition, each Building Type dictates allowable Frontage Types. Frontage Types prescribe building articulation along the street frontage and include requirements pertaining to building penetrations, shading elements and street access. These referenced Building and Frontage Types are located within the General Development Standards.



Building Type and Frontage Type Diagram

Intent: The W66 Community Commercial (W66 C-2) zone creates a mixed commercial and residential zone to increase development flexibility while continuing to provide opportunities for the sale of goods and services that serve the community. Residential uses are regulated by form based standards in order to ensure quality design.

Justification: Current platting and land uses are consistent with C-2 uses and site requirements. The majority of the parcels are currently zoned C-2. The addition of R-2 uses on these parcels increases development entitlements, promotes housing choice, and increases transit and retail demand by promoting population growth along the corridor.

Permitted Uses

1. Permissive and Conditional uses per the C-2 zone with the addition of R-2 permitted and Conditional uses. R-2 uses shall be limited to 20 du/acre.
2. Nursing and Rest homes.

Prohibited Uses

1. House, one per lot.

Permitted Residential Building Types

1. Residential uses shall comply with the following Building Types, p. 112: Stacked flats, Terrace apartments, Courtyard apartments, Townhouse, Rowhouse, Loft, Duplex/Triplex/Fourplex

Usable Open Space (for residential uses only)

1. Per R-2 zone.

Landscape Standards

1. Per §14-16-3-10 in the Zoning Code, with the exception of 5. in Additional Development Requirements.

Off Street Parking Requirements

1. Per §14-16-3-1 in the Zoning Code with the exception of 3. and 4. in Additional Development Requirements.

Building Placement

1. Setbacks, per C-2 zone.

Height

1. Per C-2 zone but not to exceed 52 feet.
2. Within 50 feet of Central Avenue, heights shall be limited to 36 feet.

Additional Development Requirements

1. General Development Standards, p. 111, shall apply to this zone.

Applicable to development fronting Central Avenue

2. In order to facilitate pedestrian circulation, no additional curb cuts shall be permitted. On parcels with no existing curb cut, one curb cut shall be permitted per 150 feet of street frontage.
3. Parking shall not be permitted between building and public R.O.W.
4. Parking shall not be located within 10 feet of the front property line. Area between front property line and parking shall be landscaped with a minimum of 50% vegetative cover.
5. At least 75% of building frontage shall be at the minimum setback. A deviation up to 20 additional feet is permitted to accommodate building articulation, patios and courtyards. Where more than one building is located per site, at least one building shall meet the 75% requirement. Other buildings shall meet this requirement where possible.
6. For lots greater than 75 feet in width, a maximum of 50% of the street frontage shall consist of parking at the minimum setback.
7. Signage shall be regulated by the General Development Standards.

W66 - CAC Zone

Intent: The W66 - Community Activity Center zone (W66 CAC) is a form based zone developed to provide a mixture of moderate density residential, commercial, entertainment, civic, and employment uses within a designated Community Activity Center which supports transit and pedestrian accessibility, while accommodating automobiles.

Justification: The Comprehensive Plan designates the Atrisco Community Activity Center in order to provide a greater variety of commercial and entertainment uses in conjunction with community-wide services, civic land uses, employment, and the most intense land uses within the community sub-area.



CAC Development Characteristics

1. Roadways and private drives with sidewalk, landscaping and on-street parking.
2. Limited curb cuts.
3. No new parking between a building and a public right of way.
4. Limited street frontage consisting of parking areas. Parking areas buffered with landscaping.
5. Aggregated public space.
6. Aggregated shared parking area.
7. Pedestrian linkages between parking areas and buildings.

CAC zone potential development diagram.

Permitted Uses

1. Any use not listed as a limited, conditional or prohibited use shall be permitted.

Limited Uses

1. New Drive up service windows shall be permitted in association with a bank or pharmacy.
2. Drive up service windows for all other uses are based on a cap and replace system limited to 3 within in the CAC. A new drive up service window shall only replace an existing use. Use is not limited to a specific site.

Conditional Uses

1. Alcohol sales for off-premise consumption shall be a conditional use.

Prohibited Uses

1. The following uses are prohibited: Vehicle sales, rental service, repair or storage; taxidermy services; single family house, one per lot; cold storage plants; drive-in theatres; kennels; mobile home development; tire capping or retreading; transfer or storage of household goods; uses permitted and conditional in the P or PR zone; truck plaza; drive-in facilities; gasoline, oil, liquefied petroleum gas or other vehicle fuel sales; the following SU-1 uses: adult amusement establishment or adult store, automobile dismantling yard, campground, cemetery, drilling, production or refining of petroleum gas or hydrocarbons, gravel, sand or dirt removal activity, stockpiling, processing or distribution, hatching plant, ore reduction, smelting, planned development area, planned residential development.

Permitted Building Types

1. Development shall occur in one or more of the following Building Types: Stacked flats, Terrace apartments, Courtyard apartments, Townhouse, Rowhouse, Duplex,/Triplex,/Fourplex Live/work Loft, Podium building, Flex building, Courtyard building, Liner building, Civic institutional building, Structured parking - no ground floor uses shall be located behind another structure which screens a minimum of 50% of the parking structure on a public façade, Structured parking - ground floor uses, Drive through/Service Station (shall meet limited use requirements above).

Usable Open Space

1. A minimum 10% of the site area shall be designated as usable open space in the form of patios, plazas, balconies, roof decks, courtyards, or exterior walkways.

Public Space

1. One public space area, a minimum of 200 square feet, shall be provided for every 10,000 square feet of building space.
2. The public space area shall be privately owned and maintained and should typically contain seating and shade.
3. An aggregate of buildings 125,000 square feet or greater shall provide pedestrian plaza space in the amount of 400 square feet for every 20,000 square feet of building space.
4. A minimum of 50% of the required public space shall be provided in the form of aggregate space that encourages its use and that serves as the focal point for the development.
5. See General Development Standards for additional public space requirements.

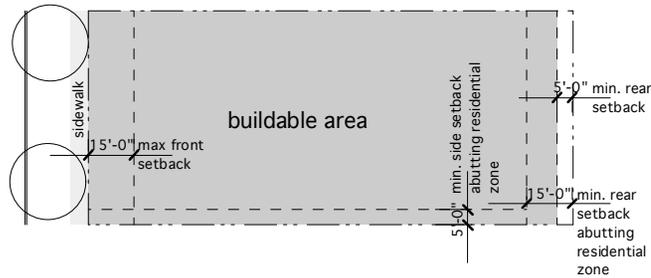
Landscape Standards

1. Landscape standards shall be per the City of Albuquerque Zoning Code (§ 14-16-3-10) except that the total landscape area required for each development shall be a minimum 5% of the net lot area.

Off Street Parking Requirements

1. Per §14-16-3-22(C) in the Zoning Code, with the following additions: Parking shall be located to the side or rear of buildings where possible. Parking area shall be setback a minimum of 10 feet from the public R.O.W. Parking area shall be buffered from public R.O.W. by a minimum 10 foot wide landscaped area and a site wall or continuous evergreen plant material a minimum of 3 feet high. See Additional Development requirements for maximum allowable parking frontage.
2. Off-street parking shall be 1 space per 1000 sq ft minimum; for all uses, except that for all lots the maximum number of allowed off-street parking spaces shall be no more than 110% of the required minimum.

W66 - CAC Development Requirements



Preliminary building placement diagram.

Building Placement

Front Setback

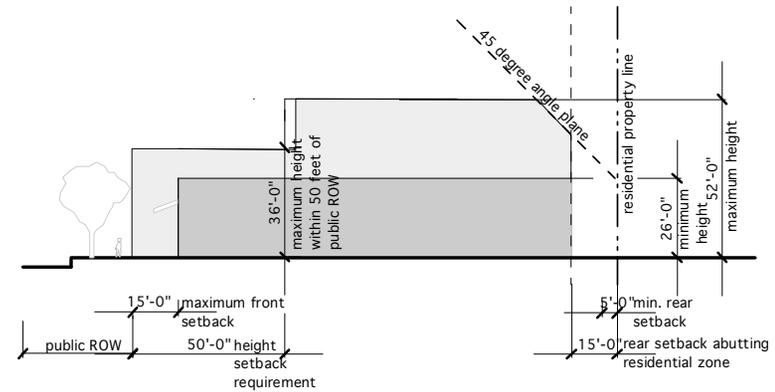
1. Maximum setback: 15 feet
- **upper floors, balconies, portals, shading devices and signage may protrude into the front setback within the property line a minimum of 8 feet above sidewalk.

Side Setback

1. Internal: none
2. Minimum side setback: 0 feet; except 5 feet minimum from abutting residential zone.

Rear Setback

1. Minimum rear setback: 5 feet; except 15 feet where site abuts the rear of a lot in a residential zone.



Building height diagram.

Building Height

1. Minimum height shall be 26 feet.
2. Heights shall be limited to 52 feet.
3. Lots abutting R-1 through R-T zone properties building heights shall not exceed a 45 degree angle plane that begins at a height of 26 feet, measured from the residential property line.
4. Within 50 feet of a public right-of-way, heights shall be limited to 36 feet.

W66 - CAC Development Requirements

Additional Development Requirements

All properties

1. The Plan's General Development Standards, p. 111, shall apply to this zone.
2. All other requirement not included in this zone shall conform to TOD-COM in the Comprehensive Zoning Code as well as the standards of this Plan.
3. In order to facilitate pedestrian circulation, no additional curb cuts shall be permitted. On parcels with no existing curb cut, one curb cut shall be permitted per 150 feet of street frontage.
4. No new parking shall be permitted between a building and a public right of way.
5. For lots greater than 75 feet in width, a maximum of 30% of the street frontage shall consist of parking areas. All other parking areas shall be located behind a building.
6. A 10 foot wide minimum landscaped setback is required between the sidewalk and parking area. Remaining street frontage shall consist of buildings, courtyards, patios and seating areas, site walls up to 4 feet in height and landscaping.
7. Stand alone ATM stations shall be walk up only. See General Development Standards for Stand alone ATM requirements.

For properties greater than 4 acres

1. In order to provide connectivity between public sidewalks, parking areas and development, no more than 400 linear feet of development shall front Central or Atrisco without a pedestrian passage to parking areas or a vehicular drive with sidewalks on both sides.
2. Platted roadways, which may remain private, shall separate blocks. Roadways or private drives shall not have more than two travel lanes a maximum of 10 feet in width. All on site roadways and private drives shall have on-street parking. On-street parking may be parallel, angled or perpendicular.
3. A minimum 6 foot wide sidewalk and 5 foot wide landscaped area, and stand up curb shall align both sides of all on site roadways and private drives. Cuts shall be permitted into curb to allow water to flow into planting areas, see Low Impact Development standards.

W66 -EPR Zone

Intent: The W66 Employment Park Residential (W66 EPR) zone creates opportunities for employment, commercial and multifamily residential uses to increase development flexibility while creating more consistent land use in the overall area. The EPR zone incorporates form based design standards to ensure quality mixed use development and provides opportunities for clustered multi-story office buildings, multi-family residential communities, and compact mixed use development.

Justification: Current demand in the area for larger parcels is for multi-family development and employment centers. There is minimal opportunity within the plan area for residential uses adjacent to employment facilities. The addition of multi-family uses on these parcels increases development entitlements, provides opportunities for people to live and work in close proximity and increases potential for transit ridership along the Central corridor.



General EPR Development Characteristics

1. Clustered parking areas separated by buildings or landscaped areas.
2. Buildings setback from public R.O.W.
3. Pedestrian and bicycle linkages throughout site
4. Opportunities for residential, commercial and employment
5. Campus style layout

EPR zone potential development diagram.

Permitted Uses

1. Permissive Uses within the R-2, C-1 and O-1 zone.
2. Hotel, auditorium, health gymnasium, clinic, small animal clinic, outdoor restaurant seating.
3. Nursing and Rest homes.

Prohibited Uses

1. House, one per lot.

Conditional Uses

1. Schools, other than public.

Usable Open Space Requirements

1. Over the entire site, at least 1 square foot of usable open space shall be provided for every 1 square foot of surface parking.
2. Balconies, patios, courtyards, plazas, parks or portals shall count toward required open space. At least 50% of the open space shall be landscaped and contain pedestrian network facilities. Courtyards and plazas shall be formally landscaped or hardscaped; and surrounded on least two sides by building(s).

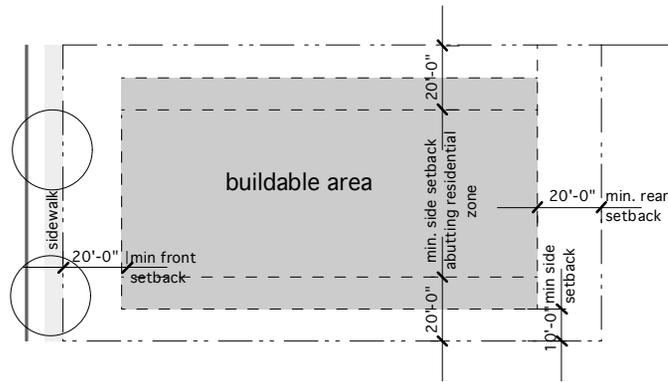
Landscape Standards

1. Landscape standards shall be per the City of Albuquerque Zoning Code § 14-16-3-10.

Off Street Parking Standards

1. Residential uses: 1.5 spaces per unit
2. Other: off-street parking regulations in the Comprehensive City Zoning Code.
3. Parking shall not be permitted in front setback areas.
4. Parking shall be located to the side or rear of building.
5. Each parking area shall not exceed 50 parking spaces.
6. Parking areas shall be spaced a minimum of 50 feet apart separated by buildings or landscaped areas.
7. Parking shall be buffered from the public R.O.W. by landscaping and a minimum 3 foot high wall or continuous landscaping a minimum of 3 feet in mature height.
8. Parking areas shall be buffered from residential areas by a minimum 10 foot landscaped buffer with trees spaced a minimum of 30 feet on center.

W66 - EPR Development Requirements



Site Layout Diagram

Building Placement

Front Setback

Minimum: 20 feet

Maximum: none

All others: 10 feet

Side setback

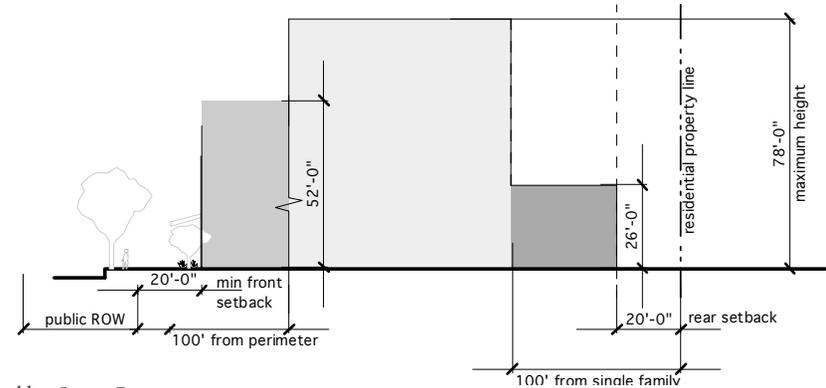
Internal: none

Abutting a zone with residential uses: 20 feet minimum

All others: 10 feet minimum

Rear setback

Setback from rear property line: 20 feet minimum



Building Section Diagram

Building Height

1. Within 100 feet of the property line of a single family residential zone, buildings shall not exceed 26 feet in height.
2. On other perimeter locations, maximum height is 52 feet.
3. Beyond 100 feet from the perimeter, height is limited to 78 feet in height.

W66 - EPR Development Requirements

Additional Development Requirements

General

1. General Development Standards, p. 111, shall apply to this zone.
2. Lot Size. No future subdivision of land shall create lots of less than one acre.
3. Pedestrian connections shall include a network of interior paths that links buildings, parking areas and open space with pedestrian walkways, bikeways, plazas, and trails.
4. Additional curb cuts are discouraged. On corner lots, additional curb cuts shall be on side streets only. For properties over 4 acres private or platted roads shall provide access to site from public R.O.W.
5. All setback areas shall be landscaped.

Campus - properties which are 4 or more acres

1. Site shall be divided into blocks of a maximum of 4 acres. Blocks shall be separated by platted or private drives, pedestrian parkways or plazas a minimum of 20 feet in width.
2. A minimum 6 foot wide sidewalk and 5 foot wide landscaped area, and stand up curb shall align both sides of all on site roadways and private drives. Cuts shall be permitted into curb to allow water to flow into planting areas, see Low Impact Development standards.

W66 - MAC Zone

Intent: The W66 Major Activity Center (W66 MAC) zone is a form based zone developed to provide a highly concentrated location of commercial, service, residential and employment uses on the Southwest Mesa in conjunction with area-wide needs in a designated Major Activity Center.

Justification: The W66 MAC area consists of over 300 acres and serves the metropolitan population and beyond, providing commercial, office, and technology centers, including medium to high density residential in sensitive relationship to employment. The W66 MAC is accessible by all modes of travel including a major transit transfer point.



General MAC Development Characteristics

1. Frontage road trail.
2. Parking visible from street is limited and buffered by landscaping.
3. Linkages to transit facility.
4. Shared parking areas.
5. Aggregated open space for parks, trails and plazas.
6. Higher density development along Central, buffered by frontage road or landscaping setback.
7. Pedestrian connectivity through site using a fine grained network of streets and trails.
8. North/South trail to improve connectivity.

MAC zone potential use

Permitted Uses

1. Any use not listed as a limited or prohibited use shall be permitted.

Limited Uses

1. Within 200 feet of the Central ROW, residential uses shall be prohibited in first floor buildings, except that motels built between 1926 and 1956 may be rehabilitated for residential use.
2. Drive through/up service windows and queuing lanes are prohibited within 150 feet of a major intersection and within 50 feet of Central Avenue. Drive through/up service windows location and queuing lanes are encouraged to be internal to the site. Queuing lanes shall not be permitted adjacent to streets.

Prohibited Uses

1. The following uses shall be prohibited: Adult amusement establishment and adult store, except in areas previously zoned C-3, IP, M-1, M-2 as of the adoption of this zone; Uses first permitted and conditional in the R-1 and M-2 zone; the following SU-1 uses: automobile dismantling yard, campground, cemetery, drilling, production or refining of petroleum gas or hydrocarbons, gravel, sand or dirt removal activity, stockpiling, processing or distribution and hatching plant, ore reduction, smelting, Planned Development Area, Planned Residential Development, truck plaza, uses permitted and conditional in the P zone.

Permitted Building Types

1. Development shall occur in one or more of the following: Stacked flats, Courtyard apartments, Podium building, Courtyard building, Loft unit, Flex building, Liner building, Civic institutional building, Structured parking - no ground floor uses, Structured parking - ground floor uses, Drive Through/Service Station.

Usable Open Space

1. A minimum 10% of the site area shall be designated as usable open space in the form of patios, plazas, balconies, roof decks, courtyards or exterior walkways.
2. Usable open space is not required if building is located within 1,500 feet of a park, plaza or other designated usable open space that is at least one acre in size and is accessible to the public.

Public Space

1. One public space area, a minimum of 200 square feet, shall be provided for every 10,000 square feet of building space.
2. The public space area shall be privately owned and maintained and should typically contain seating and shade.
3. An aggregate of buildings 125,000 square feet or greater shall provide pedestrian plaza space in the amount of 400 square feet for every 20,000 square feet of building space.
4. A minimum of 50% of the required public space shall be provided in the form of aggregate space that encourages its use and that serves as the focal point for the development.
5. See p. X for additional public space requirements.

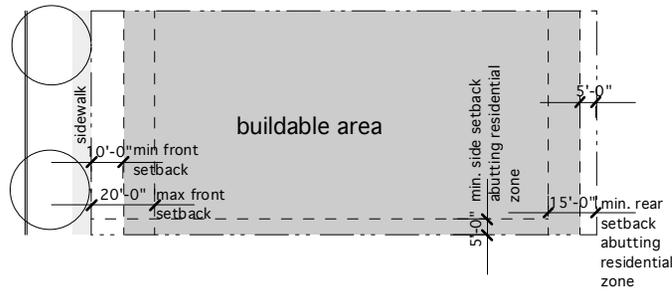
Landscape Standards

1. Landscape standards shall be per the City of Albuquerque Zoning Code (§ 14-16-3-10) except that the total landscape area required for each development shall be a minimum 10% of the net lot area.

Off-street parking Requirements

1. All uses: No minimum parking requirement.
2. All other parking requirements shall conform to TOD-MAC in the Comprehensive Zoning Code as well as the standards of this Plan.

W66 - MAC Development Requirements



Site Layout Diagram

Building Placement

Front Setback

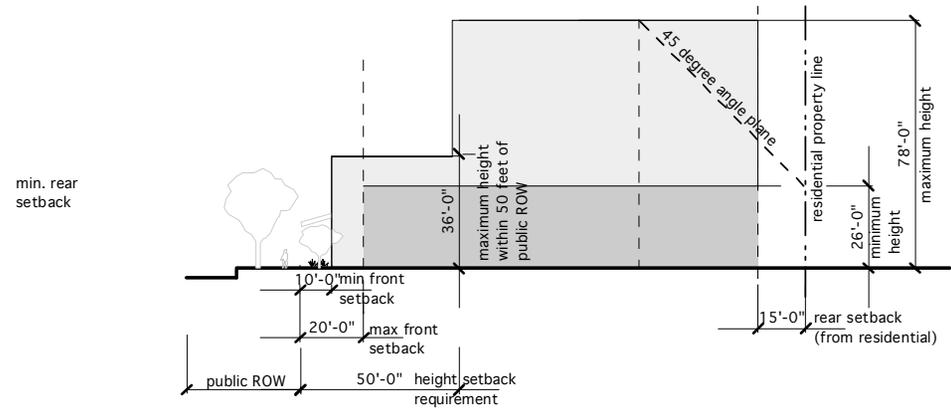
1. All development shall front platted roads or private drive.
2. Front setback from Central R.O.W.: 10 feet minimum; 20 feet maximum
3. Front setback from other arterials: 10 feet minimum; no maximum
4. All others: 5 feet minimum, 10 feet maximum.

Side Setback

none/5 feet minimum abutting a residential zone

Rear Setback

5 feet minimum/ 15 feet minimum abutting a residential zone



Building Section Diagram

Building Height

1. Minimum height shall be 26 feet.
2. Heights shall be limited to 78 feet maximum.
3. Abutting R-1 through R-T zone or corresponding SU-1 zone, building heights shall not exceed a 45 degree angle plane that begins at a height of 26 feet measured from the residential property line
4. Within 50 feet of a public ROW, heights shall be limited to 36 feet.

W66 - MAC Development Requirements

Additional Development Requirements

All properties

1. The General Development Standards, p. 111, shall apply to this zone.
2. Where this zone or the General Development Standards of this Plan area silent, the requirements of the TOD-MAC in the Comprehensive Zoning Code shall apply.
3. For parcels with over 400 linear feet of frontage along Central, 1 curb cut shall be permitted every 300 linear feet.
4. No new parking shall be permitted between a building and a public right of way.
5. For lots greater than 150 feet in width, a maximum of 50% of the street frontage may consist of parking areas. All other parking areas shall be located behind a building.
6. For lots greater than 150 feet in width, a maximum of 30% of the street frontage may consist of parking areas. All other parking areas shall be located behind a building. Remaining street frontage shall consist of buildings, courtyards, patios and seating areas, site walls up to 4 feet in height and landscaping.
7. A ten foot minimum landscaped setback is required between the sidewalk and the parking area. Remaining street frontage shall consist of buildings, courtyards, patios and seating areas, site walls up to 3 feet in height and landscaping.
8. Stand alone ATM stations shall be walk-up only. See General Standards for standalone ATM station requirements.

For properties greater than 4 acres

1. In order to provide connectivity between public sidewalks, parking areas and development, no more than 400 linear feet of development shall front Central or Atrisco other public R.O.W.'s without a pedestrian passage from public R.O.W. to parking areas or a vehicular drive with sidewalks on both sides.
2. Blocks shall be no more than 4 acres in size.
3. Platted roadways shall separate blocks. Roadways including private drives shall not have more than two travel lanes a maximum of 10 feet in width. All roadways and private drives shall have on-street parking. On-street parking may be parallel, angled or perpendicular.
4. A minimum 6 foot wide sidewalk and 5 foot wide landscaped area, and stand up curb shall align both sides of all on site roadways and private drives. Cuts shall be permitted in curb to allow water to flow into planting areas, see Low Impact Development standards.

W66 - MX Zone

Intent: The WR66 MX zone is a form based zone which builds upon the existing development pattern of this portion of the west Central corridor - small platted lots and buildings located close to and oriented to the street - in order to create a pedestrian oriented environment.

Justification: This section of the corridor is at the same elevation as the Community Activity Center and contains an existing development pattern conducive to pedestrian-oriented development where buildings line the public R.O.W. and parking is located to sides and rears of buildings to create a safe pedestrian environment.



Permitted Uses

1. Permissive uses in the C-2 and R-2 zone.
2. Nursing and Rest homes.

Conditional Uses

1. School, other than public.

Prohibited Uses

1. The sale of alcohol for off-premise consumption.
2. Drive through/up service windows are prohibited.
3. House, one per lot.
4. Conditional Uses in the C-2 zone are prohibited.

Permitted Building Types

1. Development shall occur in one or more of the following: Stacked flats; Terrace apartments; Courtyard apartments; Podium building; Flex building; Liner building, Stand alone commercial/office building; Civic institutional building; Structured parking - no ground floor uses, shall be located behind another structure; Structured parking - ground floor uses; Townhouse; Duplex,/Triplex/Fourplex, Loft.

Usable Open Space

1. A minimum 20% of the site area shall be designated as usable open space in the form of patios, plazas, exterior walkways, balconies, roof decks or courtyards.

Public Space

1. One public space area, a minimum of 200 square feet, shall be provided for every 10,000 square feet of building space.
2. The public space area shall be privately owned and maintained and should typically contain seating and shade.
3. A minimum of 50% of the required public space shall be provided in the form of aggregate space that encourages its use and that serves as the focal point for the development.
4. See General Development Standards for additional public space requirements.

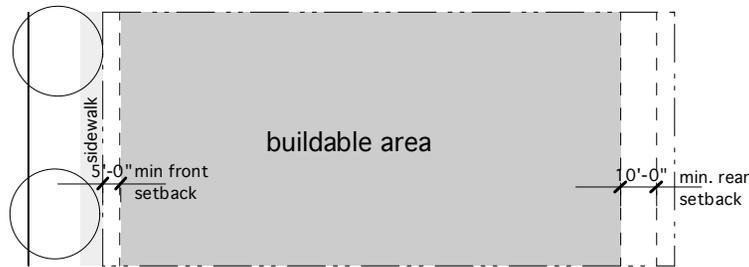
Landscape Standards

1. Landscape standards shall be per the City of Albuquerque Zoning Code (§ 14-16-3-10) except that the total landscape area required for each development shall be a minimum 10% of the net lot area.

Off Street Parking Requirements

1. For Residential Uses: 1.5 spaces/unit minimum
2. For all other uses: 1/1000 square feet minimum
3. Parking shall be located to the side or rear of building.
4. Parking area shall be setback a minimum of 10 feet from the property line.
5. Parking area shall be buffered from public ROW by a minimum 10 foot landscaped area and a site wall or continuous plant material a minimum of 3 feet high.
6. See Additional Development Requirements for maximum allowable parking frontage.

W66 - MX Development Requirements



Site Layout Diagram

Building Placement

Front Setback

Properties fronting Central: No Minimum*

All others: 5 feet minimum*

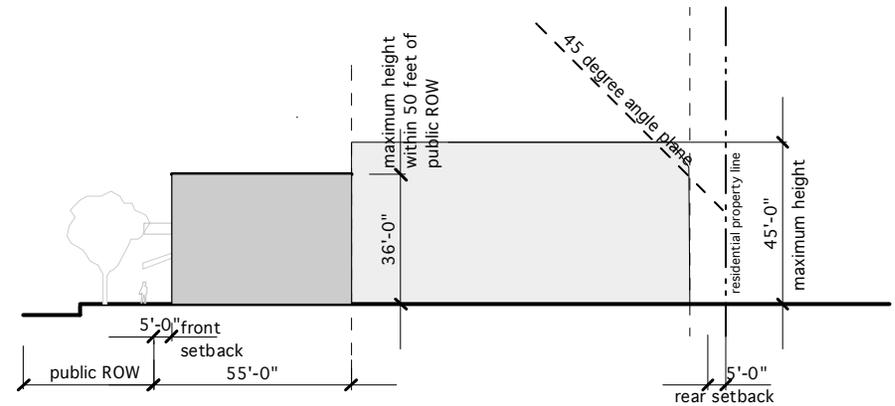
*upper floors, balconies, portals, shading devices and signage may protrude up to five feet into setback area within the property line a minimum of 8 feet above sidewalk.

Side setback

No minimum

Rear setback

10 feet minimum



Building Section Diagram

Building Height

1. Heights shall be limited to 45 feet maximum.
2. Within 75 feet of an abutting R-1 through R-T zone and corresponding SU-2 zone, building heights shall not exceed a 45 degree angle plane that begins at a height of 26 feet, measured from the residential property line.
3. Within 50 feet of Central Avenue, heights shall not exceed 36 feet.

Additional Development Requirements

All properties

1. General Development Standards, p. 111, shall apply to this zone.
2. For lots 150 feet or less in width, a maximum of 50% of the street frontage may consist of parking and drive aisles at the minimum setback.
3. For lots greater than 150 feet in width, a maximum of 30% of the street frontage may consist of parking areas. All other parking areas shall be located behind a building. Remaining street frontage shall consist of buildings, courtyards, patios and seating areas, site walls up to 4 feet in height and landscaping.
4. A 5 foot minimum landscape buffer is required between parking areas and sidewalk. All other parking areas shall be located behind a building. Remaining street frontage shall consist of buildings, courtyards, patios and seating areas, site walls up to 4 feet in height and landscaping.
5. A minimum of 75% of the first floor building frontage shall be at the minimum front setback or at the sidewalk where applicable. A deviation up to 10 feet is permitted to accommodate building articulation, patios and building entrances. Remaining street frontage shall be landscaped, see General Development Standards for requirements.
6. Stand alone ATM stations shall be walk-up only. See General Development Standards for Stand alone ATM station requirements.

W66 - RA Zone

Intent: The W66 River Activity (W66 RA) zone is a form based zone that provides for limited commercial development and public facilities near the Rio Grande, which serve retail, recreational and cultural needs of residents and visitors. The zone prescribes lower-density development that is designed to complement the natural environment of the river and Bosque, and the acequia system.

Justification: The W66 RA zone provides for a mix of non-residential land uses and for development design appropriate at this unique confluence in the city between urban Activity Centers and a Major Transit Corridor on the one hand, and the Open Space and waterways associated with the Rio Grande on the other.



Permitted Uses

1. Permissive uses of the C-1 zone.
2. Sales, service and rental of bikes and outdoor recreational goods; outdoor restaurant seating; outdoor sales of fruit, vegetables, flowers and plants and incidental related signs; community garden; recreational facilities oriented toward outdoor activities: facility for horses and riding, kayaking, lake; outdoor storage or activity if part of a permissive use on the same premises that does not negatively impact neighboring uses or premises; bed and breakfast establishment; restaurant serving liquor; uses or activities in a tent except parking is not required on the premises.
3. Residential shall be permitted up to 30 du/acre.
4. Public parking areas shall be permitted.

Prohibited Uses

1. The following uses shall be prohibited: Church; drive-up service windows; auto parts and sales; gasoline, oil, liquefied petroleum gas or other vehicle fuel sales; medical or dental lab; pawn shop; car washing; dry cleaning; taxidermy services.

Permitted Building Types

1. Development shall occur in one or more of the following: Podium building, Flex building, Stand alone commercial/office building, Civic institutional building, Stacked flats, Courtyard apartments and loft unit.

Landscape Standards

1. Landscape standards shall be per the City of Albuquerque Zoning Code (§ 14-16-3-10) except the total landscape area required for each development shall be a minimum 10% of the net lot area and lots adjoining ditches and drains along the Bosque shall use plants native to that environment (refer to City Open Space list).

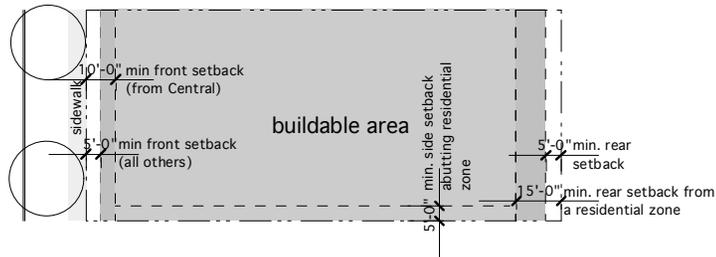
Usable Open Space Requirements

1. Due to the proximity of a major open space area, usable open space requirements for residential use are intended to provide outdoor space associated with an individual unit. A minimum of 50 square feet of usable open space shall be provided per dwelling unit.
2. Usable Open Space may be in the form of patios, courtyards, balconies, and roof gardens.

Off Street Parking Requirements

1. Required minimum parking calculations per the City of Albuquerque Zoning Code (§ 14-16-3-1) except:
 - a. Shared parking facilities. Off-street parking requirements may be met by shared parking facilities located within 800 feet.

W 66 - RA Development Requirements



Site Layout Diagram

Building Placement

Front Setback

Properties fronting Central: 10 feet minimum

All others: 5 feet minimum

Maximum front setback: 15 feet

**upper floors, balconies, portals, shading devices and signage may protrude into the front setback within the property line a minimum of 8 feet above sidewalk.

Side Setback

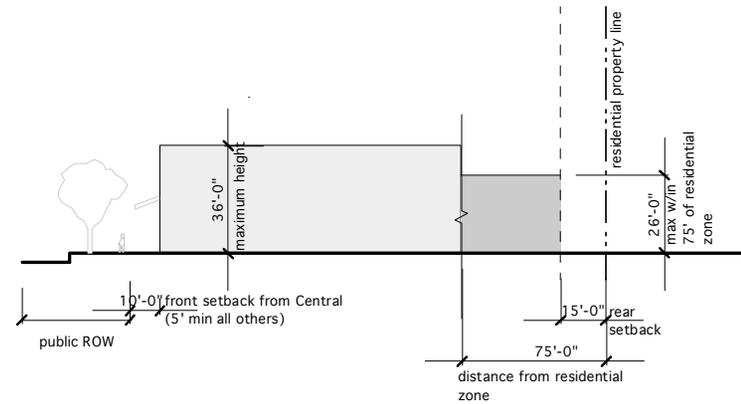
0 feet Minimum/5 feet minimum from residential zone.

Rear Setback

5 feet Minimum/15 feet where site abuts the rear of a lot in a residential zone.

Setback from abutting waterways (ditch and drain)

5 feet Minimum from MRGCD property or easement line, or 15 feet Minimum from toe of ditch or drain where neither such line exists.



Building Section Diagram

Building Height

1. Heights shall be limited to 36 feet maximum

2. Within 75 feet of an abutting residential zone for houses, building heights shall not exceed 26 feet.

Additional Development Requirements

All properties

1. General development standards, p. 111, shall apply to this zone.
2. In order to facilitate pedestrian circulation, no additional curb cuts shall be permitted. On parcels with no existing curb cut, one curb cut shall be permitted per 150 feet of street frontage.
3. No new parking shall be permitted between a building and a public right of way.
4. For lots greater than 75 feet in width, a maximum of 30% of the street frontage shall consist of parking areas.
5. A 10 foot wide minimum landscaped setback is required between the public realm and parking area. Remaining street frontage shall consist of buildings, courtyards, patios and seating areas, site walls up to 4 feet in height and landscaping.
6. Stand alone ATM stations shall be walk up only. See General Development Standards for Stand alone ATM requirements.
7. Development shall provide direct pedestrian access to the Bosque where possible.
8. Development shall be oriented to the Rio Grande and Bosque where possible. Facades which front the river and Bosque shall incorporate views of river, Bosque, mountains and acequias through the use of appropriately located glazing, balconies and outdoor pedestrian amenities including seating areas, courtyards and patios in order to take advantage of site location and to contribute to the development of an active outdoor recreation environment along the river and acequias.

W66 SAC Zone

Intent: The WR66 Special Activity Center (W66 SAC) zone is a Form based zone which provides for a 1 to 4 story mixed use environment serving neighborhood and tourism needs that is sensitive to the character of the Old Town and BioPark area. This zone prescribes medium to high density residential, shopping, service, office, and entertainment uses along a Comprehensive Plan designated Major Transit Corridor, and is within walking or biking distance of residential areas.

Justification: The W66 SAC zone provides for a mixed use environment which is supportive of the unique conditions afforded by City cultural facilities, Historic Route 66 properties and Old Town.



SAC zone potential development diagram.

Permitted Uses

1. Uses permissive in the C-1 and R-2 zone.
2. Uses not limited to 30 du/acre; density is controlled by building and site envelope.
3. The following uses are permitted: Catering, Baking, Confectionery making, Jewelry or curio making, Pottery making.
4. Bed & breakfast establishment, hotel, restaurant serving liquor.
5. Nursing and Rest homes.

Limited Uses

1. Drive up service windows are based on a cap and replace system and shall be capped at 4/42 acres. A new Drive up service windows shall be permitted only to replace an existing Drive up service window use. Drive up service windows are not limited to a specific property.

Conditional Uses

1. Schools, other than public.

Prohibited Uses

1. The following uses shall be prohibited: House, one per lot; vehicle sales, service, repair, and storage; sale of building materials; commercial surface parking lot; sample dwelling unit.

Permitted Building Types

1. Development shall occur in one or more of the following: Stacked flats, Terrace apartments, Courtyard apartments, Podium building, Flex building, Courtyard building, Liner building, Stand alone commercial/office building, Civic institutional building, Structured parking - no ground floor uses, located behind another structure, Structured parking - ground floor uses, Townhouse, Duplex, /Triplex/Fourplex, Loft unit.

Usable Open Space

1. A minimum 20% of the site area shall be designated as usable open space in the form of patios, plazas, exterior walkways, balconies, roof decks or courtyards. Open space shall be oriented to the public realm where possible.

Public Space

1. One public space area, a minimum of 200 square feet, shall be provided for every 10,000 square feet of building space.
2. The public space area shall be privately owned and maintained and should typically contain seating and shade.
3. A minimum of 50% of the required public space shall be provided in the form of aggregate space that encourages its use and that serves as the focal point for the development.
4. See General Development Standards for additional public space requirements.

Landscape Standards

1. Landscape standards shall be per the City of Albuquerque Zoning Code (§ 14-16-3-10) except the total landscape area required for each development shall be a minimum 10% of the net lot area.
2. In order to maintain a continuity of materials in the pedestrian realm to reinforce the concept of a garden district, site walls within 25 feet of Central Avenue or Rio Grande Boulevard shall be exposed adobe construction.

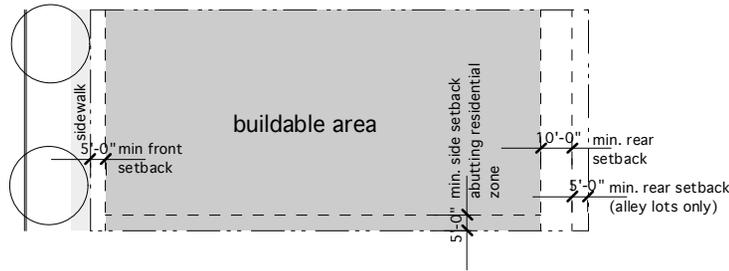


Adobe wall.

Off Street Parking Requirements

1. Required minimum parking calculations:
 - a. All uses: 1/1000 square feet
 - b. Shared parking facilities. Off-street parking requirements may be met by shared parking facilities located within 800 feet of the site.
 - c. See § 14-16-3-22(C)(5) for additional parking standards.

W66 - SAC Development Requirements



Site Layout Diagram

Building Placement

Front Setback

Minimum setback: 5 feet from front property line to create Building Amenity Zone.

*upper floors, balconies, portals, shading devices and signage may protrude into the front setback within the property line a minimum of 8 feet above sidewalk.

Side Setback

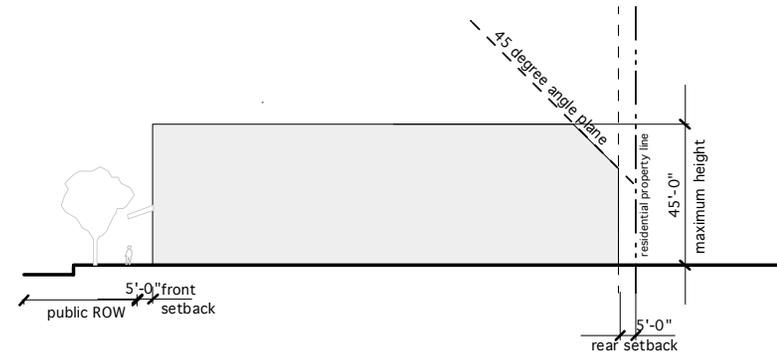
Internal side setback: no minimum.

Side setback adjoining residential zones: 5 feet minimum

Rear Setback

Rear setback: 10 feet minimum on lots without alleys

5 feet minimum on lots with alleys



Building Section Diagram

Building Height

Maximum height: 45 feet within 75 feet of an abutting R-1 through R-T zone and corresponding SU-2 zone, building heights shall not exceed a 45 degree angle plane that begins at a height of 26 feet, measured from the residential property line.

W66 - SAC Development Requirements

Additional Development Requirements

All properties

1. The General Development Standards, p. 111, shall apply to this zone.
2. For lots greater than 75 feet in width, a maximum of 50% of the street frontage shall consist of parking and driveway. Remaining street frontage shall consist of buildings, courtyards, patios and seating areas, site walls up to 4 feet in height and landscaping a minimum of 20 feet deep from the front property line.
3. A ten foot minimum setback shall be required between parking and the front setback, or the sidewalk where applicable.
4. No additional parking shall be permitted between a building and a public right of way.
5. No additional curb cuts shall be permitted.
6. For lots 5 acres or greater, Section 14-16-3-2 shall apply in addition to the above requirements. Where conflicts exist between this zone and the shopping center regulations, the regulations of this zone shall prevail.
7. Large truck parking shall be screened from street and adjacent buildings by trees and landscaping, architectural walls or other treatment approved by the Planning Director.
8. Service and loading shall not be located on any side of a structure that is adjoining residentially zoned lots.
9. See General Development Standards for Drive up service window requirements.
10. Stand alone ATM stations shall be walk-up only. See General Development Standards for Stand alone ATM station requirements.
11. See General Development Standards for Service Station requirements.
12. See General Development Standards for additional design standards.
13. Water Harvesting. In order to increase permeable area, mitigate against area flooding problems, and encourage the creation of a garden district, water harvesting on site is required. Water harvesting may consist of both active and passive measures. Passive measures include water catchment areas to collect surface runoff. Runoff from roofs should be collected into separate water catchment areas as this stormwater is cleaner than surface runoff from parking areas. Where possible catchment areas shall be located in the front setback or

in areas where their dual purpose as a ponding area and landscape amenity is most appreciated

14. Water catchment areas should be sized to achieve the first flush. See Drainage section.
15. Grading, hydrology, and landscaping plans shall be integrated to make maximum use of site storm water runoff for supplemental on site irrigation purposes. The landscape plan shall indicate use of all runoff, from individual catch basins around single trees to basins accepting flow from an entire vehicular use area or roof area
16. Storm water and runoff harvesting shall supplement drip irrigation and are required elements of the irrigation system for both new plantings and preserved vegetation
17. Catchment area shall be planted in a manner which supports the theme of a garden district. The following plants shall be used: Pending.

All development fronting Central Avenue

1. Development shall allow for a Building Amenities Zone within the first 5' of the front property line.
2. A minimum of 30% of the first floor building frontage shall be at the minimum front setback. A deviation up to 10 feet is permitted to accommodate building articulation, patios and building entrances. Remaining street frontage shall be landscaped.

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General Development Standards for All Zones

7.0 General Development Standards

This section includes mandatory standards and recommended guidelines that apply to new development within the sector plan area. (For exemptions, see Development Compliance.) Where a conflict exists between the Plan and the Comprehensive Zoning Code, the Plan will prevail. Where a conflict exists between the Plan and other applicable Rank III plans and Design Overlay Zones, such as the H-1 Historic Old Town zone, the stricter regulation will prevail.

General Development Standards Contents:

- A. Building Types
- B. Frontage Types
- C. Building Standards
- D. Multifamily Design Standards
- E. General Site Standards
- F. Pedestrian Circulation and Connectivity
- G. Off Street Parking
- H. Public Spaces
- I. Site Lighting
- J. Signage
- K. Walls and Fences
- L. Screening of Mechanical Equipment, Service and Loading Areas.
- M. Landscape Standards
- N. Best Practices Guidelines for Site Development

Building and Frontage Types

A. Building Types

1. Permitted Building Types established by zone.
2. Building Types can be found in the Comprehensive Zoning Code § 14-16-3-22(C)(1).
3. The additions/modifications to the Comprehensive Zoning Code Building Types include Townhouse, Rowhouse and Loft.

B. Frontage Types

Frontage Types are defined in the Comprehensive Zoning Code § 14-16-3-22(C)(2). Frontage Types are prescribed by Building Type.

Graphic Pending

1. Townhouse

- i. **Description.** A Townhouse is an attached or semi-attached building type consisting of at least four attached units which have varying front setbacks creating semi-private terraces and entry gardens. Townhouse may also be known as Patio Houses. Townhouses may be one or two stories and may vary architecturally from one another, though typically maintain a similar style.
- ii. **Access.** Access to each unit shall be via Stoop, Porch or Patio fronting the street, courtyard, plaza or paseo. Loading and trash disposal shall be accessed from an alley or side driveway.
- iii. **Parking.** Parking shall be located in the rear portion of the lot or a common parking area may be located interior to the block. Garages may be attached, detached or connected by a breezeway and shall be setback from the front façade a minimum of ten feet.
- iv. **Frontage.** Each dwelling unit shall include a stoop, patio, portal or porch frontage type fronting the street or fronting a courtyard, plaza or paseo.
- v. **Exposure to light and air.** Each unit shall have at least two sides exposed to the outdoors. Units may abut other units at the property line and share common walls.

Graphic Pending

2. Rowhouse

Rowhouse form shall be per City of Albuquerque Form Based Zones “Rowhouse” with the following additions and exceptions:

- i. Description. A Rowhouse building type consists of at least four attached units divided from each other by common walls. Each unit shall have a separate entrance. Typically, Row Houses have aligned repeating front facades, though porches or stoops may project. Each unit shall be articulated architecturally as an individual unit. Row Houses may be one or two stories.
- ii. Access. Access to each unit shall be via Stoop, Porch or Patio fronting the street, plaza or paseo.

Graphic Pending

3. Loft

- i. Description. A loft is a live/work attached or semi-attached building type. A loft may be 1½, 2 or 3 stories. Lofts consist of a minimum of three units and can be used for live/work, work/work or live/live. The residence and work components are intended to be shared by the same user and should be directly linked via interior circulation. Live/work units may vary architecturally; however they should have similar stylistic qualities.
- ii. Access. Each Loft building type shall have only one entry on the front façade known as the primary entry. The primary entry to each dwelling unit shall have direct access to the street, plaza, paseo or to a court. Loading and trash disposal shall be accessed from an alley or side driveway.
- iii. Parking. Parking shall be located in the rear portion of the lot or a common parking area may be located interior to the block. Garages may be attached, detached or connected by a breezeway and shall be setback from the front façade a minimum of ten feet. If the Loft is intended to be used as live/work or work/work, enough dedicated parking spaces shall be available on site for employees.
- iv. Frontage. Each dwelling unit shall include a stoop, patio, portal and/or porch frontage type fronting the street, plaza, paseo or a courtyard.
- v. Exposure to light and air. Each unit shall have at least two sides exposed to the outdoors. Units may abut other units at the property line, and/or share common walls.

General Development Standards for All Zones



Corner articulation.



Building articulation.



Balconies and entrances to street.

The following standards are intended to create an environment that is visually appealing and pedestrian friendly, by requiring building articulation that creates visual interest, variety, relates to a pedestrian scale, and provides visibility both into and out of buildings, creates sidewalk activity and provides pedestrian comfort through sidewalk shading.

C. Building Standards

1. No specific architectural style is required and a variety of styles is encouraged. Architectural elements such as walls and fences shall provide a unifying theme for the development.
2. Parking garages visible from a street shall be designed to be complementary with adjacent buildings, through the use of similar building forms and materials.
3. Building entry ways on streets. At least one entrance for each commercial/retail space on a street facade shall have direct access to the street, a street-fronting court or plaza, or a paseo. All upper level non-residential and residential spaces shall have a first floor lobby or private stair with direct access to the street or a street-fronting court or plaza.
4. Corner lots. Corner lots shall address the corner in at least one of the following ways: location of main entrance at a corner; articulation at the corner of the building relating to the corner, i.e. curve, angle, step back or projection, tower element and/or Planning Director approved detail.
5. Upper floors shall have a minimum of 20% glazing on street-facing facades and shall meet the articulation requirements of each zone.
6. Building facades next to public spaces shall provide pedestrian features as listed in § 14-16-3-18 (C) of the Zoning Code.
7. Articulation on street facades. The design standards of the Zoning Code § 14-16-3-18(D) shall apply with the following modification and exception:
 - a. Wall plane projection or recesses shall occur at least every 75 feet (ref. § 14-16-3-18(D)(2)(a))
 - b. Section 14-16-3-18 (D)(2)(b), Vertical Change of Color or Material, shall not apply.
 - c. Art such as murals and sculptures will not require coordination with City Arts Program. (ref. § 14-16-3-18(D)(2)(e))
8. Balconies and portals shall have a minimum 8 feet of vertical clearance.
9. Uncolored standard concrete masonry unit (CMU) block is not allowed.
10. Reflective glass is not allowed unless the applicant can demonstrate that the glass would not reflect the sunlight glare or solar heat build up on adjacent residential properties or rights-of-way during the hours of 7-10am and 3-6 p.m.



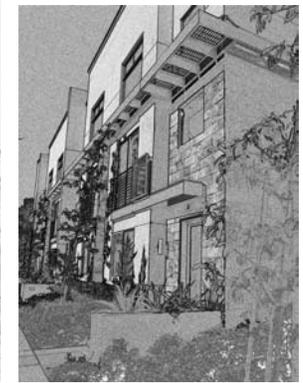
Paseo



Buildings oriented to court.



Individual articulation of units through materials and setbacks.



Site walls and landscaping define units.

D. Multi-family Design Standards

1. Buildings along the public R.O.W. or private drives shall orient to the street through the following:
 - a. Entrances to buildings or individual units shall be via stoops, patios, porches or courts.
 - b. Units and buildings shall front on public R.O.W., private drive, paseo (which leads directly to public R.O.W.) or courtyard (which is open or directly connected to a public R.O.W.).
 - c. Where sites are deep, allowing for development setback from the public R.O.W, development shall first meet minimum setback requirements of the zone before areas setback further from the public R.O.W may be developed.
2. Large (over 45,000 square feet footprint) multifamily designs are discouraged. Building mass and footprint shall be broken up through the use of smaller buildings, courtyards/courts, paseos and internal site streets.
3. In order to maintain an urban block pattern along public streets, buildings shall be no more than 400 linear feet long along a public R.O.W. and multiple buildings which front a public R.O.W on a single site shall be separated by a minimum of 50 feet to include a landscaped area with a paseo or bicycle path, a private or platted drive with sidewalks, a landscaped courtyard or play area.
4. Building articulation. A visible change in building setback, material or color shall occur a minimum of every 60 feet. Color or material change shall happen at clear breaks in mass or at inside corners.
5. Units with exterior entry ways shall have a minimum of 40 square feet of usable entry area dedicated to the unit. Minimum dimension of exterior entry way shall be 5 feet. Usable entry area may be in the form of a balcony, porch, patio, courtyard or terrace. No access path to another unit may be within the entry area. Usable entry way shall be buffered from abutting units through landscaping, site walls, or offset in building facade.
6. Units in multi-family buildings shall be individually articulated where possible. A minimum of two (2) of the following methods shall be utilized:
 - a. Building setbacks, step backs and offsets between units,
 - b. Visually separate balconies, terraces or patios,
 - c. Individual exterior entrances to units,
 - d. Majority entry element of a minimum of one of the following: a grand entry staircase, architecturally interesting overhang at entry point, public art piece, landscaped entry court, fountain, or other element approved by the Planning Director,
 - e. Color or material change to articulate individual units.
7. Gated communities shall not be permitted. Walls over 3 feet shall not be permitted within 15 feet of the public R.O.W. Gated access to interior courtyards shall not be considered a gated community.

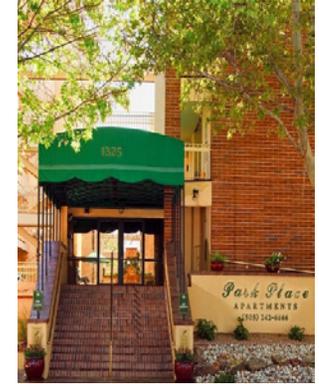
Residential Development Guidelines



Color and materials articulate individual units.

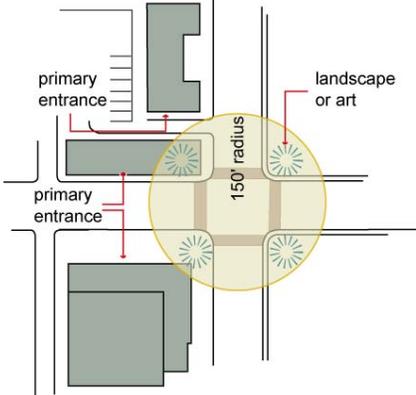


Individual balconies



Grand entry way

8. Parking shall not be visible from the public R.O.W. Where possible, parking shall be located behind buildings. Parking adjacent to a public R.O.W. shall be screened by a combination of site walls, trees and landscaping.
9. Parking areas shall be separated from residential areas by a 10 foot landscaped buffer with a minimum of 1 tree per 50 square feet and connected to residential area by a paved path a minimum of 5 feet in width. In no case shall a residential building be surrounded on more than two sides by parking.
10. Individual unit parking garages shall not front the public R.O.W. and shall be setback a minimum of 15 feet from the front facade. Double garage doors are not permitted. If a unit has more than two garage spaces, the third space shall be setback an additional 5 feet.
11. Carports shall not front the public R.O.W. Carports shall not front individual units and should be located behind residences where possible.
12. Parking structures shall not be accessed directly from the public R.O.W.
13. Sliding windows are discouraged.
14. Windows shall be recessed a minimum of 1 inch measured from building face to glazing.
15. Each unit shall have exposure to light and air on a minimum of two sides.
16. Sites with multiple residential buildings shall organize buildings around a common area, a paseo, or open space area. Pedestrian pathways shall connect all buildings on site.
17. Vehicular drop off areas shall be located to the side or rear of buildings.



Intersection with defined pedestrian crossing and primary entrance locations



Plan diagram showing pad site development with internal bays of parking, clear pedestrian access (orange lines) and street edge definition.

E. General Site Standards

The intent of the General Site Standards is to provide for minimum site development standards which will ensure cohesive layout of the project, continuity of design across all elements such as signs, outside storage, and landscaping within the property line, and a continuous street landscaping buffer along Central Avenue. Note that building placement and setbacks are determined by zone.

Site layout and design

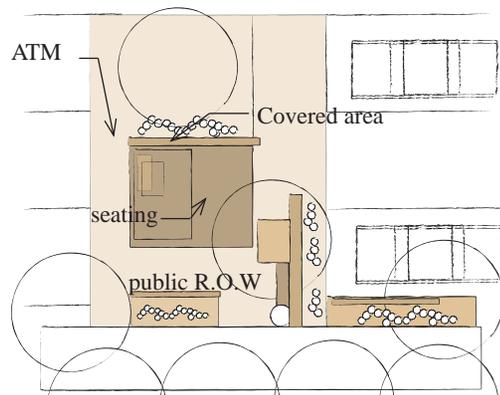
1. Development shall be designed in a manner to create the impression of a unified project and overall sense of a unique or identifiable place. A sense of entry or arrival shall be created at primary entry ways into the development. Building placement, landscaping, gates, entry monuments, specialty lighting and other design elements shall be used to create this design effect.

Guideline A. Linear “strip” development is discouraged. This type of development is characterized as one store deep with buildings and/or units arranged in a linear fashion rather than clustered.

Guideline B. Multiple buildings in single projects should be varied in size and mass.

2. Other design elements of the site, such as paving textures, seating, outdoor displays, walls and fences, and other amenities shall be compatible with the architectural and landscape treatment of the project.
3. Pad Development. The following design standards shall apply:
 - a. To the maximum extent practical, pad sites shall be clustered together to create consistent edges along streets and to provide safe and convenient pedestrian connections between buildings.
 - b. Spaces between adjacent pad site buildings shall provide pedestrian connections and amenities between sites such as a landscaped pedestrian walkway linking customer entrances between two or more pad site buildings; a public seating or outdoor eating area; sculptures or fountains; or other design features approved by the Planning Director.
 - c. Pad site buildings shall incorporate the same materials as those on the primary commercial building(s) in the development.

General Development Standards for All Zones



Conceptual Stand Alone ATM Diagram

Walk up ATM

Graphics Pending

4. Stand alone ATM standards. Stand alone ATM's are automated teller machines which are not attached to a building. Stand alone ATM's shall be walk-up only. Drive-up ATM's shall only be permitted as part of a bank drive up facility. (See Drive through/up Standards) for additional information. Stand alone ATM's shall conform to the following standards:
 - a. Stand alone ATM may be permitted in front setback.
 - b. ATM Machine shall be covered by a shading element.
 - c. ATM Machine shall be incorporated into a public space a minimum of 50 sq ft. including at a minimum: 1 tree, 1 bench, 1 trash receptacle and 1 planted area.
 - d. Stand alone ATM shall be buffered from parking areas by a 3 foot minimum wall or plant material a minimum of 3 feet in maturity.
 - e. Stand alone ATM shall be connected to public R.O.W. or buildings on site via sidewalk.
5. Drive through/up Standards. Permissive Use determined by zone. Drive through/up shall not have queuing lane abutting Central Avenue. Drive through/up lane and window shall be buffered from the public R.O.W by a minimum of a 5 foot landscape area with a 3 foot site wall or building. Where possible, Drive through/up service windows and lanes shall be located on a non-street fronting side of a building.
6. Outdoor storage. No outdoor storage is allowed within 50 feet of the Central Avenue right-of-way line. All outdoor storage shall be enclosed on all sides by a minimum six foot high solid wall or fence.
7. Encroachments. a. Encroachments in the public R.O.W. shall follow existing city regulations. b. Building encroachments in the form of balconies or structural shading elements shall not extend within 2 feet of the curb.
8. Grading. Severe changes in grade shall be minimized. Retaining walls up to 4 feet area allowed. Any changes greater than 4 feet shall be permitted only by terracing at a 3:1 slope. A combination of these techniques is allowed.

General Development Standards for All Zones



Permeable paving



Vegetative swale.



Hardscape broken up with bio retention areas.



Decorative grate and inlet.

- 9. Water harvesting areas. Surface runoff shall be directed into water collection areas located in parking lot landscape areas, landscape setback areas and patio or plaza areas where possible. The burden is on the applicant to demonstrate why water harvesting is not possible if water harvesting areas are not utilized.
- 10. Low Impact Development (LID) Requirements
 - a. All new development shall use two (2) or more of the following LID strategies.
 - b. All new development over 1 acre shall use three (3) or more of the following LID strategies.
 - c. LID strategies are as follows:
 - i. Impervious Surface. Minimize the amount of directly connected impervious material. Transition to permeable material under parking areas or create breaks in impervious material with open curbs of flush mount curbs that allow water to flow into landscaped areas.
 - ii. Internal Roadway Cross Sections. Minimize roadway cross sections where possible. Pedestrian zones should be separated from vehicular zones with a landscaped area that is bordered by open curbs or flush mount curbs to allow for water flow.

- iii. Intersection. Introduce traffic circles at large intersections that can be designed to accept storm water run off and act as a landscaped bio-retention area. At all intersections, minimize the radii to slow traffic and reduce paved area.
- iv. Looped Turnaround. Where possible along the corridor, cluster development and design a looped turn around for access. The center of loop can be bordered by flush mount curbs and act as a landscaped bio-retention area.
- v. Driveways. Where possible, share driveways.
- vi. Sidewalks. Slope/grade sidewalks horizontally so they drain towards flanking landscape.

General Development Standards for All Zones



Shade structure and fixed seating.



Children's play area.



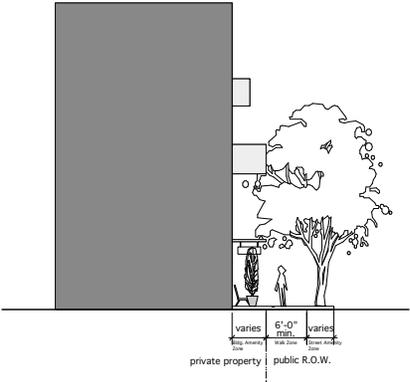
Allee of trees.



Water element/fountain.

11. Development Phasing. Construction phasing shall be indicated on the Site Development Plan. First phase development requirements: A Pedestrian Realm shall be constructed during the first phase of development. In addition to Pedestrian Realm requirements in the General Standards, undeveloped areas adjacent to the Pedestrian Realm shall include one or more of the following for every 400 linear feet of street frontage to provide visual interest until development build out.

- a. Water element (fountain, spray park, pond)
- b. Hardscaped plaza with colored scored concrete or pavers.
- c. Amphitheater/Fixed seating/movable seating 1/20 LF minimum
- d. Grove a minimum of 3 rows wide and 3 rows long
- e. 2 shrub + 1 tree /60 LF
- f. Shade structure
- g. Mural
- h. Public Art/Sculpture
- i. Children's play area
- j. Landscape Feature (planted area, garden)
- k. Allee of trees, 25 feet on center



Public Realm Diagram

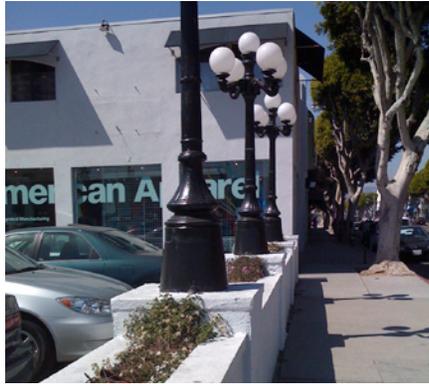
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F. Pedestrian circulation and connectivity

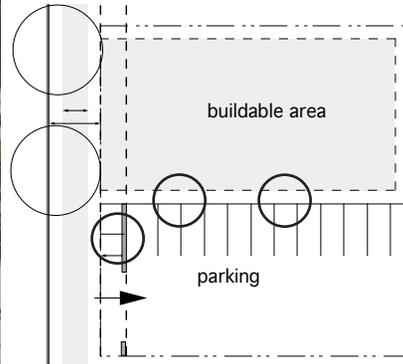
1. Sidewalks shall be located at the property line, except where necessary to meet existing adjacent sidewalk.
2. Public utilities shall not be located so as to interfere with clear pedestrian circulation.
3. Pedestrian Realm. The area from the back-of-curb dedicated to pedestrian use. The required dimensions of the Pedestrian Realm are per zone and shall contain the following areas subject to the following regulations:
 - a. Building Amenity Zone. Area adjacent to a building on private property that may be used to provide pedestrian amenities that stimulate street activity such as restaurant/café seating, retail vending, outdoor seating, fountain and public art.
 - b. Pedestrian Walk Zone. The area dedicated to pedestrian circulation within the public R.O.W. that shall remain clear at all times.
 - i. Sidewalk width. A clear pedestrian path of a minimum of six feet shall be maintained at all times.
 - ii. Sidewalk material. Sidewalks shall be a hard surface which may include concrete, brick, or pavers. Sidewalk material shall be slip resistant and of a permanent nature.
 - iii. Sidewalk alignment. The sidewalk shall be aligned where possible within the block.
 - iv. Sidewalk shall be located at property line.

- c. Street Amenity Zone. The area adjacent to the back of curb dedicated to street furnishings, bike racks, landscaping, telephones, information centers, lighting, signage, and transit facilities (benches and shelters).
 - i. Street trees. Street trees shall be located every 25 feet on center.
 - ii. The use of tree grates is permitted for all tree wells.
 - d. Limited R.O.W. Where public R.O.W. is too constrained to meet Pedestrian Realm requirements, Pedestrian Walk Zone shall be maintained and any additional R.O.W. shall be landscaped.
4. Internal Pedestrian Pathways.
 - a. For lots over one acre, separate pedestrian and vehicular access shall be provided from the public right-of-way to the building(s). Pedestrian access to buildings from the public right-of-way shall be shaded, illuminated for nighttime use, and not utilize driveways as walkways.
 - b. For sites with multiple buildings, pedestrian pathways shall connect each building and an unobstructed sidewalk measuring at least eight (8) feet in width shall be provided along the full length of all sides of buildings having customer entrances. Additional width shall be required if the sidewalk is used for seating, displays or other purposes.
 5. Site Connectivity. See MAC and SAC connectivity in Chapter 5, Transportation recommendations.

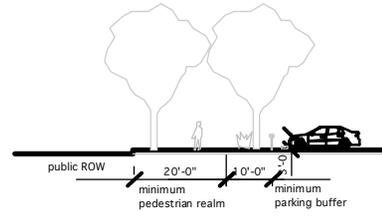
General Development Standards for All Zones



Parking with wall planter as buffer.



Parking located to side of building.



Parking diagram with landscape buffer.



Parking with landscape buffer.

G. Off-street parking

1. Buildings constructed prior to 1965 shall supply parking spaces only to the extent on-premise ground space is available.
 2. Adjacent and abutting on-street parking may count toward off-street parking requirements as permitted by zone.
 3. Standard City Transit reductions shall apply. See Zoning Code (§ 14-16-3-1(E)(6)(a)).
 4. Automobile and light truck space standards shall apply. See Zoning Code (§ 14-16-3-1(F)).
 5. Parking for Bicycles shall apply. See Zoning Code (§14-16-3-1(B)(3)).
 6. Pedestrian connections shall apply. See Zoning Code (§ 14-16-3-1(H)).
7. Off-street parking areas shall be designed and landscaped to minimize glare, reduce reflection and reduce the visual impact of large numbers of cars. Parking areas shall include the following landscaping elements:
 - a. A minimum of 15% of the parking lot area shall be landscaped. The landscaping shall consist of shade trees and shrubs and shall be distributed throughout the parking lot.
 - b. One tree shall be required per every 8 parking spaces. Tree planters shall be used at ends of double-loaded parking rows. Not more than 15 spaces side-by-side shall be allowed between planters. A minimum of 2 trees is required per parking area. No parking space may be more than 50 feet from a tree trunk.
 - c. Buildings shall be separated from parking areas and drives by a minimum 5 foot wide sidewalk and minimum 2 foot wide landscape buffer.

General Development Standards for All Zones



Sports court.



Patio dining.



Play area.



Community Garden

H. Public spaces

1. All pedestrian oriented and public spaces, including paseos and the pedestrian realm, shall comply with the following standards:
 - a. Include amenities such as benches, movable seating, drinking fountains, trash receptacles, or kiosks; A minimum of 1 bench and 1 trash receptacle is required. For public spaces greater than 100 sq ft, 1 additional seat is required for every additional 100 sq ft. 1 trash receptacle is required for every 10 seats or at least one trash receptacle every 300 lf along linear public spaces.
 - b. Consider solar angles and wind conditions in the design of the space;
 - c. Include areas along building edges for outdoor seating;
 - d. Be designed for security and be visible from the public right-of-way as much as possible;
 - e. Have pedestrian scale lighting a maximum of 16 feet in height.
 - f. Utilize distinctive paving materials, including permeable paving. Asphalt paving is prohibited.
2. Public spaces over 400 sq ft shall contain an outdoor plaza, patio, courtyard or pocket park with seating and shade covering a minimum of 25% of the area. A portion (generally at least 40%) of the square footage of this public space shall be landscaped with plant materials, including trees.
3. In addition, Public spaces over 10,000 sq ft shall contain a minimum of 1 of the following features:
 - a. Sculpture or other artwork.
 - b. Fountain or some other water feature.
 - c. Playground or other recreational amenity.
 - d. Any other amenity that meets the intent of this Plan and that meets the approval of the Planning Director.

General Development Standards for All Zones

I. Site Lighting

The intent of the site lighting standards is to ensure cohesive design, provide adequate lighting on a site for safety and to prohibit site lighting from creating a nuisance on adjacent property.

1. Exterior lighting shall be designed as part of the architectural and landscape character of a project. Fixture style and design shall be compatible and consistent with the building design.
2. Site lighting shall be per area lighting regulations in the Zoning Code (§ 14-16-3-9) with the following additions:
 - a. Pedestrian site lighting. Pedestrian street lights shall be located between 13 feet and 16 feet above grade with a maximum average spacing (per block face) of 60 feet on center. Pedestrian street lights must be placed two feet from the back of curb on each side of the street and travel lanes, unless otherwise indicated. Street lighting and street trees should not conflict.
 - b. Exterior building lights. On the street front elevation, exterior lights shall be mounted between six feet and 14 feet above adjacent grade.
 - c. Alley lighting. All lots with alleys shall have lighting fixtures within five feet of the alley's edge of pavement where it does not conflict with vehicle access and circulation. The fixture shall illuminate the alley, be between eight and 12 feet in height, and not cause glare into adjacent lots. When a structure in the lot is within five feet of the alley's edge, the lighting fixture shall be attached to the structure and not to a light pole.
 - d. Lighting elements. Lighting elements shall be compact fluorescent, metal halide, LED, or halogen only. No HID or fluorescent tube lights (excepting compact fluorescent bulbs) may be used on the exterior of buildings.
 - e. Floodlights and directional lights. Floodlights or directional lights may be used to illuminate alleys, parking garages and working (maintenance) areas, but must be shielded or aimed in such a way that they do not shine into other lots or the street.

J. Signage

The intent of the signage standards is to provide for readable, attractive signs that do not dominate the streetscape, and to encourage the use of neon and iconic signs along Central Ave. that reinforce Route 66 identity. These standards prevail over any conflicting regulations that apply to signs as a permitted use in individual zones within the plan area.

1. Signage shall be per the C-1 zone, §14-16-3-5 General Sign Regulations shall apply with the following additions and exceptions:
 - a. Neon signs
 - i. Definitions.
Neon sign. A sign that uses neon, argon, mercury or a similar gas to fill tubing made of glass or similar material, which is charged with electricity in order to create an illuminated tubular sign or illuminated elements of a sign that comprise at least 20% of the sign face area and include at minimum lettering and/or images. The tubing may contain an alternative illumination technology, such as, but not limited to, light-emitting diodes (LEDs). Any non-gaseous illumination technology, such as LEDs, must produce illumination that appears to be a continuous, uninterrupted line, similar to illumination produced by gaseous illumination technology.

Animated neon sign. A neon sign that uses changes in luminance in a sequential or radial manner to produce what appears to be movement of an element of the animated sign. Flashing and physical movement of a sign and/or its elements are not considered sequential or with radial changes in luminance.

- ii. Neon signs, including animated neon signs, are permitted on properties fronting Central Ave., or fronting on 98th Street, Unser Blvd or Coors Blvd if the signs are within 300 feet of the center line of Central Avenue, subject to regulations in §14-16-3-5 that protect residential zones. The following modifications to zone standards and general sign regulations also apply:
 - Location.
Free-standing and all types of building-mounted neon signs are allowed. A free-standing sign shall be allowed on properties designated Developing Urban by the Comprehensive Plan.

A neon sign may overhang or protrude horizontally up to 5 feet into the public right-of-way, except a marquee sign is allowed

General Development Standards for All Zones

10 feet, if it is a minimum of 12-feet above the right-of-way at grade and does not impinge on required clearance for overhead utilities.

A neon sign shall not obscure portions of a building that contribute to architectural character including, but not limited to, doors and windows.

· Number.

One free-standing or projecting neon sign per premise frontage is permitted, except that corner lots of less than 5 acres are permitted only one free-standing or projecting sign. A free-standing sign is allowed on properties designated Developing Urban by the Comprehensive Plan.

One building-mounted neon sign per business or tenant façade fronting Central, 98th Street, Unser Blvd and Coors Blvd is permitted.

· Size.

Free-standing and projecting signs. Sign face area may be up to 50% over the sign area allowed for conventional signs but shall not exceed 150 square feet total after the bonus is applied on premises of less than 5 acres, or 225 sq ft on larger premises. Lettering is allowed a proportionate size increase.

Building-mounted signs, excluding projecting signs. Total sign face area may be up to 25% over the total sign face area allowed for conventional signs but shall not exceed 20% of the façade area after the bonus is applied. Lettering is allowed a proportionate size increase.

· Height.

Free-Standing Signs. Height shall not exceed the height of the main structure, excluding towers, on the property by more than 10% or it shall not exceed 30 feet, whichever is lower.

Building-Mounted Sign. Height shall not exceed the height of the building, excluding towers, to which the sign is mounted by more than 10% or it shall not exceed 45 feet, whichever is lower.

b. Historic signs. Historic signs that are associated with the historic use of a premise are exempt from provisions of the Zoning Code (14-

16-3-5(B)(1)(d)) and of this plan that would otherwise prohibit their continued display. For the purposes of this subsection a historic sign is defined as a sign that is determined by the Planning Director to be a designated City Landmark or listed or eligible for listing in the New Mexico Register of Cultural Properties either individually or as a contributing part of a property. A historic sign may be a neon sign.

c. Iconic signs. An iconic sign is a sign whose form suggests its meaning. Such a sign is unique and creates an image and/or defines a place. An iconic sign may be sculptural in style and demonstrates extraordinary aesthetic quality, creativity or innovation. These are signs that are different from the typical sign and have elements of highly recognizable or identifiable features, sign faces or are otherwise distinguished from an average square or rectangular box design. They typically have characteristics of art, going beyond simply advertising the “what and where”. The iconic sign typically refers to an object in symbolic form (as with the road sign which shows a man at work). An iconic sign may be a neon sign.

i. Modifications to zone standards. Where an iconic building-mounted or free-standing sign is proposed, the applicable height and size limitations of the zone and/or these general standards may be modified to allow such sign based on the definition listed above and at the discretion of the approving body.

d. Conventional signs, as distinct from neon, historic or iconic, shall be regulated by standards of the Zoning Code, with the following exceptions and additions.

e. The following signs and sign elements are specifically prohibited.

i. Any off-premise sign referring to a business or merchant not doing business on the premises where the sign is displayed, unless it is allowed as a neon or historic sign (see a. and b. above). Existing off-premise signs shall be allowed to remain, until removed. No existing off-premise signs shall be altered or enlarged.

ii. Any sign which is attached to the roof of a building or structure between the eaves and the ridge line of the roof, unless it is allowed wholly as a neon, historic or iconic sign (see a., b. and c. above).

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- iii. Any sign which is a roof sign as defined in the Comprehensive City Zoning Code, unless it is allowed as a neon, historic or iconic sign (see a., b. and c. above).
- iv. Any sign which is located (painted, affixed, etc) on a water tower, storage tank, smoke stack, utility pole or other similar structure.
- v. Any sign which is located (painted, affixed, etc) on trees, rocks or other natural features.
- vi. Any sign which emits smoke, visible vapors, particles, sound or odor.

K. Walls and fences.

- 1. As per City Zoning Code (§ 14-16-3-19) with the following exceptions:
 - a. Prohibited materials. Concertina or barbed wire, sheet metal fencing and razor ribbon are prohibited. Chain link fencing is not permitted in the front setback or where adjacent to a residential use
 - b. Street walls within 20 feet of the public street right-of-way shall not exceed three feet in height above the abutting grade on the street side, except walls used for screening of mechanical equipment, loading and service areas shall not exceed six feet.

L. Screening of mechanical equipment, service and loading areas.

- 1. Pursuant to City Zoning Code (§ 14-16-3-18(C)(6)(a)) with the following additions and exceptions:
 - a. Service and loading facilities shall be combined, where possible.
 - b. Trash containers shall not be visible from a public or private street and shall be recessed or screened by a six-foot high solid wall and/or gate.
 - c. Ground mounted mechanical equipment, and service and loading areas shall be accessed from alleys or rear access easements where possible.
 - d. Exterior mounted mechanical and electrical equipment shall be architecturally screened. Location of such equipment within the building or at ground level is preferable to roof-mounting, unless such location would adversely affect the streetscape, pedestrian circulation, or open space.

M. Landscape Standards

- 1. Landscape standards shall be per the City of Albuquerque Zoning Code (§ 14-16-3-10) with the following exceptions:
 - a. The mature spread of a tree’s canopy may count toward the 75% required vegetative ground cover (§ 14-16-3-10(G)(3)) for landscape areas;
 - b. The use of tree grates is permitted for all tree wells;
 - c. Usable open space in such forms as patios, plazas and courtyards, shall have a minimum landscape area of 15%;
 - d. Building setbacks not used for pedestrian activity shall have a minimum landscape area of 50%. Asphalt is not a permitted material within the landscaped setback area.
 - e. Landscaping on roof decks may be counted toward the required area landscaping.
 - f. Standard and special landscape buffers shall be a minimum of 6 feet wide ((§ 14-16-3-10(G)(3) & (40). The special landscape buffer shall be planted primarily (at least 50%) with evergreen trees or tall shrubs, and a minimum three (3) foot high screen wall adjacent to the parking area shall be required between parking areas and abutting residential zones.
- 2. All landscaped areas shall be served by underground irrigation systems unless otherwise approved by the Planning Director.

n. Demolition

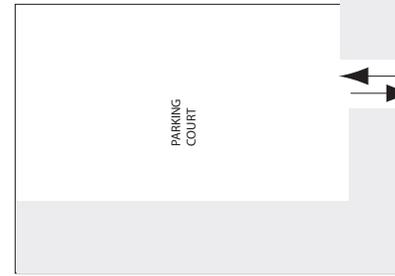
- 1. In order to limit the detrimental effect of demolition on remaining Route 66 historic resources in the plan area, a demolition review period as contained in Article 14-12-9 R.O.A. 1994 may be warranted when the demolition of a building is proposed. A review provides the time necessary to determine whether a structure that meets certain criteria shall be designated as a City Landmark consistent with Article 14-12-7, or for owners of such structures to consider alternative strategies for the preservation, rehabilitation, or restoration of an historic building. City residents and the neighborhood are required to be notified and asked to comment on the impending demolition of historic properties.

General Development Standards for All Zones

o. Best Practices Guidelines for Development

In addition to the requirements of the General Development Standards, the following Best Practices guidelines are encouraged to be incorporated in development.

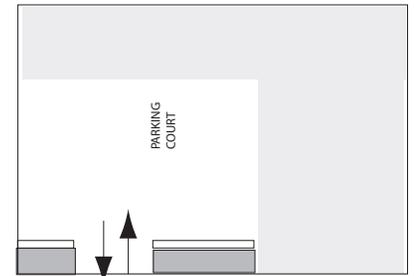
1. Building entry ways on streets. At least one entrance for each commercial/retail space on a street facade should have direct access to the street or a street fronting court or plaza. All upper level nonresidential and residential spaces should have a first floor lobby with direct access to street or a street fronting court or plaza.
2. Corner lots. Corner lots should address the corner in at least one of the following ways: location of main entrance at a corner; articulation at the corner of the building relating to the corner, i.e. curve, angle, step back or projection, tower element and/or Planning Director approved detail.
3. Where possible, in order to minimize conflicts between pedestrian and vehicular circulation, drive aisles should be one-way.
4. The following site layout diagrams suggest development organization which provides access for both pedestrian and vehicles while maintain a street frontage which contributes to the public realm. These diagrams should guide development where applicable.



CORNER - A
CORNER LOT. BUILDING WRAPS CORNER. ACCESS TO PARKING FROM SIDE STREET.

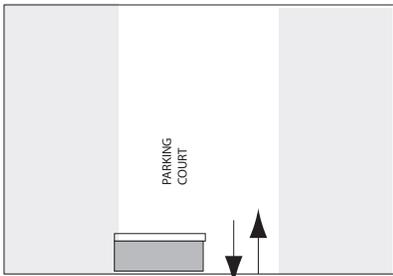


CORNER - B
CORNER LOT. PLAZA AT CORNER. ACCESS TO PARKING FROM SIDE STREET.

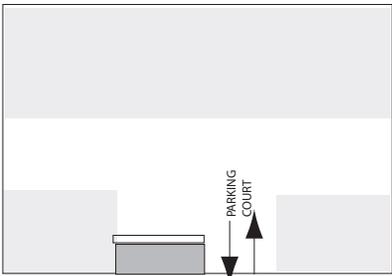


CORNER - C
CORNER LOT. BUILDING AT CORNER. ACCESS TO PARKING FROM MAIN STREET.

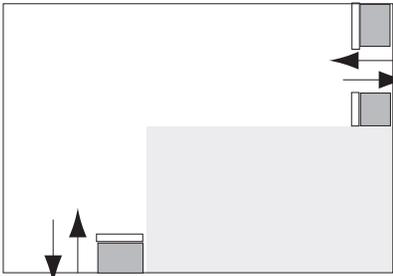
General Development Standards for All Zones



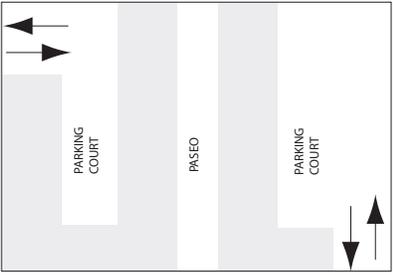
COMMERCIAL - SIDE PARKING
 PARKING LOCATED AT SIDE OF BUILDING, BUFFERED BY LANDSCAPING AND STREET WALL.



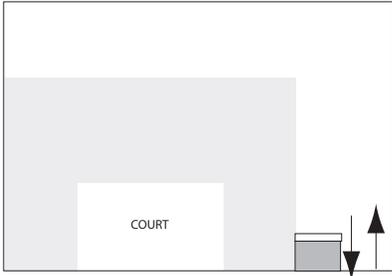
COMMERCIAL - PAD SITES
 PARKING PRIMARILY LOCATED BETWEEN BUILDINGS. SOME PARKING TOWARD STREET BUFFERED BY LANDSCAPING AND STREET WALL.



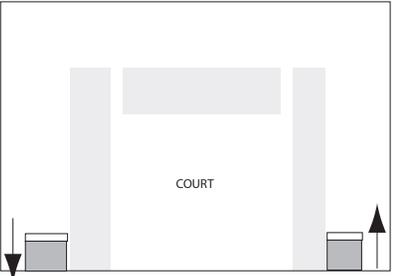
COMMERCIAL - GENERAL
 ONE BAY OF PARKING LOCATED AT SIDE OF BUILDING. MAJORITY OF PARKING LOCATED IN REAR OF BUILDING.



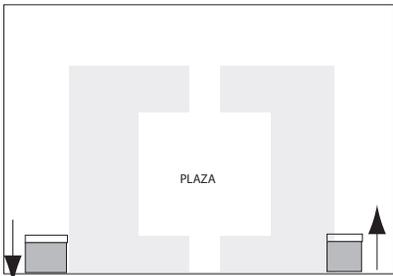
PASEO
 BUILDINGS ORIENTED TO PASEO OR MAIN STREET. ACCESS FROM SIDE OR MAIN STREET. PASEO IS A PEDESTRIAN ONLY STREET



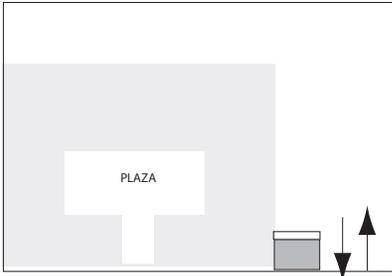
COURT- A
 BUILDINGS ORIENTED TO COURT OR MAIN STREET. ACCESS FROM SIDE OR MAIN STREET. PLAZA FOR PEDESTRIAN USE.



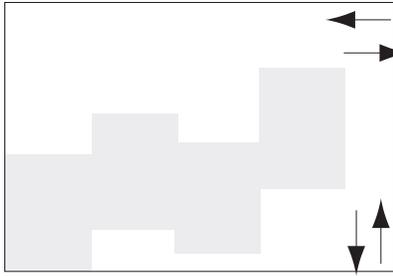
COURT- B
 BUILDINGS ORIENTED TO COURT OR MAIN STREET. ACCESS FROM SIDE OR MAIN STREET. PLAZA FOR PEDESTRIAN USE.



PLAZA -
 BUILDINGS ORIENTED TO PLAZA OR MAIN STREET. ACCESS FROM SIDE OR MAIN STREET. PLAZA FOR PEDESTRIAN USE.

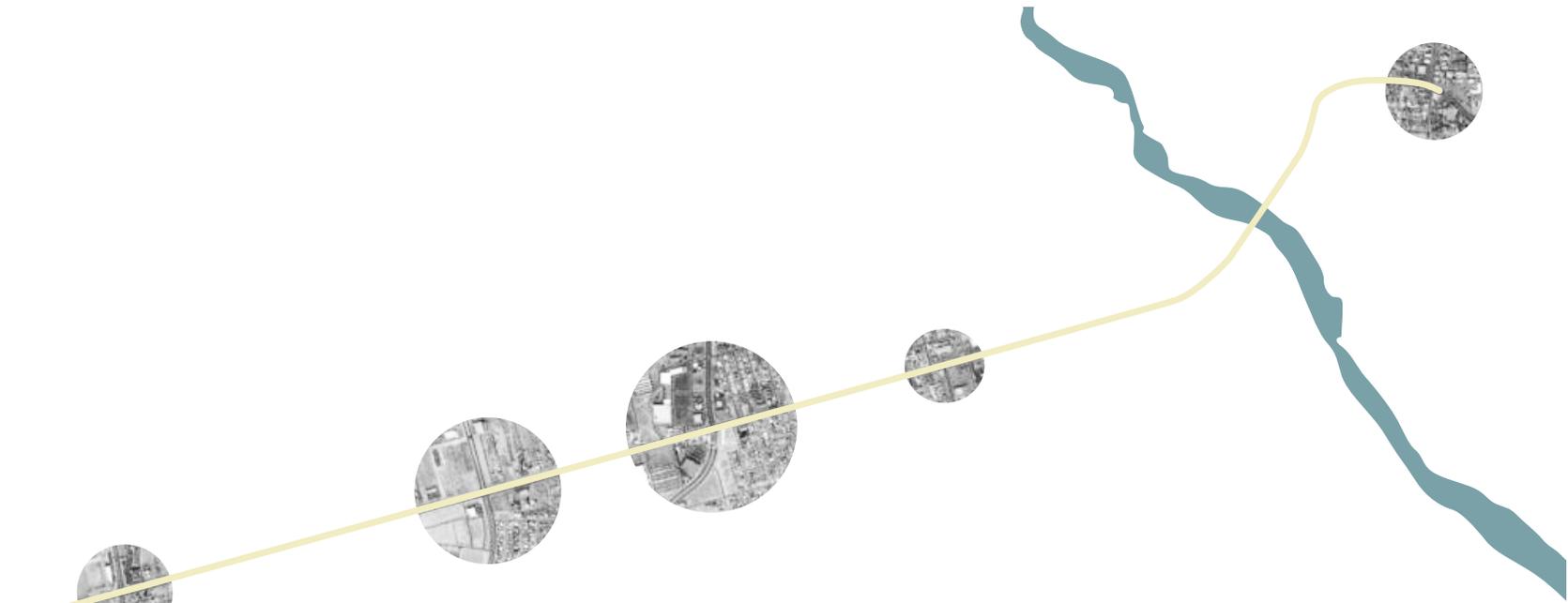


PLAZA -
 BUILDINGS ORIENTED TO PLAZA OR MAIN STREET. ACCESS FROM SIDE OR MAIN STREET. PLAZA FOR PEDESTRIAN USE.



STEPPED
 BUILDINGS STEPPED BACK TO CREATE FRONT PATIOS. ACCESS TO PARKING FROM FRONT OR SIDE.

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Chapter 5 Recommendations

This chapter contains recommendations and strategies for the Plan area that were developed through analysis of existing conditions, the application of relevant city policies and input received at community workshops and agency meetings.

The chapter is divided into the following sections:

1.0 Transportation contains recommendations regarding roadway improvements, transit, bikeways and multi-use trails. These recommendations are relevant to both governmental agencies as well as the individual developer.

2.0 City Facilities and Services contains recommendations for fire, libraries and community centers.

3.0 Drainage contains recommendations for both structural improvements as well as implementation of Low Impact Development practices.

4.0 Utilities contains recommendations for water and sewer, gas and electricity and telephone.

5.0 Parks contains recommendations for future parks.

6.0 Open Space contains recommendations for the Bosque access and enjoyment.

7.0 Trails contains recommendations for open space and MRGCD trails.

1.0 Transportation Overview

Transportation improvements in the West Central corridor are essential to promoting and incentivizing the development efforts that will further community and City goals. The investment in improvements such as new streetscapes, permanent transit infrastructure and enhanced pedestrian and bicycle infrastructure will be a building block for increasing development marketability and encouraging private redevelopment. It is important that these transportation improvements coordinate with the urban design and land use goals for the distinct areas which make up the corridor. This is particularly true in areas which are identified for pedestrian oriented development, where a key building block is the creation of “complete streets” which are identifiable by their pedestrian enhancements and transit accessibility.

The following are recommendations for transportation improvements in the West Route 66 corridor. The recommendations are based on an analysis of specific issues detailed in Chapter 3, Existing Conditions combined with a strategy for transportation improvements to support the Plan’s zoning and land use proposals.

1.1 General Overview, Segment 1: 106th Street to Coors Blvd.

The intent of the Segment 1 transportation recommendations is to preserve the sense of openness and expansive views, and to coordinate streetscape improvements and street design with the land use objectives for the area. In addition, it is important that future development in the area consider improving roadway connectivity in anticipation of increased vehicular, transit and bicycle and pedestrian use.

When travelling east, there are impressive views of the City of Albuquerque in the valley below and of the Jemez, Sandia and Manzano Mountains in the distance. When moving west the traveler gets a sense of the wide open spaces so typical of the West and becomes aware of the rugged high desert landscape that is an integral aspect of the identity of Albuquerque.

Coordinating transportation improvements with land use objectives includes creating a unique and marketable multi-modal identity for the Major Activity Center and creating appropriate multi-modal accessibility along the frontage road and sections of the corridor.

Transportation Recommendations

1.1.1 Segment One Transportation Recommendations

a. **Streetscape improvements:** Plan streetscape improvements along entire length of this segment so that as development occurs, streetscape is coordinated and reflective of distinct development conditions of this segment of Central Ave.

1. Create unique streetscape identity for the Major Activity Center (MAC), with wayfinding elements highlighting arrival into the MAC.
2. Design and implement streetscape improvements for the corridor outside of the MAC, including landscaping that is sensitive to views and the area's natural setting.

3. Increase opportunities for multi-modal connectivity by promoting biking, walking and transit use through physical improvements to the roadway.
4. As development occurs, the City should enforce its Sidewalk and Street Tree Ordinances. Areas where development has occurred and sidewalks were not constructed should be put on a CIP list in order to achieve continuous sidewalks throughout the segment. Sidewalks should be free of obstructions such as utilities, lighting and fire hydrants. Planting strips should be at the curb as required by the Street Tree Ordinance.

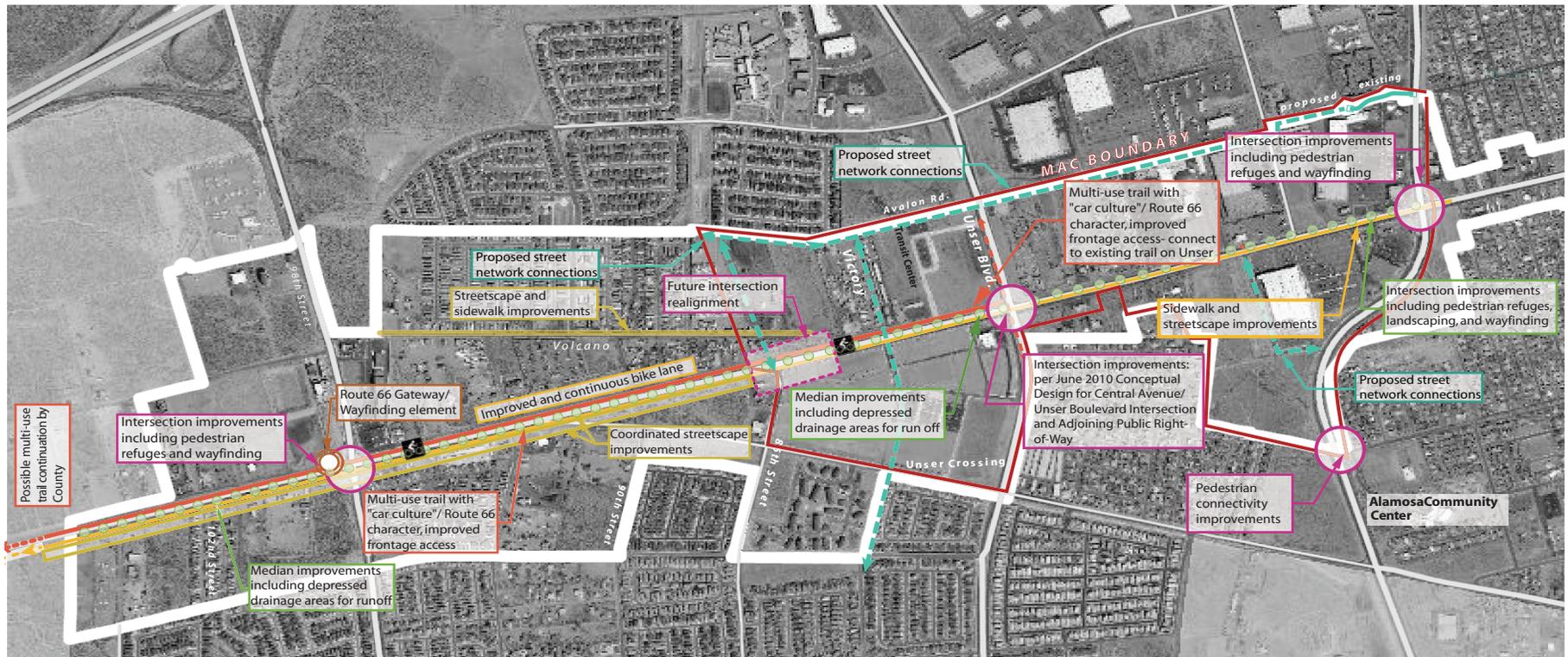
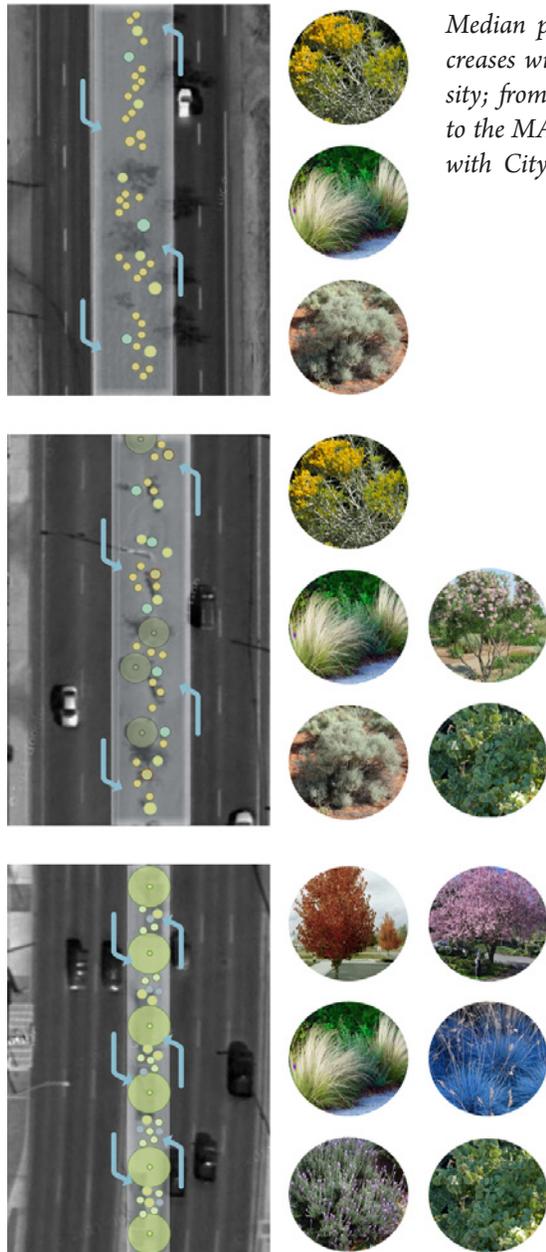


Figure 42: Segment One Transportation Recommendations

General Transportation Recommendations: Segment 1



Median plantings. Intensity of plantings increases with associated development intensity; from more open, less developed areas to the MAC. Design of plantings to comply with City Median Prototypes.

- b. Median improvements:** The unimproved medians located between Victory Rd. and 98th St. should be landscaped with plant materials that are appropriate to the high mesa setting including trees and shrubs lower in height so that views are preserved. Median design should also include depressed drainage areas for storm water runoff.

The medians between Coors and Unser should be designed with plant material that reflects the high mesa setting at an intensity of planting and with other elements like public art, that reflect the more urban nature of the Major Activity Center. Generally, the intensity of planting should increase with intensity of development.

- c. Signal timing:** As part of plans for implementing bus rapid transit (BRT) and due to Central Ave.'s nature as a high capacity corridor, the MRCOG Management Process Matrix identifies traffic signal timing and coordination and traffic signal equipment modernization as high priorities for Central Ave.

Figure 43: Median Plantings

General Transportation Recommendations: Segment 1



Figures 44-46: Concepts for Multi-Use Trail in Different Settings: MAC, night, west of MAC

d. Frontage road improvements: Plan a trail or liner park along the Central Ave. frontage road, which runs from Unser Blvd. to the City limits, that celebrates Route 66 and provides much needed recreational opportunities for the area. This trail should be a priority for the corridor's development as it creates an immediate identity for the corridor and an area wide attraction. The trail should be designed with drainage swales to capture storm-water runoff to mitigate against flooding the area. A priority for the project design should be to maintain good vehicular access to properties along the frontage road.

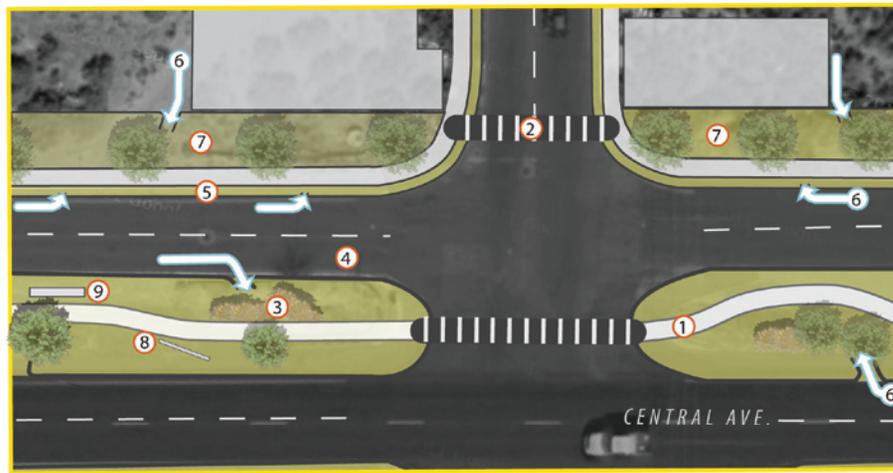
The frontage road right of way varies between of 62-72 feet wide. Within this right of way, plan for the following:

1. a 30 to 40 foot wide area which would include ponding, landscaping, public art and a 10 foot wide trail; two, 11 foot driving lanes; and a 4 foot wide landscape strip and a 6 foot wide sidewalk.
2. The 10 foot wide trail extending from Unser to 98th would initially take the place of missing sidewalks in this segment.
3. Public art celebrating Route 66, such as fun, sculptural elements (e.g.

painted old car parts lit up at night), could be located along the trail in the vegetated area between Central and the frontage road.

4. Design should include depressed drainage areas for storm water runoff.
5. Design should include crossfit stations.
6. Multi use trail would be available to pedestrians as well as recreational bicyclists, with the understanding that all intersections are stops, yielding right of way to turning vehicles. Bicyclists wanting the right of way at intersections would also have designated bike lane available to them along Central Ave.
7. Ensure coordination with the County to ensure that the multi-use trail extends west of 98th St. to connect with Atrisco Vista.
8. Ensure a connection with the north-south multi-use trail on Unser Blvd.

General Transportation Recommendations: Segment 1



- 1. multi-use trail
- 2. clearly striped crosswalks
- 3. landscape drainage swales
- 4. frontage road
- 5. landscaped setback
- 6. curb cuts and/or channeling to direct storm water to landscaped swales
- 7. 5'-15' landscaped setback
- 8. public art
- 9. cross fit station

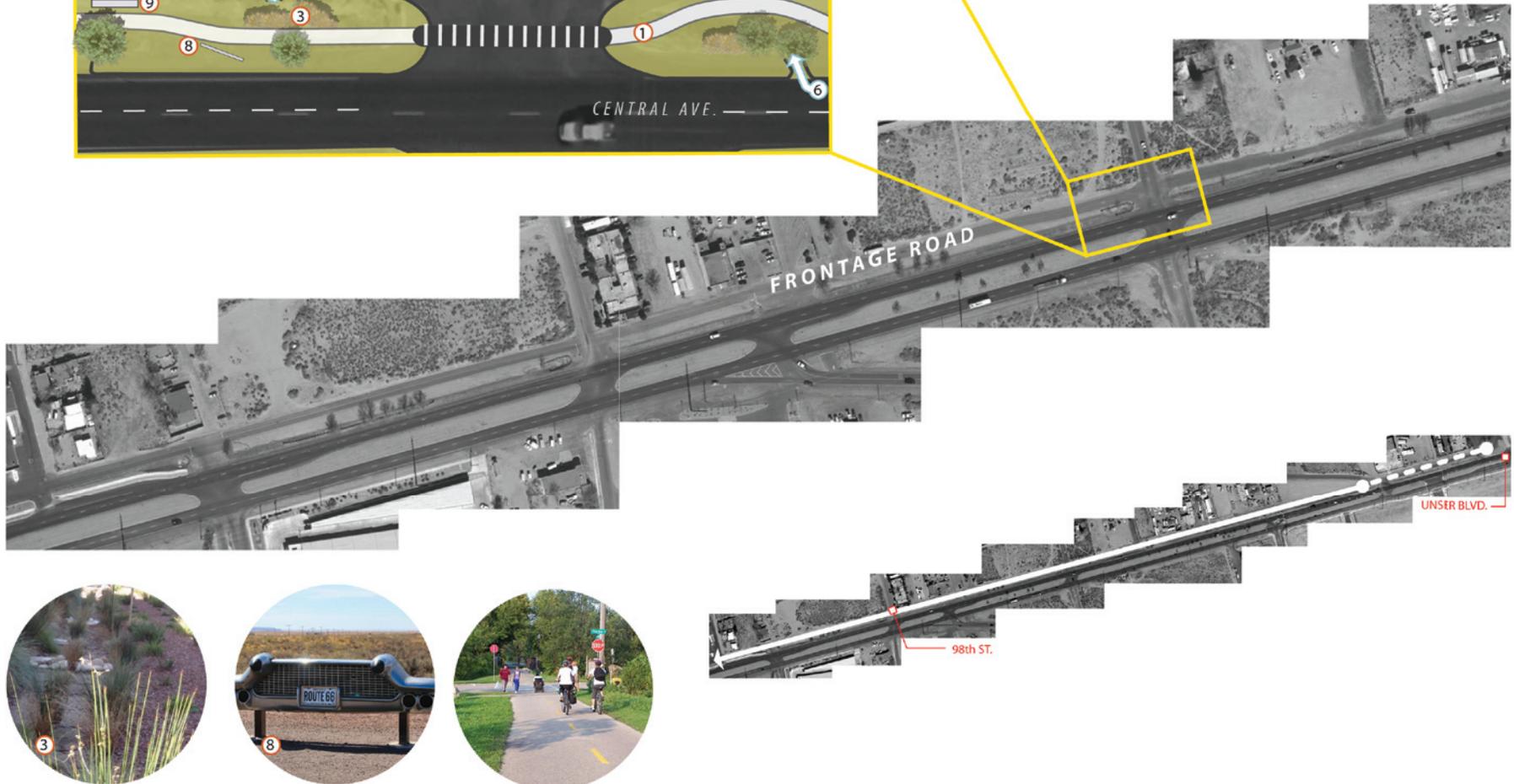


Figure 47: Concepts for Multi-Use Trail on Frontage Road

General Transportation Recommendations: Segment 1



1. continuous bike lanes
2. clearly striped crosswalks
3. enlarged and landscaped pedestrian refuges
4. narrowed turning lanes
5. landscaped buffers
6. continuous trail

Figure 48: Central and 98th Intersection

i. **Intersection improvements at Central Ave and 98th:** This intersection has excess ROW between the drive lanes and in the right turn lanes. This excess ROW could be landscaped with low lying vegetation, sensitive to views to create pedestrian refuges. To improve public safety, landscaping, widened striping and median improvements should be implemented.

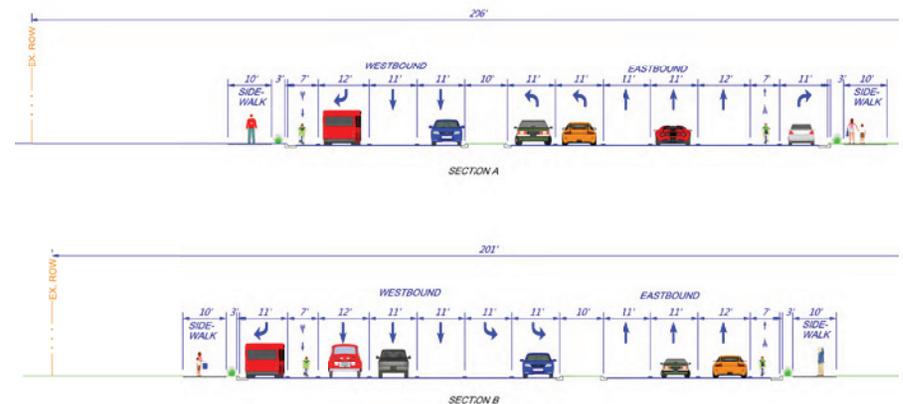


Figure 49: Central and Unser Intersection, Credit: CoA DMD, Gannet Fleming West, Inc.

- j. **Intersection improvements at Central Ave. and Unser Blvd.:** Implement the 2009 Conceptual Design for the Central Avenue/Unser intersection. In addition the following improvements are suggested:
- a. Consider adding landscaping to the striped areas located between the turning lanes and the thru lanes. This landscaping would help mitigate the intensity of the 6-lane Unser roadway and provide some visual interest to the intersection.
 - b. Ensure the project includes a connection between the multi-use trails on the Central frontage road and on Unser Blvd.
 - c. This is one of the primary intersections in the MAC; it should accommodate multi-modal traffic and include wayfinding and identity elements.



1. continuous bike lanes
2. clearly striped crosswalks
3. enlarged and landscaped pedestrian refuges
4. narrowed turning lanes /slip lanes
5. landscaped buffers

Figure 50: Central and Coors Intersection

k. Intersection improvements at Central Ave. and Coors Blvd.: This intersection should be improved with pedestrian striping, median improvements including the creation of clear pedestrian refuges, street trees/streetscape improvements, bike lane improvements and traffic calming design to slow excessive traffic speeds. The free-rights should be redesigned as right-turn slip lanes. As this is another primary intersection in the MAC, improvements should increase pedestrian safety, accommodate multi-modal traffic and include wayfinding and identity elements. A multi-use trail is proposed in the excess ROW on Coors Blvd. south of the Coors and Central intersection in order to facilitate pedestrian and bicycle connectivity between the Central Ave. corridor and the Alamosa Multi-Service Center and surrounding neighborhoods. An intersection master plan should be developed for these proposed improvements.

General Transportation Recommendations: Segment 1



Figure 53: Concept for MAC with Improved Connectivity

General Transportation Recommendations: Segment 2

1.2 General Overview Segment 2: Coors Blvd. to Rio Grande Blvd.

The goals for recommendations in Segment 2 are to build upon the existing finer grained development patterns and narrower street widths and to coordinate transportation improvements with the land use objectives for the segment, including coordinating streetscape and street design to support the more pedestrian oriented development patterns in the Community and Special Activity Centers. A key component of the improvements are enhancements that reflect the identity and landscape of the valley.

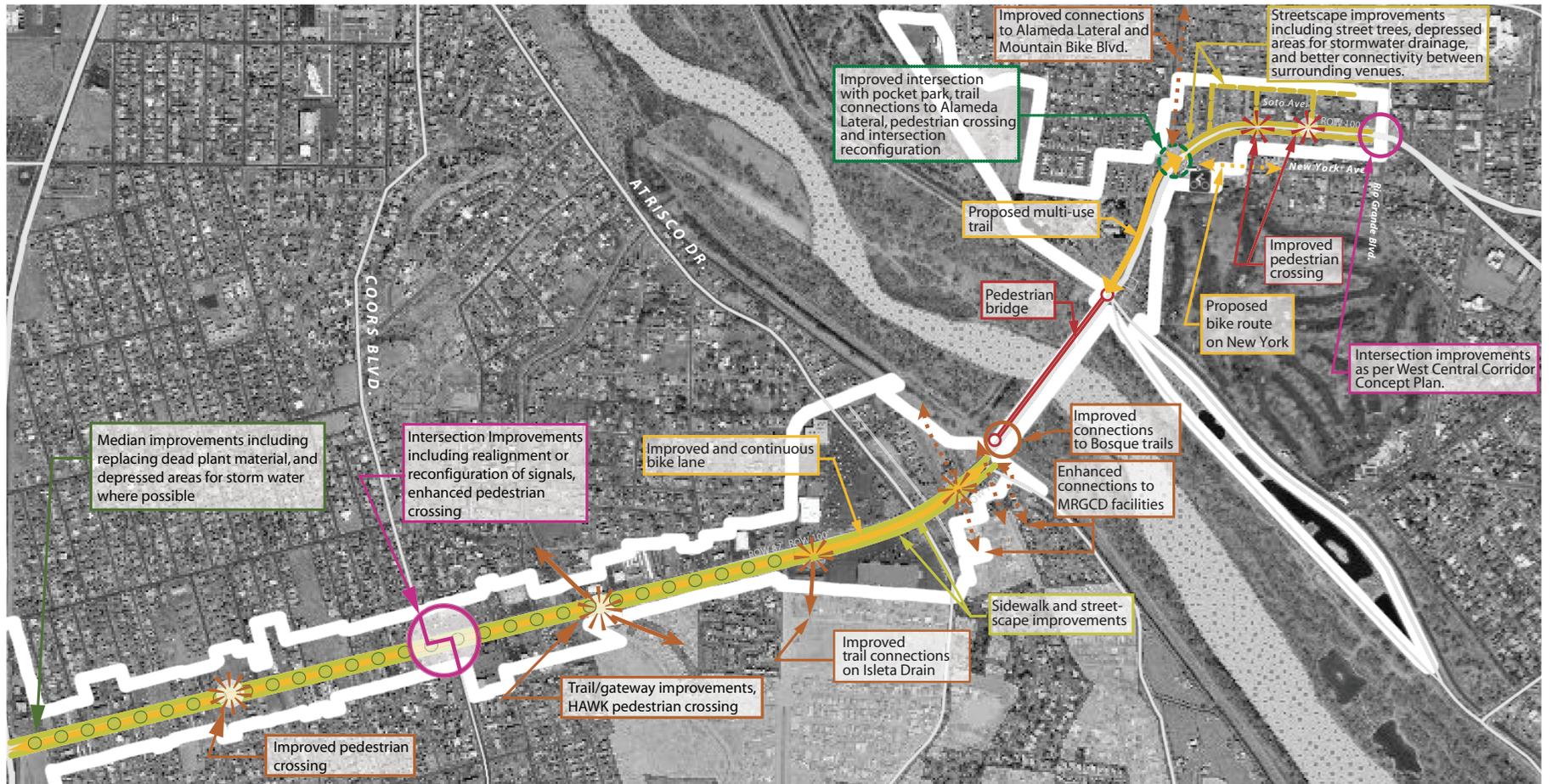


Figure 54: Segment Two Transportation Recommendations

General Transportation Recommendations: Segment 2

1.2.1 Segment 2 Recommendations:

- a. **Median improvements:** The medians from Old Coors Rd. to 58th St. should be improved, where plant materials have died, with new landscaping that is appropriate to the high desert, mid mesa setting and does not obstruct views. Medians should be retrofitted to include depressed drainage areas for storm water runoff.
- b. **General Intersection Improvements from Coors Blvd. to Rio Grande Blvd.:** The MRCOG Congestion Management Process Matrix identifies traffic signal timing and coordination and traffic signal equipment modernization as a high priority for Central Ave.
- c. **General Streetscape improvements:**
 1. Design and implement streetscape improvement plan that utilizes excess ROW from Coors to Arenal Canal. Landscape materials should provide a transition from the high desert to the valley setting.
 2. Design and implement streetscape improvement plan from Arenal Canal to Central Bridge. Landscape materials should reflect the valley setting. Streetscape improvements should be become more concentrated as they approach the CAC.
 3. Design and implement streetscape improvement plan from Central Bridge to Rio Grande Blvd. Landscape materials should reflect valley setting. Streetscape improvements should focus on promoting pedestrian activity.
 4. Follow 2010 West Central Corridor Concept Plan: multi use trail, bike lanes on New York with improved intersection at New York.
 5. Install pedestrian lighting along Central Ave. from Old Coors to the river and from New York to Rio Grande Blvd.
 6. Implement streetscape/street tree improvements for the entire length of segment. Streetscape improvement should reflect the land use and landscape transition from west to east; from a more naturalistic treatment of medians and landscaping that transition into denser plantings with street trees, wider sidewalks and other amenities intended to provide a comfortable environment for the pedestrian.
 7. Develop plan for consolidating driveways. Restrict new curb cuts unless consolidating multiple curb cuts.
 8. Implement wayfinding and identity elements to connect Old Town, the BioPark, Tingley Beach, the Bosque and Atrisco Plaza areas.
- d. **Intersection improvements at Old Coors and Yucca:** Facilitate vehicular and other traffic movements through this off-set intersection, including the option of a pedestrian crossing zone that extends from Old Coors to Yucca Dr. This might take the form of a large striped area with landscaped median.
- e. **Arenal Canal trail access/bus stop:** Install a HAWK (high intensity activated crosswalk) pedestrian activated signal as an opportunity to connect the informal trail to surrounding residential neighborhoods.
- f. **Atrisco Drive/intersection:** Plan streetscape improvements that facilitate pedestrian use and recognize the street's location and function in the activity center. Ensure improvements utilize opportunities for place making and way-finding such that the unique history and identity of the area is recognized and celebrated. The forced right should be redesigned to slow turning traffic.
- g. **Atrisco Lateral/Drain/Ditch:** The Atrisco Ditch provides a unique opportunity to connect Central Ave. pedestrians to the agricultural traditions of the Rio Grande valley. On the north side of Central Ave., the Atrisco Lateral runs through the site owned by the City's Metropolitan Redevelopment Agency and could be incorporated into the development program of the site. The Lateral goes underground under Central Ave. and resurfaces just south of Central Ave and west of Sunset Blvd. At this location it is recommended that a gateway to the south valley be developed. There is an unimproved trail along the Ditch that could be developed into a multi-use trail that would connect the plan area with Albuquerque's south valley, subject to MRGCD permitting.
- h. **Central Ave. Bridge Crossing:** Provide more opportunity for place making/identity/public art that links Route 66 and cultural/outdoor destinations and draws pedestrians over the bridge. Provide a pedestrian bridge parallel to the vehicular bridge. See Multi-Use Trail Recommendations for more information.
- i. **Multi-Use Trail at the BioPark.** Plan street improvements that encourage pedestrian connectivity between Old Town and BioPark including a multi-use trail along Central's north side at the BioPark. Ensure trail connectivity to Mountain Rd. Bike Boulevard via Panmunjon/Alameda Lateral.

General Transportation Recommendations: Segment 2

j. **Central Ave. from New York Ave. to Rio Grande Blvd.:** This section of roadway acts as a de facto gateway to some of Albuquerque's premier destinations with the BioPark, Bosque, Tingley Beach and Old Town attracting locals and visitors alike to the area. However, the run-down appearance of the area and lack of streetscape improvements make this section appear unsafe and unpleasant. Improvements to this section are critical to the redevelopment of this area as well as to the creation of a gateway from the east and north which attracts visitors not only to City destinations but across the River as well.

1. Plan streetscape improvements that create wider sidewalks with landscaping at the curb where possible for pedestrian buffering from fast moving, high volume traffic. Pedestrian improvements should also include private setback improvements that create a wider pedestrian realm and reduce lane widths through this section.
2. Specific strategies for streetscape improvements should include: reducing lane widths to gain more width for sidewalks. Currently, the street section is 85.5 feet curb to curb. This width could allow the following street section which keeps the the same number of lanes, but allows for 4 additional feet of sidewalk/planting areas at the curb. A gain of 4 feet allows for planting pits for smaller grasses and shrubbery at the street edge, softening the edge and creating a more pleasant walking environment.

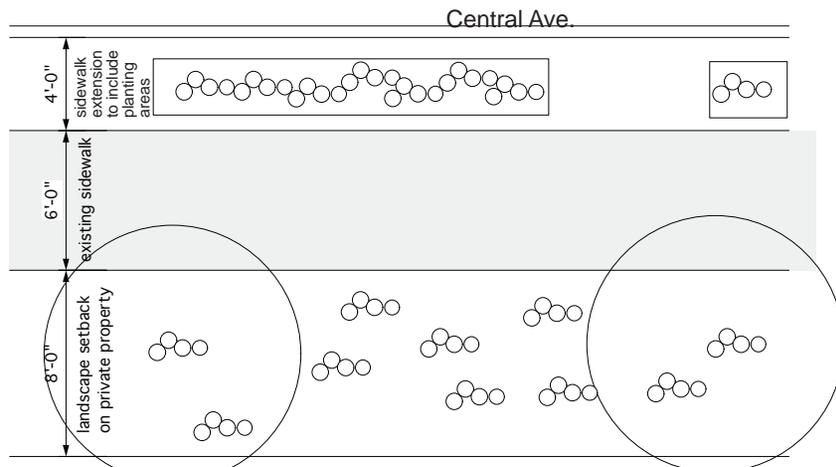


Figure 55: Sidewalk Expansion Concept

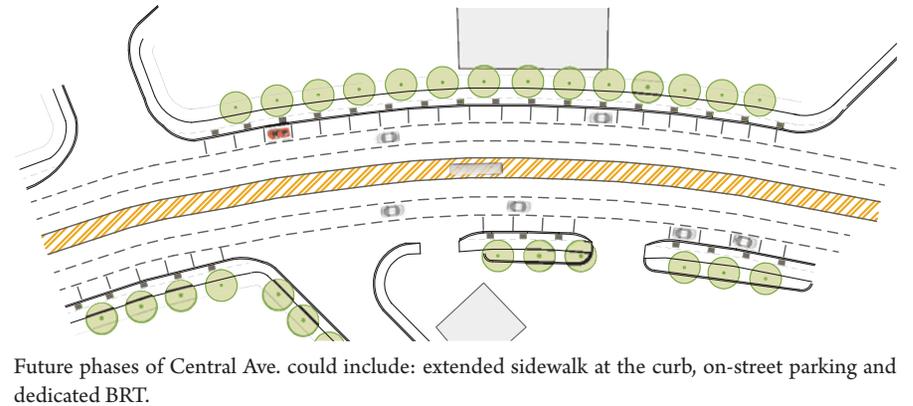
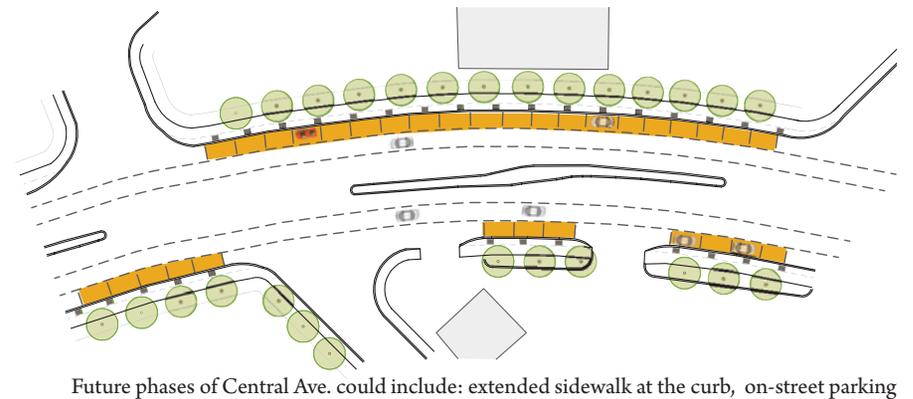
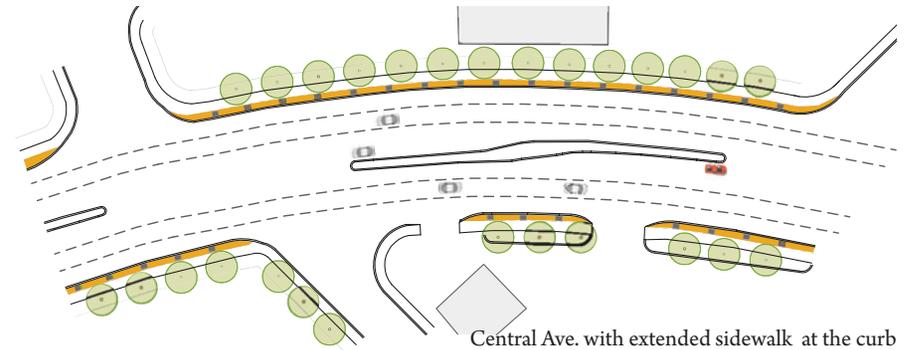


Figure 56: Concepts for Street Section Improvements

General Transportation Recommendations: Segment 2

3. Ensure a street canopy in the landscape setback area off the sidewalk. This setback ranges from 5-8 feet on the north side and 0-8 feet on the south-side of Central. By requiring street tree planting that is coordinated and continuous, a tree canopy can be developed that provides for sidewalk shading and generally improves streetscape aesthetics.
- j. R.O.W. Acquisition:** Develop a plan to acquire additional rights-of-way between New York Ave. and Rio Grande Blvd in key areas. This plan should detail existing rights of way and locations for additional right of acquisition. Pursue right of way acquisition in strategic locations such as at bus stops in order to provide space for transit while still leaving space for pedestrian movement.
- k. New York intersection:**
1. Plan intersection improvements that facilitate pedestrian use and vehicular turning into the BioPark. Improvements should include pedestrian safety refuges.
 2. Reconfigure south side of intersection in coordination with City owned MRA site redevelopment.
- l. Rio Grande Blvd. intersection:** Enhance the intersection's function as a transition/gateway between Old Town, Downtown, cultural facilities and the Bosque by implementing pedestrian safety and streetscape improvements. The pedestrian signal timing for the crosswalks across Central Ave and across Rio Grande Blvd. needs to be increased. The cross walks should be striped to clearly distinguish them as a pedestrian realm. This intersection needs signage that alerts drivers of the increase in pedestrians in the area and that notifies them that pedestrians have the right of way in crosswalks.
- m. Clayton Street intersection:** Create a signalized intersection at Central Ave. and Clayton St. This will foster pedestrian activity in this area by providing a crossing between New York Ave. and Rio Grande Boulevard.
- n. Pedestrian connectivity:** Per the 2010 West Central Corridor Concept Plan, plan a multi-use trail between the River and New York Ave.
- o. Implement the the West Central Corridor Concept Plan (2010):** (See Chapter 4, Transportation Conditions)
1. **Reduce curb cuts.** There is an abundance of curb cuts along Central Avenue between 47th St. and Rio Grande Blvd; driveways should be narrowed and curb cuts consolidated where possible in order to create a more comfortable pedestrian environment. This recommendation is in line with the Albuquerque/ Bernalillo County Comprehensive Plan policy recommendation of a spacing of one to two drives per 300 feet of frontage on principal arterials.
 2. **Pedestrian improvements for the Central Avenue/Rio Grande Blvd. intersection.** Provide a two-to-five second leading pedestrian signal in order to give pedestrians a head start when crossing Central Avenue; extend the median on the west side of the intersection in order to provide pedestrians a small refuge; eliminate the westbound turn lane in order to provide a larger median refuge and landscaping; and restripe the crosswalks.
- p. Improve area drainage, connectivity and access to local services.** Soto Ave. from Simmonds St. to Rio Grande Blvd.: The alley that runs behind the north of Central Ave. between New York Ave. and Rio Grande Blvd. currently has issues with stormwater drainage. This alley could be improved with landscaping and LID techniques, in order to solve the drainage issues, and used as an alternative pedestrian and bike route that connects Old Town with the BioPark.

General Transportation Recommendations: Segment 2



Figure 57: SAC Network Improvements

1.3 Transit Overview

Public transit plays a key role in the transportation system of the West Route 66 plan area. West Central Avenue has been identified by the Mid-Region Metropolitan Planning Organization 2035 Metropolitan Transportation Plan (MTP) as a Priority Transit Improvement Corridor. This designation identifies the West Central corridor as “well suited for further evaluation and development of potential high frequency and high volume transit service over the coming decades”. Portions of the Corridor are further designated as either an “Enhanced Transit Corridor” or “Major Transit Corridor” by the Albuquerque/Bernalillo County Comprehensive Plan. The segment of Central Avenue within the plan area is also of particular regional interest because of the Rio Grande bridge crossing which will be facing increased congestion based on projected growth trends. To reduce projected vehicular crossings, the 2035 MTP has identified a mode share goal of 10% of all river crossing trips to be taken by transit by 2025 and 20% of all trips by 2035.

The following transit recommendations were developed through an analysis of existing conditions, adherence to City and regional policy, and input received at community visioning sessions. For more information on existing conditions, see the Transit Context in Chapter 3.

The intent of the recommendations for Transit is to meet the stated community goal: *“Improve transit by increasing frequency and efficiency of service,”* and to improve the perception of transit by investing in permanent transit infrastructure that not only improves the functionality of the transit system, but also signals an investment by the City of Albuquerque in the development of this area as a recognized transit corridor punctuated by vibrant pedestrian - and transit-friendly activity centers.

The following transit recommendations were developed through an analysis of existing conditions, adherence to City and regional policy, and input received at community visioning sessions. For more information on existing conditions, see the Transit Context in Chapter 4.

1.3.1 Recommendations

- a. Plan for a Park and Ride facility in segment 2a of the plan area. An ideal location for a park and ride is one that is easily accessible by automobile and con-

veniently on the way for morning commuters. Specifically, sites near major intersections and on the “in-bound” side of the transit route are preferable. Investigate potential incentives to private landowners in exchange for allowing park and ride use of a portion of their parking.

- b. Improve bus stops at locations that are lacking seating, shade, and ADA accessibility .
- c. Consider the future integration of Bus Rapid Transit (BRT) service with such features as dedicated lanes, raised platform stations, signal prioritization, queue jumpers, and off-board fare collection. (At time of adoption of this plan, ABQ Ride is engaged in the federally-required Alternatives Analysis for such a system.) Consider locating dedicated BRT in the median ROW in the section from the River to Rio Grande Blvd. to preserve opportunities for pedestrian streetscape improvements and traffic calming features in outside lanes. This Sector Plan further supports preliminary concepts which would locate dedicated BRT lanes through the medians for the entire length of the Corridor, as this would preserve the opportunity to implement much needed pedestrian and streetscape improvements along the street edge.
- d. Coordinate structural improvements, on- and off-road, to ensure a balance between the needs of vehicles, transit, cyclists, pedestrians, including persons with sensory or mobility impairments.
- e. Ensure coordination between the Planning Department, ABQ Ride and MRCOG/Rio Metro regarding the creation of a transit system that integrates transit needs with land use and development goals and pedestrian streetscape improvements.

Transportation Recommendations: Bikeways and Multi-Use Trails

1.4 Bikeways and Multi-Use Trails

Good bicycle infrastructure and networks are an important part of creating a truly multi-modal environment. In order to encourage more trips to be by bicycle in the Plan area, it is necessary to ensure that not only is there good bike lane connectivity along Central Ave. and the intersecting north/south streets but also good connectivity regarding access to and within activity centers and popular destinations located along the corridor. There are three types of bicycle facilities within the Plan area: bike lanes maintained by DMD, multi-use trails maintained by the Parks Department, and multi-use trails maintained by the Open Space Division.

Recommendations for bike lanes and multi-use trails maintained by the Parks Department are outlined below. For information on the existing conditions these recommendations were based on, see Bikeways and Trails Context in Chapter 3. Please see the Parks, Open Space and Trails Recommendations for information on Open Space and MRGCD trails.

The intent of the recommendations for Bikeways and Trails is to create a safe and well connected bike and trail network that helps to make Central Ave. a truly multi-modal corridor that is safe for bicyclists, and to ensure trail and bikeway connectivity from Central Ave. to and within adjacent development and activity centers.

1.4.1 Recommendations:

- a. **Multi-Use trail along frontage road.** Plan a multi-use trail along the frontage road on the north side of Central Ave. from 106th St. to Unser Blvd.
- b. **County Coordination.** Coordinate with the County to continue the Old Route 66 Multi-Use Trail to Atrisco Vista. It is essential that this trail system be continued as land develops in the county to ensure connectivity with Atrisco Vista. Besides providing needed recreational opportunities for the area, this multi-use trail is a key component of establishing a unique identity for West Central Ave.
- c. **Bike lane completion:**
 1. Bike lane sections missing on Central Ave. along the frontage road should be striped.
 2. Complete missing sections of bike lanes on the south side of Central Ave.

from 98th St. to 86th St. Bring bike lanes up to the DPM standard of 7 feet for principal arterials with posted speeds of 40 mph or greater and complete multi-use trail on Unser Blvd. north of Central Ave.

- c. **Improvements to bike lane along New York Ave. from New York to Rio Grande:** Relocate the Central Ave. bike lane along New York, as the preferred bike route, due to issues of safety and limited lane options on Central in its current configuration.
- d. **Pedestrian Bridge:** Plan a multi-use trail that runs on a pedestrian bridge spanning the river at Central. A bridge at this location would have a tremendous effect on improving pedestrian and bicycle access across the river, act as another major amenity for the area, and draw people across the bridge to the activity center, on both sides of the river.

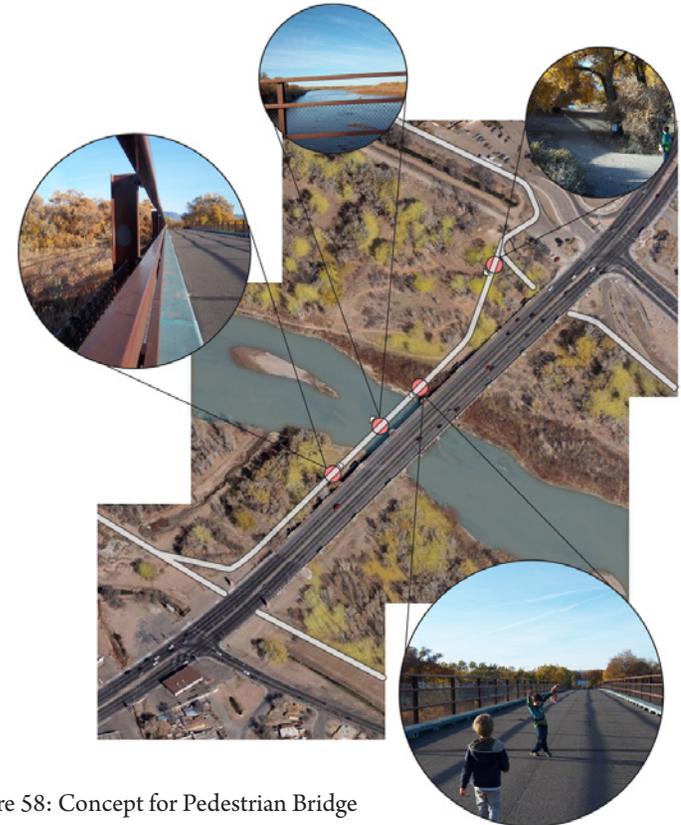


Figure 58: Concept for Pedestrian Bridge

Recommendations: City Facilities and Services

2.0 City Facilities and Services

City Services include Fire Stations, Libraries and Community Centers. The sections below contain recommendations for these services. These recommendations were developed through an analysis of existing conditions and input from community visioning sessions. For detailed information on the existing conditions that these recommendations were based on, see Chapter 5, Context: City Services.

2.1 Fire Station Recommendations

A new fire station will be built at Central Ave. and 57th St. Based on input received at community visioning services, the community feels that they are well serviced by fire protection services and there are no recommendations.

2.2 Library Recommendations

- a. Investigate the potential for a new library to serve the eastern portion of the plan area.
- b. When new facilities are built and/or older facilities are updated, ensure that these facilities include pedestrian and bicycle connectivity both within the site and to the Central Ave. corridor and surrounding residential neighborhoods.

2.3 Community Center Recommendations

Most of the plan area is well served by community centers. However the western portion of the plan area is currently underserved. While there are no community centers within the plan area, there are access issues from the Plan area to adjacent centers.

- a. Investigate the potential for a new community center in the western portion of the plan area. West of Coors there is no community center to serve the large residential population north and south of the Central Ave. area. A future joint use community center and park should be considered to serve this area.
- b. Improve pedestrian and bicycle connections at the Alamosa Community Center specifically across Coors Blvd. to serve the residential neighborhoods on the west side of Coors but also to and from Central Ave.
- c. Ensure future facilities include multi-modal access from the Central Corridor and across major thoroughfares like Coors and Unser, as well as to and from surrounding neighborhoods and adjacent services.

Recommendations: Drainage

3.0 Drainage Overview

Drainage issues vary by segment. In Segment One, there are drainage constraints related to limited capacity in the storm drain between Unser and Coors Blvds., which requires all new development to pond on site. In Segment Two, in the area between New York and Rio Grande Blvd. there are significant drainage problems related to flooding and inadequate infrastructure. Drainage recommendations for the Plan area assume, that individual development must comply with City and County Ordinances and that each development must have a City approved plan for the drainage of property and not exceed downstream capacity.

A key element of many of the following recommendations is the implementation of Low Impact Development (LID) techniques. LID seeks to mimic pre-development hydrology in order to protect waterways, habitat and ground water recharge. Conventional planning focuses on access, circulation and parking. New designs for streets can maintain high function while minimizing impervious surfaces and promoting open space and landscaping.

3.1 Drainage Recommendations:

- a. **Ponding areas along frontage road.** The City should employ stormwater capture and treatment techniques through landscape and swale improvements to the frontage road area. Plan for ponding areas in vegetated areas along new multi use trail in order to help alleviate the significant drainage problems on the road.
- b. **Ponding areas as landscape and streetscape amenities.** As new development occurs, in front and side street setbacks encourage ponding areas that coordinate with required front setback landscaping. The intent is that these ponding areas support the irrigation needs of landscape and that ponding areas become landscape amenities, not forgotten eyesores.
- c. **Private Development and Low Impact Development (LID) practices.** In anticipation of potential policy changes to City stormwater management policy which may mandate “first flush” capture and on site stormwater water treatment, all new development and redevelopment should employ LID practices.
- d. **Ponding in medians.** As new medians are installed, they should be depressed

from the roadway in order to help capture and treat stormwater run-off.

- e. **Ponding in bulbouts.** The City should investigate stormwater capture and treatment in proposed pedestrian bulb out locations.
- f. **Reuse of drainage ponds to a higher use as development necessitates.** There are two drainage ponds/jurisdictional dams that are owned by the City and managed by the Office of the State Engineer located at the northwest corner of the Central Avenue and 98th St. intersection. These ponds are mostly handling stormwater drainage from undeveloped properties in the area. These ponds are located on land that is more ideally suited for commercial and/or higher density commercial development, as the rest of the intersection has already begun to develop in this way. As more development occurs in this area, property owners will be required to manage more stormwater on site and alleviate the need for this pond. The future objective for a higher land use on this site should be kept in mind when approving site drainage plans and planning for stormwater drainage in this area.
- g. **Require LID techniques in areas to offset limited storm drain capacity in the Segment One.** In this section of the corridor, any redevelopment should manage excess stormwater through increased landscaping and Low Impact Development (LID) techniques.
- h. **Require LID techniques in areas of existing flooding.** There are significant issues with flooding in the area between New York Ave. and Rio Grande Boulevard. This is largely due to the flat topography and the abundance of impervious surfaces. In this section of the corridor, any redevelopment should manage excess stormwater through increased landscaping and Low Impact Development (LID) techniques.
- i. **Draft Mid Valley Drainage Study.** The following general principles and recommendations were made by the Draft 2012 Mid Valley Drainage Study. The section of Central Ave. from the River to Rio Grande Blvd. is included in the study as part of the Alcalde Drainage Basin. The recommendations are included because they are relevant to all areas within the Plan appropriate for LID treatments.
General Principles:
 1. The objective is to capture and treat pollutants in the “first flush” stormwater event.

2. Structural stormwater quality control is best implemented at or near the source of run-off.
3. Retention or long term detention of runoff from storms larger than the 90% equivalent rainfall (0.44 in.) may have negative downstream water rights implications.
4. As in all drainage and flood control facilities there is a tradeoff between land area consumed by the facility and its capital cost.
5. Design should be “first flush” friendly with a bypass for larger flows-otherwise “first flush” gets flushed.
6. Every reasonable opportunity to install LID techniques should be explored and exploited (almost no area is too small so long as the cost to implement and maintain is commensurate with treatment effectiveness and value received)
7. Maintenance cost and enforcement are important in planning and design considerations
8. Depending on the anticipated use of harvested water (e.g. shrubs vs. vegetable garden) water harvesting systems may need to be design and operated to bypass the “first flush” rather than capture it due to high concentrations of nutrients or pollutants-thus reducing the stormwater treatment quality.
9. Designs that are self-enforcing are the best (the owner is the first to feel/see effects when maintenance is required)
10. Public education is essential to controlling the floatables (trash) and pet related biological loads to the streets and storm drainage systems.

Micro opportunities for detention

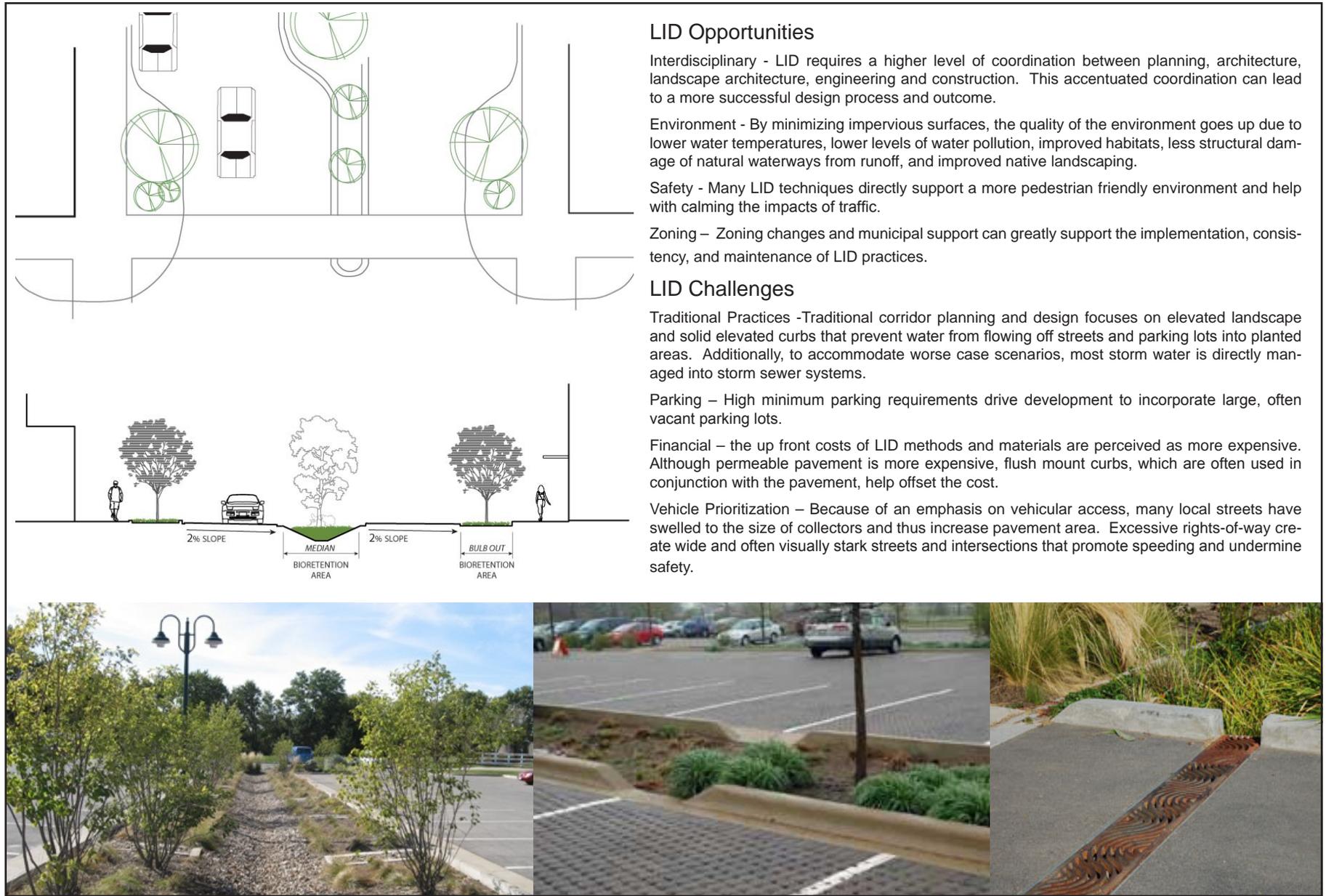
- Tree wells
- Medians
- Parking lot islands
- Pocket parks
- Backyard/front yard ponds
- Parkway between curb and sidewalk
- Area behind sidewalk
- Unused or rarely used areas of parking lots

Local opportunities for detention

- Small parks
- On-site drainage ponds retrofit for “first flush” and floatables treatment
- Re-graded parking lot landscape areas (use speed bumps/dips as diversions)
- Channel tributary entrances
- Subdivision scale detention basins
- Subdivision entry features

As any public facilities (including public buildings, parking lots, streets, medians, parks, etc.) are remodeled, expanded or refurbished or their use changes, each should be investigated for opportunities to capture and treat “first flush” stormwater for as much of the site as makes practical sense.

Recommendations: Drainage



LID Opportunities

Interdisciplinary - LID requires a higher level of coordination between planning, architecture, landscape architecture, engineering and construction. This accentuated coordination can lead to a more successful design process and outcome.

Environment - By minimizing impervious surfaces, the quality of the environment goes up due to lower water temperatures, lower levels of water pollution, improved habitats, less structural damage of natural waterways from runoff, and improved native landscaping.

Safety - Many LID techniques directly support a more pedestrian friendly environment and help with calming the impacts of traffic.

Zoning - Zoning changes and municipal support can greatly support the implementation, consistency, and maintenance of LID practices.

LID Challenges

Traditional Practices - Traditional corridor planning and design focuses on elevated landscape and solid elevated curbs that prevent water from flowing off streets and parking lots into planted areas. Additionally, to accommodate worse case scenarios, most storm water is directly managed into storm sewer systems.

Parking - High minimum parking requirements drive development to incorporate large, often vacant parking lots.

Financial - the up front costs of LID methods and materials are perceived as more expensive. Although permeable pavement is more expensive, flush mount curbs, which are often used in conjunction with the pavement, help offset the cost.

Vehicle Prioritization - Because of an emphasis on vehicular access, many local streets have swelled to the size of collectors and thus increase pavement area. Excessive rights-of-way create wide and often visually stark streets and intersections that promote speeding and undermine safety.

Figure 59: LID Opportunities and Challenges

4.0 Utilities

The following recommendations for utilities are based on an analysis of existing conditions, meetings with relevant City and governmental organizations and input received at community visioning sessions. For detailed explanation of existing conditions see Utilities Context in Chapter 3.

4.1 Water and Wastewater Recommendation

Ensure coordination between the City and the Albuquerque/Bernalillo County Water Utility Authority in order to monitor and permanently resolve any issues of odor control in the Central Ave. and Yucca Dr.

4.2 Gas Recommendation

The aesthetics of the New Mexico Gas Company border station at Central Ave. and Atrisco Dr. should be improved either through landscape screening, or removal and replacement of chainlink visible from public rights-of-way with an alternative fencing material.

4.3 Electricity and Telephone Recommendations

- a. From Cypress Dr. to Atrisco Dr. on the south side of Central Ave., and from New York Ave. to Rio Grande Blvd. on both sides of Central, utility poles are located in the sidewalk, intermittently blocking the sidewalks which creates an unsafe conditions and makes sidewalks inaccessible to wheelchairs. As new streetscape improvements are implemented utility poles should be moved out of sidewalk locations or the pedestrian walking zone should be expanded at the conflict locations.
- b. Alleviate typical voltage dips. In the section between New York and Rio Grande Blvd. there have been reports of power outages due to inadequate power transmission. Currently there are voltage dips from 282 to 215 volts.

Recommendations: Parks

5.0 Parks

The following recommendations for parks are based on an analysis of existing conditions, meetings with relevant City and governmental organizations and input received at community visioning sessions. For detailed explanation of existing conditions see Parks, Trails and Open Space Context in Chapter 5.

The current level of service standard used by the City of Albuquerque Parks Department is 2.6 acres per 1,000 population. Based on this level of service, the plan area is adequately served by parks under existing development conditions. However based on input received at community visioning sessions and the increases to residential development on the corridor it is likely that there will be an increased need for parks in the future.

5.1 Parks Recommendations

- a. While the City level of service standard for parks is currently being met, the Sector Plan is proposing land use changes that will increase density and intensity of development. These land use changes will increase the need and demand for park space in the area. In anticipation of this development, the City should plan for additional park space in the area with a preference for sites that offer multi-modal connectivity opportunities to nearby commercial services as well as residential development.
- b. Consider a joint use site with Parks and Family & Community Services for the 2.2 acre city owned vacant parcel at 90th St. and Volcano Rd.
- c. Where the Alameda Lateral intersects Central, create a pocket park with sculptural elements and an educational interpretive exhibit that explains the history and continued use of acequias for agricultural irrigation, and provides way-finding to area amenities. This intersection is across from the El Vado Metropolitan Redevelopment site and has the potential to become an important pedestrian amenity on the corridor.
- d. Encourage on-street parking, shared parking and parking reductions for parks in exchange for proximity to residential development and transit in order to reduce the acreage requirements for parks.
- e. An urban park model that offers more opportunities for passive recreation and less programming for active uses may be more appropriate for park development on the corridor.



The intersection of the Alameda lateral and Central Avenue is a cultural and historical fulcrum between the natural experience of the acequia and the built heritage of Route 66 and Central. Although small in scale, this pocket park can be a destination. The intention is to use dry sculptural materials (wood, metal, concrete...) to interpret and create a space for pedestrians to explore.

Figure 60: Concept for Pocket Park

6.0 Open Space

The Rio Grande State Park is the only Open Space adjoining the plan area. It is located in the Bosque on both banks of the Rio Grande and is managed by the City in collaboration with MRGCD and the Army Corps of Engineers. The Bosque is an important recreational and environmental resource which should be maintained to preserve the highest and best use for ecological and recreational purposes.

6.1 Open Space Recommendations

- a. Improve the Bosque access and parking on the west side of the river. Ensure that improvements enhance the aesthetics and safety of the parking area to encourage increased visitor use. By increasing visitor use, the additional “eyes on the Bosque” will provide greater safety and discourage illegal uses.
- b. Create a new parking area with ADA trail access on the west side of the river.
- c. Plan for a pedestrian bridge across the river. A pedestrian bridge is an amenity that will bring visitors and locals alike across the river and engage additional opportunities for recreational use. A well-conceived bridge could become a destination in and of itself.
- d. Ensure coordination between City of Albuquerque Open Space, the Middle Rio Grande Conservancy District and the Army Corps of Engineers regarding Bosque maintenance, trails, access and parking lots.

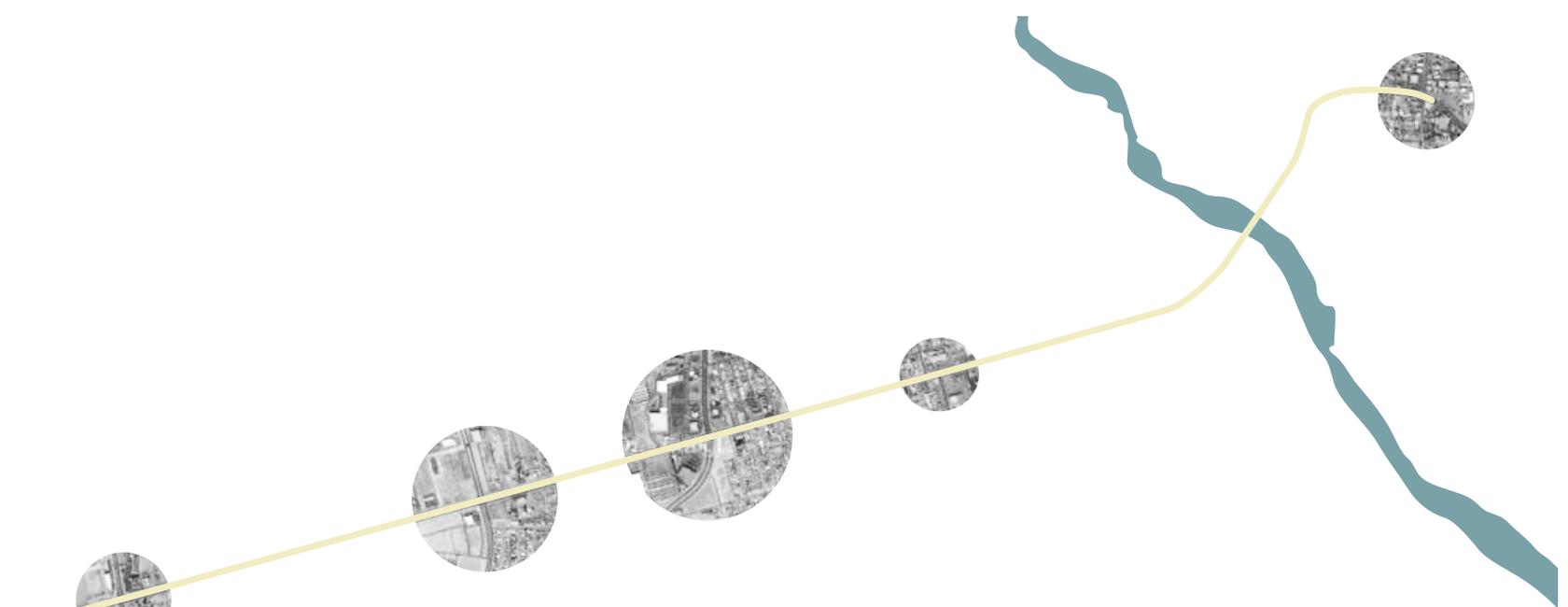
7.0 Trails

Central Ave. bisects numerous drains and ditches in the areas east and west of the river. This interface between a major urban thoroughfare and the traditional system of acequias provides a unique opportunity to enhance awareness and appreciation of the acequia system for visitors to the area. In addition, celebrating this interface highlights the identity for this section of the corridor. The informal pathways along the ditches, canals and laterals also offer unique opportunities to connect area neighborhoods to Central Ave. Formalizing these pathways would require agreements between the City of Albuquerque and the Middle Rio Grande Conservancy District. Where appropriate, this action has been recommended below. Other recommendations involve improving access to these pathways from the corridor.

7.1 Trails Recommendations

- a. Create a formal connection on Central Avenue at Arenal Canal with a HAWK signal (see Transportation Context for description) to facilitate pedestrian crossing.
- b. Enhance and extend the trail along the Atrisco Ditch. The trail could function as a pedestrian and bicycle gateway to the South Valley from the Central Ave. corridor. The Atrisco Lateral bisects the MR property on the north side of Central Ave. and Sunset Rd. This is a unique opportunity to incorporate a water feature, and potentially an agricultural theme, into redevelopment plans for the site.
- c. Any redevelopment plans for the newly created River Activity Zone on the west side of the river should include an ADA accessible parking lot with access to the ADA trails in the Bosque.
- d. Remove the existing chain-link fence where the Alameda Lateral meets Central Ave. or replace with a more attractive design.

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Chapter 6 Projects

Projects

This section identifies key projects which if implemented would significantly advance the development of the area as envisioned by this Plan. The project recommendations range from those that create the necessary impetus for redevelopment and new development investment to those that improve the overall quality of life in the area by enhancing existing amenities.

Many of the project recommendations may be realized through the City's Capital Improvement Projects (CIP) program. The CIP program administers approved capital expenditures for acquiring, constructing, replacing, upgrading and rehabilitating Albuquerque's built environment or infrastructure, such as roads, drainage systems, water and wastewater systems and parks.

Other projects may be realized through Metropolitan Redevelopment Planning.

This chapter is divided into the following sections:

1.0 Criteria for Project Priorities

2.0 Project List

3.0 MRA project recommendations

1.0 Criteria for Project Priorities

The following criteria were used to identify and prioritize potential capital improvement projects. These criteria came from a synthesis of the community and City’s most pressing ideas about how to bring new development to the corridor.

- a. Efforts should be concentrated on creating an attractive environment in activity centers and along the corridor for new businesses and the customers that frequent them.
- b. Actions should improve safety, transportation safety and public safety
- c. Projects should enhance the corridor’s identity, including acknowledging and celebrating the historic and cultural heritage of Route 66.
- d. Projects should be supportive of new and existing businesses.
- e. Projects should foster pedestrian and bicycle safety, traffic calming and improved aesthetics to the corridor.
- f. Projects should help market the corridor as an attractive place to live, work and visit.
- g. Projects should improve and develop infrastructure and facilities in activity centers and along the corridor

2.0 Summary of Projects

The following pages contain the list of projects for the Plan area.

Projects

	Topic/Segment	Page	Location	Description	Explanation	Lead Agency	Funding
	General Transportation/Traffic						
	Segment 1						
1	pedestrian safety	125, 128	Central Ave. & 98th St. Intersection	Pedestrian refuges and widen crosswalk striping.	improve pedestrian safety and comfort crossing intersection	DMD	
2	streetscape	125	Volcano Rd. (length in plan area)	Streetscape improvements with planting strip and sidewalk.	Per street tree ordinance		
3	streetscape	125	Bridge Blvd. (length included in plan area)	Streetscape improvements with planting strip and sidewalk.	Per street tree ordinance		
4	medians	125	Central Ave. between 98th St. and Victory Rd.	Landscape the unimproved medians with high mesa plant materials, and include depressed drainage areas for storm water runoff	Include trees and shrubs lower in height in order to preserve views	Parks,, DMD	
5	multi-use trail	126, 127	Central Ave Frontage Rd.	Multi-use trail or linear park that includes public art, cross fit stations and drainage swales	multi-purpose, including drainage, recreational and community identity opportunities	Parks,DMD	
6	intersection	129	Central Ave. & Unser Blvd. Intersection	Improvements per Conceptual Roadway Improvement Plan and Gannet Flemming West, Inc.		DMD	
7	streetscape	125	Central Ave. from 86th St. to Coors Blvd. (MAC)	Streetscape plan for the MAC to include elements that help support its identity as a regional destination for housing, employment and services (pedestrian improvements, wayfinding and area identity elements).	Highlight arrival into MAC; wayfinding and identity making		
8	road network	130	Central Ave. from 86th St. to Coors Blvd. (MAC)	MAC Connectivity: Require access easements in designated area to ensure overall street network improvements.	improve street network in MAC	Planning, DMD	
9	intersection	129	Central Ave. & Coors Blvd. Intersection	Intersection study to introduce pedestrian safety and traffic calming measures; pedestrian striping, median improvements-including the creation of clear pedestrian refuges, street trees/streetscape improvements, bike lane improvements and right turn slip lanes (in lieu of free-rights)	improve pedestrian safety and comfort crossing intersection; mitigate against unsafe crossings related to transit transfers	DMD, Planning	
10	intersection	12	Coors Blvd. south of the Central Ave. & Coors Blvd. Intersection	Intersection study at Amanda/Bjarne and Coors upon future development/increased traffic warrants.	facilitate pedestrian and bicycle connectivity between the Central Ave. corridor and the Alamosa Community Center.	DMD	

	Topic/Segment	Page	Location	Description	Explanation	Lead Agency	Funding
	Segment 2						
11	driveway consolidation	133	Central Ave.-all of Segment 2	Develop a plan for consolidating driveways.		Planning, DMD	
12	intersection	133	Central Ave., Old Coors Rd. and Yucca Rd. (offset intersections)	Develop plan for improved pedestrian crossing zone from Old Coors to Yucca. This might take the form of a large striped area with landscaped median.	facilitate vehicular and pedestrian movements through the intersection	DMD	
13	pedestrian safety	133	Central Ave. & Arenal Canal (and/or other location in vicinity)	Install a HAWK-pedestrian activated signal.	to facilitate pedestrian travel along the Arenal Canal trail across Central Ave.	DMD and/or Trails	
14	medians	133	Central Ave. from Old Coors Rd. to 58th St.	Replace dead plant materials in medians and include depressed areas for stormwater drainage	use plant materials appropriate to high desert/mid mesa setting	Parks	
15	lighting	133	Central Ave. from Old Coors Rd. to the river	Install pedestrian lighting	to address safety and identity issues		
16	lighting	133	Central Ave. from New York to Rio Grande Blvd.	Install pedestrian lighting	to address safety and identity issues		
17	streetscape	133	Central Ave. from Arenal. to River	Streetscape improvements to create pedestrian enhancements and community identity; excess right of way on both sides of Central to be improved with landscaping and lighting	ensure improvements utilize opportunities for placemaking and wayfinding such that the unique history and identity of the area is recognized and celebrated		
18	streetscape	133	Central Ave. & Atrisco Dr.	Streetscape improvements that facilitate pedestrian use and recognize streets location and function in the activity center. Free right should be redesigned to slow turning traffic.	ensure improvements utilize opportunities for placemaking and wayfinding		
19	wayfinding	133	Central Ave. from Atrisco Dr. to Rio Grande Blvd.	Implement wayfinding and identity measures to connect both sides of the River	Include wayfinding for destinations like Bio Park, Tingley Beach, the Bosque, Atrisco Plaza and Old Town		
20	pedestrian bridge	133	Central Ave. Pedestrian Bridge	Build a pedestrian bridge across the Rio Grande that parallels the Central Ave. Bridge.	Make connections between Bio Park and MR site on west side of river. Create an area attraction.		

Projects

	Topic/Segment	Page	Location	Description	Explanation	Lead Agency	Funding
	<i>Segment 2 Continued</i>						
21	multi-use trail	133	Central Ave. from River to New York	Plan and design multi-use trail along BioPark at Central Ave.	per West Central Corridor Concept Plan; creates bike route connection to New York and Mountain Road Bike Boulevard via Alameda Lateral		
22	wayfinding	133	Central Ave. & Tingley Dr. Intersection	Install wayfinding elements that identify entrances and connect Tingley Beach and the Bio Park.			
23	intersection	135	Central Ave. & New York Ave. Intersection	Reconfigure the south side of the intersection and implement improvements that facilitate and enhance pedestrian activity at this intersection		DMD	
24	streetscape	134	Central Ave. from New York Ave. to Rio Grande Blvd.	Streetscape improvements that encourage pedestrian connectivity between Old Town and Bio Park including sidewalk/planting strip extensions at the curb and right of way acquisition in key areas. Commission masterplan for this section to implement a cohesive and comprehensive pedestrian oriented streetscape for this section.	Other improvements may include sidewalk extension with landscape strips, street trees in front setback, lighting. Traffic calming to lessen negative impact of fast moving, high volume vehicular travel in this area.		
25	streetscape	135	Soto Ave. from Simmonds St. to Rio Grande Blvd.	Improve roadway with landscaping and LID techniques, and use as an alternative pedestrian and bike route from Old Town to the BioPark	LID techniques will help to solve drainage and flooding issues; encourages pedestrian travel between Old Town and BioPark		
26	intersection	135	Central Ave. and Rio Grande Blvd. Intersection	Implement wayfinding/gateway elements that announce the BioPark, Tingley Beach and the Bosque. Increase pedestrian signal timing. Improve cross walk striping. Install signage that alerts drivers of pedestrians in the area and gives them the ROW in crosswalks			
27	streetscape	135	Central Ave. Locations TBD	Develop plan to ensure adequate sidewalk widths in areas where utility poles block or limit access.	sidewalk expansion into street via curb side sidewalk extension or target aquisition of ROW in blocked areas.		

	Topic/Segment	Page	Location	Description	Explanation	Lead Agency	Funding
	Bikeways and Multi-use Trails						
	<i>Segment 1</i>						
28	bike lane	138	Central Ave. from 98th St. to 86th St.	Complete missing sections of bike lanes on the south side of Central Ave. Bring bike lanes up to the DPM standard of seven feet for principal arterials with posted speeds of 40mph or greater.		DMD and Planning	
29	bike lane	138	Central Ave. along the Frontage Rd.	Stripe missing bike lanes		DMD	
30	intersection	138	Central Ave. & Unser Blvd. Intersection	Install bike box, bike lane striping	Intersection is slated for other improvements, ensure bikeway improvements simultaneously	DMD	
31	multi-use trail	138	Central Ave. & Unser Blvd. Intersection	connect unser multi-use trail to proposed frontage road multi-use trail	Intersection is slated for other improvements, ensure bikeway improvements simultaneously	DMD	
32	intersection	138	Central Ave. & Coors Blvd. Intersection	Install bike box, bike lane striping	Intersection is slated for other improvements, ensure bikeway improvements simultaneously	DMD	
33	multi-use trail	126, 127	Central Ave Frontage Rd.	Trail or linear park that includes public art, cross fit stations and drainage swales	multi-purpose, including drainage, recreational and community identity opportunities	Parks	
	<i>Segment 2</i>						
34	bike lane	138	Central Ave. & New York Ave. Intersection	Move bike lane from Central Ave. to New York Ave. per the West Central Corridor Concept Plan		DMD	
35	bike connection	138	Central Ave. & New York Ave. Intersection	Sign a bike route connection to Mountain Road Bike Boulevard via Alameda Canal		DMD	
36	multi-use trail	133	Central Ave. from River to New York	Plan and design multi-use trail along BioPark	per West Central Corridor Concept Plan		

Projects

	Topic/Segment	Page	Location	Description	Explanation	Lead Agency	Funding
	Transit					DMD	
	Segment 1						
37	bus stop	137	Central Ave. just east of Unser Blvd.	Install ADA accessible bus stop with shade structure		Transit	
38	bus stop	137	Central Ave. & Airport Dr. (at Verizon Wireless)	Install seating and shade structure		Transit	
39	bus stop	137	Central Ave. west of Airport Dr. (at Legacy Church)	Install seating and shade structure		Transit	
	Segment 2						
40	bus stop	137	Central Ave. & 64th St.	Install seating and shade structure		Transit	
41	bus stop	137	Central Ave. & 52nd St.	Install seating and shade structure		Transit	
42	bus stop	137	Central Ave. & Cypress St.	Install seating and shade structure		Transit	
43	bus stop	137	Central Ave. & 48th St.	Install seating and shade structure		Transit	
44	park and ride	137	Central Ave. & Atrisco Dr. Area	Plan for a Park and Ride facility to serve eastbound transit service	Investigate potential incentives in exchange for park and ride area.	Transit	
45	bus stop	137	Central Ave. just east of Tingley Dr. (in front of The Beach Apartments)	Install seating and shade structure		Transit	

	Topic/Segment	Page	Location	Description	Explanation	Lead Agency	Funding
	Parks						
	<i>Segment 1</i>		Location	Description	Explanation	Lead Agency	
46	park	144	West of Coors Blvd. Location TBD	Consider a future joint use community center/community park to serve populations west of Coors.		Parks and/or Family & Community Services	
	<i>Segment 2</i>						
47	park	144	Central Ave at New York	Pocket park/joint use with MRGCD interpretative site.	provides gateway element to Bio Park, improves New York intersection, ditch history and information for visitors	Parks, MRGCD	
	Drainage						
	<i>Segment 1</i>						
48	medians	140	Locations TBD	As medians are installed or retrofitted, ensure they are depressed from the roadway	Depressions will help capture and treat stormwater run-off	DMD, Parks	
49	multi-use trail	140	Central Ave. Frontage Rd.	Employ stormwater capture and treatment techniques as part of multi-use trail improvements		DMD, Parks	
	<i>Segment 2</i>						
50	medians	140	Locations TBD	As medians are installed or retrofitted, ensure they are depressed from the roadway		DMD, Parks	
51	private development/setback	140	Locations TBD	pocket gardens which pond stormwater (surface runoff or roof runoff). Encourage coordination with BioPark staff to identify plantings and sustainable design concepts.	Increases pervious area to mitigate against flooding in area, helps create identity for SAC which complements location near BioPark. Ponding areas as amenity.	DMD	
	Utilities						
	<i>Segment 2</i>						
52	border station	143	Central Ave. & Atrisco Dr.	Use landscape screening, and/or removal and replacement of chainlink visible from public rights-of-way with an alternate appropriate fencing material.	Improve aesthetics of NM Gas Co. Border Station	NM Gas Company	
53	utility poles	143	Locations TBD	As new streetscape improvements are made investigate moving utility poles out of the sidewalk			

Projects

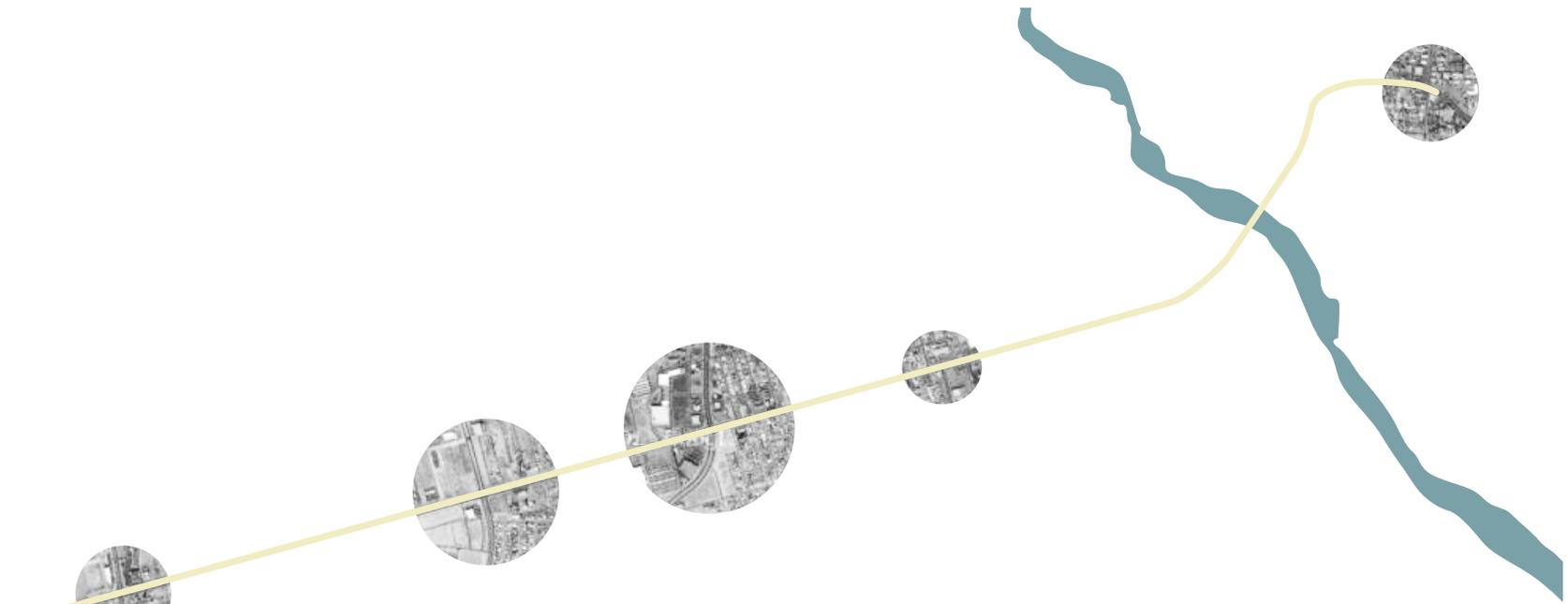
	Topic/Segment	Page	Location	Description	Explanation	Lead Agency	Funding
	Trails (MRGCD and Open Space)						
	<i>Segment 2</i>						
54	trails	145	Central Ave. & Atrisco Ditch	Enhance and extend the trail along the Atrisco Ditch.	Pedestrian and bicycle gateway to the South Valley from the Central Ave. corridor.	CoA & MRGCD	
55	pocket park	145	Central Ave. & Alameda Lateral	Create an educational interpretive exhibit that explains the history and continued use of acequias for agricultural irrigation, and provides way-finding to area amenities. Remove existing chain-link fence or replace with a more attractive design.		MRGCD	
	Open Space						
56	parking	145	Central Ave. & Sunset Dr.	Improve Bosque access parking on the west side of the river, enhance the aesthetics and safety of the parking area	Encourage more visitor use	Open Space	
57	ADA	145	Central Ave. & Sunset Dr.	make ADA connection between parking area and ADA trail.			
	Historic Route 66						
58			Plan area	Develop a conservation easement program for preserving historic facades and signs		Planning	
59			Plan area	Develop a neon sign incentive programs		Planning	
60			New York and Central	Redevelop the El Vado Motel with a strong interpretive component and public access		Planning/MR	

3.0 Metropolitan Redevelopment Projects

The following is a list of potential Metropolitan Redevelopment Projects in the Plan area:

- a. The multi-use trail along the frontage road from Unser Blvd. to the a. west end of the plan area. The project could offer the community a unique recreational opportunity.
- b. Pedestrian bridge across the river at Central Ave. and enhance vehicular bridge
- c. MR projects that bring mixed income and senior housing corridor to the corridor in partnership with Albuquerque Housing Authority &/or Family & Community Services.
- d. MR projects that bring opportunities for tourism to the corridor:
 - i. SAC streetscape improvements (intersection treatments, extended sidewalk/planting areas, LID rain gardens, adobe wall theme)
 - ii. SAC signage and wayfinding
 - iii. SAC MR sites redevelopment: visitor friendly and public oriented, restaurants, visitor center, museums, galleries, shops
 - iv. SAC Soto/Simmonds ped/bike connection between Rio Grande Blvd. and Central Ave. (supporting connectivity to Old Town/Museum District) in coordination with DMD
 - v. River/Bosque visitor destination: restaurant, iconic structure, river activity and viewing amenities, equestrian opportunities
- e. MR projects that bring pedestrian oriented, mixed use development:
 - i. MAC LID implementation: Plazas, linear park
 - ii. MAC signage and wayfinding
 - iii. MAC MR sites redevelopment: mixed use component, public space, plazas
 - iv. MAC identity and development marketing
- f. Other projects:
 - i. Route 66/Car culture museum, area for display, parking, drive in opportunities (restaurant, theatre). Area to celebrate car culture in New Mexico (including Lowrider, classic cars), market as part of the overall corridor identity.

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Appendices

The appendices contains the following sections:

Definitions

Planning Process

1.0 Definitions

Access. Permission or ability to enter, approach or pass to and from public and private property.

Access Easement. Easement for vehicular or pedestrian access across private property.

Accessibility. Approachability and usability by people with disabilities. Degree of compliance with the federal Americans with Disabilities Act.

Amenities, pedestrian. Pedestrian amenities serve as informal gathering places for socializing, resting, and enjoyment of a particular area and contribute to a walkable district. Typical amenities include extra wide sidewalks, street trees, sitting spaces, weather protection (awnings or canopies), pedestrian scale lighting, bus stop seating, etc.

Articulation. Off-sets, projections, recessed walls, windows, doors, etc. that provide variation to a building façade.

Automobile-dependent uses or activities. Land uses that contain automobiles and/or motor vehicles as integral parts of the uses.

Bollard. A post of metal, wood, or masonry that is used to separate or direct traffic (vehicles, pedestrians and/or bicycles). Bollards are usually decorative and may contain sidewalk or pathway lighting.

Build-to-Line. An alignment established a certain distance from the curb line to a line along which the building shall be built. Front porches and handicap ramps are exempt. Front porches and handicap ramps are exempt.

Building mass. The aggregate size of a building, or the total height, width, and depth of all its parts.

Building orientation to the street. Primary facade of building fronting/facing street.

Bulb Out or Curb Extension. This is a traffic calming measure, intended to slow the speed of traffic and increase driver awareness. They also allow pedestrians and vehicle drivers to see each other when vehicles parked in a parking lane would otherwise block visibility. A curb extension comprises an angled narrowing of the roadway and a widening of the sidewalk. This is achieved using the addition of pavement.

(CIP). Capital Implementation Program

Clear Height. Unobstructed vertical distance between two objects.

Compact development. Buildings, parking areas, streets, driveways, and public spaces are developed in a way that shortens trips, and lessens dependence on the automobile; thereby reducing levels of land consumption, energy use, and air pollution. Compact development promotes full utilization of urban services such as water lines, sewers, streets, and emergency services, by taking advantage of existing public facilities and minimizing the need for new facilities.

Density. A measurement of the number of dwelling units in relationship to a specified amount of land.

Development intensity. The amount or magnitude of a use on a site or allowed in a zone. Generally, it is measured by floor area. It may also be measured by such things as number of employees, amount of production, trip generation, or hours of operation.

(DRB). Design Review Board

Drive aisle/Driveway. An improved (e.g., paved) driving surface for one line of vehicles.

(EPC). Environmental Planning Commission.

Façade. The face or front of a building.

Fenestration. The openings which form a part of a building façade.

Forecourt. The outer or front court of a building or of a group of buildings which may

Definitions

contain a vehicular drop-off or parking.

Frontage Types. The category of building facades, as described on pages 47-48.

Human scaled. Site and building design elements that are dimensionally related to pedestrians, such as: small building spaces with individual entrances (e.g., as is typical of downtowns and main street developments); larger buildings which have articulation and detailing to break up large masses; narrower streets with tree canopies; smaller parking areas or parking areas broken up into small components with landscaping; and pedestrian amenities, such as sidewalks, plazas, outdoor seating, lighting, weather protection (e.g., awnings or canopies), and similar features. These features are all generally smaller in scale than those which are primarily intended to accommodate automobile traffic.

Infill and redevelopment. The development of vacant, bypassed or under-utilized lands in an area that is mainly developed.

Mixed use development. Development in which multiple land uses are permitted such as retail and residential.

(MRCOG). Mid-Region Council of Governments.

Node. An area of concentrated activity, often involving higher densities and a mix of uses, that encourages alternatives to automobile travel.

Pedestrian-friendly/pedestrian-oriented. Development which is designed with an emphasis primarily on the street sidewalk and on pedestrian access to the site and building, rather than on auto access and parking areas. The building is generally placed close to the street and the main entrance is oriented to the street sidewalk. There are generally windows or display cases along building façades which face the street. Typically, buildings cover a large portion of the site. When parking areas are provided, they are generally limited in size and they are not emphasized by the design of the site.

Plaza, plazuela. A public square or extra-wide sidewalk (e.g., as on a street corner) that allows for special events, outdoor seating, sidewalk sales and similar pedestrian

activity.

Primary entrance. The entrance to a building that most pedestrians are expected to use. Generally, each building has one primary entrance. It is the widest entrance of those provided for use by pedestrians. In multi-tenant buildings, primary entrances open directly into the building's lobby or principal interior ground level circulation space. When a multi-tenant building does not have a lobby or common interior circulation space, each tenant's outside entrance is a primary entrance. In single-tenant buildings, primary entrances open directly into lobby, reception, or sales areas.

Right-of-way (ROW). Land that is owned in fee simple by the public, usually for transportation facilities.

Streetscape. The portion of the right-of-way that is between the lot line and the edge of the vehicular lanes. The principal streetscape components are curbs, sidewalks, planters, street trees and street lights.

Structured parking. A covered structure or portion of a covered structure that provides parking areas for motor vehicles. It includes parking on top of a structure where there is gross building area below the parking, but nothing above it. The structure can be the primary structure for a Commercial Parking facility or be accessory to multi-dwelling residential, commercial, employment, industrial, institutional, or other structures.

Transportation mode. The method of transportation (e.g., automobile, bus, walking, bicycling, etc.)

Urban. Relating to, characteristic of, or constituting a city.

Urban design. The conceptualization of the built environment in response to human needs and desires.

(ZHE). Zoning Hearing Examiner

Definition Sources:

CABQ Form Based Code

Model Development Code and User's Guide for Small Cities, 1999 (Oregon TGM Program)

Lexicon of the New Urbanism, Duany Plater-Zyberk & Company, Version 2.1 (1999)

Webster's New Collage Dictionary (1995)

Planning Process

2.0 Planning Process Overview

The planning process was initiated by Albuquerque City Councilors Ken Sanchez-District 1, and Isaac Benton-District 3, in coordination with City Councilor Debbie O'Malley-District 2. The Planning Team was led by City Planning staff in conjunction with Council staff and architecture, landscape and planning consultants, Strata Design.

The year-long planning process for the 2011 West Route 66 Sector Development Plan began with a kick-off meeting in September 2010. Throughout the fall of 2010, the Planning Team held a series of community meetings that were referred to as Listening Meetings. These meetings were designed to elicit general input from the community on topics that included community perception and identity, zoning and land use, transportation and transit and other public infrastructure and services such as parks, plazas, ditches and drains, and public art. Listening Meetings also provided the community the opportunity to express their opinions on topics that while important, may not be addressed by the Sector Plan such as code and police enforcement issues.

Following the Listening sessions during the winter of 2010, the Planning Team held a series of workshops called Visioning Sessions. Visioning Sessions were designed to guide the community through activities that took the broad topics that were defined during the Listening Meetings and began to distill these ideas into visions, goals and objectives for the Plan area. During the same timeframe, in addition to the community participation portion of the planning process, the Planning Team commissioned a retail market study, met with governmental departments and agencies, gathered and analyzed data and conducted research that informed the content of the Sector Development Plan.

In order to ensure that the Planning Team had accurately captured the ideas and visions of the community, the Team held community meetings in early spring 2011 to present the results of the Visioning Meetings and to offer potential strategies for achieving those visions. The feedback meetings were followed by Liaison Committee meetings during summer 2011, at which the team presented a working draft of the Plan to neighborhood representatives and other stakeholders, whose community connections would assist the planning team in gaining broader community feedback on the initial draft of the Plan.

2.1 Community Participants

It was the intent of the Planning Team to achieve a broad participatory planning process that included a wide spectrum of both property and business owners on the corridor as well as residents of adjacent neighborhoods. Property-owners, and the following Neighborhood Associations and other community organizations were invited to participate in the community meetings that helped to inform the vision and content of this plan:

Alamosa NA
Anderson Hill HOA
Anderson Hills NA
Avalon NA
Blossom Ridge HOA
Bluewater Point HOA
Crestview Bluff NA
El Rancho Grande HOA
Encanto Village HOA
Historic Old Town POA
Huning Castle NA
Los Altos Civic Assoc.
Los Volcanes NA
NAIOP
Northwest Alliance of Neighbors
NM Route 66 Association
Orchards at Anderson Heights
Pat Hurley NA
Riverview Heights NA
Skyview West NA|
South Valley Coalition of Neighborhood Associations
Stinson Tower NA Sunrise HOA
Sunstar NA
SWAN (SW Alliance of Neighbors)
Tapia Meadows NA
Valley Gardens NA
Vecinos del Bosque NA
Vista Magnifica

Vista Sandia HOA
 WCCDG (West Central Community Development Group)
 West Mesa NA
 West Park NA
 Westgate Heights NA
 WSCNA (Westside Coalition of NAs)

Participants in Liaison Committee Meetings (July – September 2011)

Alamosa NA	Jerry Gallegos
Anderson Hills NA	Pat & Joe Risbeck
Avalon NA	Kelly Chappelle
Los Altos Civic Assoc.	Diane Beserra
NAIOP	Ron Bohannan
Pat Hurley NA	George Holly
Riverview Heights NA	Pat Baca
Stinson Tower NA & Encanto Village HOA	Norm & Belinda Mason
SWAN	Klarissa Pena
SWAN	Nancy Montano
West Park NA	Elaine Faust
Property-owner (Seg 1)	Curtis A. Brewer
Business & Property-Owner (Seg 1)	Van Barber
Business & Property-Owner (Seg 1)	Mike & Lisa Stewart
Business & Property-Owner (Seg 2)	Lonnie Yanes
Business & Property-Owner (Seg 2)	Nate Archuleta

2.3 Plan Boundary Revisions

During the planning process the West Route 66 Sector Development Plan boundary has been modified from the 1987 plan boundary. The most significant change is that portions of the former plan area that are located in Bernalillo County have been removed from the plan, as the City of Albuquerque does not have jurisdiction over these lands. In the fall of 2011, Bernalillo County began a separate planning process for the portion of the corridor that is located in the County. The Planning Team is working with Bernalillo County in order to achieve coordination between the two plans. Properties that are zoned for single family residential have also been removed from the plan. The West Route 66 SDP is a corridor plan and the Planning

Team felt that removing the very few R-1 and R-T properties that were within the plan area would not have a very large impact on the plan as a whole. Another important change is the addition of the segment of Central Ave. that crosses the river and extends east to Rio Grande Blvd. Existing physical and socio-demographic conditions here are similar to the area west of the river. This segment also has strong potential to provide a more attractive link between existing amenities— Old Town, the Bio Park, Tingley Beach and the Bosque—and the west side of the river. Other changes that were made to the boundary include the correction of mapping errors and cleaning up the boundary so that it follows parcel lines.

2.4 UNM Advanced Planning Studio Project

In the Fall 2010, graduate students from the University of New Mexico Community and Regional Planning Program collaborated with the West Central Community Development Group and other local stakeholders to produce redevelopment proposals for the West Central Ave. and Old Coors Rd. corridors. They include design and policy recommendations for the Central Ave. corridor between the River and Unser Blvd. Recommendations focused on economic stabilization and community development and have been considered in formulating this plan.