



VOLCANO HEIGHTS

Sector Development Plan



EPC Redline 2014

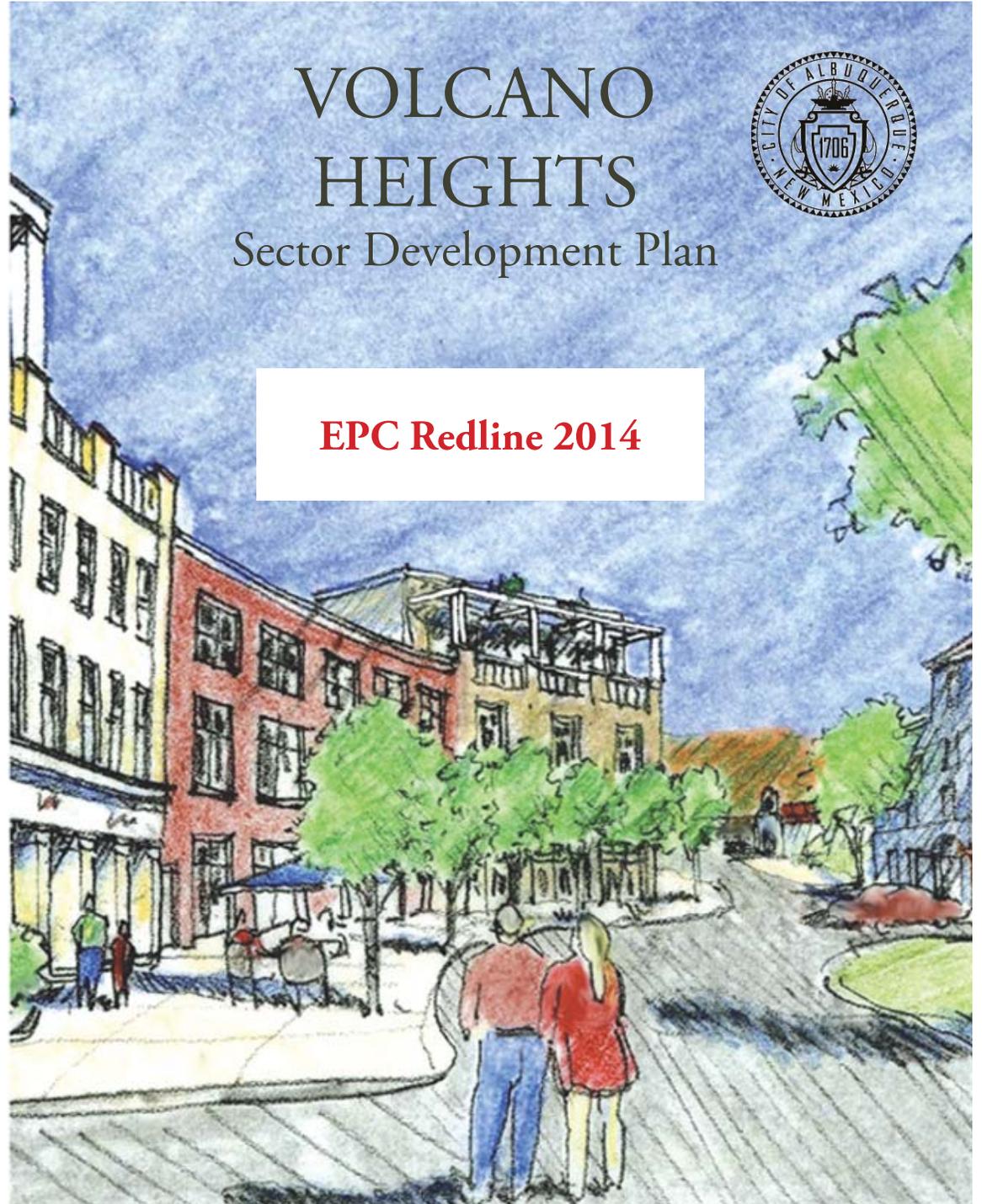


TABLE 3.1 – APPLICABILITY OF PLAN SECTIONS BY DEVELOPMENT TYPE (Cont'd)

Plan Section	4. 1 Land Use Table	5.X.2 Building Placement	5.X.4 Height Standards	5.X. 6 Parking & Service	5.X.7.i Encroachments	5.X.7.iii Single-Family Buffer	5.X.9-12 Building Design Standards by Zone	6.0 General Site Development Standards	7.0 General Building Design Standards	8.0 Signage	9.0 Open Space Standards	9.6 Landscaping & Lighting	10.0 Street Standards	11.0 Streetscape Standards
Expansion of parking area only (not in conjunction with expansion of building or use)														
Up to 10 spaces				X				X	X	X		X		
11 or more additional spaces				X		X		X	X	X	X	X		
Façade changes to existing buildings*														
'A' Street façade					X		X	X	X	X		X	X	X
All other street façades					X		X	X	X	X		X		
Signage														
Modification of an existing sign where the cost of the modification is valued at <u>more than</u> 50% of the replacement value of the sign								X	X	X				
New sign or complete replacement of an existing sign								X	X	X				

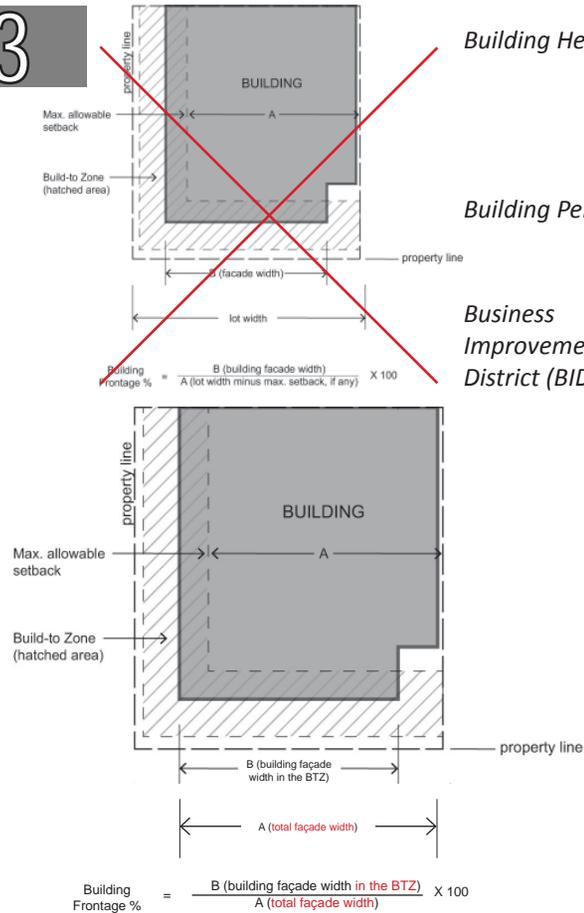
* Restuccoing with a color other than originally approved requires a permit in order to ensure compliance with color regulations per Section 7.5 starting on page 125.

CHAPTER II: REGULATIONS

3.0 Administration *Building Frontage:*

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The percentage of ground floor façade width located within the **Build-to Zone** as a proportion of the **total building façade lot-width** along an **'A'** or **'B' Street**. **Civic space that meets the criteria in Table 9.4, including parks, plazas, improved forecourts, and pedestrian breezeway frontages paseos**, shall be considered as buildings for the calculation of building frontage. [See calculation diagram.]

The height of a **building** measured from approved grade to the eave line in the case of a pitched roof or to the roof line in the case of a **parapet**. See also **structure height**.

An official certificate of Entitlement issued by the City to an **applicant** in order to construct, enlarge, or alter a structure.

Business Improvement Districts (BIDs) are created by petition of at least ten or more business owners comprising at least 51% of the total business owners in the proposed district. Upon receipt of the petition, a municipality typically passes an ordinance creating the district and establishing the time and terms for paying the BID fee, which could be assessed as part of property taxes or paid annually by tenants. The municipality appoints a management committee, typically an existing revitalization nonprofit or a committee of nominees submitted by business owners, to manage collected fees and act as the legal entity to provide ongoing maintenance, services, and liability for a self-defined area. BIDs often employ a property manager that can act as a recruiting, marketing, and brokering agent for the district. [See also State of New Mexico BID Act, Sections 3-63-1 to 3-63-16 NMSA 1978.]

Bus Rapid Transit (BRT):

A public transportation system with some dedicated infrastructure and additional resources that enables service that is timely and more efficient than an ordinary bus line can provide. These systems approach the service quality of rail transit with the cost savings and flexibility of bus transit.

Center Zones:

Character Zones within Volcano Heights meant to provide gravity to hold together surrounding development. Center Zones are intended to have the most dense, urban, walkable built environment and the most intense activity, particularly for pedestrians. In this Plan, Center Zones include Town Center, Regional Center, and Village Center zones.

Character Zone:

A zone within Volcano Heights that creates an urban form distinct from other zones in the Plan area. Character Zones are identified in the zoning map in **Section 4.1 starting on page 57**.

Civic Space:

See Open Space, Civic.

Colonnade:

A row of regularly spaced columns supporting a major horizontal element above.

Commercial Ready:

A space constructed at a minimum ground floor height as established by character zone that may be used for non-commercial uses and/or be converted into retail/commercial use. The space must comply with all building and construction codes for retail use in place at the time of site plan **approval**. Commercial-ready spaces are intended to provide additional flexibility to use space according to market demand.

3.0 Administration

Porte Cocheres: A roofed structure covering a driveway at the entrance of a building to provide shelter while entering or leaving a vehicle.

Primary Street: See **Streets, Primary.**

Private Open Space Amenity: Could include a pool (swimming, lap pool, spa area), play courts (basket ball, tennis), picnic areas with shade structures, etc.

Property Owners Association (POA): Typically a non-voluntary organization that collects regular dues from all property owners within a self-defined area to pay for ongoing maintenance, liability, and operations of privately owned amenities that benefit properties within the self-defined area. A POA that chooses to register with the City ONC shall be included in official **notification** requirements of projects per **Table 3.4 on page 36** and **Table 3.5 on page 37** of this Plan.

Public Improvement District (PID): A method of funding subdivision improvements, such as roads, public buildings, drainage infrastructure and recreational facilities through special assessments added to property taxes for properties within the defined boundaries of the district. See **Section 14.3.2**

Public Utility Structure: Per City Zoning Code §14.16.1.5.

Recreation Area: A **civic open space** intended for passive, unprogrammed, low-impact recreation not typically defined spatially by building façades. Recreational areas are typically naturalistic with minimal improvements.

Retail Sales: Retail establishments are the final step in the distribution of merchandise. They are organized to sell items in small quantities to many customers. Establishments in stores operate as fixed point-of-sale locations, which are designed to attract walk-in customers. Retail establishments often have displays of merchandise and sell to the general public for personal or household consumption, though they may also serve businesses and institutions. Some establishments may further provide after-sales services, such as repair and installation. Included in, but not limited to, this category are durable consumer goods sales and service, consumer goods, other grocery, food, specialty food, beverage, dairy, etc., and health and personal services.

Reverse-angle Parking: **On-street, angled parking where cars back into a parking space. The procedure is generally easier than parallel parking and safer for multimodal streets – particularly for cyclists – because of greater visibility when cars pull into the travel lane. [See Exhibit 13.2 for examples.]**

Right-of-Way (ROW): The area of land dedicated to or acquired by the city, county, or state primarily for the use of the public for the movement of people, goods, and vehicles.

Rock Outcropping: Bedrock or other stratum a minimum of 6 feet high on its steepest side as measured from the adjacent 10% slope line and in excess of 500 SF in surface area.

Roof Terraces/Gardens: Flat areas on top of a building that are accessible for use as a recreation or gardening space for the residents and users of the building.



Playground on the West Side



Portal on the West Side



Rock Outcropping in Volcano Heights



Roof Terrace in East Downtown

6.0 Site Development Standards General to All Zones

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6.5. Grading

- 6.5.1. **Purpose / Intent:** This Plan restricts grading in order to protect natural topography and distinct topographic and natural character to the extent possible. Natural terrain and geological features make the area unique, desirable, and of value to residents and the larger community.
- 6.5.2. Cut and fill slopes shall be no steeper than 3:1 on average, and retaining walls shall not exceed 4 feet in height unless incorporated within a building's foundation or approved by the City Hydrologist as necessary for development.
- 6.5.3. Graded areas shall maintain the character of the natural terrain by varying gradients, undulating contours, and rounding the toe and crest of any slope greater than 10 feet in height.
- 6.5.4. Fill shall be limited to four (4) feet except as deemed necessary for site development and drainage by the City Hydrologist. When more than 4 feet of fill is proposed, a project shall require **DRB approval** in order to ensure coordination with the City Hydrologist, City Open Space Division, and other relevant City staff.
- 6.5.5. Fill shall not exceed the existing highest natural grade point on site, unless approved by the City Hydrologist for required drainage. Fill height shall be measured from the natural grade.

- 6.5.6. In the selection of alignment and in site design, grading plans shall demonstrate that cut and fill has been kept to a minimum consistent with the standards in this Plan. Generally, the overall topography of the site is not to be substantially altered. Development must comply with the joint Albuquerque-Bernalillo County Fugitive Dust Ordinance found in the New Mexico Administrative Code 20.11.20, as well as the most recently approved City Air Quality Control Division standards and requirements.

6.6. Construction Mitigation

- 6.6.1. **Purpose / Intent:** Any damage to the vegetation, slope, or placement of boulders due to or related to construction shall be mitigated as provided herein.
- 6.6.2. Grading permits ~~for commercial lot developments in the Volcano Heights Sector Development Plan area~~ shall only be issued concurrently with ~~the respective commercial building permits for non-residential and mixed-use developments, as well as residential developments over two lots.~~ Grading permits for ~~residential~~ developments greater than 10 acres shall require soil stabilization, approved by the Environmental Health Department, which shall be applied to the disturbed area within three months after grading of the site commences. Grading within public rights-of-way or public drainage easements is exempt.

6.0 Site Development Standards General to All Zones

- 6.6.3. Prior to beginning any construction, the property owner, developer, or infrastructure contractor (whichever is the most relevant) shall complete the following requirements.
 - (i) A temporary silt fence shall be constructed at the site boundary adjacent to sensitive lands (i.e. the Escarpment buffer, Major Public Open Space, archeological site, or public or private conservation area to be maintained in natural desertscape) to effectively protect them from heavy equipment and vehicles.
 - a. Construction or silt fencing shall be placed no less than 12" from the Petroglyph National Monument boundary. [76]
 - b. The temporary silt fence shall comply with the most recently approved City Air Quality Control Division standards and requirements.
 - (ii) ~~Clear limits of construction shall be established so that construction activities do not encroach on Petroglyph National Monument.~~
 - (iii) Photographs of the project site in its original condition shall be submitted with the application for **building permit**, subdivision, and/or site development plan. Within the Escarpment Transition zone only, in lieu of photographs, a comprehensive video recording of existing conditions and situations of rocks and boulders shall be submitted to inform compliance with permitting and other approvals.

- (iv) ~~For any construction within the Escarpment Transition zones that includes blasting, the City Open Space in coordination with the National Park Service may require monitors to be placed between the blast site and the Petroglyph National Monument boundary in order to record blast data to ensure that disturbance remains within approved levels. Where required, monitor data may be required to be maintained for up to one year following final completion of the subject project.~~ Applications for public- and private-sector projects, especially infrastructure, within the Escarpment Transition zone or **adjacent** to archeological sites shall include geotechnical data and analysis that demonstrate to the satisfaction of the City Engineer, in coordination with the City Open Space Division and National Park Service, that proposed trenching or blasting will not affect the face of the Escarpment, whether by potentially causing erosion or caving of the slopes and boulders, threatening public safety or welfare, or otherwise damaging archeological resources.
 - a. Clear limits of construction shall be established so that construction activities do not encroach on Petroglyph National Monument. The National Park Service and/or City Open Space Division shall be permitted to monitor any construction staking ~~and/or blasting activities~~ within the Escarpment Transition zone. No construction easements on the Monument shall be granted.



6.0 Site Development Standards General to All Zones

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- b. For any construction within the Escarpment Transition zone that includes blasting, the City Open Space in coordination with the National Park Service may require monitors to be placed between the blast site and the Petroglyph National Monument boundary in order to record blast data to ensure that disturbance remains within approved levels. Monitor data may be required to be maintained for up to one year following final completion of the project.

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- 6.6.4. Replacement of boulders shall approximate the original location, angle, and surface exposure, based on pre-construction photographs and/or comprehensive video recording.
- 6.6.5. Revegetation to approximate original cover with native or xeric plants as appropriate from the Plant List in **Table 9.5** is required within 90 days of project completion. [See **Section 9.7.8 starting on page 157** to determine whether native or xeric species are required.] A deviation may be granted by the Planning Director or his/her designee if the type of vegetation or time of year make revegetation within 90 days impossible or undesirable. For infrastructure projects on public lands, the construction company shall post a warranty bond effective for three years after completion of the infrastructure work to ensure successful revegetation.

~~6.6.6. Applications for public and private sector projects, especially infrastructure, within the Escarpment Transition zone or adjacent to archeological sites shall include geotechnical data and analysis that demonstrate to the satisfaction of the City Engineer, in coordination with the City Open Space Division and National Park Service, that proposed trenching or blasting will not affect the face of the Escarpment, whether by potentially causing erosion or caving of the slopes and boulders, threatening public safety or welfare, or otherwise damaging archeological resources.~~

~~6.6.7. The National Park Service and/or City Open Space Division shall be permitted to monitor any construction staking and/or blasting activities within the Escarpment Transition zone. No construction easements on the Monument shall be granted.~~

6.6.8. To minimize the negative impact of fugitive dust on petroglyphs, no stockpiling of rock or basalt or rock crushing shall be permitted within 1320 feet (1/4 mile) of the Petroglyph National Monument boundary.

6.7. Infrastructure

6.7.1. Utilities

(i) Easements

- a. In order to facilitate pedestrian movement and maintain accessibility, utility infrastructure such as poles, transformers, boxes, and access panels shall be planned for minimal visual intrusion and mobility impediment to pedestrians. Overhead and underground electric distribution lines are typically located within public utility easements (PUE) or private easements. They are compatible with other “dry” utilities such as cable, telephone, and fiber optic facilities. The width of the

6.0 Site Development Standards General to All Zones

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6.8. Street Screens

- 6.8.1. Except as noted elsewhere in this Plan, all required street screens shall be made up of (a) the same material as the principal building or (b) a combination of masonry and **living fence**. Required street screens shall be located at the setback line unless otherwise noted in this Plan. [See also **Section 9.7.3 starting on page 154** for requirements for wall design.]
- 6.8.2. Where provided, the **living fence** shall be within a minimum 3-foot wide planting strip located on the pedestrian, residential, or public street side. [See also **Section 9.7.3 starting on page 154.**]
- 6.8.3. ~~Any frontage along all 'A' Streets and Civic Space not defined by a building at the BTZ shall be defined by a 4-foot high street screen. The required street screen shall be located at the setback line along the corresponding frontage.~~
- 6.8.4. Any off-street loading, unloading, storage, service, or trash pick-up areas shall be screened using a **street screen** at least as tall as the trash containers and/or service equipment it is screening at the **BTZ**.

- 6.8.5. Parking visible from the public **ROW** along an **'A'** or **'B'** **Street** shall have a **street screen** of masonry and/or metal railing 3-6 feet high combined with a landscape strip at least 3 feet wide on the pedestrian, residential or public street side.
 - (i) Along a **'B'** Street frontage, this requirement may be waived as a minor deviation if the applicant can show sufficient evidence of intent to build a parking structure within 2 years. [See **Table 3.2** starting on page 31.]
 - (ii) If construction of a parking structure has not commenced within 2 years, a street screen shall be required.
- 6.8.6. Drive-through facilities, where allowed by zone and located on a **'B'** **Street**, shall be screened from view via location behind a building on-site or a **street screen**, specifically:
 - (i) **Screening:** A street screen shall be provided for drive-up service windows and associated drive-up queue lanes that abut public streets or pedestrian areas. Screening may also be provided by placing the drive-up service windows and/or queue lanes between two adjacent and parallel buildings.
 - (ii) **Drive-up Service Windows:**
 - a. Drive-up service windows shall not be on a façade that faces or fronts an **'A'** Street.
 - b. Drive-up service windows shall be oriented away from pedestrian areas, such as sidewalks and plazas, and residentially-zoned areas, where possible.

7.0 Building Design Standards General to All Zones



7.4. Zero Lot Line Structures

- 7.4.1. Prior to being issued a building permit, a property owner wanting to build a structure with a wall on a lot line shall submit to the City a maintenance easement agreement signed by the abutting property owner, unless the building is part of an attached unit development.
- 7.4.2. Where there is no setback from the property line, gutters and downspouts shall drain to the street or water harvesting area to avoid impact to abutting lots.

7.5. Structure Color

- 7.5.1. In order to minimize the visual impact of development, colors shall be restricted to light reflective values between 20 and 50 percent. Metal items such as vents, cooling units, and other mechanical devices on roofs are subject to this regulation.
 - (i) This range of color general includes yellow ochres, browns, dull reds, and grey-greens, similar to the natural colors found on the mesa and escarpment. This middle range of reflectance is intended to avoid very light and very dark colors.
 - (ii) Stucco, block, and/or brick shall have integral color other than the standard grey.
 - (iii) Other materials with colors similar to those illustrated in Exhibit 7.1 may be used, as long as they have integral color other than the standard grey and meet the standards for reflectivity and harmony with colors in the natural landscape. [See Section 9.7.3 starting on page 154 for more details about restrictions for walls and fences.]



LIGHT REFLECTANCE VALUES

Chart Color	LRV %	Chart Color	LRV %
122 Straw	41.83	135 Sahara	34.63
106 Buckskin	34.73	116 Adobe	24.03
118 Suede	37.67	124 Coral	34.93
117 Fawn	45.48	115 Cottonwood	32.18

LRV data is from El Rey Stucco; El Rey does not guarantee the LRV data provided. Colors are illustrative from El Rey Standard Color for Premium Stucco Finish. Color reproduction in this Plan is not an exact representation of the El Rey color chart.

Exhibit 7.1 – Sample Colors

NOTE: These sample colors are stucco with integrated color as manufactured by El Rey traditional cementitious stucco in Albuquerque. This compound includes cement, hydrated lime, sand aggregates, and iron oxide pigments. Since the stucco is integrally colored, it will never need to be painted. Like many natural landscapes, the traditional cement stucco is breathable and appears slightly different during each season and at alternate times of the day.

Exhibit 7.1 – Sample Colors

7.0 Building Design Standards General to All Zones



7.7. Design of Residential Garages

- 7.7.1. See “Other Uses” items **OU-6** and **OU-7** in the **Table 4.4 starting on page 64** to determine whether garages are permitted in the relevant character zone. The following standards apply where residential garages are permitted by right or conditionally.
- 7.7.2. Where alleys are available, residential garages shall be accessed via the alley.
- 7.7.3. Townhouses and courtyard apartments shall use rear-loaded garages.
- 7.7.4. Front-loaded garages on residential lots less than 40 feet wide shall be prohibited. Garages on these lots shall be either Garage Type A or D in **Exhibit 7.2** per the standards in **Table 7.1**.
- 7.7.5. Lots equal to or greater than 40 feet shall use any of the garage types in **Exhibit 7.2** per the requirements in **Table 7.1**.

- 7.7.6. Where front-loaded garages are allowed, the garage façade shall not exceed 50% of the total front façade area, inclusive of porches, so that garages do not dominate the front façade.
- 7.7.7. Front-loaded three-car garages are not permitted on **lots** equal to or less than 70 feet wide. Three-car garages on lots greater than 70 feet wide shall have a third garage setback of three (3) feet minimum from the primary garage **façade**.
- 7.7.8. ~~Where there is no setback from the property line, gutters and downspouts shall drain to the street or water harvesting area to avoid impact to abutting lots.~~
- 7.7.9. Individual garage bays shall be no greater than 12 feet wide. For garages with multiple bays, all garage doors shall be divided into single bays separated by at least a 16-inch pier or column.
- 7.7.10. The color of garage doors shall blend with or complement the exterior wall color in order to minimize the prominence of the garage door.

TABLE 7.1 – GARAGE TYPES*

Lot Width	Allowable Garage Types*	Front Garage Setback from Main Façade (Front-loaded)	Side Garage Setback from Property Line (Side-accessed)	Rear Garage Setback from Property Line (Rear-loaded)
40+ Feet	A,B,C,D,E,F	10 ft. Minimum	5 ft. Minimum	2 ft. Minimum
Less than 40 Feet	A,D	10 ft. Minimum	5 ft. Minimum	2 ft. Minimum

* See **Exhibit 7.2** – Garage Type Diagrams

- Note 1: Garage Type D shall have a minimum of 5 linear feet of fenestration on the street façade and be articulated to resemble the main structure.
- Note 2: Garage Types D and F may be accessed from either front or side.
- Note 3: Driveway access from a ‘B’ Street, including drive pad but exclusive of wings, is limited to 12 feet for Garage Types B, C, D, E, and F except where providing access from **alleys**.

9.0 Open Space, Landscaping, and Site Lighting Design Standards

9.7.3. Plant Lists

- (i) **Purpose/Intent:** The purpose of regulating plants is to reduce water use, maintain the character of native plants now existing in the Petroglyph National Monument, and provide a harmonious landscape.
- (ii) **Landscaping:** Appropriate plants shall be chosen from the Plant List in Table 9.5 for landscaping within the Plan area. Shrubs and trees shall be nursery grown in order to minimize poaching from the Monument and Major Public Open Space.
 - a. **Native Species Requirement:** Within 200 feet of the Petroglyph National Monument or other **Major Public Open Space**, only plants and trees listed as native in the Plant List in Table 9.5 shall be used in order to limit impact of invasive and/or non-native plants on native vegetation, except for street trees. [See Section 11.3.5 starting on page 206. for details about street trees.] Native plant species were selected from a list compiled during an inventory within the Petroglyph National Monument by the National Park Service in 1994-1995, including almost 200 plants (amended).

- b. **Plant List Requirement:** Two-hundred (200) feet or more from the Petroglyph National Monument or other Major Public Open Space, any plant from the Plant List in Table 9.5 may be used. In addition to species native to the Petroglyph National Monument, additional plant species were selected from the official xeric or low-water use plant list of the Albuquerque Bernalillo County Water Utility Authority (ABCWUA).
- (iii) **Streetscaping:** See Section 11.3.5 starting on page 206.
- (iv) **Construction Mitigation:** Land disturbed in development shall be re-vegetated using either native or xeric plants as appropriate from the Plant List in Table 9.5. [See also Section 6.6 starting on page 116.]

9.0 Open Space, Landscaping, and Site Lighting Design Standards

- d. Protection of canyons from erosion through control of developed flows and through stabilization techniques that are consistent with the visual character of the open space.

9.7.9. **Plant Lists**

- (i) **Purpose/Intent:** ~~The purpose of regulating plants is to reduce water use, maintain the character of native plants now existing in the Petroglyph National Monument, and provide a harmonious landscape.~~
- (ii) **Landscaping:** ~~Appropriate plants shall be chosen from the Plant List in Table 9.5 for landscaping within the Plan area. Shrubs and trees shall be nursery grown in order to minimize poaching from the Monument and Major Public Open Space.~~
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- (iii) **Streetscaping:** ~~See Section 11.3.5 starting on page 206.~~
- (iv) **Construction Mitigation:** ~~Land disturbed in development shall be re-vegetated using either native or xeric plants as appropriate from the Plant List in Table 9.5. [See also Section 6.6 starting on page 116.]~~



10.0 Street Standards

[Note: A text amendment in 2014 added cross sections, an additional map, and some text changes, which affected the page numbers for the remainder of this Plan.]

10.0 Street Standards

- 10.3.2. **Primary Streets:** The character of streets in Volcano Heights will vary based on location. The Primary Streets and Designations Map [see **Exhibit 10.1**] illustrates the street network within the Plan area. Street alignments as shown coincide with the existing 20-foot access easement on the edge of each parcel wherever possible. **Section 10.6 starting on page 171** includes cross sections for the typical configuration of each **Primary Street** type. The specifications address vehicular lane width, parkway widths, **ROW** widths, number of travel lanes, on-street parking, and pedestrian accommodation.
- (i) Cross sections for each type of **Primary Street** depict specific elements and associated dimensions. The arrangement of these elements may change to suit local conditions, particularly when a **Primary Street** is intended for **Bus Rapid Transit (BRT)**. The final location of these lanes — whether the **BRT** will run in the median, the outside, or the inside lanes, or a combination of the above based on **adjacent** land uses, ROW, etc. — will be determined during the road design process.
 - (ii) See **Table 10.1** for allowable adjustments to the **Primary Street** standards. Adjustments should be considered carefully to ensure that the intended character of each street type is maintained or enhanced.
 - (iii) See **Section 10.5.4 starting on page 170 for more on intersection design.**
- 10.3.3. **Secondary Streets:** This Plan specifies standards for all new local streets in Volcano Heights in **Section 10.7 starting on page 197**. The platting and construction of new streets will be addressed on a project-by-project basis and reviewed by the City Department of Municipal Development (DMD) **and/or the City Engineer, as relevant.**

10.0 Street Standards

TABLE 10.2 – STREET TYPES AND CROSS SECTIONS

Cross Section	Programmed ROW	# Vehicular Lanes	Vehicular Travel Lane Widths / Transit Lane Widths	Bike Lane / Buffer*	On-Street Parking*	Pedestrian Sidewalk Width	Landscape / Tree Well	Applicable Sector Plan Area
ST 1.1: Typical Retail Collector <i>Applicable Streets:</i> <ul style="list-style-type: none"> Rosa Parks Universe Blvd. south of Rosa Parks 	58 feet	2	10 feet	None	7 feet	12 feet	(Within sidewalk width)	VCSDP
ST 1.2: Typical Residential Collector <i>Applicable Streets:</i> <ul style="list-style-type: none"> Kimmick Dr. 	58 feet	2	10 feet	None	7 feet	6 feet**	6 feet	VCSDP
ST 2.1: Neighborhood Street A	70 feet	2	11 feet	4 feet / 3 feet	7 feet	10 feet	(Within sidewalk width where required)	VHSDP
ST 2.2: Neighborhood Street B <i>Applicable Streets:</i> <ul style="list-style-type: none"> Kimmick from Paseo to Rosa Parks Calle Nortena from Paseo to Valiente 	72 feet	2 (+ 14' median/turn bay)	10 feet	None	7 feet	6 feet	6 feet	VCSDP
ST 2.3: Neighborhood Street C <i>Applicable Street: Urraca Rd.</i>	64 feet	2	10 feet	None	7 feet	5 feet	5 feet	VCSDP

* Includes curb and gutter dimension where element is at the edge of the paved section of the roadway.

** Except where platted at 5 feet at the time of the original adoption of this Plan (May 2011), in which case the total programmed ROW is reduced by 2 feet overall.

10.0 Street Standards

TABLE 10.2 – STREET TYPES AND CROSS SECTIONS (CONT'D)

Cross Section	Programmed ROW	# Vehicular Lanes	Vehicular Travel Lane Widths / Transit Lane Widths	Bike Lane / Buffer*	On-Street Parking*	Pedestrian Sidewalk Width	Landscape / Tree Well	Applicable Sector Plan Area
ST 3: Minor Arterial <i>Applicable Street: Universe Blvd.</i>	77 feet	2 (+ 16' median/center turn bay)	11 feet	5 feet	None	7 feet	5 feet	VCSDP VHSDP
ST 4: Connector Street	84 feet	2 (+ 12' median/turn bay)	10 feet	4 feet / 3 feet	7 feet	12 feet	(optional within sidewalk width)	VHSDP
ST 5: Suburban Boulevard <i>Applicable Streets:</i> • <i>Unser Boulevard at Escarpment</i> • <i>Rainbow Boulevard</i>	128 feet	4	11 feet	7 feet	None	10 feet	10 feet	VCSDP
ST 6: Paseo del Norte at Escarpment	135 feet	4 (+2 transit lanes)	11 / 12 feet	9 feet	None	(See cross section)	None	VCSDP VHSDP

* Includes curb and gutter dimension where element is at the edge of the paved section of the roadway.

10.0 Street Standards

TABLE 10.2 – STREET TYPES AND CROSS SECTIONS (CONT'D)

Cross Section	Programmed ROW	# Vehicular Lanes	Vehicular Travel Lane Widths / Transit Lane Widths	Bike Lane / Buffer*	On-Street Parking*	Pedestrian Sidewalk Width	Landscape / Tree Well	Applicable Sector Plan Area
ST 7.1: Urban Boulevard A <i>Applicable Streets:</i> <ul style="list-style-type: none"> Paseo del Norte from Calle Nortena to Kimmick Unser Boulevard from southern boundary of Volcano Cliffs to Escarpment Unser Boulevard from Kimmick to Woodmont 	156 feet	4 (+ 50' median)	11 feet	6 feet	None	10 feet	10 feet	VCSDP
ST 7.2: Urban Boulevard B <i>Applicable Streets:</i> <ul style="list-style-type: none"> Unser Boulevard from Escarpment to Kimmick 	156 feet	4 (+36' median)	12 feet	8 feet	None	None	None	VCSDP
ST 7.3: Urban Boulevard C <i>Applicable Streets:</i> <ul style="list-style-type: none"> Unser Boulevard from Woodmont to northernmost right-in/right-out in Volcano Heights 	156 feet	4 (+2 slip lanes)	11 / 12 feet	4 feet / 2 feet	None	(See cross section)	(See cross section)	VHSDP
ST 7.4: Urban Boulevard D <i>Applicable Street:</i> Paseo del Norte from Kimmick to western boundary of Volcano Trails	156 feet	6 (+2 transit lanes)	11 / 12 feet	6 feet / 2 feet	None	10 feet	(Within sidewalk width)	VHSDP VTSDP

* Includes curb and gutter dimension where element is at the edge of the paved section of the roadway.

10.0 Street Standards

TABLE 10.2 – STREET TYPES AND CROSS SECTIONS (CONT'D)

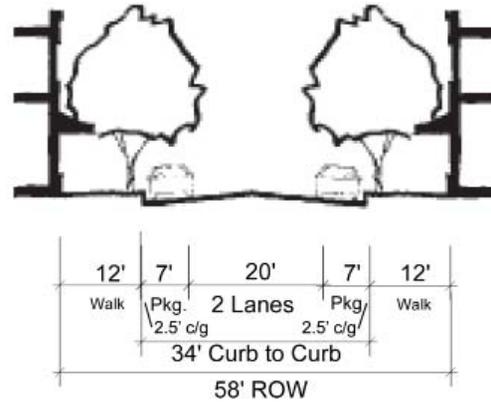
Cross Section	Programmed ROW	# Vehicular Lanes	Vehicular Travel Lane Widths / Transit Lane Widths	Bike Lane / Buffer*	On-Street Parking*	Pedestrian Sidewalk Width	Landscape / Tree Well	Applicable Sector Plan Area
ST 8: Transit Boulevard	120 feet	2 (+2 transit lanes)	10 / 12 feet	4 feet / 3 feet	7 feet	12 feet	6 feet	VHSDP
ST 9: Town Center	96 feet	2	11 feet	4 feet / 3 feet	18 feet (reverse-angle)	12 feet	(Within sidewalk width)	VHSDP
ST 10.1: Park Edge – Single-loaded	76 feet	2 (+12' median/turn bay)	11 feet	6.5 feet / 2.5 feet	7 feet (one side only)	10 feet	(Within sidewalk width)	VHSDP
ST 10.2: Park Edge – Double-loaded	70 feet	2	11 feet	4 feet / 3 feet	7 feet	10 feet	(Within sidewalk width)	VHSDP
ST 11: Local Street	52 feet	2	9 feet	None	6 feet	6 feet**	5 feet	VCSDP

* Includes curb and gutter dimension where element is at the edge of the paved section of the roadway.

10.0 Street Standards

[Note: Street Types 1.1 and 1.2 are not used within the Volcano Heights SDP]

ST 1.1: Retail Collector

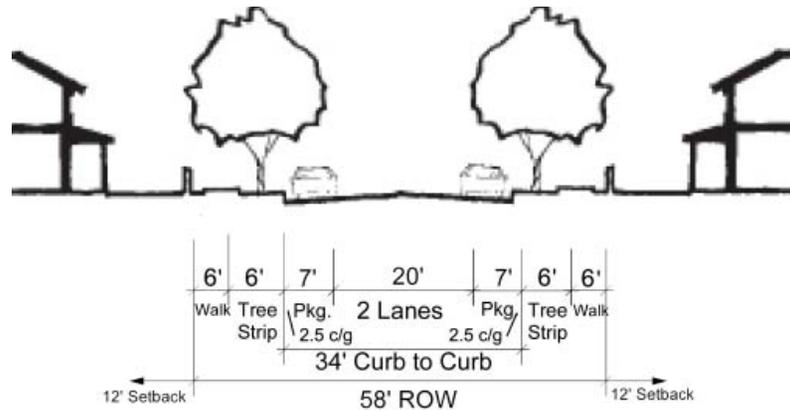


Street Type 1.1 – Typical Retail Collector (2 Lanes)

* Note: c/g stands for “curb and gutter”

10

ST 1.2: Residential Collector



Street Type 1.2

10.0 Street Standards

- 10.6.1. **Street Type 2.1: Neighborhood Street A**
- (i) **Intent/Purpose:** These streets are intended to access local uses, predominantly businesses and residences within the Plan area.
 - (ii) **Cross Section:** See **Exhibit 10.8.**
 - (iii) **Frontage Standards:** See Site Development Standards in **Section 5 starting on page 77.**
 - (iv) **Streetscape Standards:** See **Section 11 starting on page 203.**

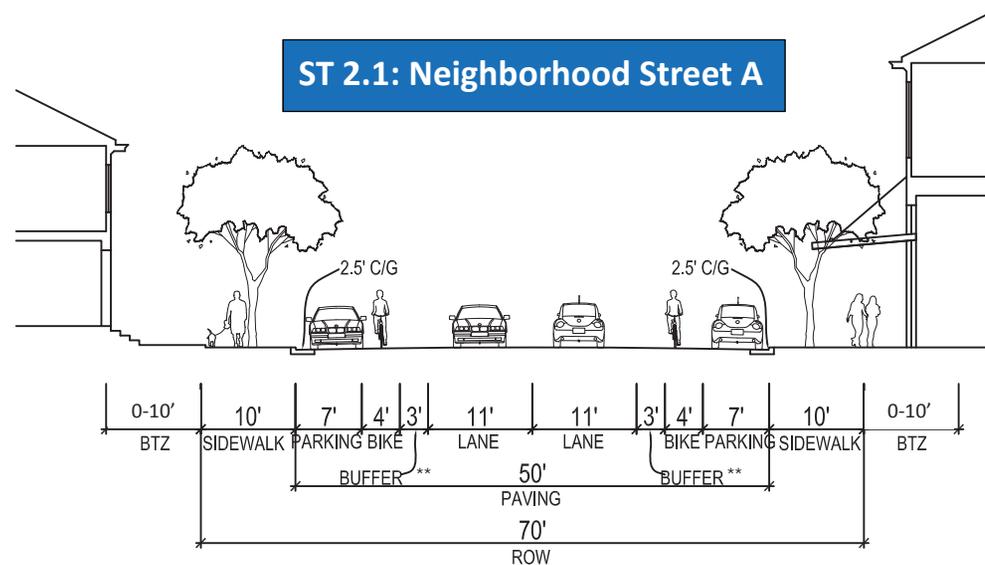


Exhibit 10.4 – Street Type 2.1: Neighborhood Street A

* Note: C/G = Curb and Gutter

** Note: Buffer = Separation between the bicycle and vehicle lanes.

10.0 Street Standards

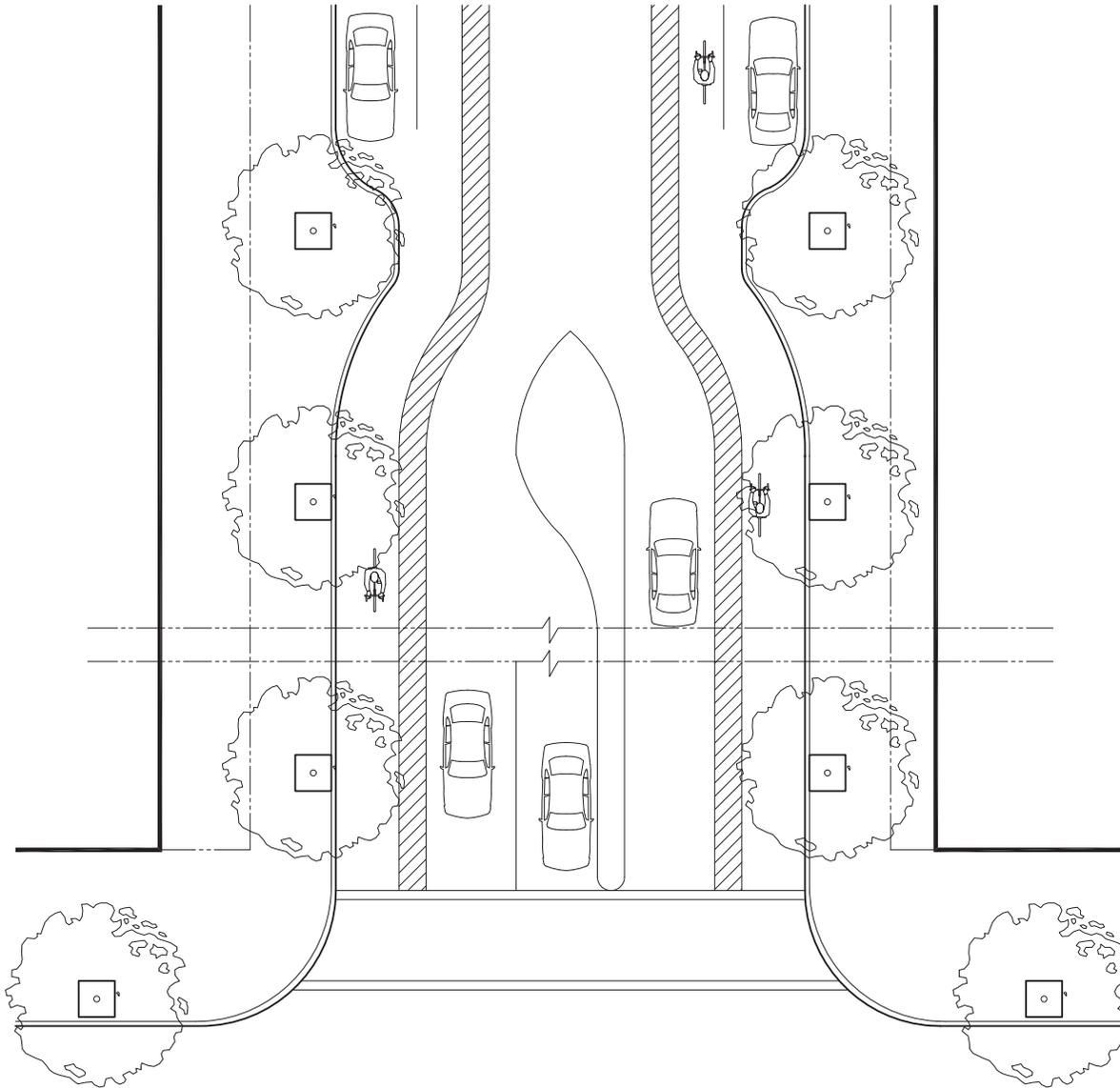


Exhibit 10.5 – Street Type 2.1: Neighborhood Street A – Typical Intersection (Plan View)

10.0 Street Standards

ST 2.2: Neighborhood Street B

[Note: Street Types 2.2 and 2.3 are not used within the Volcano Heights SDP]

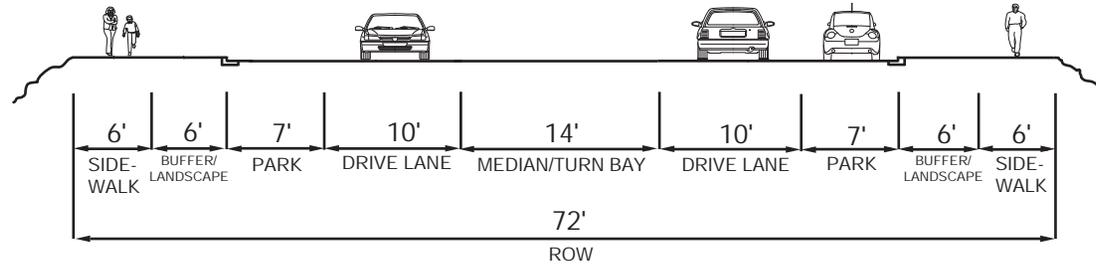


Exhibit 10.6 – Street Type 2.2: Neighborhood Street B

* Note: C/G = Curb and Gutter

** Note: Buffer = Separation between the bicycle and vehicle lanes.

ST 2.3: Neighborhood Street C

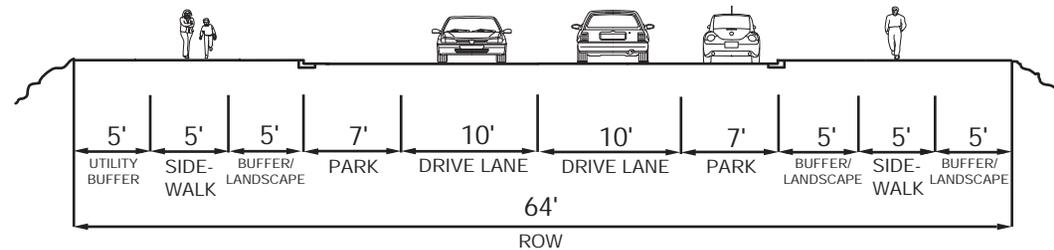


Exhibit 10.7 – Street Type 2.3: Neighborhood Street C

10.0 Street Standards

10.6.2. **Street Type 3: Minor Arterial**

- (i) **Intent/Purpose:** Universe Boulevard is a minor arterial carrying significant traffic volumes to facilitate regional movement. At the same time, it connects many predominantly residential areas and therefore must be safe and comfortable for pedestrian and cyclists.
- (ii) **Cross Section:** See **Exhibit 10.18.**
- (iii) **Frontage Standards:** See Site Development Standards in **Section 5 starting on page 77.**
- (iv) **Streetscape Standards:** See **Section 11 starting on page 203.**

ST 3: Minor Arterial

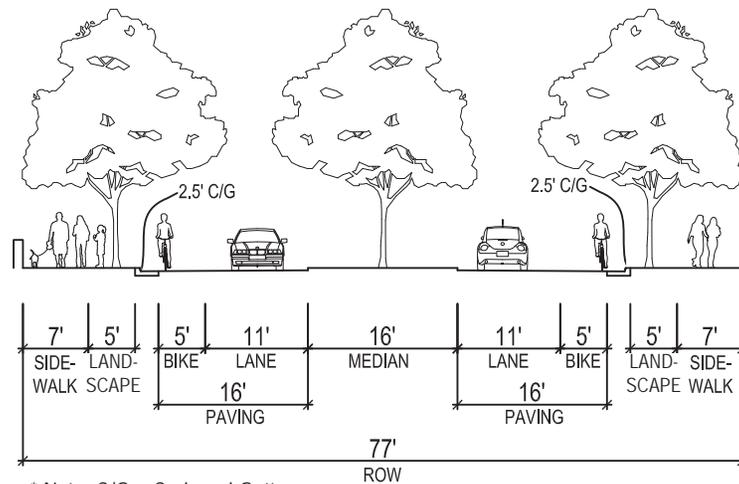


Exhibit 10.8 – Street Type 3: Minor Arterial

10.0 Street Standards

10.6.3. **Street Type 4: Connector Street**

- (i) **Intent/Purpose:** These streets are intended to access **neighborhood streets** and promote multi-modal transportation to reach businesses and residences within the Plan area.
- (ii) **Cross Section:** See **Exhibit 10.6**.
- (iii) **Frontage Standards:** See Site Development Standards in **Section 5 starting on page 77**.
- (iv) **Streetscape Standards:** See **Section 11 starting on page 203**.

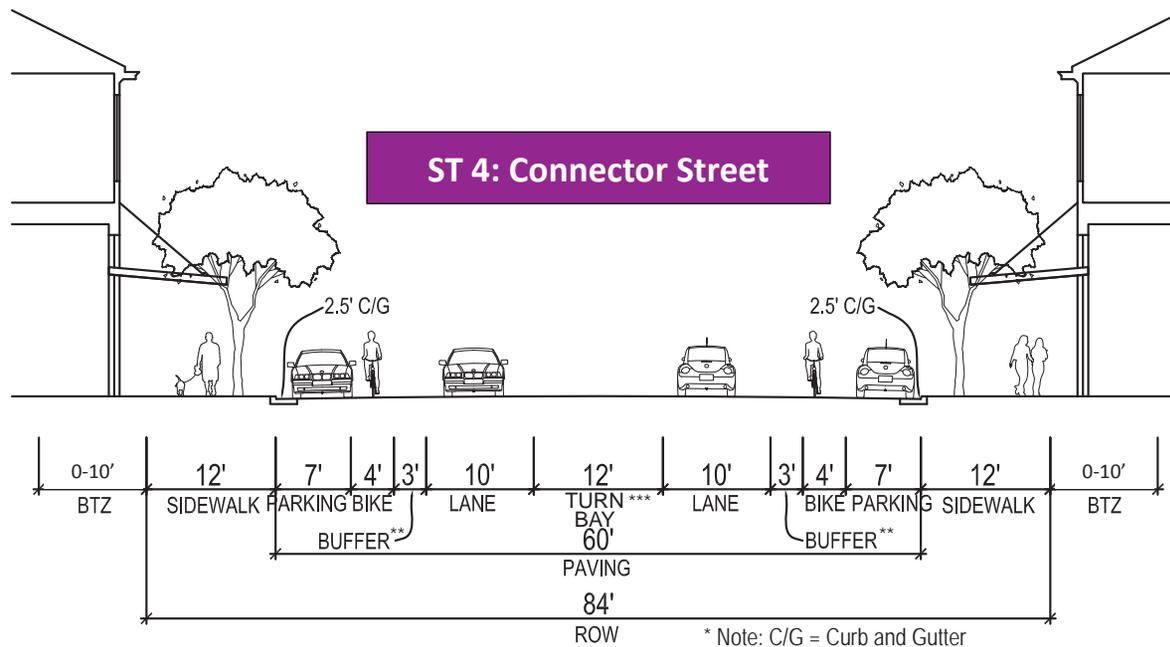
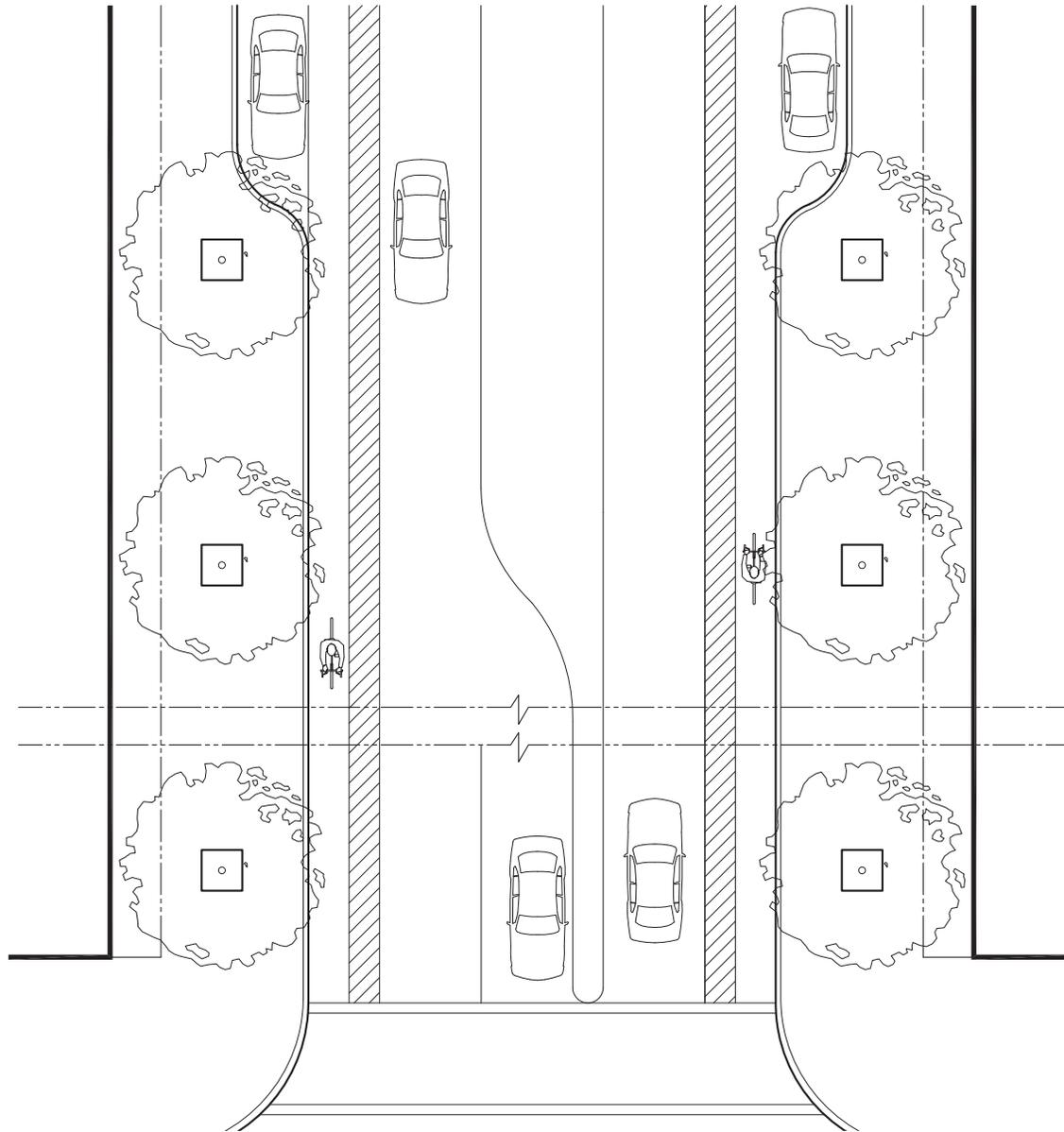


Exhibit 10.9 – Street Type 4: Connector Street

* Note: C/G = Curb and Gutter
 ** Note: Buffer = Separation between the bicycle and vehicle lanes.
 *** Note: The center lane is a two-way left-turning lane and/or median as appropriate.

10.0 Street Standards



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Exhibit 10.10 – Street Type 4: Connector Street – Typical Intersection (Plan View)

[Note: Street Type 5 is not used within the Volcano Heights SDP]

ST 5: Suburban Boulevard

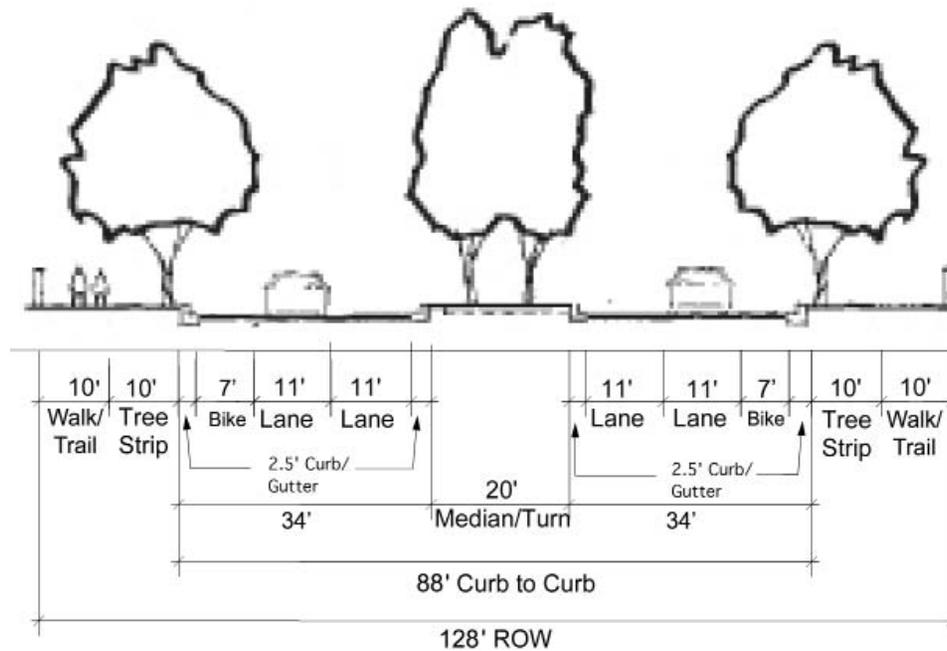
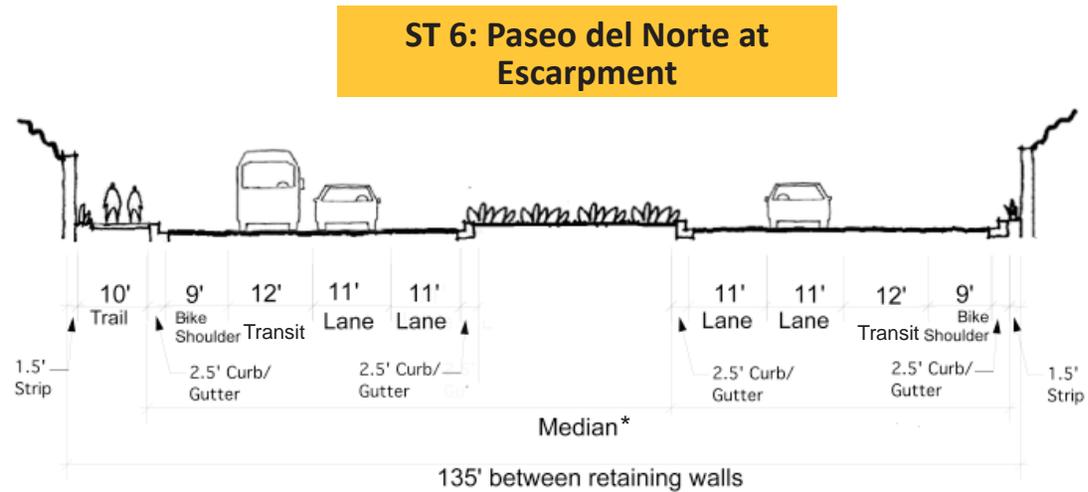


Exhibit 10.11 – Street Type 5: Suburban Boulevard

10.0 Street Standards

[Note: Street Type 6 is not used within the Volcano Heights SDP]



** Note: Median width varies according to right-of way through the Petroglyph National Monument*

Exhibit 10.12 – Street Type 6: Paseo del Norte at Escarpment (6 lanes with Transit)

ST 7.1: Urban Boulevard A

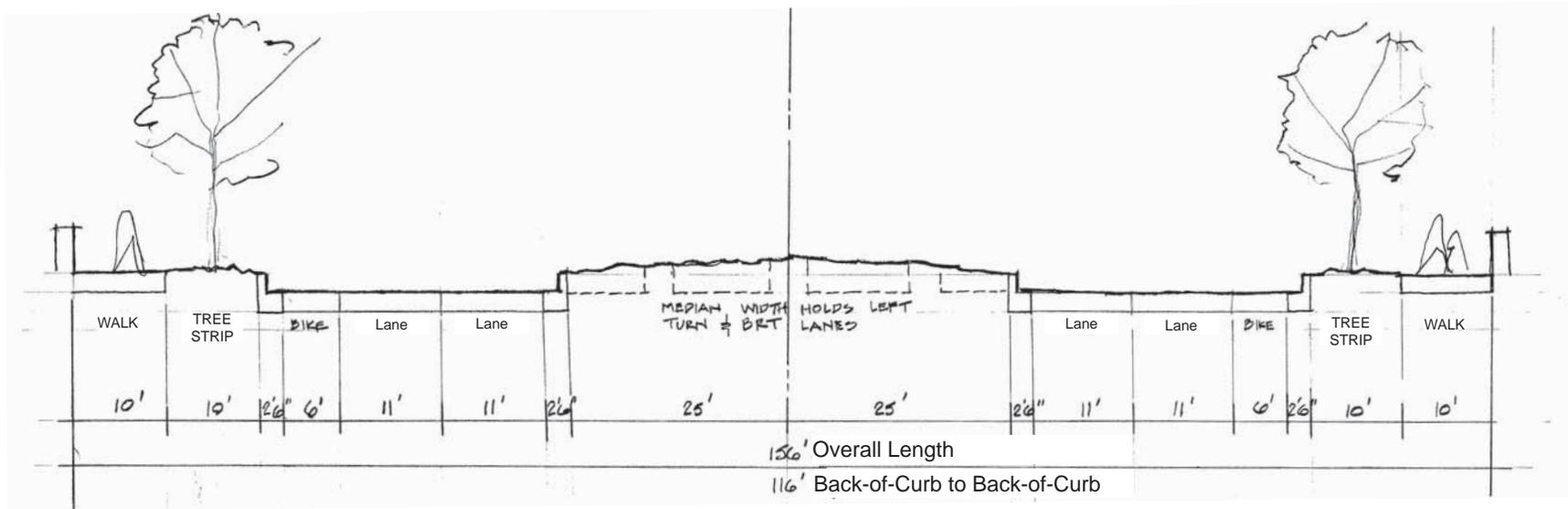
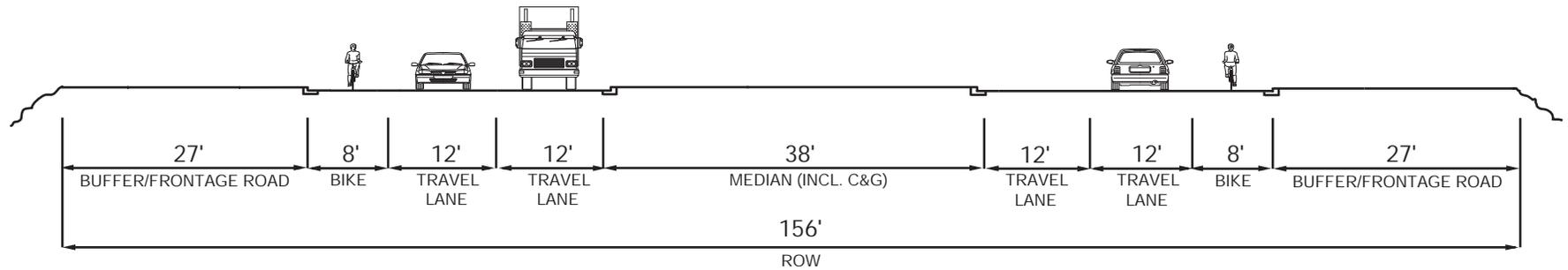


Exhibit 10.13 – Street Type 7.1: Urban Boulevard A

10.0 Street Standards

[Note: Street Type 7.2 is not used within the Volcano Heights SDP]

ST 7.2: Urban Boulevard B



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Exhibit 10.14 – Street Type 7.2 – Urban Boulevard B

10.0 Street Standards

10.6.4. **Street Type 7.3:** Urban Boulevard C

(i) **Intent/Purpose:** Unser Boulevard is primarily a regional road, serving residents and businesses within Volcano Heights as well as the surrounding region.

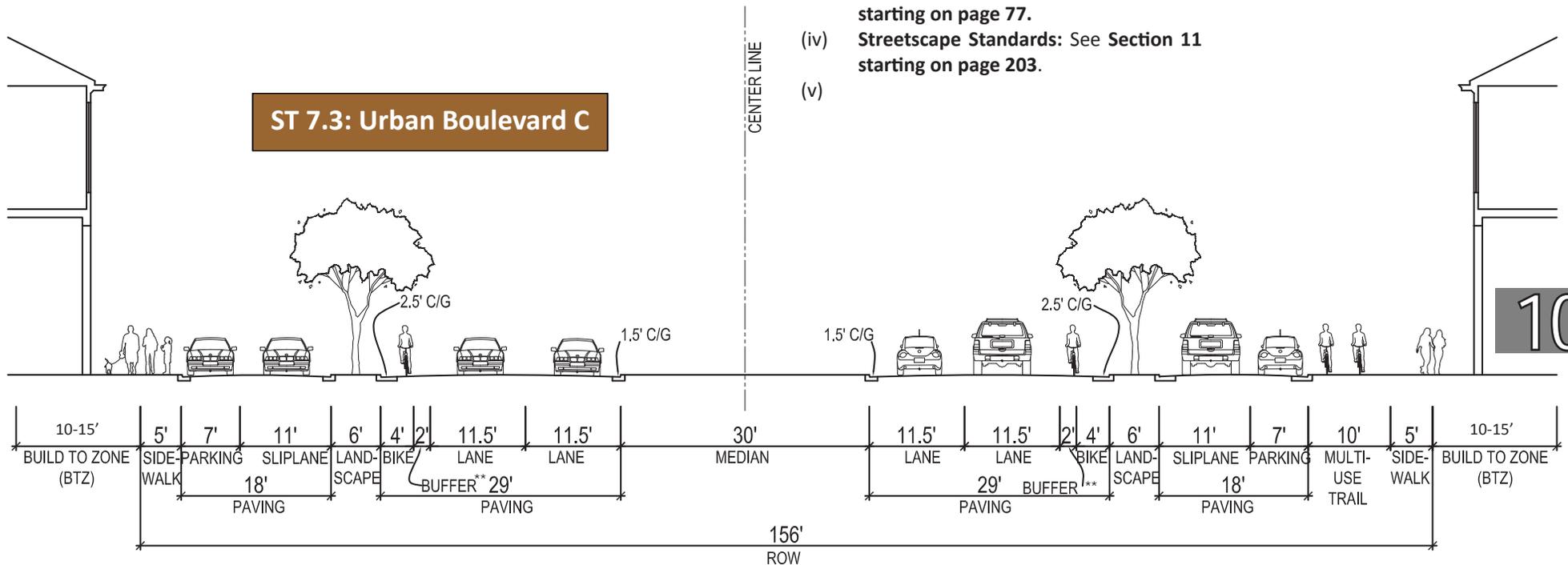
(ii) **Cross Section**

- a. See **Exhibit 10.16**.
- b. Slip lanes are for one-way movement only. Directional signage is needed.
- c. Beginning 500 feet from an intersection with Paseo del Norte, the Transit Boulevard, or any potential station locations, an extra 36 feet in the **ROW** may be needed for **BRT** lanes and/or station platforms.

(iii) **Frontage Standards:** See Site Development Standards in **Section 5** starting on page 77.

(iv) **Streetscape Standards:** See **Section 11** starting on page 203.

(v)



* Note: C/G = Curb and Gutter

** Note: Buffer = Separation between the bicycle and vehicle lanes.

Exhibit 10.15 – Street Type 7.3: Urban Boulevard C

10.0 Street Standards

- 10.6.5. **Street Type 7.4: Urban Boulevard D**
- (i) **Intent/Purpose:** Paseo del Norte is primarily a regional road, serving residents and businesses within Volcano Heights as well as the surrounding region.

- (ii) **Cross Section**
 - a. See **Exhibit 10.17.**
 - b. Beginning 500 feet from an intersection with Unser Boulevard, the Transit Boulevard, or any potential station locations, 36 feet in the **ROW** may be needed for **BRT** lanes and/or station platforms.
- (iii) **Frontage Standards:** See Site Development Standards in **Section 5 starting on page 77.**
- (iv) **Streetscape Standards:** See **Section 11 starting on page 203.**

ST 7.4: Urban Boulevard D

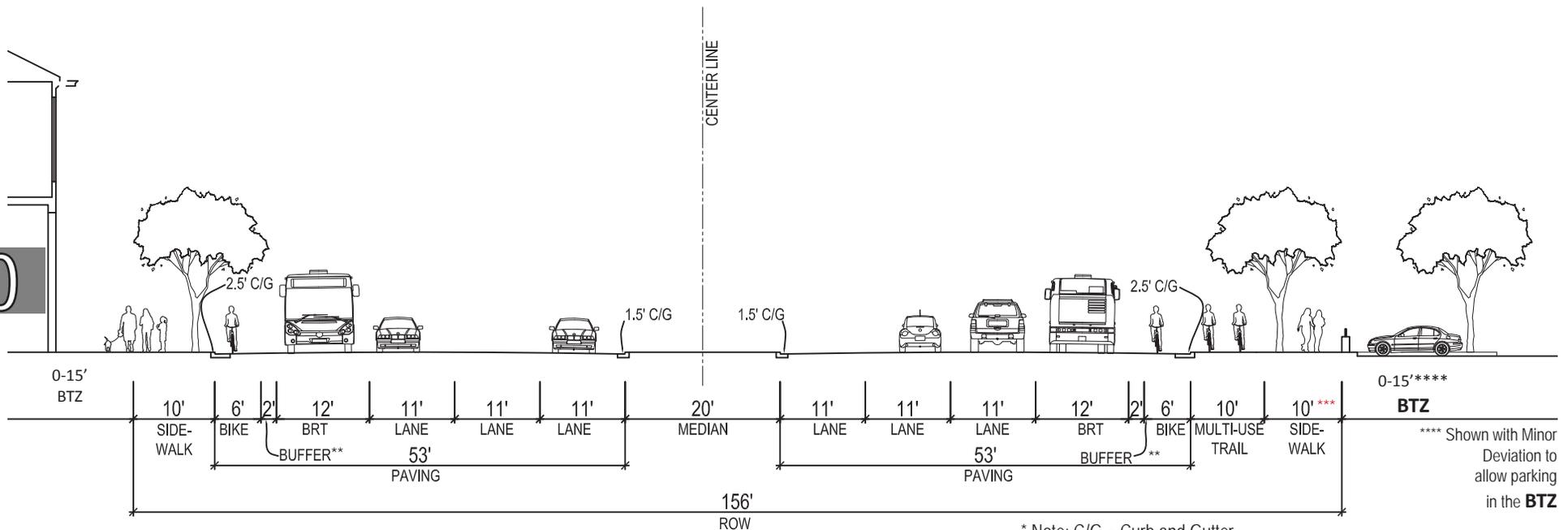


Exhibit 10.16 – Street Type 7.4: Urban Boulevard D

* Note: C/G = Curb and Gutter
 ** Note: Buffer = Separation between the bicycle and vehicle lanes.
 *** Note: Width may vary to accommodate trail and/or trail buffer.
 **** Shown with Minor Deviation to allow parking in the BTZ

10.0 Street Standards

10.6.6. **Street Type 8: Transit Boulevard**

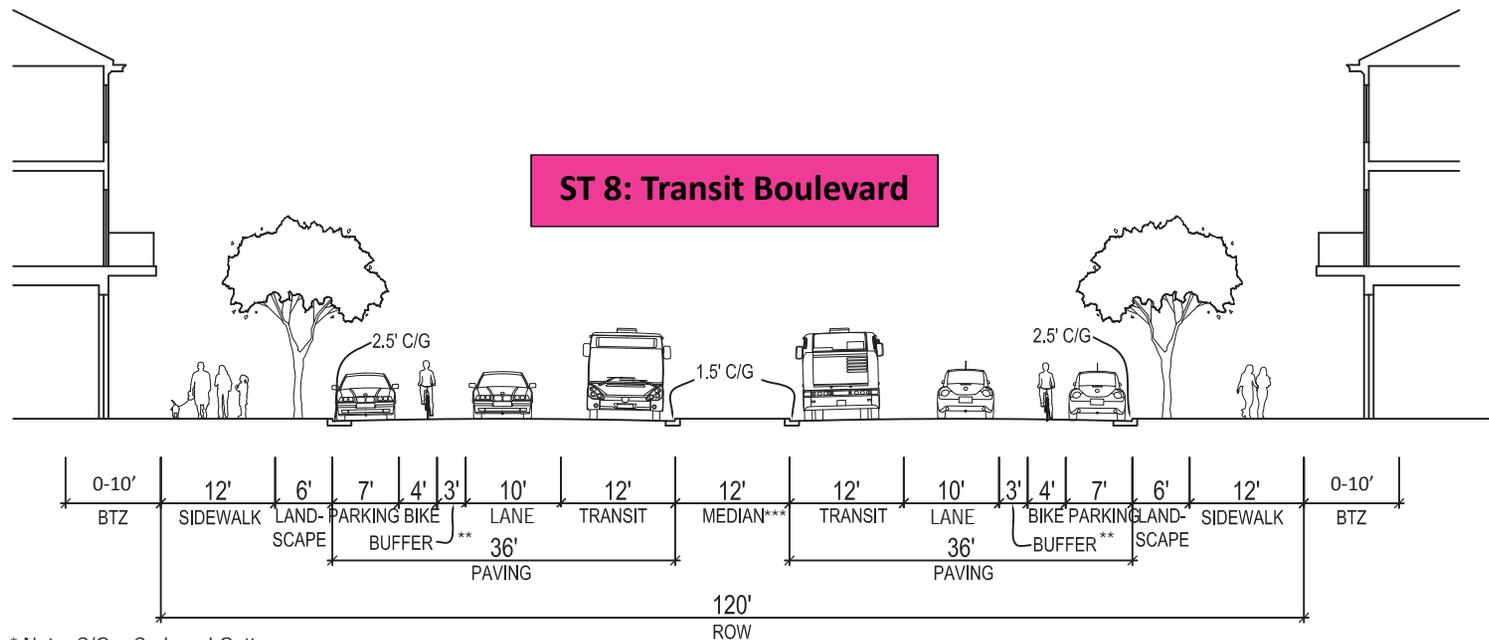
(i) **Intent/Purpose:** The Transit Boulevard serves multiple modes of transportation, including the proposed **BRT**. The walkable, dense, urban Town Center is organized around this Transit Boulevard, which acts as a “Main Street” for Volcano Heights.

(ii) **Cross Section**

a. See **Exhibit 10.14**.
 b. Beginning 500 feet from intersections on Paseo del Norte, Unser Boulevard, and any other potential station locations, an extra 36 feet of ROW may be needed for **BRT** lanes and/or station platforms.

(iii) **Frontage Standards:** See Site Development Standards in **Section 5** starting on page 77.

(iv) **Streetscape Standards:** See **Section 11** starting on page 203.



* Note: C/G = Curb and Gutter

** Note: Buffer = Separation between the bicycle and vehicle lanes.

*** Note: Median becomes Turn Bay or equivalent in 'B' Street segments.

Exhibit 10.17 – Street Type 8: Transit Boulevard

10.0 Street Standards

ST 8: Transit Boulevard

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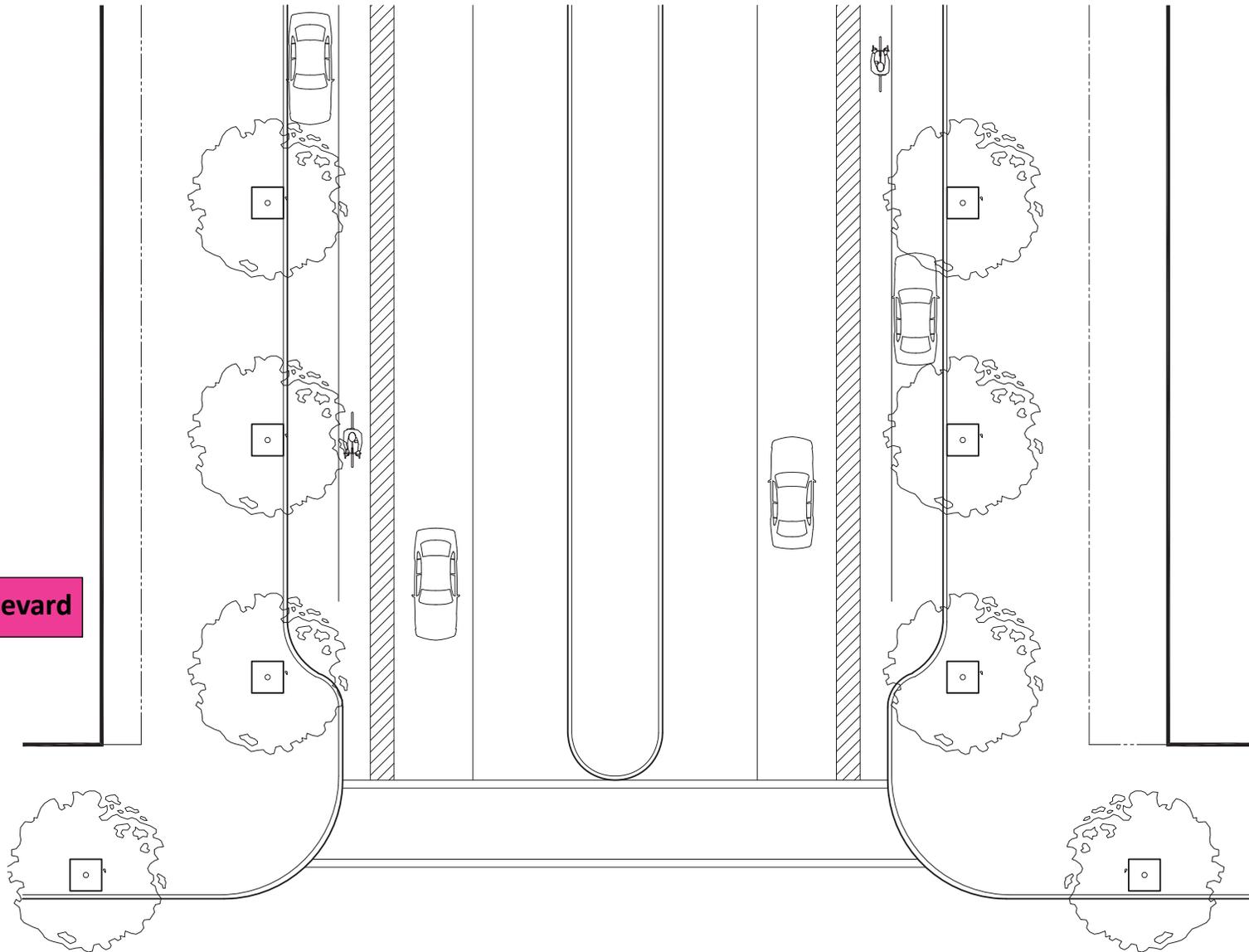
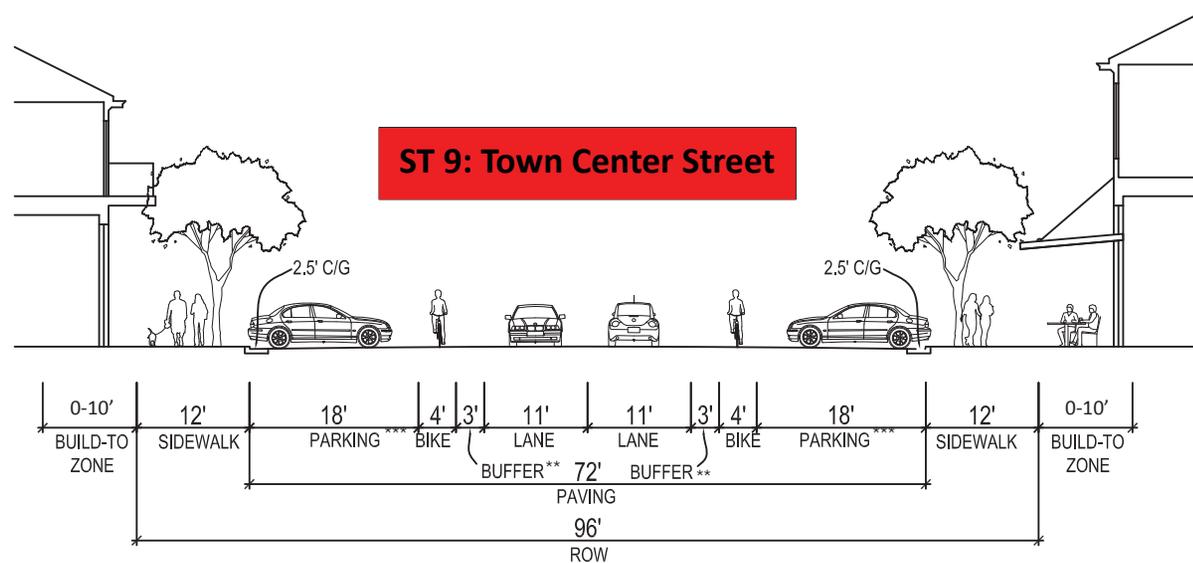


Exhibit 10.18 – Street Type 8: Typical Intersection (Plan View)

10.0 Street Standards

10.6.7. **Street Type 9: Town Center Street**

- (i) **Intent/Purpose:** These streets are intended to be the most pedestrian-friendly while supporting multiple modes of transportation circulating throughout the Plan area and surrounding region.
- (ii) **Cross Section:** See **Exhibit 10.4.**
- (iii) **Frontage Standards:** See Site Development Standards in **Section 5.1** starting on page 78.
- (iv) **Streetscape Standards:** See **Section 11** starting on page 203.
- (v)



* Note: C/G = Curb and Gutter

** Note: Buffer = Separation between the bicycle and vehicle lanes.

*** Note: Parking shown is reverse-angle parking. See **Section 13.3.10(viii)**.

Exhibit 10.19 – Street Type 9: Town Center Street

10.0 Street Standards

ST 9: Town Center Street

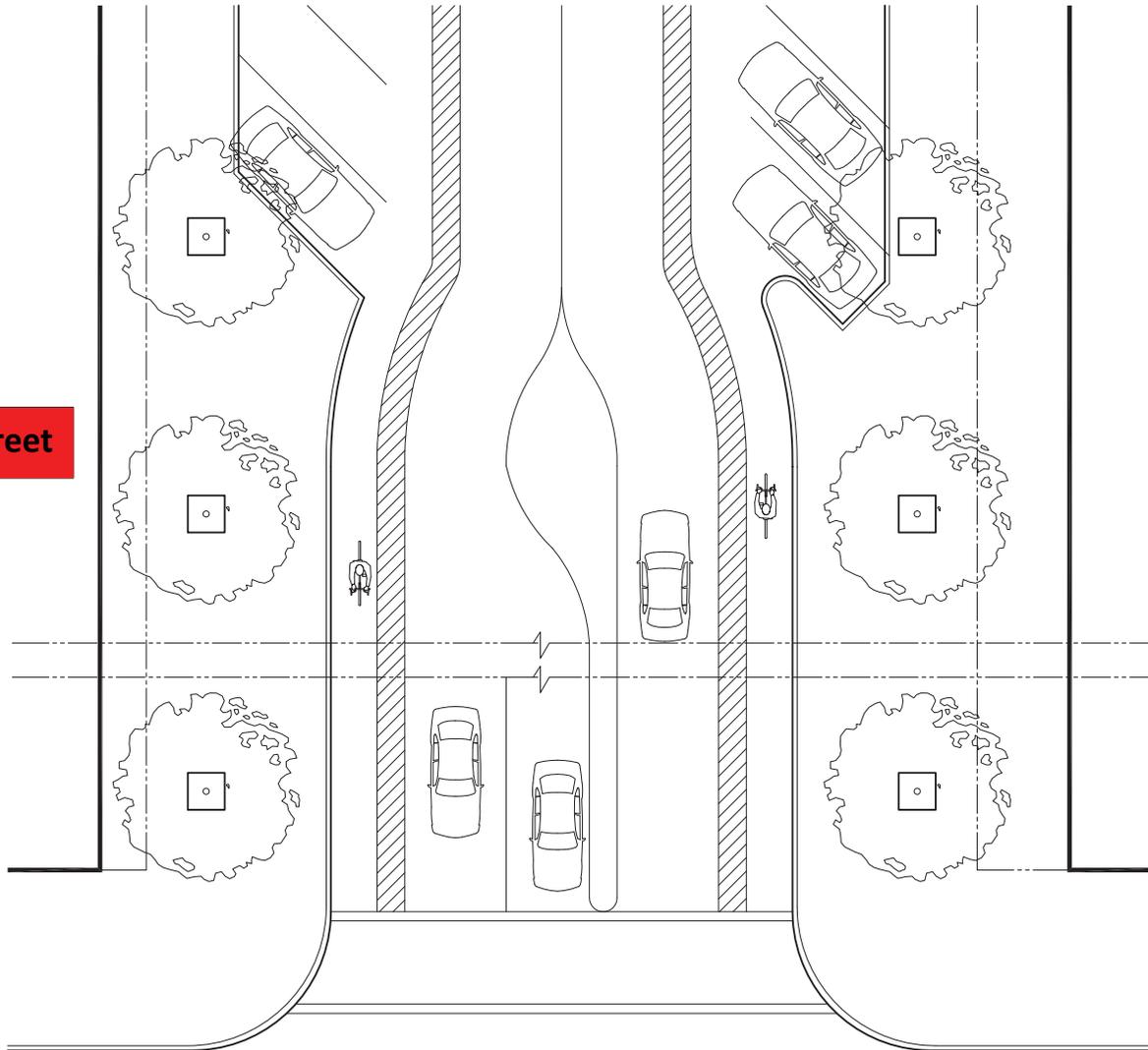
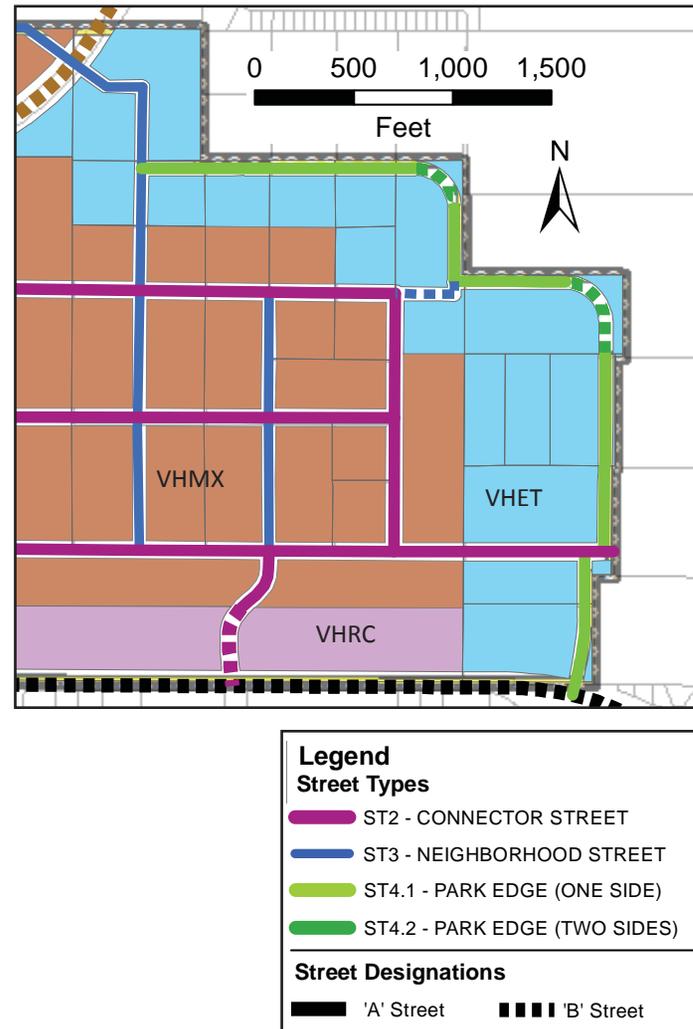


Exhibit 10.20 – Street Type 9: Typical Intersection (Plan View)

10.0 Street Standards

10.6.8. Street Type 10: Park Edge

- (i) **Intent/Purpose:** The Park Edge road is intended to access local development, predominantly residences and businesses within the Plan area, by multiple modes of transportation, including on-street bicycle lanes as well as connections to an off-street trail along the Petroglyph National Monument. **Major Public Open Space** is best enhanced and protected as a public amenity when buffered from development by a **single-loaded road** along Major Public Open Space edges (i.e. development only occurs on the side of the road farthest from the Major Public Open Space). The City Open Space Division and the National Park Service prefer the single-loaded road as the best transition between development and sensitive lands within the Petroglyph National Monument. Single-loaded roads increase safety for open space users and nearby property owners by providing visibility for surveillance and monitoring, as well as improving accessibility for park users. Single-loaded roads are also the most effective means of protecting important views into and out of the Monument, as well as to the Sandia Mountains to the east. The Park Edge road cross sections are designed to incorporate landscaping and medians that act as transitions from the built environment to **sensitive lands** within the Monument.



Detail of Exhibit 10.2 – Character Zones and Street Types: Park Edge Road

10.0 Street Standards

ST 10.1: Park Edge Single-Loaded

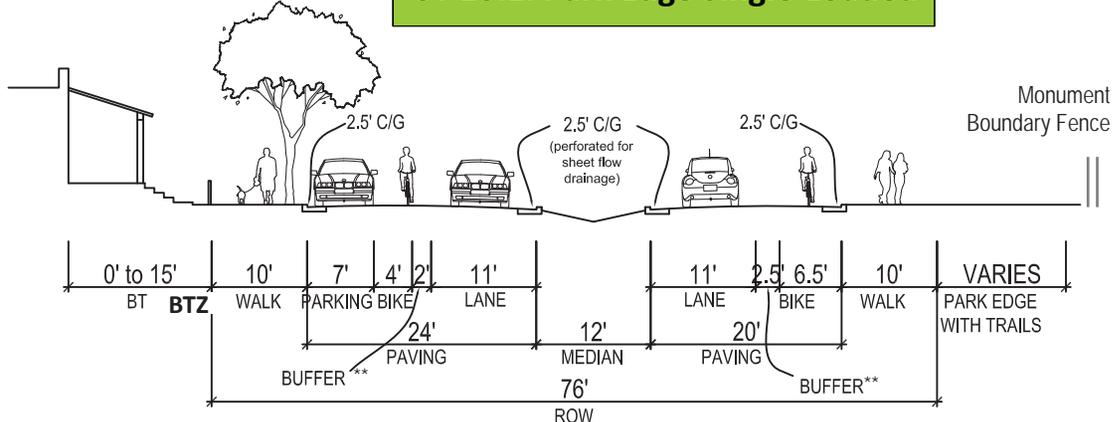


Exhibit 10.21 – Street Type 10.1: Park Edge Single-Loaded

* Note: C/G = Curb and Gutter
 ** Note: Buffer = Separation between the bicycle and vehicle lanes.

ST 10.2: Park Edge Single-Sided

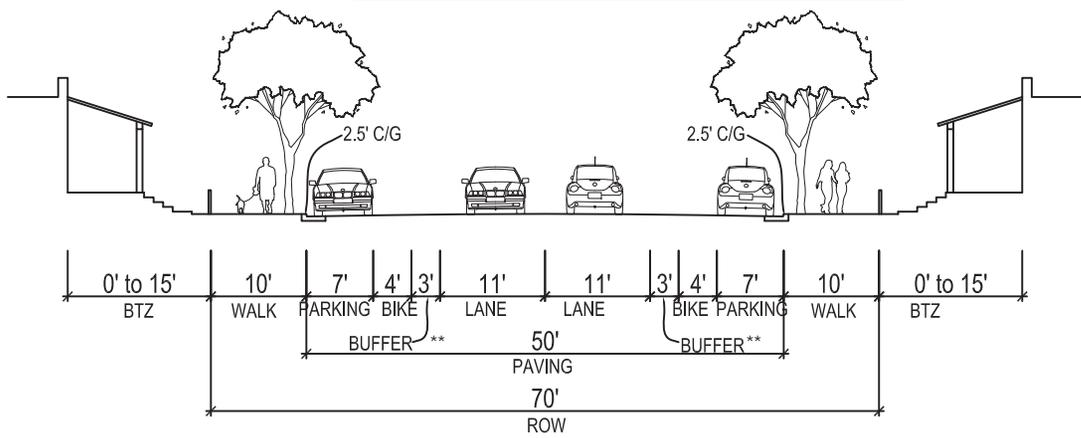


Exhibit 10.22 – Street Type 10.2: Park Edge Double-Loaded

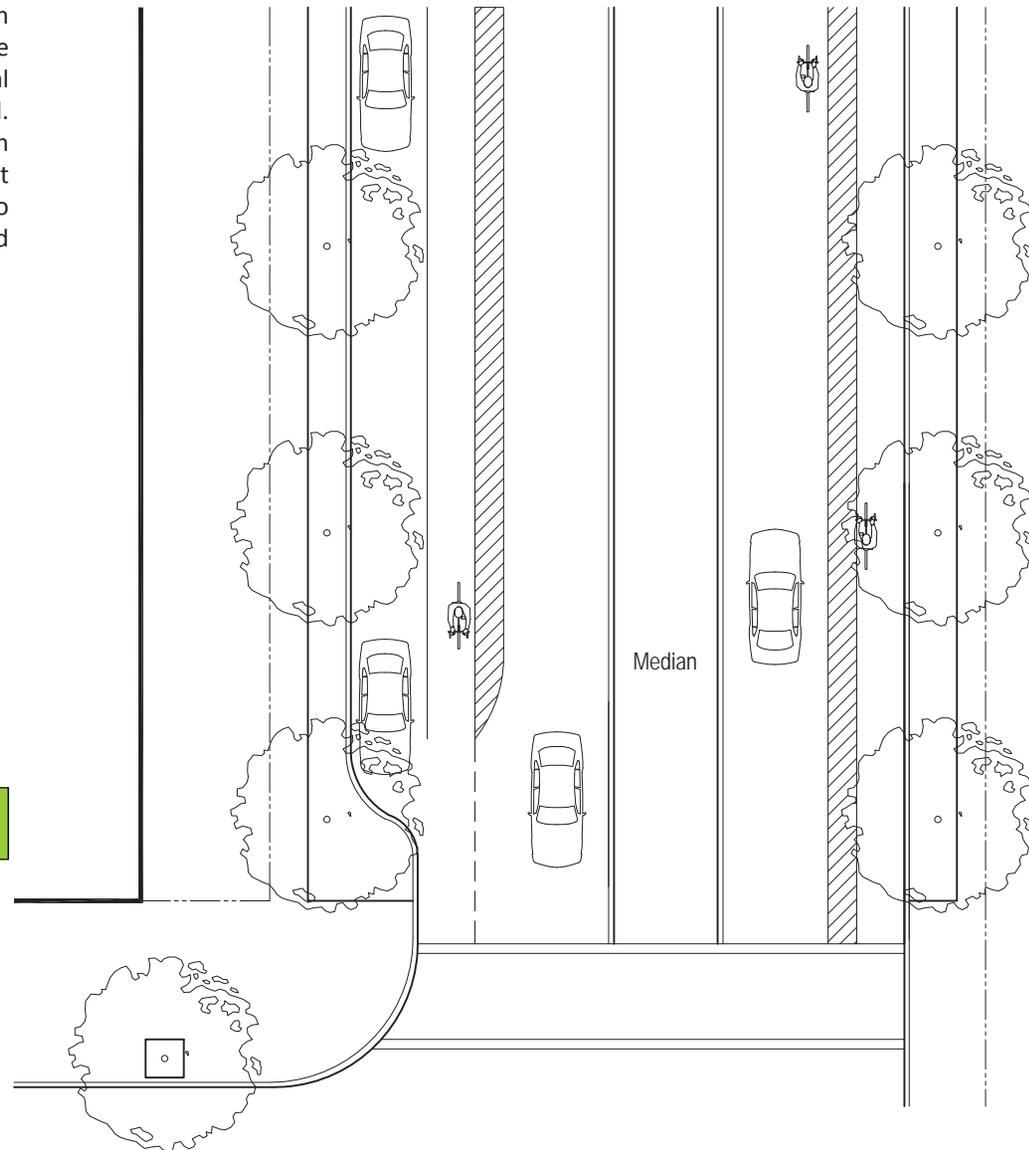
* Note: C/G = Curb and Gutter
 ** Note: Buffer = Separation between the bicycle and vehicle lanes.

- (ii) **Cross Sections:** Two cross sections are provided for the Park Edge Street.
 - a. Where the street abuts the Petroglyph National Monument and/or where development is only intended on the west side of the road, Street Type 4.1 is to be constructed. [See **Exhibit 10.10.**]
 - b. Where development will occur on both sides of the street, Street Type 4.2 is to be used. [See **Exhibit 10.11.**]
 - c. The Park Edge and additional east-west streets in the SU-2 Volcano Heights Escarpment Transition (VHET) zone should ~~shall~~ be sited to provide pedestrian access to the Petroglyph National Monument. Access shall be determined by the National Park Service Monument Visitor Plan and/or by the City Open Space Division in lieu thereof.
 - d. Where a median is incorporated, it should be perforated for hydrology and rainwater drainage and control, subject to approval by the City Hydrologist.
- (iii) **Frontage Standards:** See Site Development Standards in **Section 5 starting on page 77.**
- (iv) **Streetscape Standards:** See **Section 11 starting on page 203.**

10.0 Street Standards

- (v) **Linear pond/bioswale:** The median and/or eastern edge of the Park Edge Road is an appropriate and beneficial location for a bioswale/linear pond. Such a pond, designed in consultation with the City Engineer can help to meet the City’s water quality goals. [See also **Goal 12.5.5 starting on page 220** and **Policy 13.5.3 starting on page 239.**]

ST 10.1: Park Edge Single-Loaded



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Exhibit 10.23 – Street Type 10.1: Typical Intersection (Plan View)

10.0 Street Standards

ST 10.2: Park Edge Double-Loaded

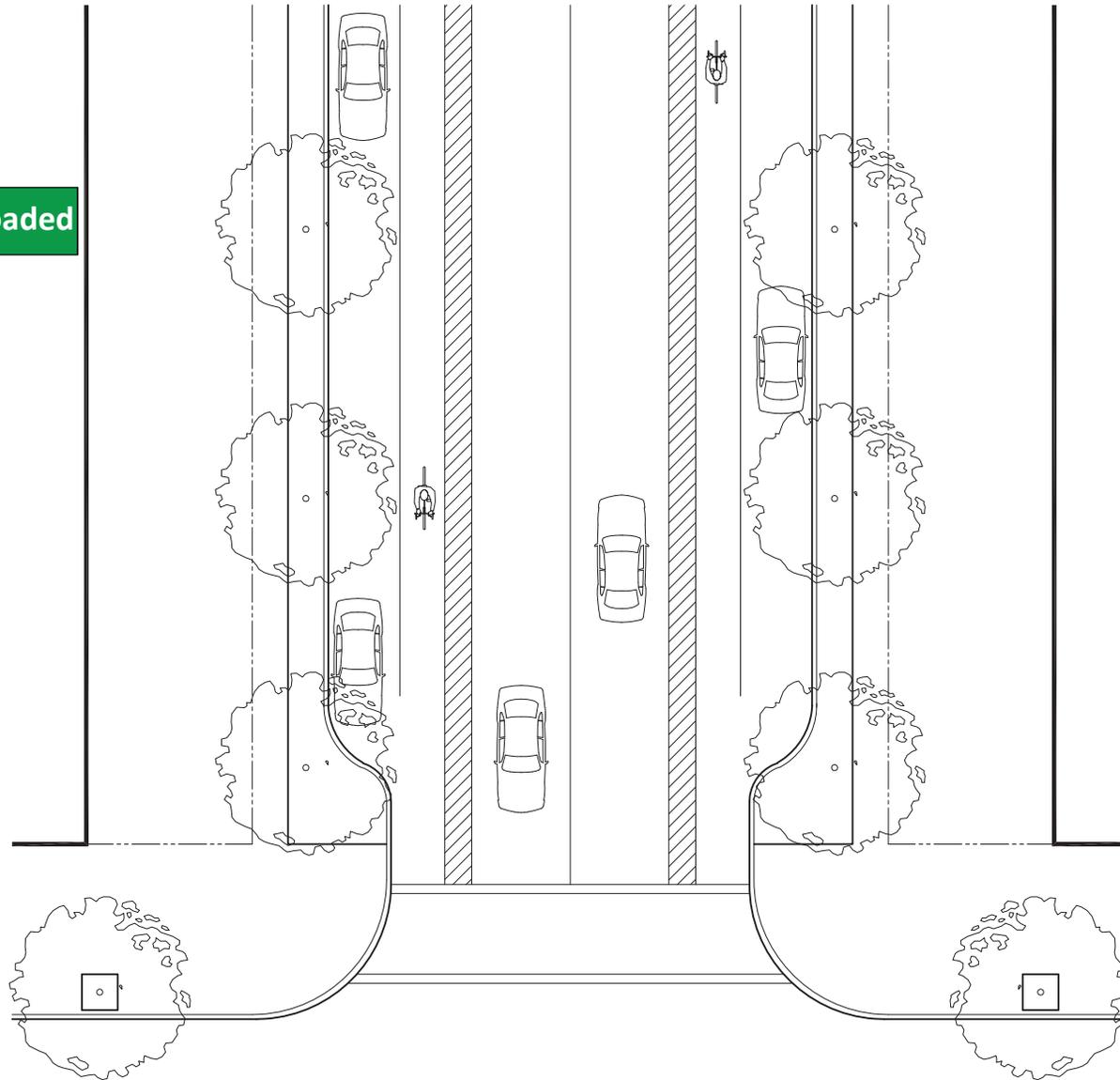


Exhibit 10.24 – Street Type 10.2: Typical Intersection (Plan View)

10.0 Street Standards

10.7. Secondary Street Design Standards

10.7.1. The platting of new dead-end streets and culs-de-sac that terminate the road is prohibited.

- (i) Stub streets or “knuckle” culs-de-sac are allowed where necessary to reach no more than 4 parcels beyond a corner or intersection.
- (ii) Mid-block “bubble” culs-de-sac without throats are allowed.
- (iii) Pedestrian/bike connections shall be provided to open space and/or road networks beyond knuckle or bubble culs-de-sac.

10.7.2. Required ‘A’ vs. ‘B’ Streets

(i) New development shall include Secondary Streets to serve projects where needed to supplement Primary Streets, per the requirements in Table 10.2, which includes both Primary and Secondary Streets.

a. ‘A’ vs. ‘B’ Percentage: The percentages given are ratios for the minimum number of ‘A’ Streets and maximum number for ‘B’ Streets. For example, for a project within Town Center, a minimum of 1 of every 2 Secondary Streets shall be planned as ‘A’ Streets. If more than half the streets are planned as ‘A’ Streets, the ‘B’ Street percentage would be reduced accordingly. In Regional Center, for example, at least 1 of every 4 roads shall be an ‘A’ Street. See Exhibit 10.19.

b. Connectivity: Secondary Streets added in Volcano Heights shall maintain or improve street connectivity to serve pedestrians as well as dispersing auto traffic.

- (ii) The first development in shall determine Secondary Street designation (i.e. whether ‘A’ or ‘B’) for the block. Streets shall be platted as such during the TIDD/SAD/PID and/or Site Development Plan for Subdivision process. Street designations may only change at intersections.



TABLE 10.2 – SECONDARY STREET REQUIREMENTS

Character Zone	Secondary Streets	
	‘A’ Street (min.)	‘B’ Street (max.)
Town Center	50%	50%
Regional Center	25%	75%
Village Center	25%	75%
Mixed Use	25%	75%
Neighborhood Transition	0%	100%
Escarpment Transition	25%	75%

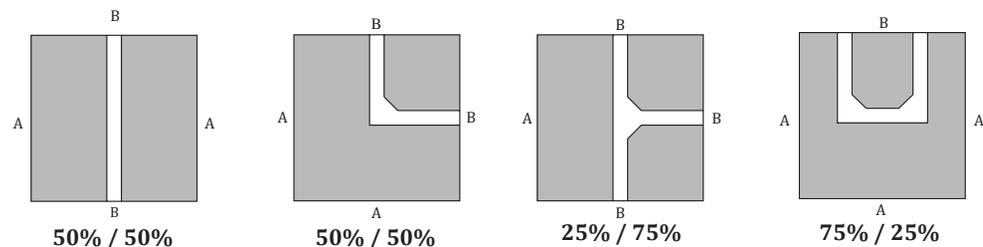


Exhibit 10.19 – Example Diagrams: Alley Configurations with ‘A’ vs. ‘B’ Street Percentages