

V. PLAN IMPLEMENTATION



PLAN IMPLEMENTATION / PROJECTS AND PROGRAMS

Abbreviations and Acronyms used in the following table:

APD = Albuquerque Police Department	MRCOG = Mid-Region Council of Governments
APS = Albuquerque Public Schools	NHHRC = Nob Hill Highland Renaissance Corporation
CIP = Capital Improvements Project	NHBA = Nob Hill Business Association
CNM = Central New Mexico Community College	NHNA = Nob Hill Neighborhood Association
COA = City of Albuquerque	NTMP = Neighborhood Traffic Management Program (COA)
DMD = Department of Municipal Development (COA)	RFP = Request for proposals
GABAC = Greater Albuquerque Bicycling Advisory Committee	SBA = Small Business Administration (federal)
GARTC = Greater Albuquerque Recreational Trails Committee	SHPO = State Historic Preservation Office
HBNA = Highland Business and Neighborhood Association	STEPS = Southeast Team for Entrepreneurial Success
L RTP = Long-range Transportation Plan	UNM = The University of New Mexico
MRA = Metropolitan Redevelopment Agency (COA)	

All new and reconstructed sidewalks and streetscapes shall be designed and built in accordance with the best practices described and illustrated in the American Association of State Highway and Transportation Officials (AASHTO) Guide for the Planning, Design, and Operation of Pedestrian Facilities.

A. Projects and Programs

Element	ID	Policy Implemented	Type	Action	Lead Agency	Coordination Required	Potential Funding Sources
A. MOVEMENT SYSTEMS							
Pedestrian/ Bicycle/ Vehicle Circulation	A-1	PC1, PC2, PC3, PC5, BC1, VC1, VC2	Capital Improvement Project	Develop a Streetscape Schematic Design Plan for Central Avenue between Girard and San Mateo Boulevards. Address the following elements: <ul style="list-style-type: none"> - Landscape buffering (shade trees, medians, bulb-outs, planting strips) - Pedestrian improvements (signalized crossings and crosswalks, sidewalks, ADA accessibility, signage alerting motorists of pedestrian and cyclist activity, addition of benches and seating along sidewalks, evaluation of unused curb cuts for closure) - Pedestrian-scale lighting - Traffic calming measures - Lane widths - Transit stops with seating and shade (coordinated with Modern Streetcar plan) - Way-finding - On-street parking - Public art - Impact of Central Blvd. lane reductions on Silver and Copper Aves. between Washington St. and San Mateo Blvd. 	Planning Department (COA)	Parks Management (COA); DMD (COA); Traffic Engineering Division (COA); City Forester (COA)	CIP; Individual projects already funded – see Appendix C



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Element	ID	Policy Implemented	Type	Action	Lead Agency	Coordination Required	Potential Funding Sources
Pedestrian Circulation	A-2	PC1	Capital Improvement Project	Develop safe, visible crosswalks at existing traffic signals on Lead and Coal.	DMD (COA); Traffic Engineering Division (COA)	Lead-Coal Study (DMD, COA)	CIP
Pedestrian Circulation	A-3	PC1, PC3	Capital Improvement Project	Modify Coal and Lead to shorten crosswalks, build level sidewalks, create buffers between sidewalks and vehicles, and slow traffic to improve pedestrian safety, accessibility and comfort.	Traffic Engineering Division (COA); Planning Department (COA)	Lead-Coal Study (Planning Department, COA)	CIP
Bicycle Circulation	A-4	BC3	Capital Improvement Project	Modify Lead and Coal Avenues to allow for installation of protected commuter bike lanes.	DMD (COA); Traffic Engineering Division		CIP
Pedestrian Circulation	A-5	PC4, PC5	Regulation - See Section 5B	Modify zoning to allow future development to orient toward alleyways, but only where street orientation would not be compromised as a result. In these situations, double-fronted buildings are required.	Planning Department (COA)		N/A
Pedestrian Circulation	A-6	PC4	Program Enhancements	Maintain alleys. Residents should identify problem areas through Streets Hotline complaint system.	DMD (COA)	Solid Waste (COA); Residents	General Fund
Pedestrian Circulation	A-7	PC3, PC4	Community Initiative	Identify alleys suitable for public art or community gardens.	Community Organizations; Alley Gardens project	Property Owners; MRA/Planning (COA)	NA
Pedestrian Circulation	A-8	PC3, PC5	Capital Improvement Project	Establish Night Sky Friendly pedestrian-scale lighting on Central, Lead, Coal, Zuni Silver and Washington.	DMD (COA); MRA/ Planning (COA)	PNM, Planning Department (COA)	CIP
Pedestrian Circulation	A-9	PC3	Capital Improvement Project	<p>In cooperation with local community organizations, conduct an inventory of all sidewalks and crosswalks in the plan area to identify barriers and other access limitations for people with disabilities.</p> <p>Develop a plan to remediate existing access deficiencies in the walkway network and widen sidewalks to the standards adopted in this plan, with higher priority accorded to safe travel routes along arterial and collector streets, and along local roads in close proximity to schools, bus stops, and other key pedestrian travel destinations. Incorporate into the plan a detailed description of all actions needed to achieve compliance with U.S. Access Board guidelines, a schedule to achieve compliance, a funding strategy, and an assignment of official responsibility for plan implementation.</p>	DMD (COA)	Planning Department (COA); Walk Albuquerque; Alliance for Active Living; UNM	CIP



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Element	ID	Policy Implemented	Type	Action	Lead Agency	Coordination Required	Potential Funding Sources
Pedestrian Circulation	A-10	PC3, safety	Program Enhancement	Identify pedestrian pathways connecting schools, transit and commercial areas and ensure their safety as walking routes, such as the Highland walkway. Coordinate with community organizations for on-going surveys of sidewalks to ensure walkability.	Planning Department (COA); NHNA; NHHRC; NHBA; HBANA	DMD (COA); Walk Albuquerque; Alliance for Active Living; UNM	Safe Routes to School (federal program administered by NM-DOT)
Pedestrian Circulation	A-11	PC3, safety	Program Enhancement	Improve code enforcement of private landscaping to ensure unobstructed sidewalk right-of-way for pedestrian use.	Code Enforcement (COA)	Residents; Neighborhood Associations	General Fund
Pedestrian Circulation	A-12	PC3, safety	Community Initiative	Report traffic problems to improve enforcement of traffic law violations (speeding and failure to stop), especially around schools.	Neighborhood and Business Associations	APD; Traffic Analyses (COA)	
Pedestrian Circulation	A-13	PC5	Community Initiative	Encourage area restaurants to enhance street vitality by providing outdoor seating for patrons, but only in cases where a 5-foot clear passageway for pedestrians can be maintained at all times. Encourage businesses to provide awnings or shade trees over sidewalks to provide shade and relief from heat, subject to the vertical and horizontal clearance guidelines of the U.S. Access Board.	Property Owners	NHRC; NHBA; HBNA; CCR Zoning	
Pedestrian Circulation	A-14	PC5	Program Enhancement	Establish public art as part of all Capital Improvements in the plan area.	DMD/Public Art (COA)	Property Owners; Community Organizations; MRA/Planning (COA)	Public Art Program (COA)
Bicycle Circulation	A-15	BC2, BC3	Capital Improvement Project	Complete the installation of bike lanes and routes in plan area as designated in the Bikeways Master Plan (routes on Copper Ave. and Valverde Dr.; lanes on Carlisle Blvd., Lead Ave., Coal Ave., and Zuni Rd.).	DMD (COA)	Planning Department (COA); MRCOG; LRTP; GARTC; GABAC	CIP
Bicycle Circulation	A-16	BC2	Program Enhancement	Designate Silver Ave. between Washington St. and San Mateo Blvd. as a bike route; remove Central Ave. from Bikeways Master Plan as bike corridor due to safety concerns.	Parks and Recreation (COA)	MRCOG (LRTP process)	General Fund
Bicycle Circulation	A-17	BC1	Community Initiative	Provide public bicycle racks in existing shopping areas.	Property Owners; NHHRC; NHBA; HBNA; NHNA; Community Organizations	Zoning enforcement	
Bicycle Circulation	A-18	BC1	Community Initiative	Promote bicycle commuting and provide all-day bike storage for area employees.	Community Organizations; NHHRC; NHNA; NHBA; HBNA	Property Owners	



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Element	ID	Policy Implemented	Type	Action	Lead Agency	Coordination Required	Potential Funding Sources
Vehicular Circulation	A-19	VC1, PC2	Program Enhancement	Assess the need for traffic calming measures on residential streets throughout plan area to slow through-traffic (i.e., stop signs, bulb outs, diverters, etc.).	DMD (COA); Traffic Engineering Division (COA)	Neighborhood Associations	CIP, NTMP
Transit	A-20	T1, PC5	Regulation – See Section 5B	Establish area around Hiland Theater as a TOD node with high density commercial and residential uses, consistent with Bernalillo County goal for the property. Work with streetscape planning efforts to coordinate adjacent Rapid Ride stop (and Streetcar stop in future phases).	Bernalillo County; ABQ Ride (COA)	ABQ Ride (COA); Planning Department (COA); Modern Streetcar Project	Modern Streetcar Project
Transit	A-21	T1, PC3, PC5	Capital Improvement Projects	Consolidate transit stops where possible (for Rapid Ride and Streetcar). Improve transit stops with shade, seating, and lighting. Coordinate stops with signalized mid-block crossings.	ABQ Ride (COA)	Modern Streetcar Project; Community Organizations	CIP
Transit	A-22	T1, PC3	Program Enhancement	Monitor and participate in HDR design guidelines for modern streetcar transit stop in the Nob Hill Historic Core. Coordinate with signalized mid-block crossings.	ABQ Ride (COA)	Modern Streetcar Project; Community Organizations	Modern Streetcar Project
Parking	A-23	P1	Capital Improvement Projects	Establish feasibility of north/south “parking streets” (such as Amherst St.) through a survey of existing rights-of-way and access requirements. Survey 100-blocks north and south of Central that appear to have sufficient right-of-way to verify right-of-way.	DMD (COA)	Planning Department (COA); Property Owners	CIP
Parking	A-24	P1, P3, CF-V7	Capital Improvement Project	Establish public parking courts/structures to promote ‘park once’ nodes. Wrap structures with office, retail, or residential structures. Create, fund and define a timeline for a public process to identify the location of the first and subsequent public/private funded parking courts/structures.	Planning Department (COA)	DMD (COA); Nob Hill and Highland Neighborhood Associations and Developer Stakeholders	CIP, On-site parking fee for waivers, TIF district
Parking	A-25	P2	Program Enhancement	Promote development to eliminate large, unused parking lots.	Planning Department (COA); MRA/Planning (COA)	Property Owners	General Fund
Parking	A-26	P3	Community Initiative	Consider parking permits in residential areas that restrict parking for residents only between 5 pm and 8 am.	Residents; Community Organizations (to make request)	Parking Division (COA)	General Fund



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B. COMMUNITY FORM							
Commercial – District 3	B-1	CF-District 3-1, CF-District 3-2	Program Enhancement	Develop a Corridor Plan for Lomas, including streetscape, land-use, and zoning to accommodate adequate planning for all four corners of the intersection with San Mateo.	Planning Department (COA)		General Fund, CIP
Residential - University Heights & Granada Heights	B-2	CF-University Heights & Grenada Heights-1	Program Enhancement	Survey University Heights and Granada Heights for potential eligibility as historic district and State or National Register nomination of individual properties.	Planning Department (COA)	SHPO	General Fund, SHPO
Residential	B-3	CF-University Heights & Grenada Heights-1; CF-Monte Vista / College View-1, 2	Program Enhancement	Develop and support initiatives that promote awareness and preservation of the historic character and streetscapes of the neighborhood (e.g., a handbook on historic character and guidelines for rehabilitation, development of walking routes with interpretive monuments, and signage which gives identity to historic districts.	Planning Department (COA)	Neighborhood Associations; Community Organizations	General Fund, SHPO
Public Facilities/ Community Services	B-4	PF/CS1	Program Enhancement	Establish a system for periodically meeting with the community and APS to explore ways to share grounds, recreational facilities, and buildings with the broader community.	Planning Department (COA)	APS; Parks & Recreation (COA); Family & Community Services; Community Organizations; Neighborhood Associations	General Fund
Streetscapes and Parks	B-5	S&P-1, S&P-2	Capital Improvement Project	Landscape existing medians throughout the Plan area to prevent run-off, address heat island effect, and assist in safe pedestrian crossing.	DMD (COA)	Parks Management (COA); City Forester (COA)	CIP
Streetscapes and Parks	B-6	S&P-4	Capital Improvement Project	Establish public plazas (possibly at the De Anza Motor Lodge and/or the Hiland Theater).	Property Owners	MRA/Planning (COA); City Forester (COA)	CIP
Streetscapes and Parks	B-7	S&P-5	CIP	Improve streetscape of Carlisle Boulevard to soften impact of traffic on adjacent residential properties and improve pedestrian safety and aesthetic environment.	DMD (COA)	City Forester (COA)	CIP
Streetscapes and Parks	B-8	S&P-6	CIP	Add landscaped median on MonteVista Boulevard, and develop Campus Boulevard as a landscaped, pedestrian friendly street.	DMD (COA)		CIP



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C. INFRASTRUCTURE							
Infrastructure	C-1	I1	Capital Improvements Project	Make upgrades to the drainage system in the Campus Wash Drainage Basin, taking into account future road construction in the Plan area and results of the Campus Drainage study, particularly regarding improving the water carrying capacity of Central Avenue with additional inlet infrastructure, as well as changing the flood designation of Central Avenue.	DMD (COA)	Planning Department (COA)	CIP
D. ECONOMIC VITALITY							
Economic Vitality	D-1	EV-2	Program Enhancement/Community Initiative	Establish TIF District to support the redevelopment of the De Anza Motel and other catalytic projects.	Planning Department (COA)	Private Developers	General Fund
Economic Vitality	D-2	EV-1, EV-2	Community Initiative	Create an incentive plan for commercial property owners with a low occupancy rate to encourage leasing of shops to small and independently-owned businesses.	NHHRC; NHBA	Property Owners	
Economic Vitality	D-3	EV-1, EV-2	Community Initiative	Actively recruit new businesses to the area.	NHHRC; HBNA; NHBA		
Economic Vitality	D-4	EV-1, EV-2	Community Initiative	Create a Business Improvement District (BID) in the area, where property owners and businesses share the costs of implementing specific improvements and programs.	Business and property owners		
Economic Vitality	D-5	EV-1, EV-2, EV-3, EV-4	Program Enhancement	Fund land acquisition and public improvements in support of catalytic projects within the Metropolitan Redevelopment area.	MRA/Planning (COA)	NHHRC; NHBA; HBANA	General Fund; TIF district; Grants; State Legislature Appropriations; City Metropolitan Redevelopment Fund
Economic Vitality	D-6	EV-1	Community Initiative	Gather and disseminate information on small business development resources.	NHHRC; HBANA; NHBA	Department of Economic Development (COA); CNM	Federal SBA; STEPS



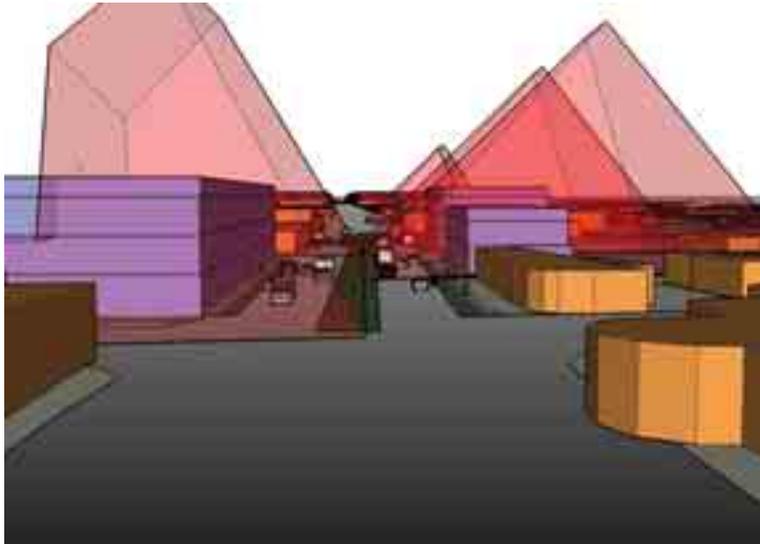
B. Regulations

1. Zoning – Commercial Districts

As a result of the 1987 Nob Hill Sector Plan the primary zoning along Central Avenue in the Historic Core and Emerging Nob Hill districts was changed to CCR (Community Commercial Residential) and OR (Office Residential). The primary purpose of these zone changes was to create the opportunity for mixed residential and commercial use along the corridor and to provide a better transition of uses with surrounding residential areas. Over time the community became interested in modifying the zones to provide more design control and predictability to support their vision for the future of these commercial districts. One of the objectives was to develop better control of building height since the underlying R-2, R-3, C-2 and O-1 zones utilize a 45 degree and 60 degree solar envelope to allow buildings to exceed 26 feet. Analysis of the development potential under

the existing CCR zone, particularly if half or full block developments were to occur, showed the potential for five to seven story building heights in those locations. Another objective was to provide additional design standards to help insure the creation of the future character of the commercial districts as described in the 1987 Nob Hill Sector Plan and the more recent Metropolitan Redevelopment Master Plan for Central – Highland - Upper Nob Hill area. This would include standards governing building height and massing, façade design, building set back and parking area design. The existing commercial zoning in the “Highland” district is primarily C-2 with a small amount of C-1 adjacent to Copper Avenue. This zoning does not allow for the mix of residential and commercial activity that is a major community goal for the Boulevards and Avenues in the Nob Hill Highland Sector Plan Area and that was described in the Metropolitan Redevelopment Master Plan for Central/Highland/Upper Nob Hill area.

The zoning proposed by this Sector Development Plan creates a “form based” version of the CCR zone (CCR-1, CCR-2 and CCR-3) and develops three distinct zoning areas to allow for the variation of requirements for the three commercial districts: Route 66 Historic Core, Emerging Nob Hill and the Highland District. There are two OR zones (OR-1 and OR-2) proposed to allow for the same variation of requirements as you move toward the future metropolitan redevelopment area west of Carlisle. The new zones that are proposed do not change the current permissive uses of the existing zones CCR and OR zones. The proposed CCR-3 does modify the existing C-2 and C-1 zones in that area by the elimination of alcoholic beverage sales for off-premise consumption, drive up windows and outdoor motor vehicle and trailer sales as permissive uses. The new zone does, however, add R-3 permissive uses to the current C-2 and C-1 zoned areas.



A key component of these proposed zoning requirements in the commercial districts is a map that establishes the Allowable Building Heights in the area. (See Figure 36)

The objective of these commercial zoning proposals is to realize the goals of the Nob Hill/Highland community as described in this Plan while at the same time creating opportunities, not hardships, for existing property owners. As stated many times at community meetings the desire is to stimulate private and public investment in this unique area that will build on the success of the Historic Nob Hill area and help the Emerging Nob Hill and Highland areas transition from a suburban auto oriented environment to a walkable, pedestrian oriented urban setting with mixed residential, commercial and entertainment opportunities.

CCR-1, CCR-2, CCR-3 and OR-1, OR-2

All of the properties within the CCR-1,2&3 and the OR-1&2 will be governed by a maximum height requirement that will replace the existing solar envelope regulation and will create a consistency of “street wall” massing in these areas. These heights will be defined by an Allowable Building Heights map.

Avenues and Boulevards

The properties fronting on the “Avenues” (Central, Copper, Silver, Coal) and “Boulevards” (Girard, Monte Vista, Carlisle, Lomas and San Mateo) that are within the CCR zone area will have a specific set of requirements that will be focused on enhancing their role as an “Urban Street”. The CCR-1,2 &3 zones will regulate the location of buildings and parking relative to the street and internal property lines and will require conformance to specific architectural form and design requirements directed at creating an active and pedestrian oriented urban street.

Streets, Drives and Roads

The properties fronting on the “Streets” (Washington, Adams, Jefferson, Madison, Monroe, Quincy, Jackson, Manzano and Truman), the “Drives” (Dartmouth, Richmond, Bryn Mawr, Wellesley, Tulane, Amherst, Hermosa, Solano Aliso, Morningside, Montclair, Sierra, Graceland and Val Verde) as well a small section of Zuni Road that are within the CCR or OR zone areas will have two options within the zone. These areas are important to the creation of the mixed use urban vision of the Sector Plan but in some cases specific properties present unique development challenges due to their size, existing buildings, lack of abutting alleys, etc. This is especially true in the Highland area. In response to this the CCR and OR zones offer two

alternative development options, “Urban Streets” and “Conventional”. If a property owner chooses to redevelop his or her property under the “Urban Street” option then they will take advantage of the benefits of lower parking requirements and building setback requirements, etc. that are intended to help the property owner “do the right thing” relative to the goals of the community and of this Plan. If, on the other hand, the property owner decides that they don’t want to conform to the “Urban Street” requirements they have the option to develop their property under the “Conventional” option. This means that they will be required to meet the requirements of the traditional C-2, R-2, R-3 and O-1 zones, as defined in the City of Albuquerque Zoning Code, with the exception of those requirements governing use and height which has been redefined by the CCR-1,2,3 and OR-1,2 zones.

The following are more detailed descriptions of elements of the proposed Commercial District Zones.



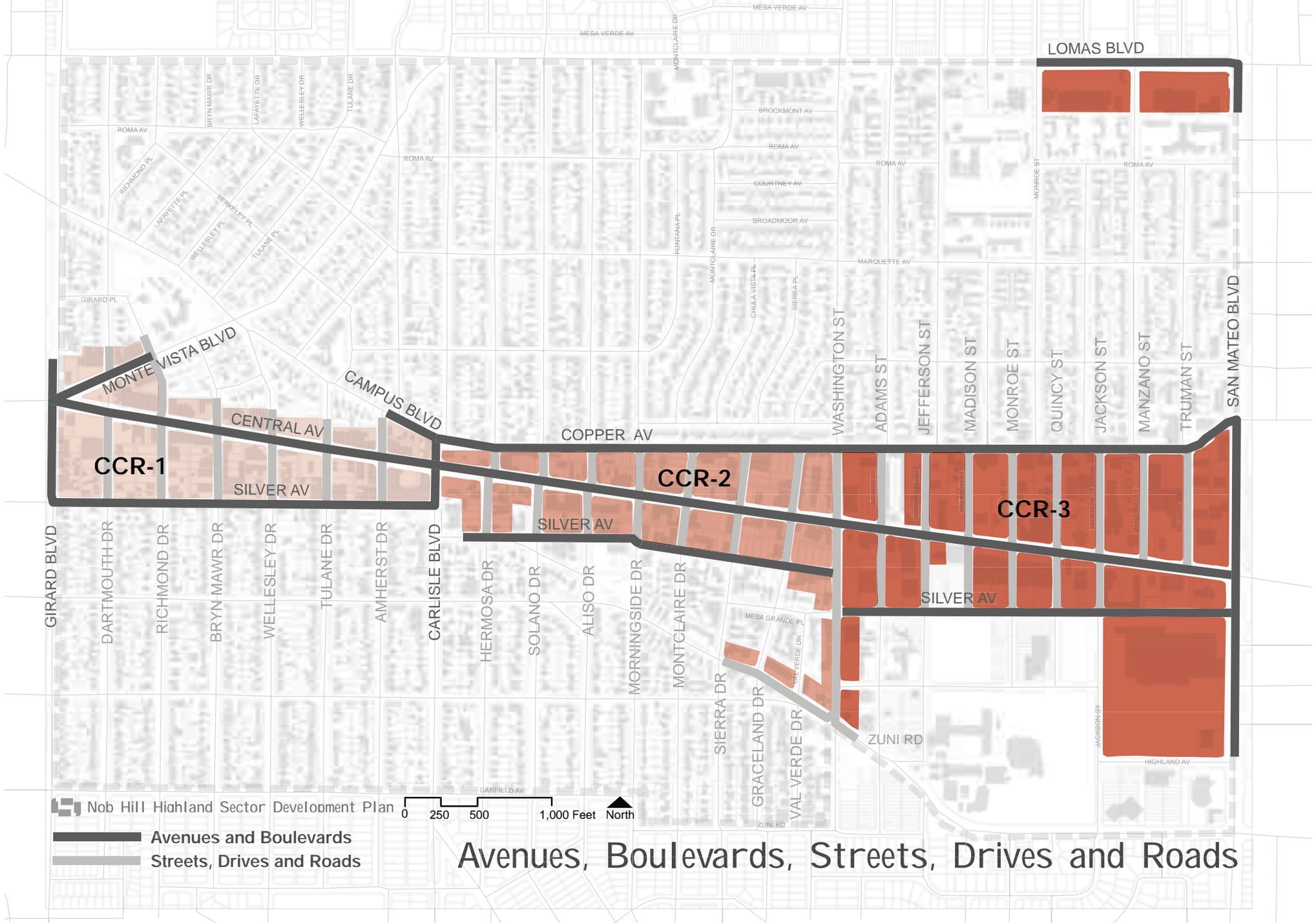
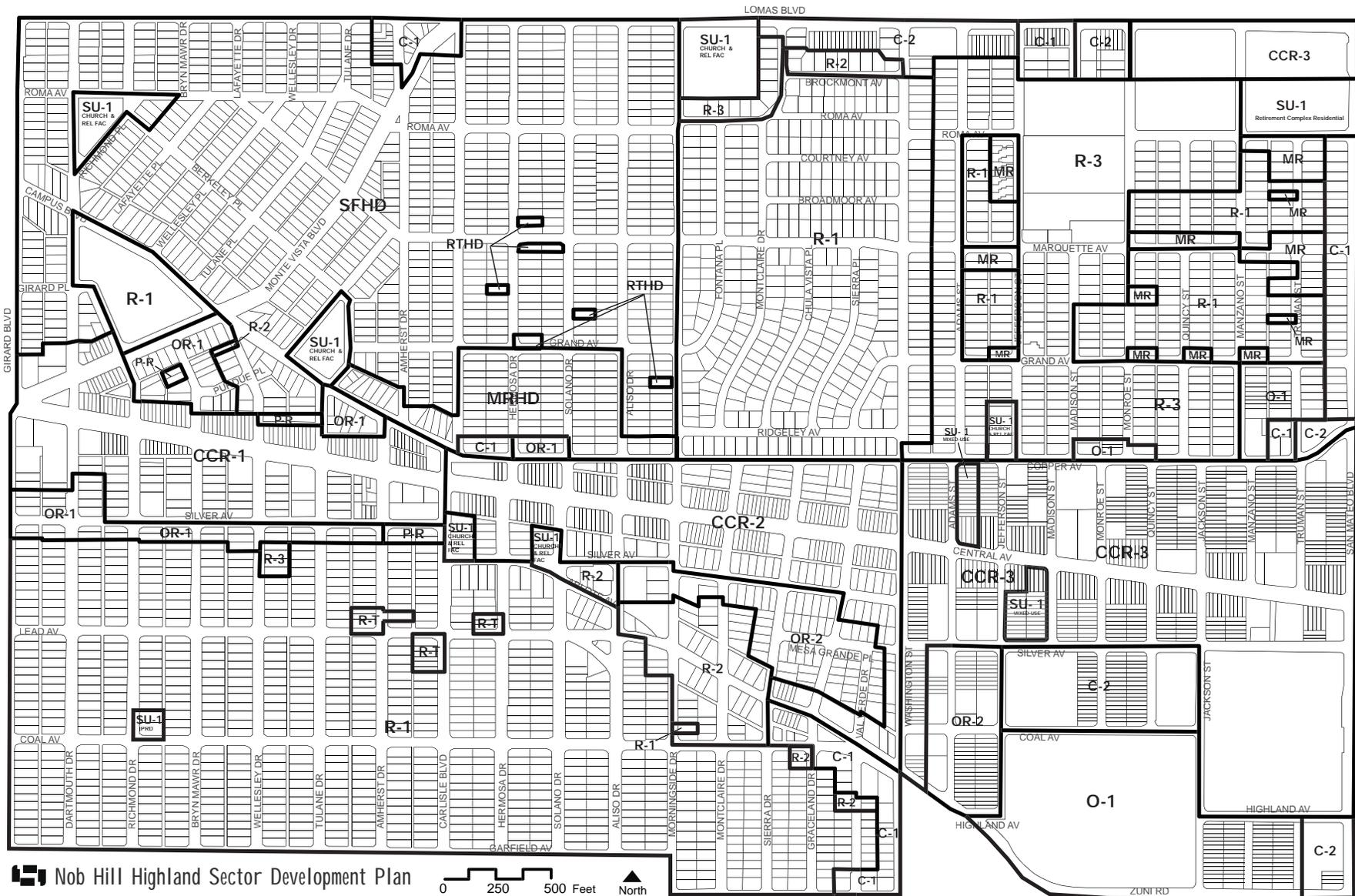


Figure 33: Avenues, Boulevards, Streets, Drives and Roads





Nob Hill Highland Sector Development Plan

- | | | | | | |
|-------|--------------------------------------|------|---|------|---|
| C-1 | Neighborhood Commercial | MRHD | Mixed-Use Residential Historic District | R-2 | Multi-Family |
| C-2 | Community Commercial | O-1 | Office and Institutional | R-3 | Multi-Family |
| CCR-1 | Community Commercial / Residential 1 | OR-1 | Office / Residential 1 | R-T | Residential Townhouse |
| CCR-2 | Community Commercial / Residential 2 | OR-2 | Office / Residential 2 | RTHD | Residential Townhouse Historic District |
| CCR-3 | Community Commercial / Residential 3 | P-R | Reserve Parking | SFHD | Single Family Historic District |
| MR | Mixed-Use Residential | R-1 | Single Family | SU-1 | Special Use |

Figure 34: Comprehensive Zoning

Comprehensive Zonina



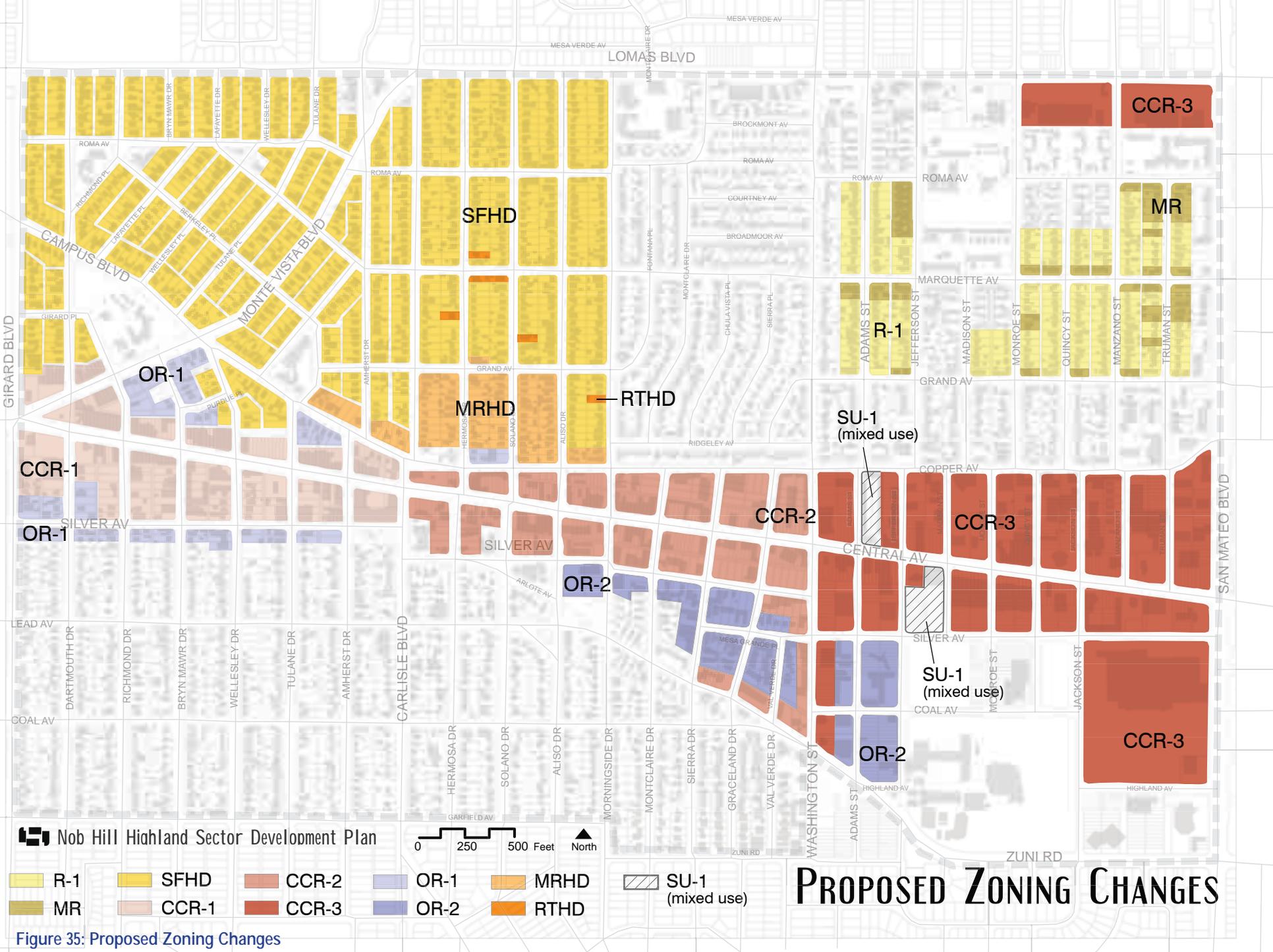


Figure 35: Proposed Zoning Changes



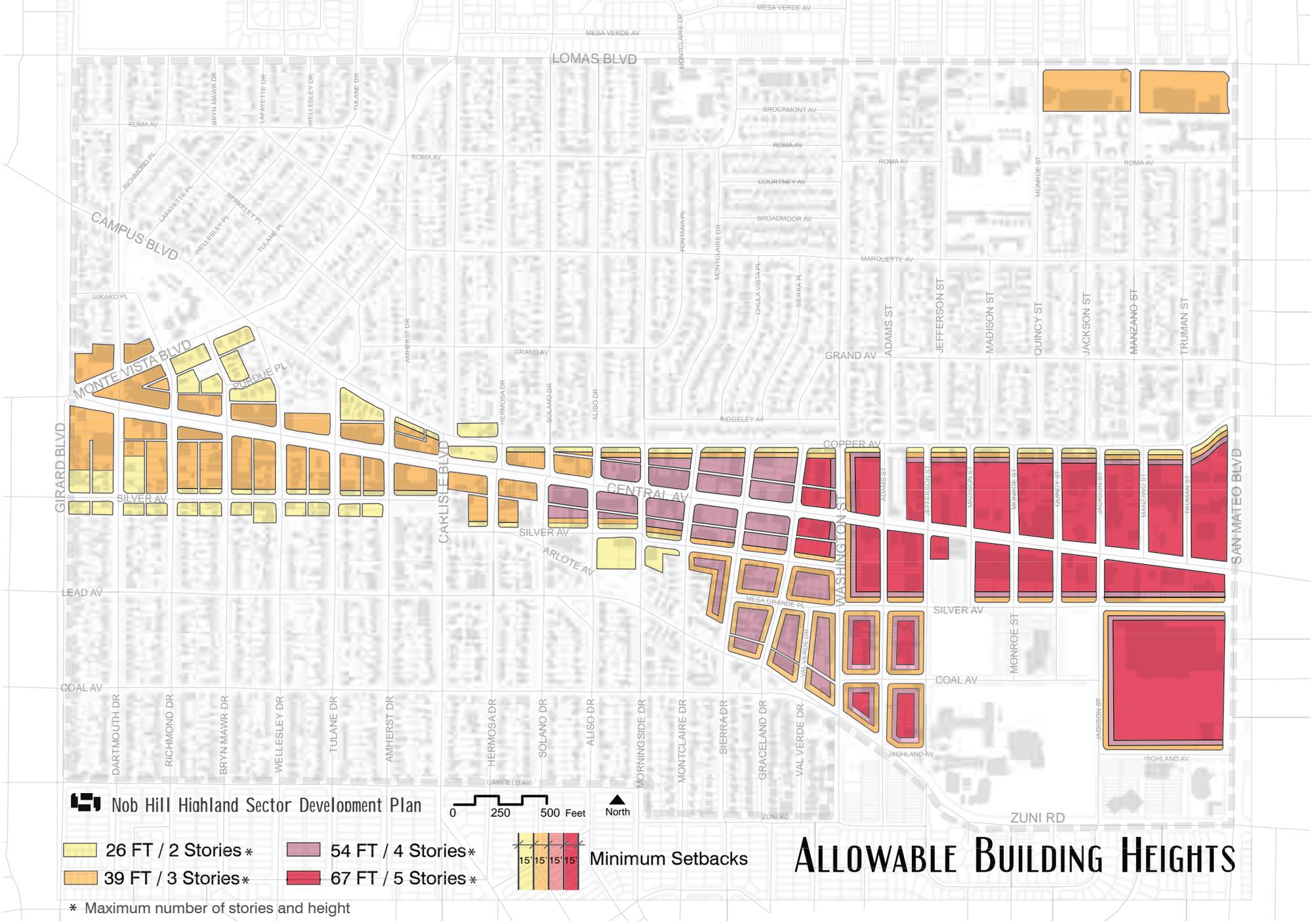


Figure 36: Allowable Building Heights



Community Commercial/Residential One - CCR-1

1. General Rules: Uses

The Community Commercial/Residential One (CCR-1) land use category corresponds to the R-3 Zone for residential uses and the C-2 zone for non-residential uses with the following exceptions:

- A. Alcoholic beverage sales for off-premise consumption are not allowed.
- B. Drive-up service windows are not allowed.
- C. Outdoor motor vehicle or trailer sales, retail, service, repair, and/or storage shall be separated from Central Avenue by a building. For the purpose of the CCR Land Use Category, "building" means a structure enclosed on all side by walls and roofs.
- D. There is no maximum floor area ratio or density limit for R-3 residential uses.



2. Architecture: Form & Elements

- A. Existing facades that abut a public right-of-way for buildings that are designated on State and National Historic Registers or as City Landmarks or Characteristic Buildings in Figure 3 shall comply with the following:
 1. Façade rehabilitation or remodeling shall maintain the historically characteristic window and door proportion and character of the building.
 2. Any new materials shall be consistent with historically employed materials on such facades.

* Fenestration (Def.) - "The arrangement, proportioning of doors and windows in a building"

** Façade (also Frontage): The exterior wall of a building set parallel to a lot line bordering a public right-of-way or clearly visible from a public right-of way. An architectural element is not clearly visible if it is visible only through an opening in a street or an opening in a courtyard wall.

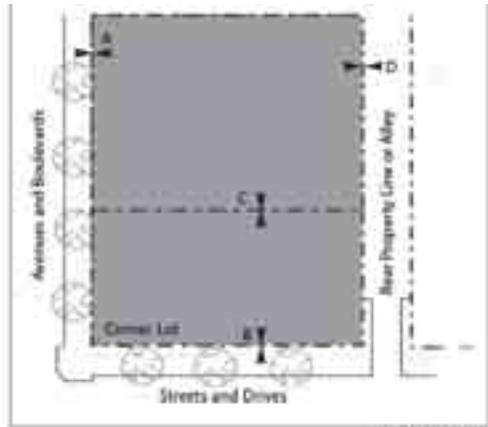
3. Additions to contributing buildings shall reflect the historic façade demarcations (i.e., walls, pilasters or similar elements.)
 4. Additions above contributing buildings shall be vertically aligned with the historic demising walls of the storefronts below (see photo p. 90)
 5. Colors of paint or stucco may be modified without a limit to frequency.
- B. The ground stories facing Central Avenue shall be built to appear as shopfronts and have front fenestration* of 60% minimum of facade**, with window sills no higher than 30 inches above the finished floor.
 - C. Regarding the ground stories facing intersecting side streets to a depth of 150' or to an alley or street, whichever occurs first, when the building or buildings facing Central at a minimum length along Central of 50' are redeveloped or rebuilt, the 40' along the side street from Central shall have at least one entrance and the remaining portion of the side street frontage shall be built to appear as shopfronts or residential facades and shall be consistent in design and materials as to the major facades facing Central. Where it is impractical, as determined by the Planning Director, to locate parking ingress and egress from the alley: (a) one ingress no more than 12' in width is allowed from the side street in situations where egress from the alley is practical; or (b) one ingress / egress location no more than 30' in width is allowed to the side street should option "a" be infeasible. Where storefronts or residences due to the adjacent sidewalk grades make pedestrian access infeasible, as determined by the Planning Director, this requirement may be waived. The side street facade shall have fenestration of 60% minimum of façade** with window sills no higher than 30 inches above the finished floor.
 - D. All other ground floor and upper level building frontage shall be designed with 40-90% of the building frontage glazed, with the window sill no higher than 30 inches above the finished floor.
 - E. Building frontages on alleys shall have ground floor windows no higher than 60" above finished grade at building and shall be designed with 40%-90% of the building frontage glazed. All public alleys shall include pedestrian scale lighting and be maintained free of pedestrian and vehicular obstacles.
 - F. Window glass shall be clear with light transmission between exterior and interior:
 1. 90% minimum for the ground story (within nine feet of the sidewalk elevation); and
 2. 75% minimum for the upper stories (modification permitted as necessary to meet any applicable building and energy code requirements).
 - G. Façade fenestration design shall maintain the established window and door proportions of the block.
 - H. Placement- openings shall occur along street façades at twenty-five feet (25') on center minimum; openings to be three foot (3') wide minimum.
 - I. Entrance - each ground floor building shall have 1 entrance minimum for each twenty five feet (25') or less of building frontage length on street frontage.
 - J. Articulation - building façade at front and side street shall change each twenty five (25') minimum in height, or setback, or material.
 - K. Awnings, arcades and balconies are permitted.
 - L. Property walls & fences - as allowed in current City regulations at fronts, sides & rears of buildings.
 - M. Where setback buildings currently exist the streetwall shall be strengthened through use of solid vertical landscaping and low walls.

3. Signage and Lighting

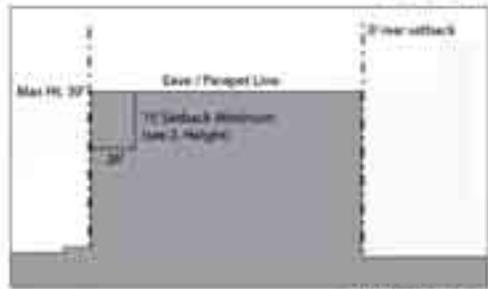
(See General Regulations page)



Building Placement and Height



Plan Diagram



Section Diagram

4. Setbacks

- A. Buildings shall be placed within the shaded area as shown in the above diagram.
 1. Front Build to Line: 0" to twelve inches (12")
 2. Side Street Setback: 0"
 3. Side yard Setback: 0"
 4. Rear Setback: 0"

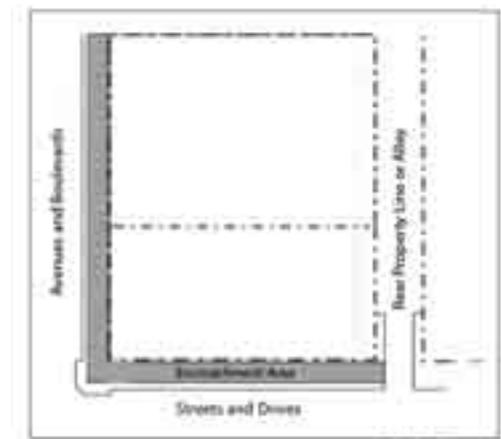
- B. Notwithstanding the other provisions of this Plan, two forecourts are allowed on each side of Central (within the reach of the Route 66 Historic Core and within the reach of Emerging Nob Hill), setback to the following specifications: between ten feet (10') and twenty feet (20') deep from the right-of-way, between twenty-five feet (25') and seventy-five feet (75') wide parallel to the right-of-way, and sited not less than fifty feet (50') from any block corner.

5. Height

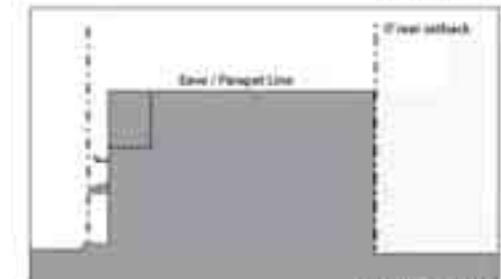
Building height shall be measured in feet from grade as defined in the zoning code to top of parapet or midpoint of pitch.

- A. Maximum: 3 stories/39' transition to 26', see Allowable Building Heights Plan. If 75% or more of the block frontage along Central is being developed or redeveloped, one third of the new development is limited to 2 stories (26 feet) in height.
- B. Decorative variations in the parapet height are allowed up to an additional 5%; and
- C. Architectural features, such as a towers, are allowed up an additional 15% height (limited to 5% of building foot print) above the decorative variations in parapet height.

Building Frontage and Profile



Plan Diagram



Section Diagram

6. Encroachments Allowed

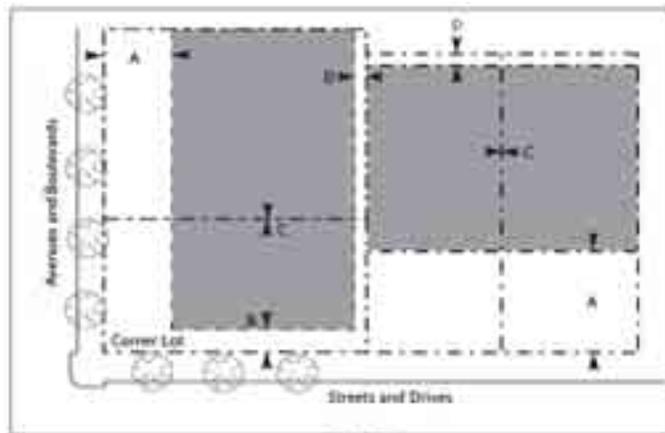
Arcades, balconies and awnings may encroach over the Public Right of Way [ROW] at streets. Encroachments into public ROW shall follow existing COA regulations.

7. Frontage Types

Arcade, shop front and forecourt (see Frontage Types Page 104). Not less than 50% of the building facades shall be accompanied by arcades, cantilevered shed roofs, or awnings.



Parking Placement



Plan Diagram

8. Parking Requirements

- A. On-site parking is allowed only in the shaded areas as shown.
 1. Front Setback: 30% of lot depth minimum or 50', whichever is less. Parking lots and off street parking shall be behind or within a building or buildings adjacent to the fronting street.
 2. Side Street Setback: six foot (6') minimum with exception of parking completely below grade whose setback is 0" minimum
 3. Side Setback: 0" minimum
 4. Rear Setback: five foot (5') minimum
- B. Parking requirements below are subject to reductions due to credits allowed by the zone code:
 1. 1 space/bedroom, 2 spaces maximum/DU
 2. 3 parking spaces: commercial or retail per 1000 s.f.
 3. 1 parking space: restaurants, per each 4 seats
 4. 1 parking space: per hotel room

- C. Commercial uses 3,000 square feet or less in size are exempt from parking requirements.
- D. Vehicular access is permitted only from side street or alley. Parking garages shall have liner buildings along all side street frontages and solid 3'-0" minimum high walls [all levels] or solid landscape at side and rear property lines; however, if the side or rear property line is adjacent to a residentially-zoned lot, the wall of the parking structure must be entirely solid, without opening.
- E. Surface parking areas at rears of properties shall be shielded from streets by wall, fence, or solid, vertical landscaping. Screening shall be a minimum of 30 inches in height but shall not exceed 36 inches in height.

9. Landscape Requirements

The landscape design should reflect the more urban character of this area in its programming, detailing, and planting intensity. 10% of parking lot areas shall be planted with irrigated shade producing trees. All side street setback areas shall be landscaped to screen parking areas. All properties must comply with Section 14-16-3-10, Landscaping Regulations, of the Zoning Code.

Option: Urban Street/Conventional

10. Urban Street

- A. All properties fronting on an *avenue* or a *boulevard* in this zone will be required to conform to these "urban street" regulations.
- B. Properties fronting on a *drive* or a *street* have the option of developing under these urban street regulations or choosing the conventional option.

11. Conventional Option

- A. The conventional option means that the project will be required to meet the requirements of the traditional C-2, R-2, R-3 and O-1 zones, as defined in the City of Albuquerque Zoning Code, with the exception of use and height which has been redefined by this CCR zone (or OR Zone depending on which zone is applicable).



Community Commercial/Residential Two - CCR-2

1. General Rules: Uses

The Community Commercial/Residential Two (CCR-2) land use category corresponds to the R-3 Zone for residential uses and the C-2 zone for non-residential uses with the following exceptions:

- A. Alcoholic beverage sales for off-premise consumption are not allowed.
- B. Drive-up service windows are not allowed.
- C. Outdoor motor vehicle or trailer sales, retail, service, repair, and/or storage shall be separated from Central Avenue by a building. For the purpose of the CCR Land Use Category, "building" means a structure enclosed on all side by walls and roofs.
- D. There is no minimum floor ratio or density limit for R-3 residential uses.



4. Additions above contributing buildings shall be vertically aligned with the historic demising walls of the storefronts below (see photo p. 93)
5. Colors of paint or stucco may be modified without a limit to frequency.
- B. The ground stories facing Central Avenue shall be built to appear as shopfronts and have front fenestration* of 60% minimum of facade**, with window sills no higher than 30 inches above the finished floor.
- C. Regarding the ground stories facing intersecting side streets to a depth of 150' or to an alley or street, whichever occurs first, when the building or buildings facing Central at a minimum length along Central of 50' are redeveloped or rebuilt, the 40' along the side street from Central shall have at least one entrance and the remaining portion of the side street frontage shall be built to appear as shopfronts or residential facades and shall be consistent in design and materials as to the major facades facing Central. Where it is impractical, as determined by the Planning Director, to locate parking ingress and egress from the alley: (a) one ingress no more than 12' in width is allowed from the side street in situations where egress from the alley is practical; or (b) one ingress / egress location no more than 30' in width is allowed to the side street should option "a" be infeasible. Where storefronts or residences due to the adjacent sidewalk grades make pedestrian access infeasible, as determined by the Planning Director, this requirement may be waived. The side street facade shall have fenestration of 60% minimum of facade** with window sills no higher than 30 inches above the finished floor.
- D. All other ground floor and upper level building frontage shall be designed with 40-90% of the building frontage glazed, with the window sill no higher than 30 inches above the finished floor.
- E. Building frontages on alleys shall have ground floor windows no higher than 60" above finished grade at building and shall be designed with 40%-90% of the building frontage glazed. All public alleys shall include pedestrian scale lighting and be maintained free of pedestrian and vehicular obstacles.
- F. Window glass shall be clear with light transmission between exterior and interior:
 1. 90 % minimum for the ground story (within nine feet of the sidewalk elevation); and
 2. 75% minimum for the upper stories (modification permitted as necessary to meet any applicable building and energy code requirements).
- G. Placement- openings shall occur along street façades at twenty-five feet (25') on center minimum; openings to be three foot (3') wide minimum.
- H. Entrance - each ground floor building shall have 1 entrance minimum for each twenty five feet (25') or less of building frontage length on street frontage.
 - I. Articulation - building façade at front and side street shall change each twenty five feet (25') minimum in height, or setback, or material.
 - J. Awnings, arcades and balconies are permitted.
 - K. Property walls & fences - as allowed in current City regulations at fronts, sides & rears of buildings.
 - L. Where setback buildings currently exist the streetwall shall be strengthened through use of solid, vertical landscaping and low walls

2. Architecture: Form & Elements

- A. Existing facades that abut a public right-of-way for buildings that are designated on State and National Historic Registers or as City Landmarks or Characteristic Buildings in Figure 3 shall comply with the following:
 1. Façade rehabilitation or remodeling shall maintain the historically characteristic window and door proportion and character of the building.
 2. Any new materials shall be consistent with historically employed materials on such facades.
 3. Additions to contributing buildings shall reflect the historic façade demarcations (i.e., walls, pilasters or similar elements.)

* Fenestration (Def.) - "The arrangement, proportioning of doors and windows in a building"

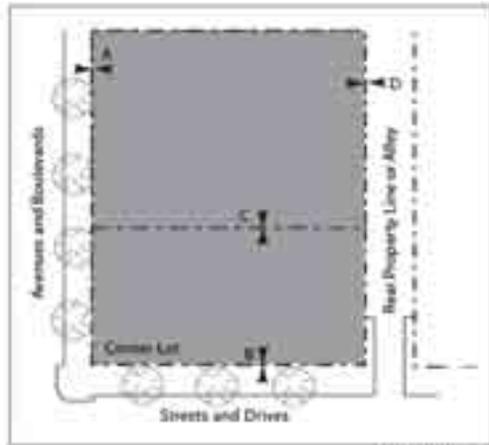
** Façade (also Frontage): The exterior wall of a building set parallel to a lot line bordering a public right-of-way or clearly visible from a public right-of-way. An architectural element is not clearly visible if it is visible only through an opening in a street or an opening in a courtyard wall.

3. Signage and Lighting

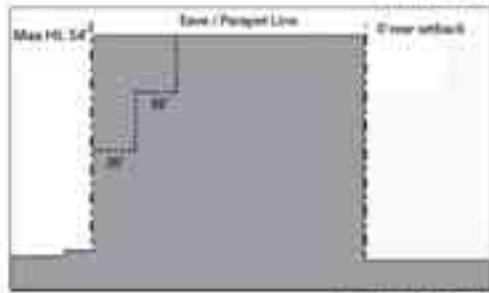
(See General Regulations page)



Building Placement and Height



Plan Diagram



Section Diagram

4. Setbacks

- A. Buildings shall be placed within the shaded area as shown in the above diagram.
 1. Front build to line: 0" to twelve inches (12")
 2. Side Street Setback: 0"
 3. Side yard Setback: 0"
 4. Rear Setback: 0"

- B. Notwithstanding the other provisions of this Plan, two forecourts are allowed on each side of Central (within the reach of the Route 66 Historic Core and within the reach of Emerging Nob Hill), setback to the following specifications: between ten feet (10') and twenty feet (20') deep from the right-of-way, between twenty-five feet (25') and seventy-five feet (75') wide parallel to the right-of-way, and sited not less than fifty feet (50') from any block corner.

5. Height

Building height shall be measured in feet from grade as defined in the zoning code to top of parapet or midpoint of pitch.

- A. Maximum: 4 stories/54' with lower heights, see Allowable Building Heights Plan. If 75% or more of the block footage along Central is being developed or redeveloped, twenty-two percent (22%) of the new development is limited to 3 stories (39 feet) in height. No elevation of a building across Central Avenue frontage shall be more than 2 stories (26 feet) greater than an adjoining building (measured from adjacent building not from grade of adjacent lot or sidewalk).
- B. Decorative variations in the parapet height are allowed up to an additional 5%; and
- C. Architectural features, such as a tower, are allowed up an additional 15% height (limited to 5% of building footprint) above the decorative variations in parapet height.

Building Frontage and Profile



Plan Diagram



Section Diagram

6. Encroachments Allowed

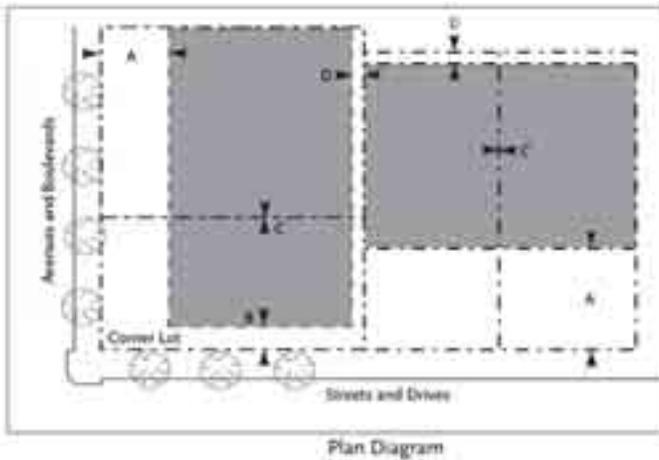
Arcades, balconies and awnings may encroach over the Public Right of Way [ROW] at side streets. Encroachments into public ROW shall follow existing COA regulations.

7. Frontage Types

Arcade, shop front and forecourt (see Frontage Types Page 104). Not less than 50% of the building facades shall be accompanied by arcades, cantilevered shed roofs, or awnings.



Parking Placement



8. Parking Requirements

- A. On-site parking is allowed only in the shaded areas as shown.
1. Front Setback: 30% of lot depth minimum or 50' whichever is less if parking is located completely behind the structure. If parking is located within the structure or if a parking structure is wrapped, the Front Setback shall be 20% of lot depth minimum or 25 feet whichever is less provided that the front of building contains active uses visible from the front and side street(s) if on a corner parcel(s) including retail, office, residential, entertainment, and civic. Parking lots and off street parking shall be behind or within a building or buildings adjacent to the fronting street.
 2. Side Street Setback: six foot (6') minimum with exception of parking completely below grade whose setback is 0" minimum
 3. Side Setback: 0" minimum
 4. Rear Setback: five foot (5') minimum

- B. Parking requirements below are subject to reductions due to credits allowed by the zone code:
1. 1 space/bedroom, 1.5 spaces per 2-bedroom unit 2 spaces maximum/DU
 2. 3 parking spaces: commercial or retail per 1000 s.f.
 3. 1 parking space: restaurants, per each 4 seats
 4. 1 parking space: per hotel room
- C. Commercial uses 3,000 square feet or less in size are exempt from parking requirements.
- D. Uses within 650 feet of a Bus Rapid Transit or Streetcar stop may factor a 20% reduction in parking requirements. Uses from 650 feet to 1300 feet of a Bus Rapid Transit or Street car stop or within 650 feet of another bus stop may factor a 10% reduction in parking requirements. Routes lengths that have bus shelters at all stops may factor in an additional 5% reduction in parking requirements.
- E. Vehicular access is permitted only from side street or alley. Parking garages shall have liner buildings along all side street frontages and solid 3'-0" minimum high walls [all levels] or solid landscape at side and rear property lines; however, if the side or rear property line is adjacent to a residentially-zoned lot, the wall of the parking structure must be entirely solid, without opening.
- F. Surface parking areas at rears of properties shall be shielded from streets by wall, fence, or solid, vertical landscaping. Screening shall be a minimum of 30 inches in height but shall not exceed 36 inches in height.

9. Landscape Requirements

The landscape design should reflect the more urban character of this area in its programming, detailing, and planting intensity. 10% of parking lot areas shall be planted with irrigated shade producing trees. All side street setback areas shall be landscaped to screen parking areas. All properties must comply with Section 14-16-3-10, Landscaping Regulations, of the Zoning Code.

Option: Urban Street/Conventional

10. Urban Street

- A. All properties fronting on an *avenue* or a *boulevard* in this zone will be required to conform to these urban street regulations.
- B. Properties fronting on a *drive* or a *street* have the option of developing under these urban street regulations or choosing the conventional option.

11. Conventional Option

- A. The conventional option means that the project will be required to meet the requirements of the traditional C-2, R-2, R-3 and O-1 zones, as defined in the City of Albuquerque Zoning Code, with the exception of use and height which has been redefined by this CCR zone (or OR Zone depending on which zone is applicable).



Community Commercial/Residential Three - CCR-3

1. General Rules: Uses

The Community Commercial/Residential Three (CCR-3) land use category corresponds to the R-3 Zone for residential uses and the C-2 zone for non-residential uses with the following exceptions:



- A. Outdoor motor vehicle or trailer sales, retail, service, repair, and/or storage shall be separated from Central Avenue by a building. For the purpose of the CCR Land Use Category, "building" means a structure enclosed on all side by walls and roofs.
- B. There is no maximum floor area ratio or density limit for R-3 residential uses.

2. Architecture: Form & Elements

- A. Existing facades that abut a public right-of-way for buildings that are designated on State and National Historic Registers or as City Landmarks or Characteristic Buildings in Figure 3 shall comply with the following:
 1. Façade rehabilitation or remodeling shall maintain the historically characteristic window and door proportion and character of the building.
 2. Any new materials shall be consistent with historically employed materials on such facades.
 3. Additions to contributing buildings shall reflect the historic façade demarcations (i.e., walls, pilasters or similar elements.)

* Fenestration (Def.) - "The arrangement, proportioning of doors and windows in a building"

** Façade (also Frontage): The exterior wall of a building set parallel to a lot line bordering a public right-of-way or clearly visible from a public right-of way. An architectural element is not clearly visible if it is visible only through an opening in a street or an opening in a courtyard wall.

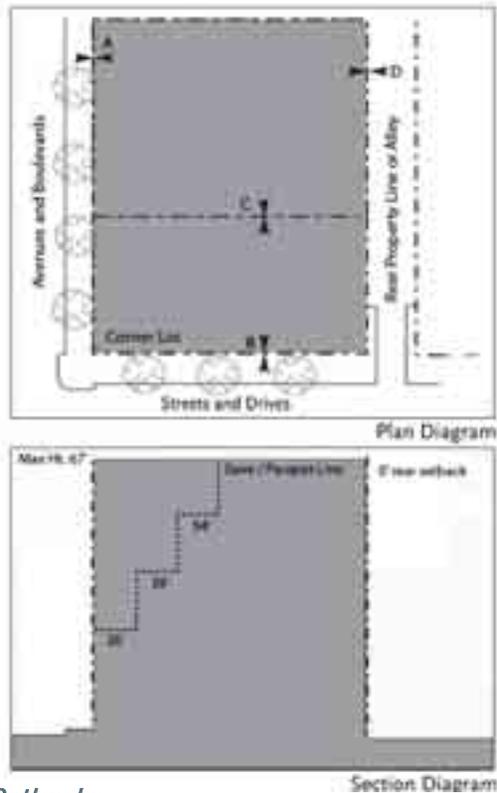
4. Additions above contributing buildings shall be vertically aligned with the historic demising walls of the storefronts below (see photo p. 96)
 5. Colors of paint or stucco may be modified without a limit to frequency.
- B. The ground stories facing Central Avenue shall be built to appear as shopfronts and have front fenestration* of not less than 60% minimum of facade**, with window sills no higher than 30 inches above the finished floor.
 - C. All other ground floor upper level building frontage shall be designed with 40-90% of the building frontage glazed, with the window sill no higher than 30 inches above the finished floor.
 - D. Window glass shall be clear with light transmission between exterior and interior:
 1. 90% minimum for the ground story (within nine feet of the sidewalk elevation); and
 2. 75% minimum for the upper stories (modification permitted as necessary to meet any applicable building and energy code requirements).
 - E. Placement- openings shall occur along street façades at forty feet (40') on center minimum; openings to be three foot (3') wide minimum. Entrance - each ground floor use shall have 1 entrance minimum for each forty feet (40') or less of building frontage length on street frontage.
 - F. Articulation - building façade at front and side street shall change each forty feet (40') minimum in height, or setback, or material. Awnings, arcades and balconies are permitted.
 - G. Property walls & fences - as allowed in current City regulations at fronts, sides & rears of buildings.
 - H. Drive-thru type buildings allowed only in rears of properties or on sides of properties provided that the service window and any associated order board are located at least fifty feet (50') from any residential zone and screened by a structure or wall at least six feet (6') in height.

3. Signage and Lighting

(See General Regulations page)



Building Placement and Height



4. Setbacks

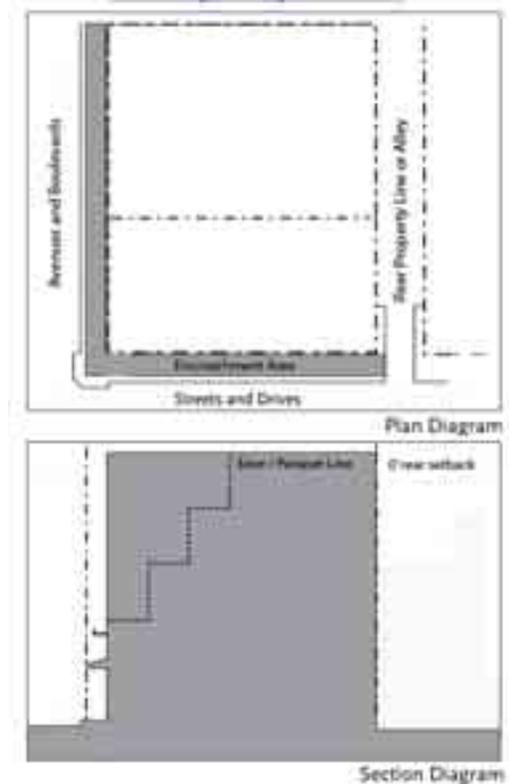
- A. Buildings shall be placed within the shaded area as shown in the above diagram.
 1. Front build to line: 0" to twelve inches (12")
 2. Side Street Setback: 0"
 3. Side yard Setback: 0"
 4. Rear Setback: 0"

5. Height

Building height shall be measured in feet from grade as defined in the zoning code to top of parapet or midpoint of pitch.

- A. Maximum: 5 stories/67' and transitions to lower heights, see Allowable Building Heights Plan. If 70% or more of the block frontage along Central is being developed or redeveloped, at least 20% must be five stories (67 feet) in height and 40% of the new development is limited to 4 stories (53 feet) in height.
- B. Decorative variations in the parapet height are allowed up to an additional 5%; and
- C. Architectural features, such as a tower, are allowed up an additional 15% height (limited to 5% of building footprint) above the decorative variations in parapet height and are limited to 5% of floor area.

Building Frontage and Profile



6. Encroachments Allowed

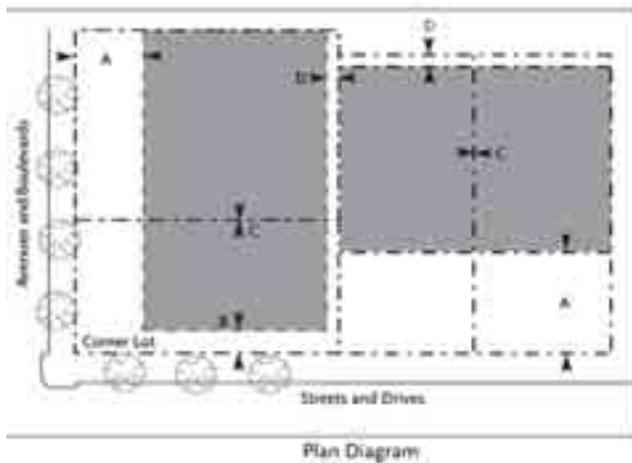
Arcades, awnings and balconies may encroach over the Public Right of Way [ROW] at side streets. Encroachments into public ROW shall follow existing COA regulations.

7. Frontage Types

Arcade, shop front and forecourt (see Frontage Types Page 104). Not less than 50% of the building facades shall be accompanied by arcades, cantilevered shed roofs, or awnings.



Parking Placement



8. Parking Requirements

- A. On-site parking is allowed only in the shaded areas as shown.
1. Front Setback: 30% of lot depth minimum or 50', whichever is less if parking is located completely behind the structure. If parking is located within the structure or if a parking structure is wrapped, the Front Setback shall be 20% of lot depth minimum or 25 feet whichever is less provided that the front of building contains active uses visible from the front and side street(s) if on a corner parcel(s) including retail, office, residential, entertainment, and civic. Parking lots and off street parking shall be behind or within a building or buildings adjacent to the fronting street.
 2. Side Street Setback: six foot (6') minimum with exception of parking completely below grade whose setback is 0" minimum
 3. Side Setback: 0" minimum
 4. Rear Setback: five foot (5') minimum
- B. Parking requirements below are subject to reductions due to credits allowed by the zone code:
1. 1.5 spaces per 2-bedroom unit, 1 space/bedroom, 2 spaces maximum/DU
 2. 3 parking spaces: commercial or retail per 1000 s.f.
 3. 1 parking space: restaurants, per each 4 seats
 4. 1 parking space: per hotel room
- C. Uses within 650 feet of a Bus Rapid Transit or Streetcar stop may factor a 20% reduction in parking requirements. Uses from 650 feet to 1300 feet of a Bus Rapid Transit or Street car stop or within 650 feet of another bus stop may factor a 10% reduction in parking requirements. Route lengths that have bus shelters at all stops may factor in an additional 5% reduction in parking requirements.
- D. Vehicular access is permitted only from side street or alley on block frontages of 300 feet or less. Blocks longer than 300 feet may have one access point from fronting street. Parking garages shall have liner buildings along all side street frontages and solid 3'-0" minimum high walls [all levels] or solid landscape at side and rear property lines; however, if the side or rear property line is adjacent to a residentially-zoned lot, the wall of the parking structure must be entirely solid, without opening.
- E. Surface parking areas at rears of properties shall be shielded from streets by wall, fence, or solid, vertical landscaping. Screening shall be a minimum of 30 inches in height but shall not exceed 36 inches in height.

9. Landscape Requirements

The landscape design should reflect the more urban character of this area in its programming, detailing, and planting intensity. 10% of parking lot areas shall be planted with irrigated shade producing trees. All side street setback areas shall be landscaped to screen parking areas. All properties must comply with Section 14-16-3-10, Landscaping Regulations, of the Zoning Code.

Option: Urban Street/Conventional

10. Urban Street

- A. All properties fronting on an *avenue* or a *boulevard* in this zone will be required to conform to these urban street regulations.
- B. Properties fronting on a *drive* or a *street* have the option of developing under these urban street regulations or choosing the conventional option.

11. Conventional Option

The conventional option means that they will be required to meet the requirements of the traditional C-2, R-2, R-3 and O-1 zones, as defined in the City of Albuquerque Zoning Code, with the exception of use and height which has been redefined by this CCR zone (or OR Zone depending on which zone it is in).



Office/Residential One - OR-1



1. General Rules: Uses

The OR Office/Residential One (OR-1) land use category corresponds to the R-2 Zone for residential uses and the O-1 Zone for non-residential uses:

- A. Conditional Uses
 - 1. Uses listed as permissive in the R-C Zone; floor area requirements do not apply.
 - 2. Existing non-conforming uses are to be treated as approved conditional uses.
- B. Drive-thru building type not allowed in this category.
- C. There is no maximum floor area ratio or density limit for R-2 residential uses.

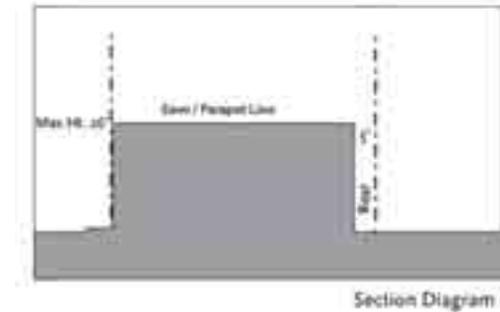
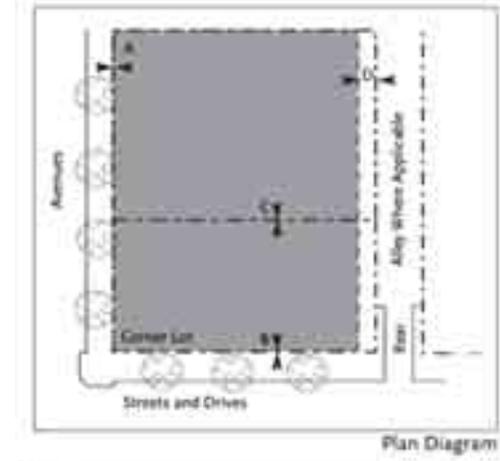
2. Architecture: Form & Elements

- A. Placement- openings shall occur along street façades at thirty feet (30') on center minimum; openings to be three feet (3') wide minimum.
- B. Ground floor building frontage shall be designed with 30-70% of the building frontage length glazed, with the window sill no higher than thirty inches (30") above the finished floor.
- C. Entrance - There shall be at least 1 functioning entry door for every thirty feet (30') of building frontage length on street frontages.
- D. Articulation - building façade at front and side street shall change each twenty five feet (25') minimum in height, or setback, or material.
- E. Property walls & fences - as allowed in current City regulations at fronts, sides & rears of buildings.

3. Signs

According to general sign regulations in the COA Zoning Code.

Building Placement and Height



4. Setbacks

- A. Buildings shall be placed within the shaded area as shown in the above diagram.
 - 1. Front Build to Line: 0' - 8' per lot
 - 2. Side Street Setback: five feet (5')
 - 3. Side yard Setback: 0'
 - 4. Rear Setback: five feet (5')

5. Height

Building height shall be measured in feet from grade as defined in the zoning code to top of parapet or midpoint of pitch.

- A. Maximum: 2 stories/twenty six feet (26') high maximum, see Allowable Building Heights Plan.



Building Frontage and Profile

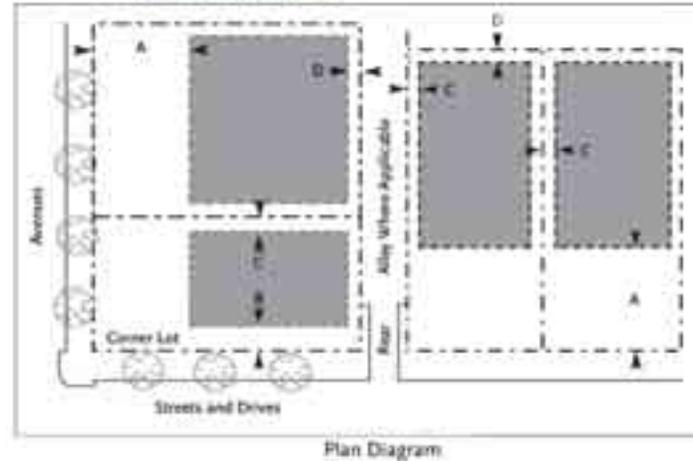
6. Encroachments Allowed

No encroachments allowed.

7. Frontage Types

Arcade, shop front and forecourt (see Frontage Types Page 104). Not less than 50% of the building facades shall be accompanied by arcades, cantilevered shed roofs, or awnings.

Parking Placement

**8. Parking Requirements**

- A. On-site parking is allowed only in the shaded areas as shown above and defined in the text below.
 1. Front Setback: 30% of lot depth minimum, unless below grade. Parking lots and off street parking shall be behind or within a building or buildings adjacent to the fronting street.
 2. Side Street Setback: six foot (6') minimum with exception of parking completely below grade whose setback is 0" minimum
 3. Side yard Setback: five feet (5') minimum
 4. Rear Setback: five feet (5') minimum
- B. Parking requirements below are subject to reductions due to credits allowed by the zone code:
 1. Office: 3 per 1,000 square feet
 2. Residential: 1 space/bedroom, 2 spaces maximum.
- C. Vehicular access is permitted only from the side streets and from an alley.

- D. Existing garages at rears of properties of existing access drives from streets are permitted.
- E. Surface parking areas at rears of properties shall be shielded from streets by wall, fence, or solid, vertical landscaping. Screening shall be a minimum of 30 inches in height but shall not exceed 36 inches in height.

9. Landscape Requirements

The landscape design should reflect the more urban character of this area in its programming, detailing, and planting intensity. 10% of parking lot areas shall be planted with irrigated shade producing trees. All side street setback areas shall be landscaped to screen parking areas. All properties must comply with Section 14-16-3-10, Landscaping Regulations, of the Zoning Code.

Option: Urban Street/Conventional

10. Urban Street

All properties fronting on an *avenue* or a *boulevard* in this zone will be required to conform to these urban street regulations.

Properties fronting on a *drive* or a *street* have the option of developing under these urban street regulations or choosing the conventional option.

11. Conventional Option

The conventional option means that the project will be required to meet the requirements of the traditional C-2, R-2, R-3 and O-1 zones, as defined in the City of Albuquerque Zoning Code, with the exception of use and height which has been redefined by this OR zone (or CCR Zone depending on which zone is applicable).



Office/Residential Two - OR-2



1. General Rules: Uses

The OR Office/Residential land use category corresponds to the R-2 Zone for residential uses and the O-1 Zone for non-residential uses:

- A. Conditional Uses
 - 1. Uses listed as permissive in the R-C Zone; floor area requirements do not apply.
 - 2. Existing non-conforming uses are to be treated as approved conditional uses.
- B. Drive-thru type building type not allowed in this category.
- C. There is no maximum floor area ratio or density limit for R-2 residential uses.

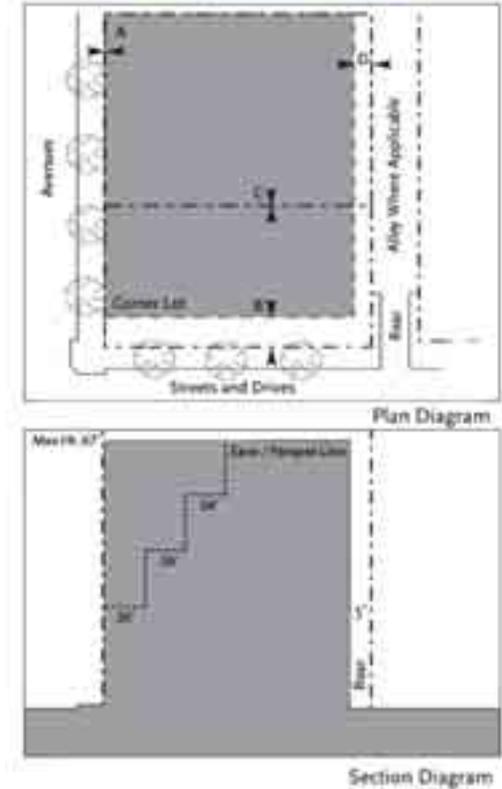
2. Architecture: Form & Elements

- A. Placement- openings shall occur along street façades at thirty feet (30') on center minimum; openings to be three feet (3') wide minimum.
- B. Ground floor building frontage shall be designed with 30-70% of the building frontage length glazed, with the window sill no higher than thirty inches (30") above the finished floor.
- C. Entrance - There shall be at least 1 functioning entry door for every thirty feet (30') of building frontage length on street frontages.
- D. Articulation - building façade at front and side street shall change each fifty feet (50') minimum in height, or setback, or material.
- E. Property walls & fences - as allowed in current City regulations at fronts, sides & rears of buildings.

3. Signs

According to general sign regulations in the zoning code.

Building Placement and Height



4. Setbacks

- A. Buildings shall be placed within the shaded area as shown in the above diagram.
 - 1. Front Build to Line: 0' - 8' per lot
 - 2. Side Street Setback – Adjacent to Surface Parking Only: 10'
 - 3. Side yard Setback: 0'
 - 4. Rear Setback: five feet (5')

5. Height

Building height shall be measured in feet from grade as defined in the zoning code [on site] to top of parapet or midpoint of pitch.

- A. Maximum: 3 stories/thirty-nine feet (39') high to 5 stories/sixty-seven (67') high, see Allowable Building Heights plan.



Building Frontage and Profile

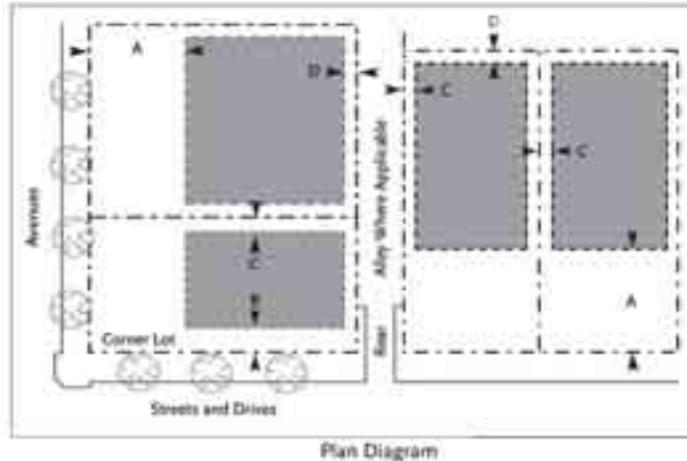
6. Encroachments Allowed

No encroachments allowed.

7. Frontage Types

Arcade, shop front and forecourt (see Frontage Types Page 104). Not less than 50% of the building facades shall be accompanied by arcades, cantilevered shed roofs, or awnings.

C. Parking Placement



Plan Diagram

8. Parking Requirements

- A. On-site parking is allowed only in the shaded areas as shown above and defined in the text below.
 1. Front Setback: 30% of lot depth minimum, unless below grade. Parking lots and off street parking shall be behind or within a building or buildings adjacent to the fronting street.
 2. Side Street Setback: six foot (6') minimum with exception of parking completely below grade whose setback is 0" minimum
 3. Side yard Setback: five feet (5') minimum
 4. Rear Setback: five feet (5') minimum
- B. Parking requirements below are subject to reductions due to credits allowed by the zone code:
 1. Office: 3 per 1,000 square feet
 2. Residential: 1 space/bedroom, 2 spaces maximum/DU
- C. Vehicular access is permitted only from sides streets and the alley. Parking garages shall have liner buildings at all street frontages and solid 3'-0" minimum high walls [all levels] or solid landscape at side and rear property lines;

provided, however, if the side or rear property line is adjacent to a residentially-zoned lot, the wall of the parking structure must be entirely solid, without opening.

- D. Existing garages at rears of properties of existing access drives from streets are permitted.
- E. Surface parking areas at rears of properties shall be shielded from streets by wall, fence, or solid, vertical landscaping. Screening shall be a minimum of 30 inches in height but shall not exceed 36 inches in height.

9. Landscape Requirements

The landscape design should reflect the more urban character of this area in its programming, detailing, and planting intensity. 10% of parking lot areas shall be planted with irrigated shade producing trees. All side street setback areas shall be landscaped to screen parking areas. All properties must comply with Section 14-16-3-10, Landscaping Regulations, of the Zoning Code.

Option: Urban Street/Conventional

10. Urban Street

- A. All properties fronting on an *avenue* or a *boulevard* in this zone will be required to conform to these urban street regulations.
- B. Properties fronting on a *drive* or a *street* have the option of developing under these urban street regulations or choosing the "conventional" option.

11. Conventional Option

The conventional option means that the project will be required to meet the requirements of the traditional C-2, R-2, R-3 and O-1 zones, as defined in the City of Albuquerque Zoning Code, with the exception of use and height which has been redefined by this OR zone (or CCR Zone depending on which zone is applicable).



General Regulations

1. Signage

- A. One projecting sign, not to project more than 30" may be allowed for each business provided:
 - 1. Sign is separated by at least 25' from any other projecting sign, and
 - 2. There is not another sign for the same business on the premises.
 - 3. Sign heights shall not exceed 5' above the height of a building [parapet or midpoint of pitched roof].
- B. Wall signs shall not exceed 20% of a building front's façade area.
- C. Illumination, motion: in addition all regulations in the City's sign code [Section 14-16-3-5 ROA 1994], change of illumination may produce apparent motion of the visual image on signs.
- D. Allowable types of signage: 3-dimensional, blade, or projecting signs, wall-mounted signs w/ recessed or raised letters; letters [individual] of metal or light box construction; masonry or metal plaques; street address numbers; awning signage - cloth, metal, or glass, shall be open at sides & bottom [no boxed or 1/4 round shapes]; neon; window paint - names or logos may be painted on the street level windows. Signage details:
 - 1. Individual letters shall not exceed 18 inches in height or width and 3 inches in relief.
 - 2. Painted window signs shall fit in 8 sq. ft. area or less.
 - 3. Masonry or bronze plaques shall fit in an 18 sq. ft. area or less.

2. Lighting

- A. Storefronts - street level storefronts shall be lit @ the exterior w/ building-mounted fixtures [in compliance with City Dark Sky standards] during dark hours of the day.
- B. Building sides & rears - if lit, may use floods or directional lights and must be shielded or aimed to prevent view of lamp filament/source from areas off of the property.
- C. Lighting Lamp elements: shall be incandescent, metal halide, or halogen only. HID or fluorescent not allowed [except compact fluorescents which screw directly into standard sockets].
- D. Public Streets - in addition to City standard lighting for traffic, sidewalk areas lighting shall be pedestrian-scaled lamp on shaft/base, w/ head @ 12' to 15' high, funded as Public infrastructure.
- E. All light fixtures shall be in compliance with Section 14-16-3-9 ROA 1994, Area Lighting Regulations of the Comprehensive City Zoning Code.

3. Historic Preservation

- A. All buildings on the State and National Historic Registers, City Landmarks, and Characteristic Buildings shall be preserved within the project area. Alteration, demolition, and/or new construction to buildings on the State and National Historic Registers, City Landmarks and Characteristic Buildings in Figure 3 shall be reviewed by the LUCC staff and a report provided in a timely way to the Development Review Division of the Planning Department. LUCC staff shall proceed to have designated Characteristic Buildings, the Nob Hill Business Center, Monte Vista Fire Station, De Anza Motor Lodge, and Solar Building as City Landmarks.

4. Environmental Planning Commission Review

- A. Sites larger than 5 acres require review and approval from the Environmental Planning Commission. Those sites over 5 acres in size at the adoption of this plan shall comply with this standard regardless of platting action.

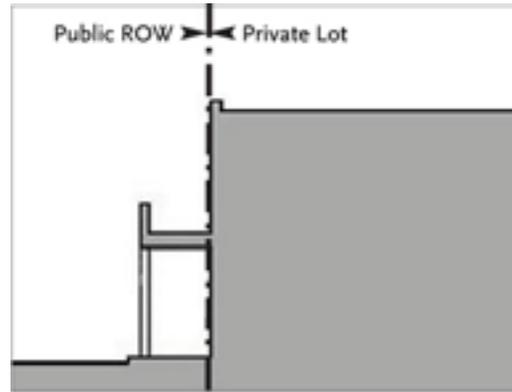


5. Frontage Types

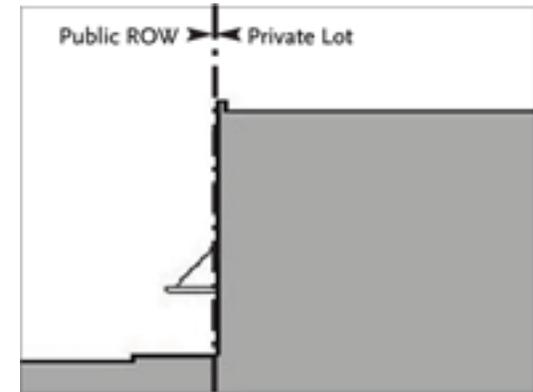
Frontage Types are applied to each zone. These represent a range of additions to the basic facade of the building, in the following illustrations, "ROW" means the public street right-of-way.

Allowed Frontage Types

Type	CCR-1	CCR-2	CCR-3	OR-1	OR-2
Arcade	★	★	★		
Shop Front	★	★	★	★	★
Stoop				★	★
Forecourt	★	★	★		



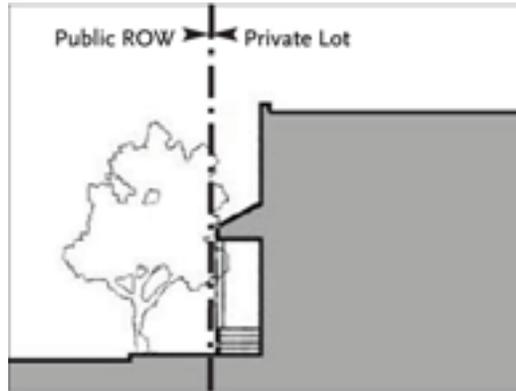
A. Arcade
The facade of a building with an attached colonnade. Balconies may overlap the sidewalk while the ground floor remains set at the lot line. This type is ideal for retail use, but only when the sidewalk is fully absorbed within the arcade so that a pedestrian cannot bypass it. An easement for private use of the right-of-way is usually required. To be useful, the arcade should be no less than 8 feet wide clear in all directions.



B. Shop front
The facade is placed at or close to the right-of-way line, with the entrance at sidewalk grade. This type is conventional for retail frontage. It is commonly equipped with cantilevered shed roof or awning. The absence of a raised ground floor story precludes residential use on the ground floor facing the street, although this use is appropriate behind and above.

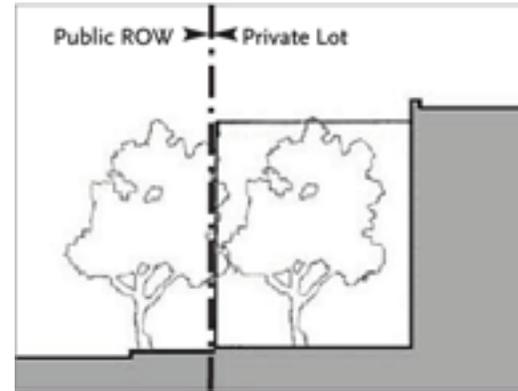


PLAN IMPLEMENTATION / REGULATIONS



C. Stoop

The facade is placed close to the frontage line with the ground story elevated from the sidewalk, securing privacy for the windows. This type is suitable for ground floor residential uses at short setbacks. This type may be interspersed with the shop front. A porch may also cover the stoop.



D. Forecourt

The facade is aligned close to the frontage line with a portion of it setback. The resulting forecourt is suitable for gardens, vehicular drop offs, and outdoor dining. This type should be used sparingly and in conjunction with the shop fronts. A fence or wall at the property line may be used to define the private space of the yard. The court may also be raised from the sidewalk, creating a small retaining wall at the property line with entry steps to the court.



2. Zoning – Residential Districts

Monte Vista/College View Historic District

Issues and Analysis

The Monte Vista and College View Historic District has distinctive characteristics in architectural styles, subdivision pattern and streetscape. Preservation of the district’s historic character is a primary goal of this plan. Of particular interest to the residents of the neighborhood are scale and massing of buildings, which have historically been one-story with uniform front yard setbacks, varying by block from 20 feet up to 40 feet.

The historic architectural styles in the district, with distinctive details and materials, height and massing, and the historic streetscape are important features to preserve. Individual freedom to renovate and upgrade the homes over time is also important to the community, as long as the overall character of the neighborhood is maintained. Height, massing and streetscape are regulated through zoning. The approach to architectural styles, materials and other details is education and voluntary guidelines.

Solar access is a primary concern. Special neighborhood zoning incorporates the City’s existing Building Height Limitations to Preserve Solar Access for all lots regardless of the age of the subdivision.

The properties that are proposed for rezoning are shown on the enlarged segment of the Zoning Map.

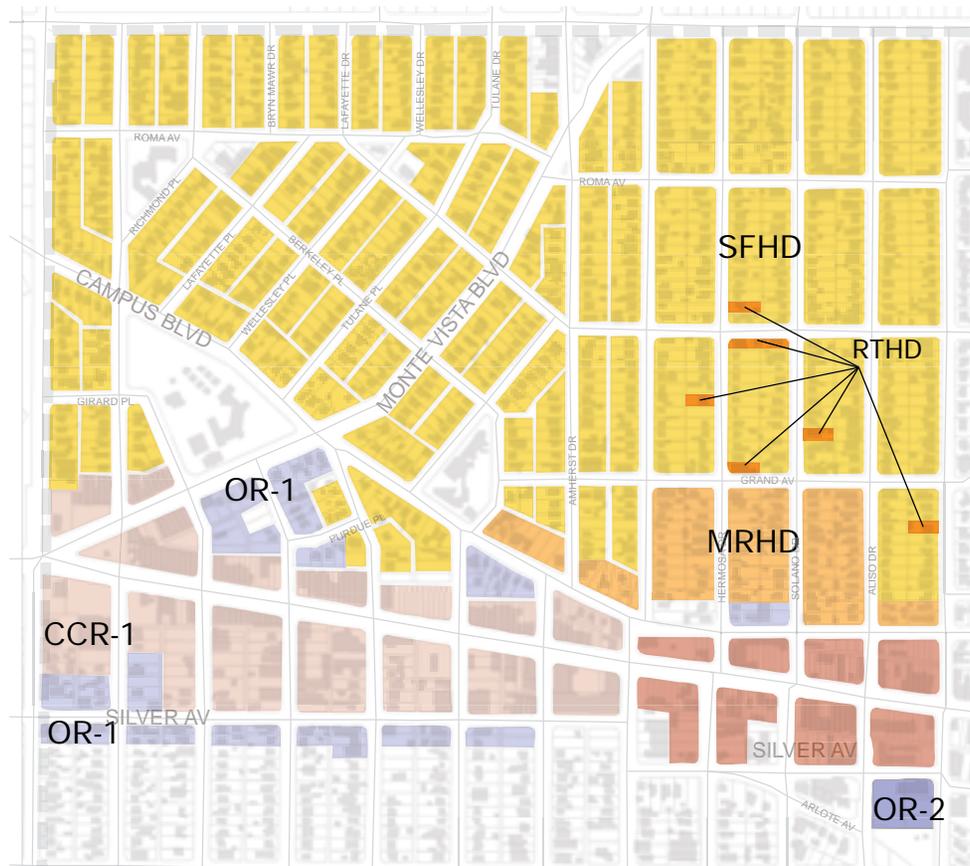
Zoning Proposals

The plan designates the residential portion of the historic district as part of the SU-2 Special Neighborhood Zone for the plan area. This zone is based upon the existing residential zones with minor modifications. The new zone will

- Preserve the historic streetscape;
- Preserve the scale of buildings from the street; and
- Preserve solar access to properties, while allowing homeowners the flexibility to renovate and expand their homes.

Zoning Districts

New SU-2 zoning districts are added to the Sector Development Plan zone map for properties within the Monte Vista and College Heights Historic District and zoned R-1, R-T or MR prior to the adoption of this plan.



SU-2/SFHD

The SU-2/SFHD Single Family Historic District zone corresponds to the R-1 Zone in the Zoning Code with the following exceptions:

A. Conditional Uses

1. A carport within the front yard setback is not allowed.
2. Walls, fences and retaining walls greater than three feet in height are not allowed in the front yard setback.
3. Existing nonconforming uses are to be treated as approved conditional uses.

B. Height

1. Buildings shall comply with the Building Height Limitations to Preserve Solar Access, §14-16-3-3 (A) (7) of the Zoning Code. The additional height limitations of this section shall apply to any lot regardless of the age of the building, except that structures existing as of the date of adoption of the Plan are allowed subject to Zoning Code regulations for buildings that are nonconforming as to height.
2. Building height above 15 feet and second story additions to existing buildings must be set back a minimum of five feet from the front façade to preserve the scale of the original building.

SU-2/RTHD

The SU-2/RTHD Townhouse Residential Historic District zone corresponds to the RT Zone in the Zoning Code with the following exceptions:

A. Conditional Uses

1. A carport within the front yard setback is not allowed.
4. Walls, fences and retaining walls greater than three feet in height are not allowed in the front yard setback.
5. Existing non-conforming uses are to be treated as approved conditional uses.

B. Height

1. Buildings shall comply with the Building Height Limitations to Preserve Solar Access, §14-16-3-3 (A) (7) of the Zoning Code. The additional height limitations of this section shall apply to any lot regardless of the age of the building, except that structures existing as of the date of adoption of the Plan are allowed subject to Zoning Code regulations for buildings that are nonconforming as to height.
2. Building height above 15 feet and second story additions to existing buildings must be set back a minimum of five feet from the front façade to preserve the scale of the original building.

SU-2/MRHD

The SU-2/MRHD Mixed Residential Historic District zone corresponds to the RT Zone in the Zoning Code with the following exceptions:

A. Conditional Uses

1. Uses listed as permissive in the R-2 Zone are conditional uses in this zone.
2. A carport within the front yard setback is not allowed.
3. Walls, fences and retaining walls greater than three feet in height are not allowed in the front yard setback.
4. Existing non-conforming uses are to be treated as approved conditional uses.

B. Height

1. Building height shall not exceed 26 feet. Buildings shall comply with the Building Height Limitations to Preserve Solar Access, §14-16-3-3 (A) (7) of the Zoning Code. The additional height limitations of this section shall apply to any lot regardless of the age of the building, except that structures existing as of the date of adoption of the Plan are allowed subject to Zoning Code regulations for buildings that are nonconforming as to height.

2. Building height above 15 feet and second story additions to existing buildings must be set back a minimum of five feet from the front façade to preserve the scale of the original building.

C. Parking

No more than two parking spaces shall be allowed in the 15 foot front yard setback of any premises, in addition to a driveway. The remaining front yard setback shall be landscaped.



R-1 – West of Washington Street

A. Solar Access

1. Properties west of Washington Street zoned R-1 shall comply with the Building Height Limitations to Preserve Solar Access, Sec. 14-16-3-3(A) (7) of the Zoning Code. The additional height limitations of this section shall apply to any lot regardless of the age of the building, except that structures existing as of the date of adoption of the Plan are allowed subject to Zoning Code regulations for buildings that are nonconforming as to height.



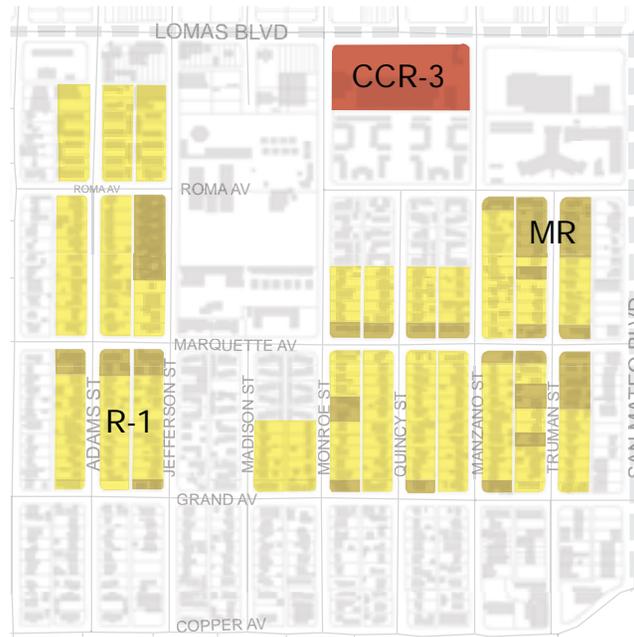
Highland Neighborhood

Issues and Analysis

In the late 1950's and through the 1960's the concept of "urban centers" along major arterial corridors was very prevalent in the southwest. Phoenix, in particular, developed a plan to establish a number of these centers along their Central Avenue west of downtown. Albuquerque in an apparent attempt to model this concept established the area around Central and San Mateo Avenues as this community's first urban center. This idea coincided with the establishment of zoning in Albuquerque in 1959 and led to the establishment of high intensity yet segregated land use zoning in the Central and San Mateo area. In the Highland Neighborhood this took the form of R-3 zoning for the interior of the neighborhood to provide the opportunity for high density housing to support this coming center of activity. Apparently this R-3 zoning was put into place even though there were many single family housing units already built in the area. Today the majority of non-commercial areas in the neighborhood have been built as single family dwelling and the existence of the R-3 zoning is an ever present destabilizing influence in the neighborhood.

In an attempt to stabilize the zoning and future land use of the Highland Neighborhood this Sector Plan proposes to accomplish changes to protect single family blocks. The R-3 properties that are currently developed as single family to R-1 are rezoned from R-3 to R-1. Existing duplexes, townhouses, or homes with second units within these blocks are rezoned from R-3 to MR. Parcels and blocks developed with apartment buildings or predominantly higher density multi-family, remain R-3.

The properties that are proposed for rezoning are shown on the enlarged segment of the Zoning Map.



Zoning Proposals

In the Highland area between Copper and Lomas from Washington to San Mateo, the plan designates single family homes as R-1. Properties that have existing apartments, duplexes, and accessory dwelling units are rezoned as SU-2/MR, a zoning category existing in the 1987 Nob Hill Sector Development Plan. The intent of these changes is to support the overall health of the area, and ensure stable land use for homeowners. The zoning maintains the mixed densities in the neighborhood, but discourages the deterioration of existing single family homes.

Zone Change

Areas rezoned from R-3 to R-1 and MR are shown in the zoning map.

The R-1 Single Family residential zone is the same as the R-1 zone in the Zoning Code.

The MR Mixed Residential zone corresponds to the RT Zone in the Zoning Code with the following exceptions:

- A. Conditional Uses:
 1. Uses listed as permissive in the R-2 Zone.
 2. Existing non-conforming uses are to be treated as approved conditional uses.
- B. No more than two parking spaces shall be allowed in the 15-foot front yard setback of any premises, in addition to a driveway. The remaining front yard setback shall be landscaped.

