

Exhibit A. Sawmill Sector Plan Amendment



Sawmill/Wells Park Sector Development Plan Transportation and Design Amendments

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City of Albuquerque

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1. Overview.

1. Content and Format

This exhibit is an amendment to Chapters 6 and 7 of the Sawmill/Wells Park Sector plan. The amendment replaces Sections 2 and 3, of Chapter 6 and inserts two additional sections (12 and 13) into the chapter. A new section pertaining to land use and building form recommendations is also inserted into Chapter 7 following the Issues and Analysis Section. The exhibit is formatted into 5 sections that address issues in two focus areas, through regulations that are followed by recommendations for implementation.

The Regulations include:

- Minimum block sizes and connectivity nodes.
- Streetscape design.
- Design of roadway cross sections, intersections, sidewalks, planting strips, driveway access and off-street parking facilities.

The Recommendations include:

- A roadway connectivity scenario derived from a network analysis.
- A neighborhood traffic management plan
- Transit service improvements
- Improved Bicycle and Pedestrian Facilities.
- Land use and building forms that complement the near and long-term connectivity standards along with coordinating with the streetscape and recreational trail design standards.

The Sections Include:

Section 1 is the Overview. **Section 2** has two parts; the **first** establishes roadway regulations that facilitate connectivity for automobile, transit, bicycle and foot travel while mitigating adverse impacts from traffic noise and congestion. These regulations mandate maximum block sizes, multiple connections from development projects to adjacent streets and the design rules for street cross sections. The **second** part presents recommended roadway connectivity schemes that are the product of a network analysis required by the Sawmill Wells Park Sector Plan. **Section 3** provides regulations and recommendations for off-street parking located within a cluster of small lots separated by public areas or within parking structures that are shielded by building liners. **Section 4** identifies mechanisms to create funding to implement public projects. **Section 5** offers land use and zoning recommendations that better integrate land uses and transportation facilities.

The amendment covers both the Western and Eastern Focus Area which are subsets to the Sawmill/Wells Park Sector Plan as amended in 2002. The **Western Focus Area** is in transition from heavy industrial uses to commercial and residential uses. It encompasses the entire Sawmill Neighborhood and a small portion of the Wells Park Neighborhood.

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The Western Focus Area is defined on the north by Interstate 40 (I-40) to the west by Rio Grande Boulevard from I-40 to Bellamah Avenue, east on Bellamah to 20th Street, and south on 20th to Mountain Road. The south and east boundary lines are set along Mountain Road from Rio Grande Boulevard to 12th Street, north on 12th to Bellamah Avenue, east on Bellamah to 8th Street, north on 8th to the railroad spur, west along the railroad spur to 12th Street and then north to I-40 (see figure 1).

The **Eastern Focus Area** includes the remaining portion of the Wells Park Neighborhood. The neighborhoods south of Aspen Avenue primarily reflect stable commercial and residential areas that are undergoing home renovations and residential infill on vacant lots. The section north of Aspen is a stable, but aging, warehouse district that has the potential to transition to a mixed use district.

A large parcel bordering 12th Street and the I-40 frontage road is excluded from the circulation plan. This land was recently deeded to the Indian Pueblo Federal Development Corporation and was designated by congress as trust lands of the Department of Interior. This places the property beyond the City's jurisdiction. Fourth Street was also excluded from this amendment. Policies and recommendations for Fourth Street are included in the Fourth Street Corridor Plan.

2. A new Section 2 is created for Chapter 6 of the Sawmill/Wells Park Sector Plan to regulate the roadway and streetscape development.

B 2.a Plan Description. The Sawmill/Wells Park Sector Development Plan functionally links transportation to land use and encourages street designs that promote walking, bicycling and transit while still accommodating automobiles and the large-vehicle traffic generated by industrial uses. These goals are consistent with Policy II-4-q of the Albuquerque Bernalillo Comprehensive Plan which states,

“Transportation investments should emphasize overall mobility needs and choice among modes in the regional and intra-city movement of people and goods. This includes providing adequate street capacity and right-of-way to meet access and mobility needs and improving the effectiveness of the existing street system by encouraging bicycling, walking, and use of mass transit in and between the activity centers.”

An interconnected roadway system is crucial to redeveloping the Sawmill/Wells Park Area. Interconnected roadways forge intra and inter-neighborhood connections that tie communities by using multiple modes (foot, bicycle, transit, or automobile) to provide a convenient means for residents to interact with each other. The benefits include reduced travel distances; redundant access for emergency vehicles; and improved travel-time for the residents and service providers, such as mail delivery, parcel package service, transit routes, and solid waste pickup.

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In contrast, roadway systems with few street connections and discontinuous networks are a barrier to travel between neighborhoods. A dearth of street connections results in limited access to essential services and sidewalk networks that are non-continuous. This provides motorists few alternative travel routes, funnels automobile trips onto a limited number of roadways, and discourages travel by foot and bicycle.

As required by the Sawmill/Wells Park Sector Plan a transportation network and land-use analysis was conducted. The analysis is based on the existing sector plan's policies, the property owners' intended use of their lands, and the impacts of the recommendations proposed in this task for the land uses and building forms in the focus areas. Included in the study were interviews with private property owner's, meetings with transportation and planning officials from the City of Albuquerque and the Mid Region Council of Governments (MRCOG), City planning staff, area residents, and the Sawmill Community Land Trust, (a non-profit agency that redevelops properties in the area).

From the analysis a recommended interconnected roadway system scenario was developed that also insulates residential and mixed-use areas from heavy traffic, noise, vibrations, and other environmental impacts of industrial zones. It provides an efficient street system for moving vehicles and pedestrians and conducting commerce.

2.b Definitions. For the purposes of this task the following definitions shall apply.

Central Core: Generally located west of 12th Street, east of 20th Street, north of Bellamah Road and South of Aspen Avenue.

Collector Street: As defined in §14-14-1-6 ROA 1994.

Curb Cut: Narrow curb cuts placed at street intersections allow someone in a wheelchair, on a toddler's tricycle etc., to move onto or off a sidewalk without difficulty. Wider curb cuts are used for motor vehicles to enter a driveway or parking lot on the other side of a sidewalk. Accessible pedestrian curb cuts transition from the low side of a curb to the high side (usually 15 centimeters change in level). Accessible curb ramps are a minimum of 1 meter wide. For every twelve meters of horizontal distance, they rise no more than one meter. Driveway curb cuts transition from the low side to the high side of the curb at a verticle angle to allow a sedan to cross the curb cut without the bottom of the car contacting the surface.

Edge Zone: The pedestrian right-of-way between the curb and either a planting strip or tree well that provides space for pedestrians to ingress or egress from a parked vehicle, with room for a vehicle door to open and a transit stop.

Liner Building: A specialized building, parallel to the street, which is designed to conceal an area such as a parking lot or loading dock. While liner buildings may include commercial or residential uses, their limited depth (from front to back) makes them more disposed to residential use.

Local or Major Local Commercial Street: A local or major local street as defined in §14-14-1-6 ROA 1994, where the right-of-way is bordered by properties zoned commercial, industrial or manufacturing and where the predominant land uses are not residential.

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Figure 1: Sawmill Wells Park Sector
Development Plan Focus Area

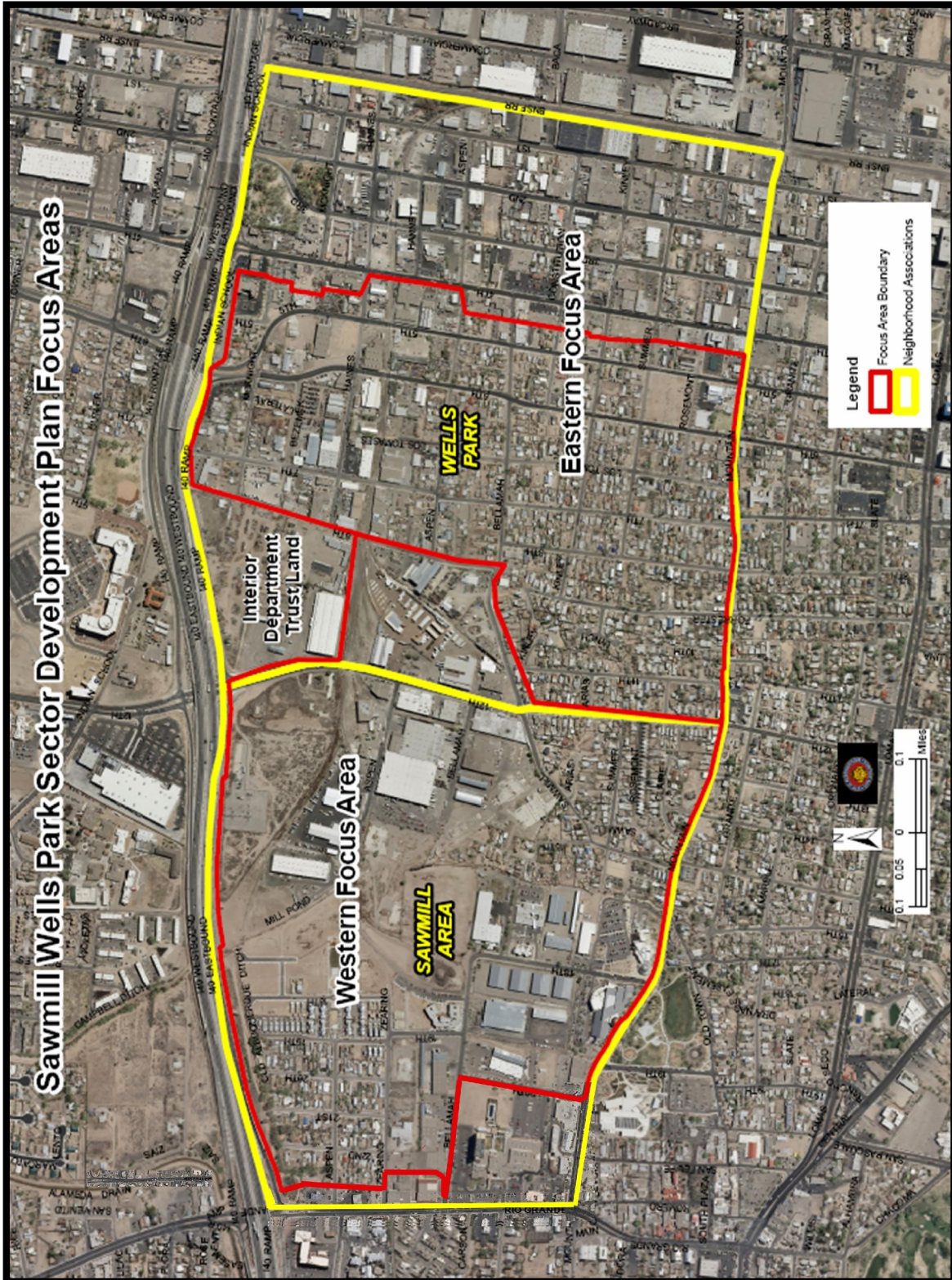


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Local or Major Local Residential Street: A local or major local street as defined in §14-14-1-6 ROA 1994, where the right-of-way is bordered by properties zoned residential or multi-family, and where the predominant land uses are residential.

Minor Arterial Street: A street as defined in §14-14-1-6 ROA 1994.

Pedestrian Lighting: Exterior lighting applications where people travel by foot, or on bike along pathways, walkways, bikeways and roadways.

Permeable Pavers: Pavers that allows water to percolate to the underlying soils while providing a stable surface for vehicles. Permeable pavers are made from many different types of materials.

Redevelopment: Any proposed project that will change the external building square footage by more than 10% or remodel or rehabilitate more than 25% of a structure and requires a City Building Permit.

Security Light: Outdoor light with infrared or microwave sensors that light up whenever someone or something passes within a certain range.

Sidewalk Commercial Uses: Commercial activities permitted to occur on a sidewalk per §14-16-3-18-(C) of the City Zoning Code.

Street Connectivity: Streets with multiple routes and connections serving the same origins and destination; the connectivity level increases or decreases as the number of routes and connections increase or decrease.

Streetscape: The area within a roadway public right-of-way or public roadway easement that includes hardscaping, landscaping, public art, and street furniture.

Traffic Management Plan: Traffic calming is the combination of mainly physical measures that reduce the negative effects of motor vehicle use, alter driver behavior and improve conditions for non-motorized street users.

2. c Regulations: Development Process Manual (DPM) Procedures:

c.1. Sector plans may adopt alternative design standards to the DPM procedures subject to review and approval by the Environmental Planning Commission (EPC). The EPC and/or the Design Review Board (DRB). The Planning Director shall be added to all sector plan area project signature blocks to ensure compliance with sector plan policies. Construction plans for public projects in the Sawmill/Wells Park Sector Plan area shall proceed through the normal Design Review Committee (DRC) process. The standards shall be consistent with the DPM. The Planning Director shall be included in all design review and added to the signature blocks for project within the sector plan area to ensure compliance with the plan's policies.

c.2. During each phase of the City design review process, proposed tree species, bench and lighting fixture designs and other design details shall be reviewed by a Plan Implementation Committee and adjacent property owners. The Plan Implementation Committee will be established by the City Planning Director and is to be comprised of residents of the neighborhood, property owners and City Staff.

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c.3. City contracts for designing and developing public projects shall specify that contractors are required to notify and include the residents and property owners in the neighborhood in all phases of project design review from concept to final design and construction. A representative of the committee shall be invited to attend the DRB and/or DRC meetings to ensure committee inclusion in design decisions.

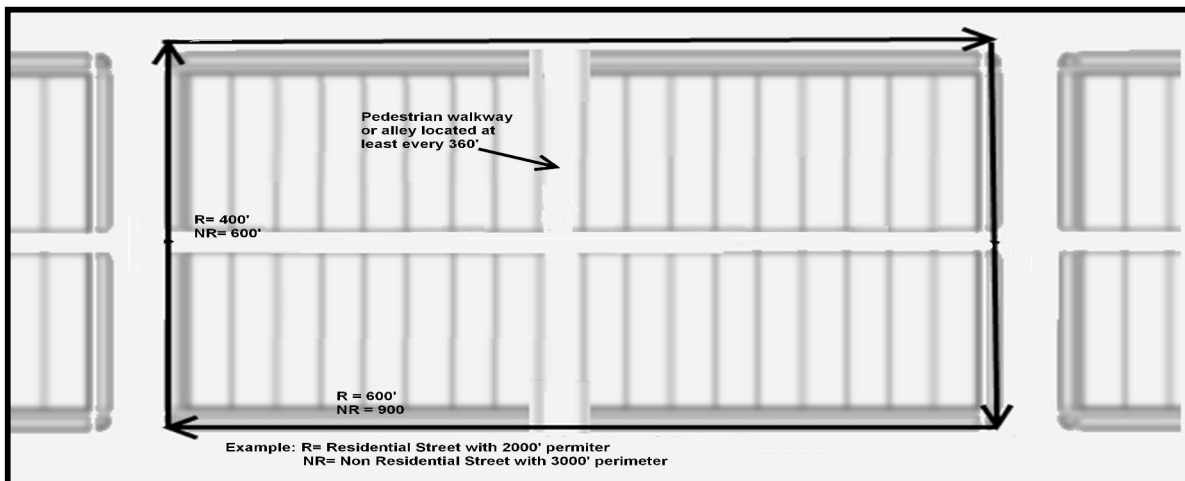
2.d Regulations: Western Focus Area Street Connectivity.

d.1. For site plans over 4 acres that require construction of a street per the DPM, the DRB, or the EPC there shall be at least one street within the site plan area with two street connections to the existing street system. Each street connection shall be to a different roadway. If a property has an irregular shape or borders existing structures that prohibits two street connections, the EPC or the DRB may require only one connection.

d.2. If the property is subdivided either pedestrian walkways or alleyways shall be provided.

d.3. Blocks shall be platted into rectangular shapes. If a property is irregularly shaped prohibiting subdivision into rectangular blocks, the EPC or the DRB may waive this requirement. The perimeter of a block shall be no greater than 2,000 linear feet for lands zoned for residential uses and 3,000 linear feet for lands zoned for commercial uses. Block lengths greater than 360 linear feet shall be divided by an alley or a pedestrian corridor.

Figure 2: Approved Sawmill Artisan Village Project Interior Road Connectivity



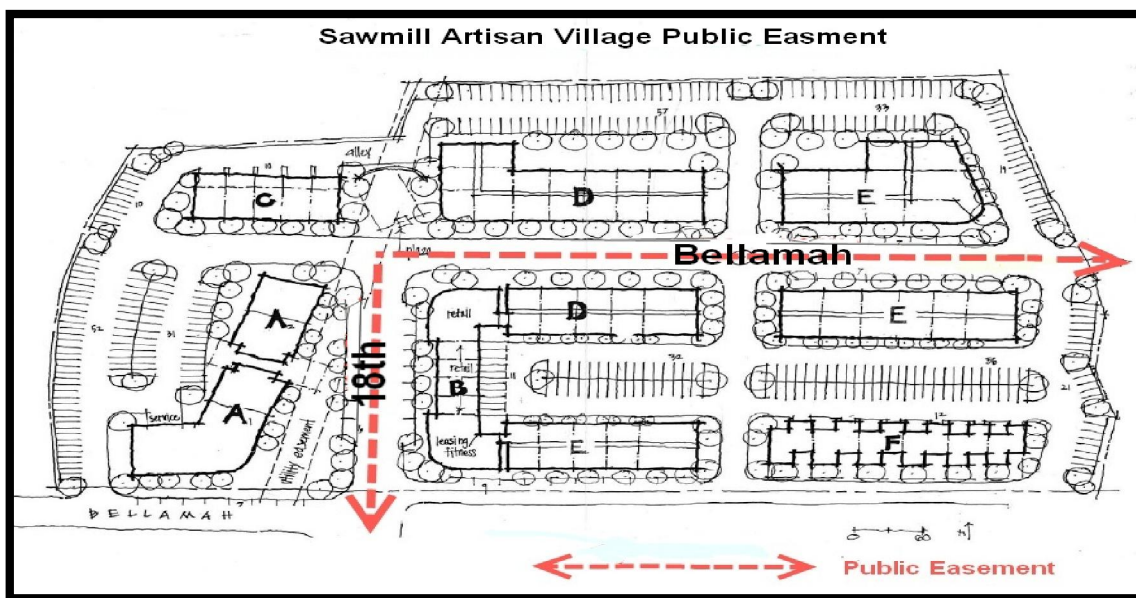
2.e Regulations: Western Focus Area Local and Major Local Roadways and Intersection Design General Procedures for Existing Streets.

e.1. The City, as resources and funding permits, shall reconstruct the cross-section of local and major local streets between the back of sidewalks or easements as set forth in the design regulations in 2.e. and 2.f. of this task. This is consistent with the

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provisions of Ordinance-02-39 (2) that the City shall “foster community in older and newer neighborhoods and prioritize the needs of the older parts of Albuquerque in terms of vitality and development.

Figure 3: Block Lengths and Perimeters
Example Perimeters, Residential and Non Residential Blocks Per



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e.2. The property owners shall construct the cross-section of local and major local streets behind the curbs including the planting strips and sidewalks. This shall occur once the ordinance requirement to install perimeter landscaping is triggered.

Figure 4: Cross Section Responsibilities City and Property Owners

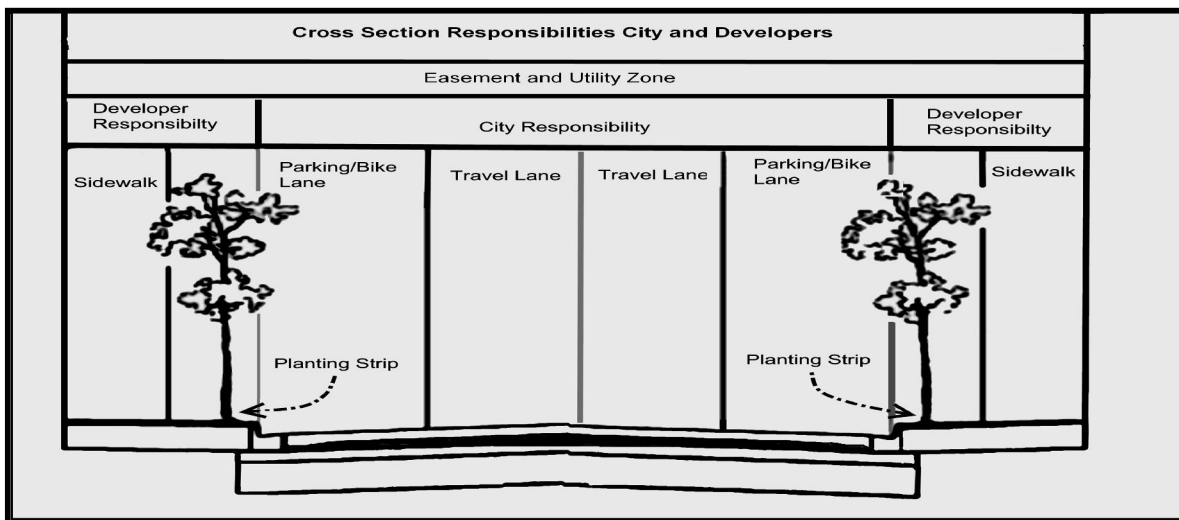
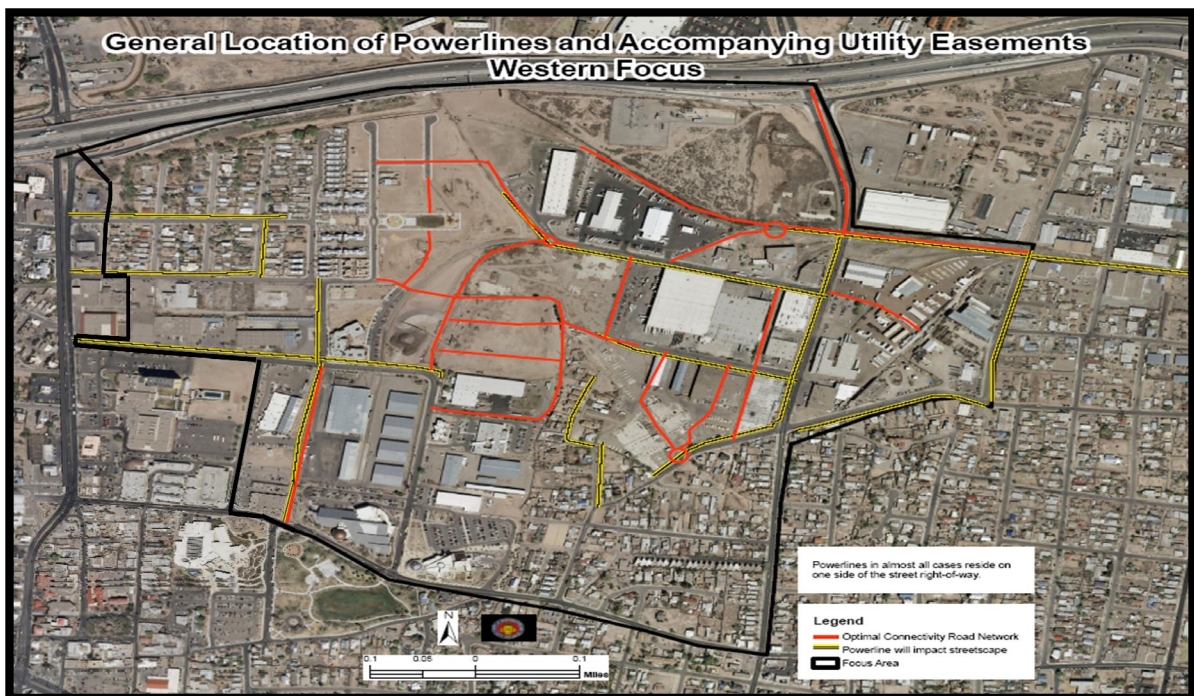


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e.3. Normal right-of-way width for local and major local streets is 50 feet unless otherwise specified in this task.

e.4. Power line corridors, whenever possible, shall be located within alley ways instead of streets. Where the street right-of-ways include power lines and utility easements, cross section designs shall accommodate the maintenance and repair of utilities and provide for the safety and ease of use by motorists and pedestrians. On arterial and collector streets, utilities shall be buried where financially and technically feasible.

Figure 5; General Location of Power lines and Accompanying Easements Western Focus



e.5. To improve the focus area's attraction for pedestrians the City shall implement traffic calming strategies including narrowing lanes and adding on-street parking and medians. Rights-of-way shall include ample sidewalk space, landscaped planting strips and pedestrian lighting capable of lighting all pedestrian areas in the public right-of-way without casting light on private properties. Benches shall be located near the museums and businesses to accommodate people who are unable to walk long distances. Utility poles, signs and other items that often obstruct sidewalks shall be placed within landscaped strips when possible.

e.6. Historic markers shall be erected where appropriate, along collector and arterial streets, public trails, and on public properties.

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e.7. 12th Street: Streetscape Design (Interstate 40 to Mountain Road).

Existing Conditions: 12th Street is a gateway from I-40 into the Sawmill/Wells Park Neighborhoods and the Central Business District. The street is a four-lane minor arterial between I-40 and Sawmill Road, carrying more than 12,000 vehicles per day through a commercial and industrial corridor. South of Sawmill Road, 12th Street becomes a two-lane minor arterial carrying 8,500 vehicles per day through a residential neighborhood of single-family homes. Along this segment the sidewalks have been repaired and pedestrian lighting was installed through a joint project of the Sawmill/Wells Park Metropolitan Redevelopment Area (MRA) Infrastructure Committee and the City.

One bus route operates on 12th Street. ABQ Ride Route 36 which is a one direction circular route that proceeds northbound on 12th Street and San Ysidro Road and south on Rio Grande Boulevard. The Mid Region Council of Governments (MRCOG) long range bikeway system-map identifies 12th Street as a primary bicycle route, although bike lanes are not yet installed.

Improved Roadway Design: 12th Street will provide space for automobiles, busses, and bike lanes. Design regulations are established for the intersections, sidewalks, driveway access and the alley along the part of the corridor between I-40 and Mountain Road.

12th Street between Mountain Road and Sawmill has been improved to the level of service set forth in the goals of the Sector Plan, including appropriate pedestrian and bike facilities and landscaping the planting strips.

Figure 6: 12th Street Cross Section Between Mountain Road and Sawmill Road

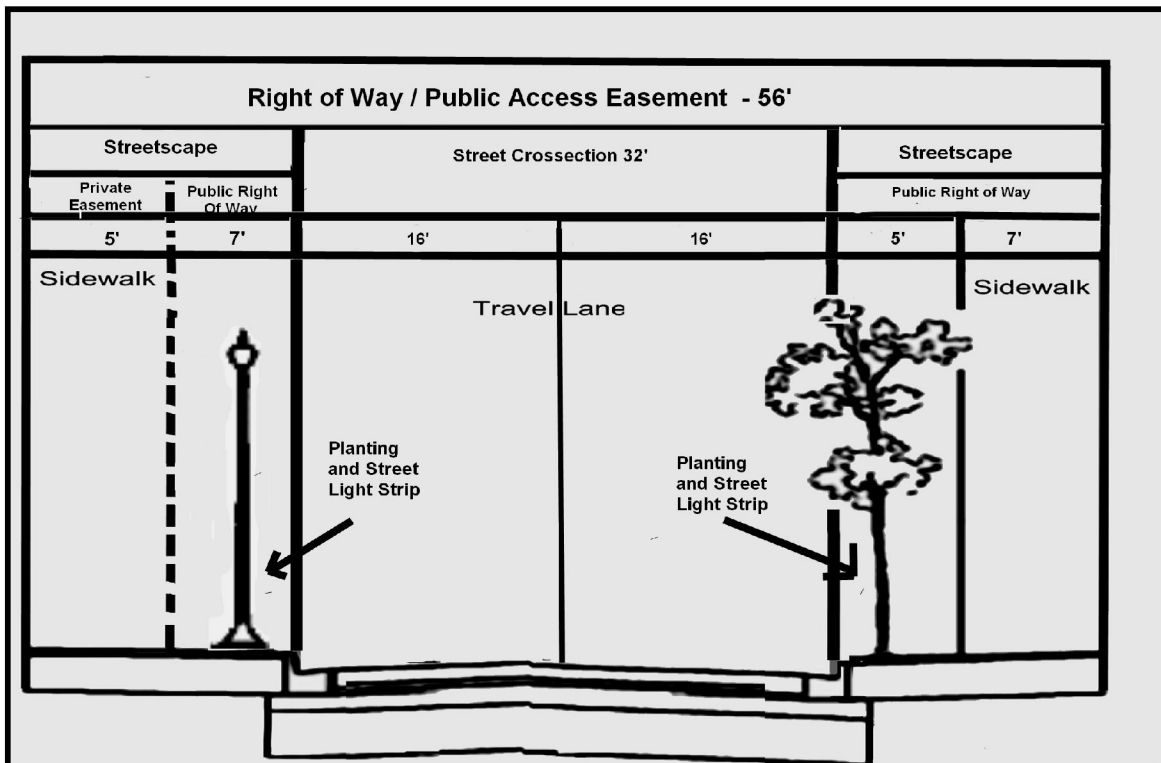


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North of Sawmill Road to where 12th Street intersects with the segment of Bellamah east of 12th Street it will transition to a four lane commercial arterial cross section that supports traffic generated by neighborhood retail and multi-family housing.

North of where 12th Street intersects with the segment of Bellamah west of 12th Street the roadway will be a four lane arterial adequate for commercial truck traffic. The median width will increase north of the railroad spur.

Figure 7: 12th Street Cross Section Between Sawmill / Bellamah Road to a Point 144 Feet South of the Centerline of Bellamah Road

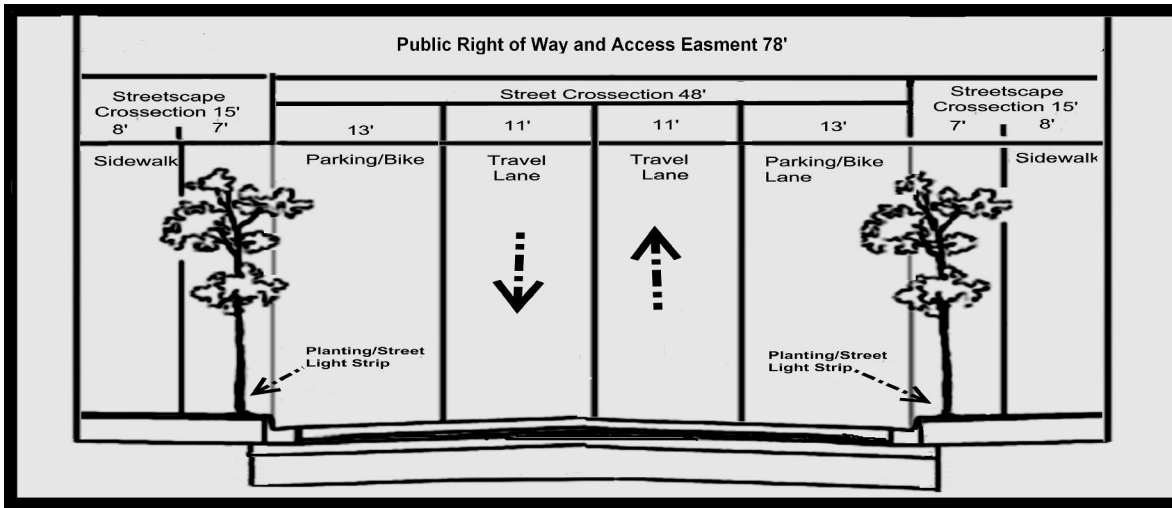


Figure 8: 12th Street Cross Section 140 feet South of Bellamah Road to 250 Feet North of the Railroad Crossing Located Between Aspen and Haines

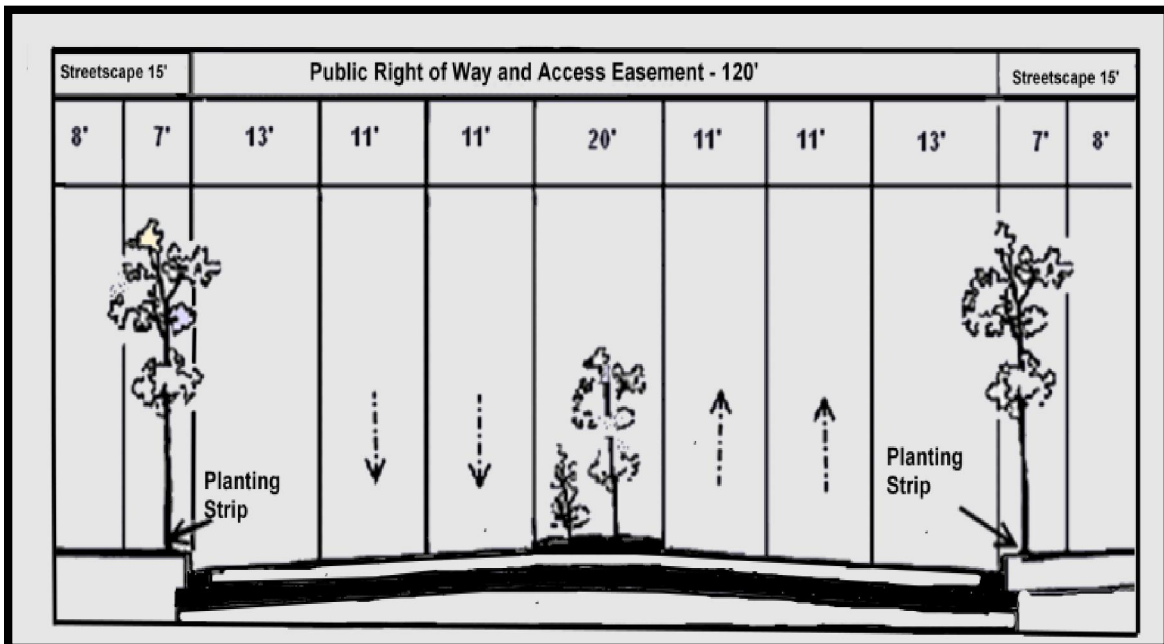
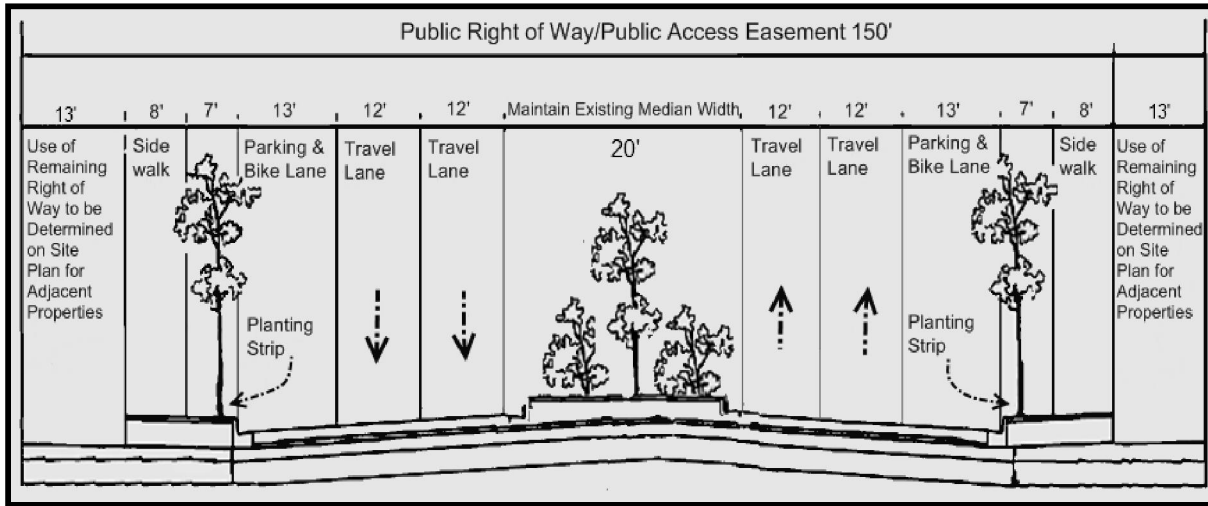


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Figure 9: 12th Street Cross Section 250 Feet North of the Railroad Spur
 Located Between Aspen and Haines to I-40



Intersection Design: The curb return radius at the northwest corner of the intersection of 12th Street with Aspen Avenue shall have a minimum radius that accommodates vehicles with a wheel base of 50-feet (tractor trailer trucks). The City shall conduct traffic signal warrant studies at Aspen Avenue, Bellamah Avenue, and Bellamah/Sawmill Avenue. and upon meeting warrants, signals should be installed at these intersections. Signals, would protect pedestrians crossing 12th Street, and improve safety for vehicles, especially tractor trailer trucks turning onto 12th Street from Aspen or Bellamah. The City shall also design, engineer, and reconstruct the intersection of 12th Street and Sawmill Road. The project may include a traffic signal and/or a westbound to northbound slip lane.

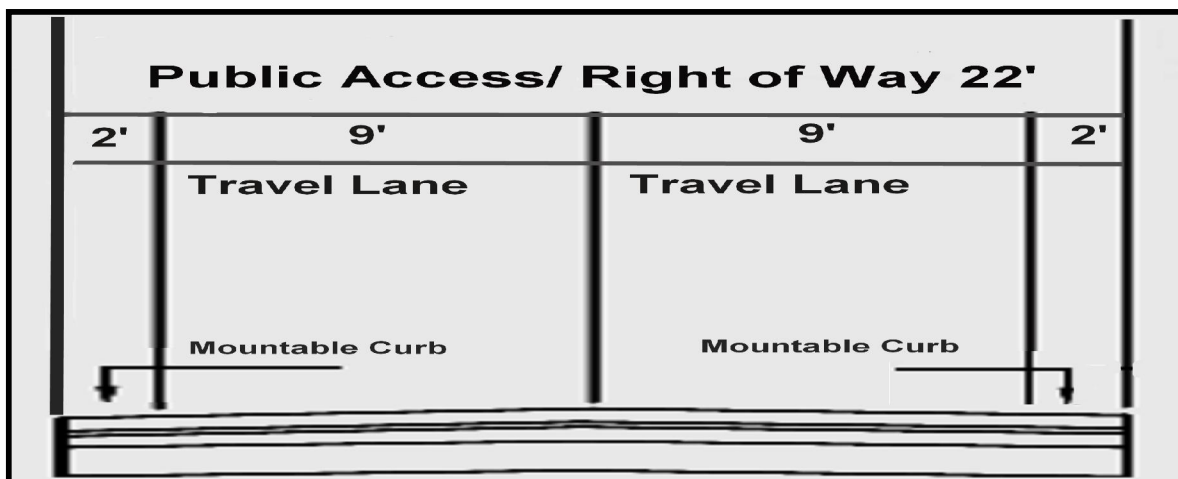
Sidewalk Edge Zone Design (Sawmill Road to I-40): Projects that include redeveloping or subdividing properties adjacent to 12th Street shall construct a minimum 15-foot edge zone behind the street curb. The edge zone shall include a 7-foot planting strip between the curb and the sidewalk or a 7-foot paved area with street trees in tree wells that are spaced apart as recommended by the City Forrester and an 8-foot sidewalk behind the planting strip. If the edge zone exceeds 15-feet the excess space shall be utilized as a pedestrian plaza, or otherwise, as permitted in the City Zoning Code.

Driveway Access: Between Sawmill/Bellamah Road and I-40, upon redevelopment of a property only one new curb cut is allowed every 250-feet. The minimum allowed width for a new or reconstructed curb cut shall be built to meet DPM standards.

e.8 Design of Alley West of 12th Street North of Sawmill Road: The alleyway behind the structures on the west side of 12th Street, upon redevelopment of the buildings between 12th and the alley, shall be constructed with a minimum of two 9-foot travel lanes abutted by 2-foot mountable shoulders. Each property owner is responsible for developing the segment of the alleyway adjoining their property. This does not include the vacated alley section between Bellamah and Sawmill Road.

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Figure 10: Cross Section of Alley West of 12th Street.



e.9. 18th Street Cross-Section Design. (Bellamah to Mountain Road):Existing.

Conditions: 18th Street from Bellamah to Mountain Road is a major local street. This segment of the roadway is approximately one quarter mile long. 18th Street is also a primary connection between the Village Center, Tiguex Park, and the museums.

Improved Roadway Design: The 18th Street cross-section shall foster street level activities including entertainment, dining, shopping, recreational opportunities, and public space.

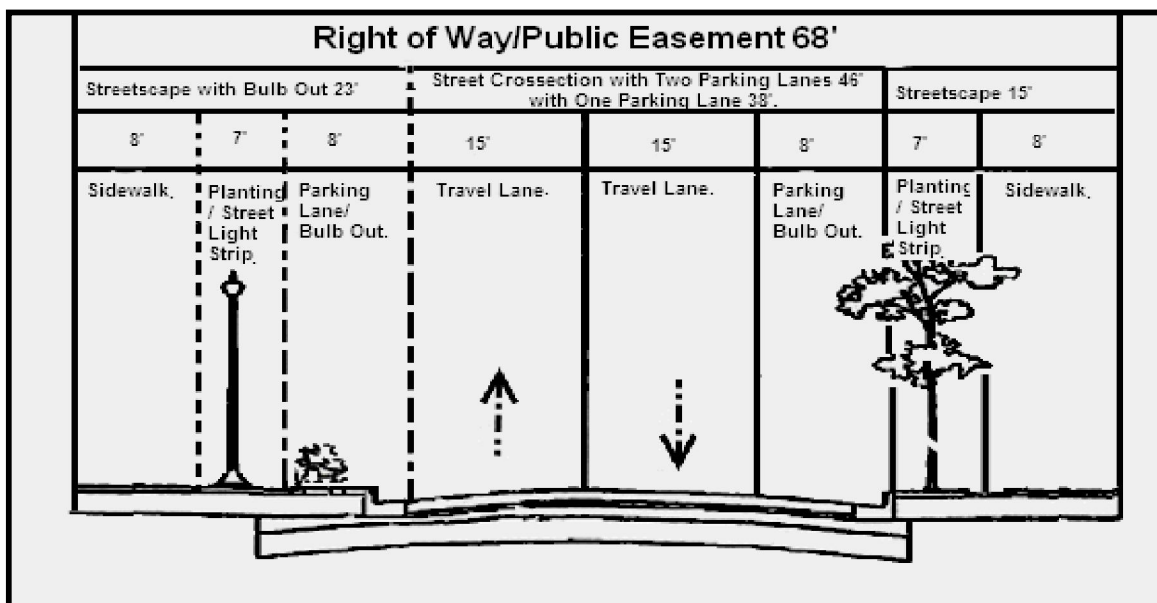
Intersection Design: Where roadways intersect with 18th Street the intersections shall be configured to slow traffic and ensure that pedestrians and motorists are visible to each other. There shall be bulb outs installed on all intersection corners, and the street corners shall have small curb return radius of 10 feet to 15 feet to slow turning traffic and reduce pedestrian crossing distances. Crosswalks shall be installed on all approaches using high-visibility markings, such as ladder striping, contrasting-colored pavement and/or textured pavement. If a new local street is constructed that intersects with 18th Street, it shall be an all-way stop control intersection.

Sidewalk Edge Zone Design: Projects that redevelop or subdivide properties adjacent to 18th Street shall construct a minimum 15-foot edge zone that includes a 7-foot planting/street light strip between the curb and the sidewalk or a 7-foot paved area with street trees and street-lights spaced per the recommendation of the City Forester and the City Traffic Engineer. Within the planting strip or paved area there shall be one seating space for every 1,000 square feet of net leaseable space on a site. The sidewalk behind the planting strip shall be a minimum of 8 feet.

To narrow the crossing distances for pedestrians every 100-feet there shall be crosswalks built with patterned concrete and flanked by bump outs bisected by curb cuts. When the edge zone exceeds 20-feet the excess space may be used for a pedestrian plaza or any sidewalk commercial uses permitted by the City zoning code.

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Figure 11: Proposed Cross Section 18th Street.



Driveway Access: Existing driveway curb cuts may remain until a property is redeveloped. Upon redevelopment one curb cut is allowed for every 250-feet of road frontage. New curb cuts shall be evenly spaced from the closest existing driveway as site constraints allow. The maximum allowed width for a new or reconstructed curb cut is 22-feet.

e.10. Bellamah Avenue Design.

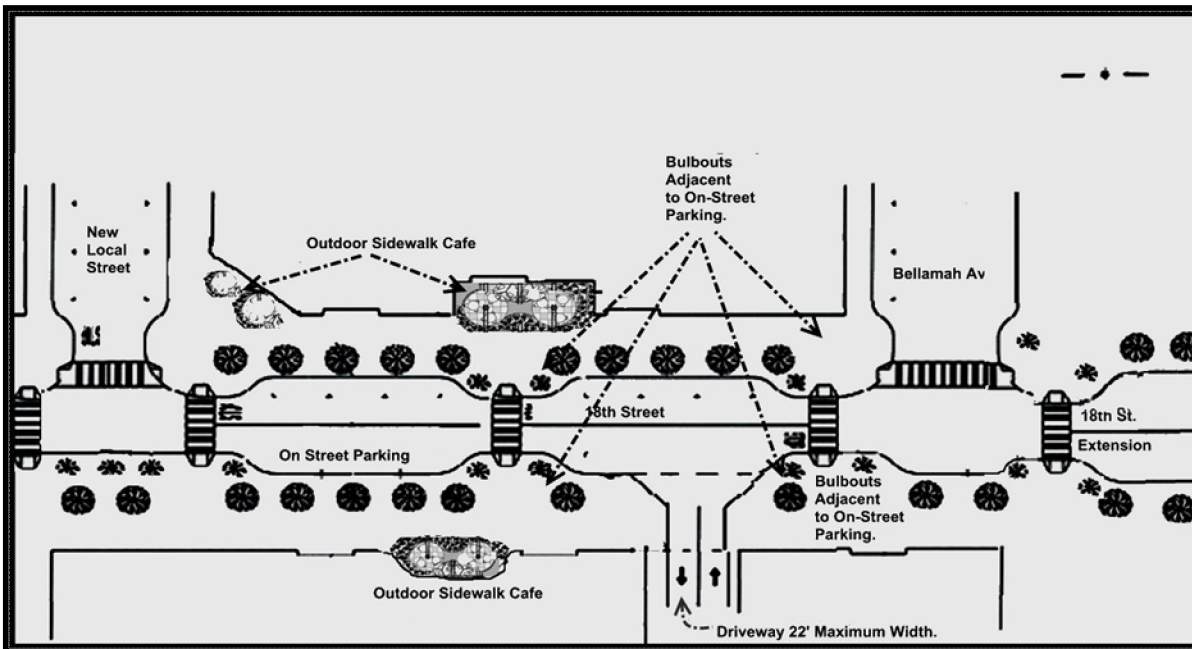
Existing Conditions: Bellamah, is a major local street, abutted by commercial land uses that are the primary east to west access to the Western Focus Area. The street is not continuous; one segment runs from Rio Grande Boulevard and 18th Street, the other from 12th Street to 820-feet west of 12th. The street segment between Rio Grande Boulevard and 18th Street has a 60-foot right-of-way. The cross section is 40-feet from curb to curb with 6-foot sidewalks located directly behind the curb. 18th Street and 20th Street intersect from the South and 19th Street from the north. The segment between 12th Street and a point 820 feet west of 12th street has a 67-foot right-of-way that is paved from right-of-way line to right-of-way line. This segment has neither curbs nor sidewalks.

Improved Roadway Design: The design for Bellamah must accommodate truck traffic accessing the industrial properties located in the Western Focus Area and commuter traffic traveling to the Central Core and Village Center. It should also shield pedestrians from this traffic to encourage walking and biking.

Sidewalks Edge Zone Design: Projects that redevelop or subdivide properties adjacent to Bellamah Avenue shall include a minimum 13-foot edge zone. The edge zone shall include a 7-foot planting strip between the curb and a 6-foot sidewalk or a 14-foot paved area that includes street trees spaced per the recommendation of the City Forester. There shall be at least one seating space for every 1,000 square feet of heatable space.

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Figure 12: Proposed Cross Section 18th Street Plan View.



Driveway Access: Existing driveway curb cuts may remain until a property is redeveloped. Upon redevelopment one curb cut is allowed for every 250-feet of road frontage. New curb cuts shall be evenly spaced from the closest existing driveways as site constraints allow. The maximum allowed width for a new or reconstructed curb cut is 22-feet.

Figure 13: Cross section Bellamah Avenue from 12th Street to Rio Grande Boulevard

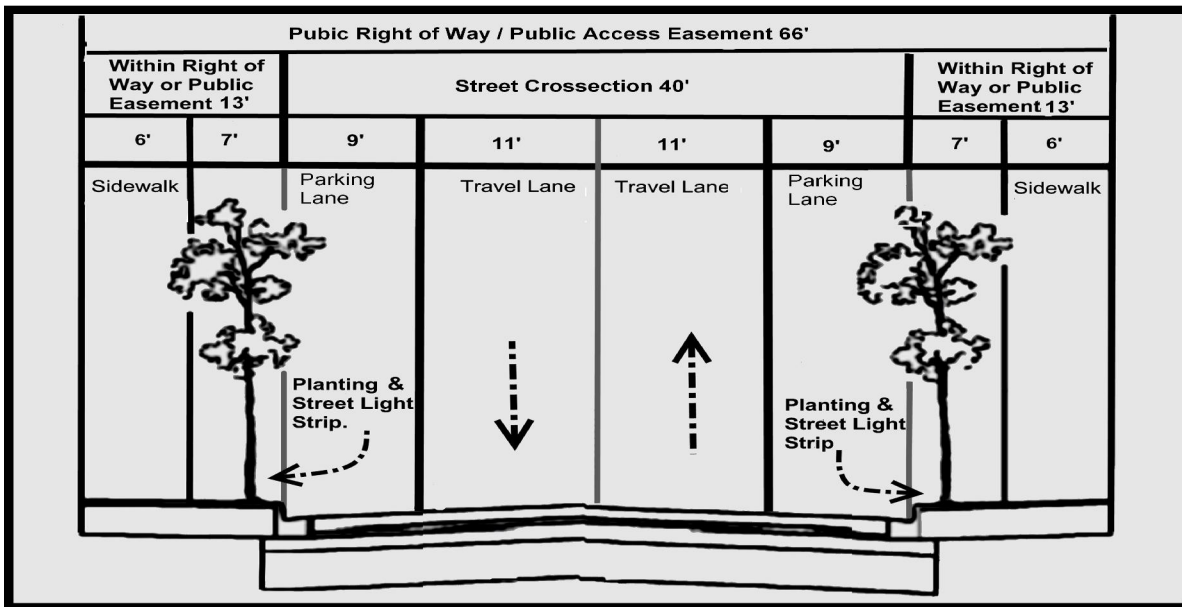


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e.11. Mountain Road between 20th Street and Old Town Road.

Existing Conditions: Mountain is a collector roadway that spans from Gabaldon Road to Interstate 25 (I-25) and connects the Western and Eastern Focus Areas linking the Sawmill and Wells Park Neighborhoods to the adjoining neighborhoods of Old Town and Martinez Town/Santa Barbara. This segment of the roadway is a bike boulevard where the speed limit is posted for 18 miles per hour.

The cross-section varies in width depending on whether or not a given segment includes a parking lane. Where there is a parking lane the cross section is 59-feet with two 21-foot travel lanes and an 11-foot parking lane on varying sides of the road. Where there is not a parking lane the cross section is 48-feet with two 21-foot through lanes between the curbs. A planting strip exists along the south side from 20th Street to Old Town Road and on the north side between 18th Street and Old Town Road

Improved Roadway Design: Construct a 7-foot wide median and an 8-foot parking lane on both sides of the road, and narrow the two lanes to 11-feet and install bump outs every 100-feet. Install pattern pavement treatments at the crosswalk. Remove the existing sidewalk on the section of the north side of the road that is in front of the Museum of Natural History and plant a 6-foot planting strip behind the curb line and reinstall an 8-foot sidewalk behind the sidewalk.

Figure 14: Cross section Mountain Road and 20th Street to Old Town Road:

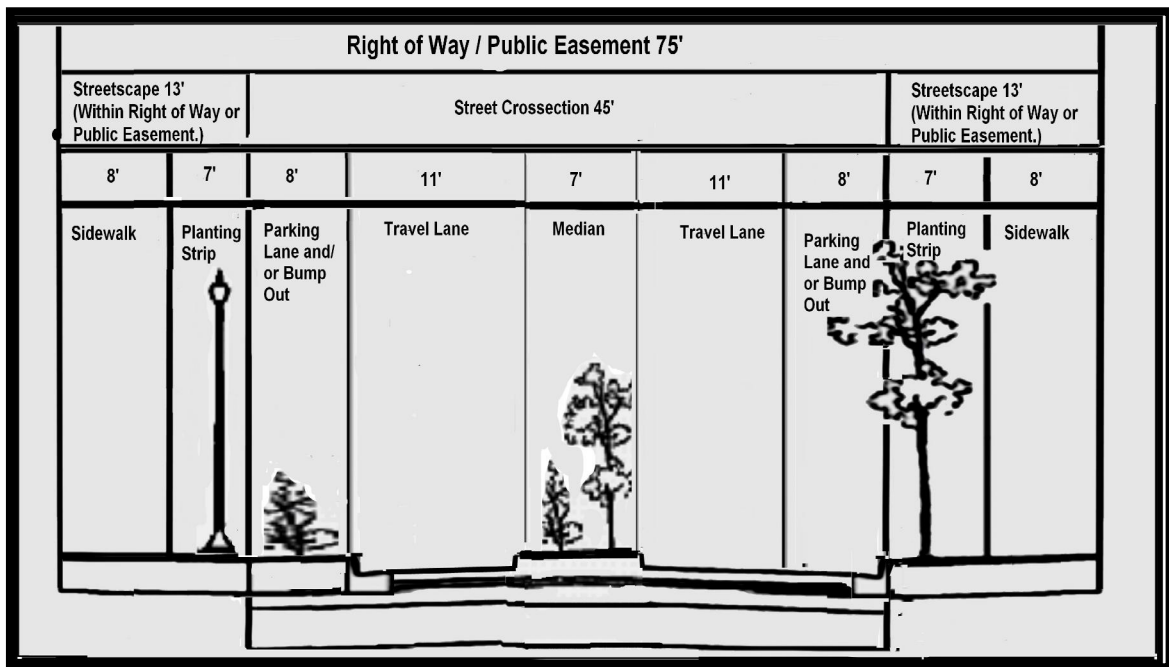


Exhibit A. Sawmill Sector Plan Amendment

e.12. Mountain Road between Old Town Road and 12th Street

Existing Conditions: The right-of-way is designed for 25 miles per hour for this segment of Bellamah which is 50-foot wide with a 32-foot curb-to-curb paved cross section. The City has constructed new 6-foot sidewalks behind the curb and installed pedestrian lighting on this segment of Mountain Road and a pedestrian refuge is located at 15th Street and Mountain. Between 14th Street and Rio Grande Boulevard, Mountain is bicycle-boulevard with an 18 mile-per-hour speed limit.

Improved Roadway Design: The Downtown Neighborhood Sector Plan will address additional pedestrian crossings across Mountain as part of an update (see 2.f.1).

Demolition: Buildings abutting Mountain Road shall not be removed to increase the right-of-way for street paving, sidewalk, or other public improvements. Increases in the cross section width are to be acquired through easements or purchasing a portion of a parcel(s) that does not contain a structure.

2.f Regulations: Eastern Focus Area: Collectors and Minor Arterials

f.1. Mountain Road: 12th Street to 4th Street.

Existing Conditions: The cross section is divided into the following three zones; 12th Street to 11th Street, 11th Street to the alley between 8th Street and 7th and from the alley between 8th and 7th Streets to 4th Street. Between 11th and 12th Street the right-of-way is 49-foot wide with a 30-foot street cross section comprised of two 15-foot travel lanes and a 5-foot 5-inch sidewalk. From 11th Street to the alley between 8th Street and 7th Streets, the right-of-way is 37-foot wide with a street cross section of 26-foot which is comprised of two 13-foot travel lanes and a 5-foot sidewalk. From the alley between 8th and 7th Streets to 4th Street, the right-of-way is 62-foot wide with a street cross section of 32-foot comprised of two 10-foot travel lanes, a 11-foot continuous left turn lane, a 5-foot sidewalk, and an 11-foot planting strip located either between the sidewalk and the street or the sidewalk and the fence line. The City has also constructed new sidewalks and installed pedestrian lighting on this segment.

Improved Roadway Design: The roadway cross section will be 56-foot with a 6-foot planting strips and sidewalks, two 5-foot bike lanes and two 11-foot travel lanes. Buildings on Mountain Road shall not be removed to increase the right-of-way for street paving, sidewalk, or other public improvements. Increases in the cross section width are to be acquired through easements or purchasing the portion of a parcel(s) that does not contain a structure.

Demolition: Buildings abutting Mountain Road will not be removed to increase the right-of-way for street paving, sidewalk construction, or other public improvements. Increases in the cross section width are to be acquired through easements purchasing the portion of a parcel(s) that does not contain a structure.

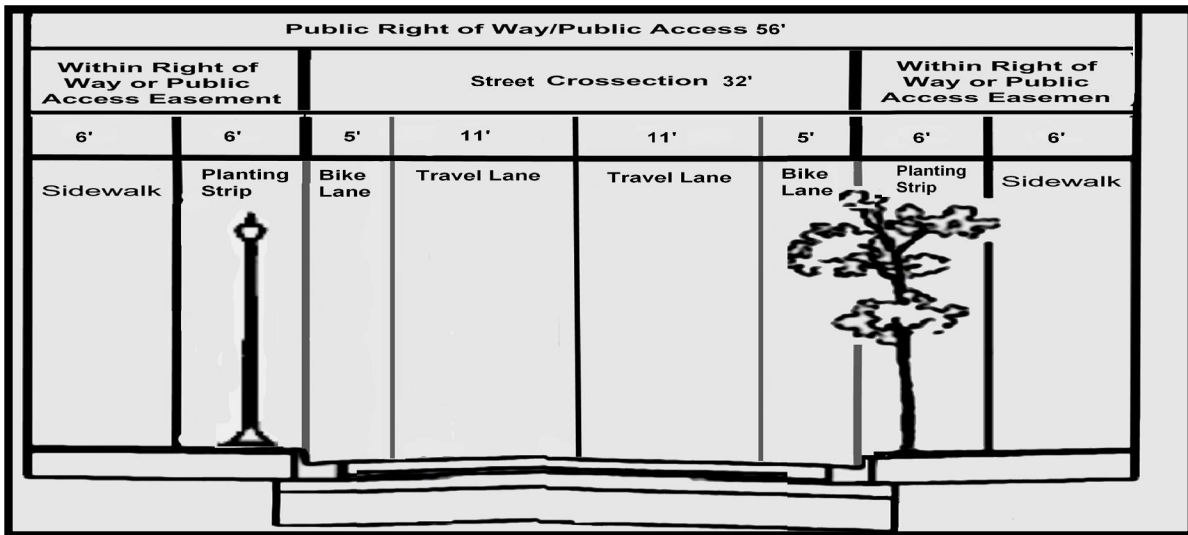
Exhibit A. Sawmill Sector Plan Amendment

Future Amendments: The updated Downtown Neighborhood Association (DNA) Sector Plan will address additional pedestrian crossings at Mountain Road. The Sawmill/Wells Park Sector Plan will be amended to include the recommendations from the DNA Sector Plan. Additional pedestrian crossings shall be developed through a joint committee of neighborhood representatives from the section of the DNA, located north of Lomas Boulevard, and also residents from the Sawmill and Wells Park Neighborhoods and City Staff.

f.2. 5th and 6th Streets: Mountain Road to I-40.

Existing Conditions: 5th Street and 6th Street are one-way arterial roadways; 6th Street proceeds southbound and 5th Street northbound. From Mountain Road to the railroad spur between Aspen and Haines these roadways, have 60-foot right-of-ways, a 32-foot street cross-section with one 13-foot and one 12-foot through lane. There is a 7-foot parking lane, on the west side of the road for 5th Street and on the east side of the road for 6th Street. Each road has a 6-foot Sidewalk and an 11-foot planting strip either between the sidewalk and street or the sidewalk and the property line.

Figure 15: Adopted Cross Section Old Town Road to 4th Street



The road curves to the west approximately 50-feet north of where Bellamah intersects with 5th and 6th Streets. This causes a blind spot for motorists heading west on Bellamah. Currently the City has a yellow flasher facing 6th Street to alert drivers to the presence of the intersection. From the railroad spur between Aspen and Haines to the confluence of the two roadways, 5th Street and 6th Street have 60-foot right-of-ways, a 43-foot street cross section, with two 18-foot through lanes, a 7-foot parking lane, a 6-foot sidewalk and an 11-foot planting strip either between the sidewalk and street or the sidewalk and the property line.

Exhibit A. Sawmill Sector Plan Amendment

Sixth Street from I-40 south to the confluence of 5th Street and 6th Street has a 120-foot right-of-way, with a 98-foot street cross section. There are four 14-foot through lanes and a 20-foot right turn slip lane on each side of the street. The sidewalks abut the curb and are 7-feet wide. The cross section also includes a 24-foot rockscaped median with a neighborhood marquee sign.

Improved Roadway Design: The City will convert 5th Street and 6th Street into two way streets with a parking lane on one side of the roadway and a north and south through lanes. Between the railroad spur and Mountain Road where local streets intersect with 5th Street and 6th Street road bump outs will be installed at all four corners of the intersection. This will create a protected parking lane and improve the clear sight triangle for motorists entering the roadway.

The City plans to install pedestrian lighting, and plant a mixture of approximately 150 ornamental and shade trees in existing dirt strips between the sidewalk and the curb to provide intermittent shade along approximately 1500-feet on 5th Street and approximately 3,000-feet on 6th Street.

2.g Regulations: Eastern and Western Focus Area Local Streets Designs Residential and Commercial.

g.1. Intent:

These regulations will develop additional local streets that provide connections and direct access to properties throughout the Western Focus Area. A number of these streets would be constructed within abandoned railroad easements since the focus areas are the location of railroad spurs that are used less and less. The rail spurs will only be abandoned once the owners of the properties served by the spur have issued a letter to the City stating that the use of the spur is no longer needed. The properties served by the spurs are zoned, for residential or light industrial uses, or are likely to be rezoned and redeveloped as such.

g.2. Improved Roadway Design:

Residential Street cross-sections shall have a maximum 56-foot right-of-way with curb to curb pavement width of 32-feet, and 6-foot planting strips abutted by 6-foot sidewalks on both sides of the roadway. Commercial Street cross-sections shall have a maximum 64-foot right-of-way with a 40-foot curb to curb pavement width, 6-foot planting strips, and 6-foot sidewalks on both sides of the roadway. If the roadway will be located within a narrow railroad easement that is narrower than the required right-of-way, the cross section can be the width of the railroad easement and parking lanes will not be required.

g.3. Sidewalk/Planting Strip Design:

Sidewalks will be a minimum of 6-feet and shall be set back a minimum of 4-feet from the curb to create a planting strip. Where site conditions preclude the 4-foot planting strip, the sidewalks may abut the curb. If the right-of-way width is constricted by a narrow railroad easement, a 6-foot sidewalk on one side of the street or a 4-foot sidewalk on both sides of the street is sufficient. The planting strips shall be landscaped

Exhibit A. Sawmill Sector Plan Amendment

and utility poles, signs, and other items that can obstruct sidewalks will be placed within landscaped strips when possible.

Figure 16: Proposed 5th and 6th Street Cross Sections: Confluence of 5th and 6th Street to Mountain Road

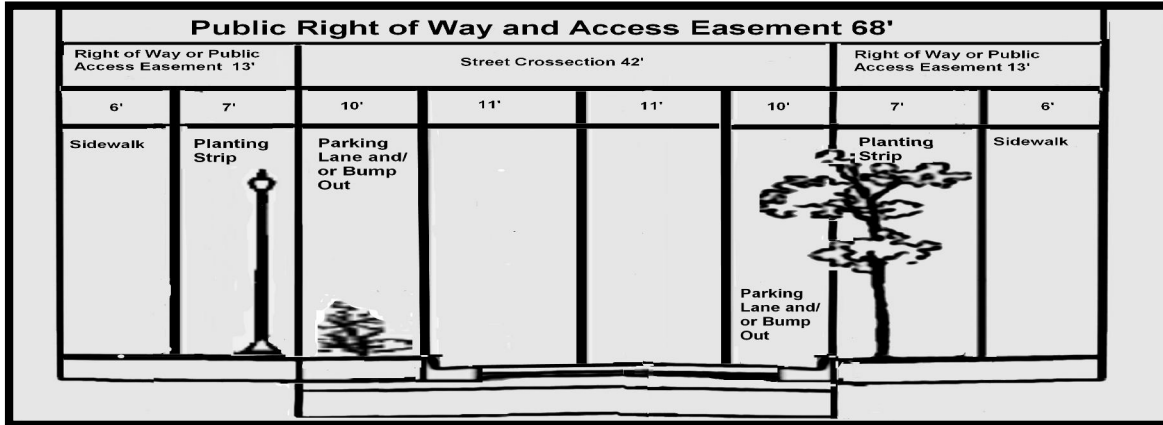


Figure 17: Cross section Local and Major Local Residential Street Design

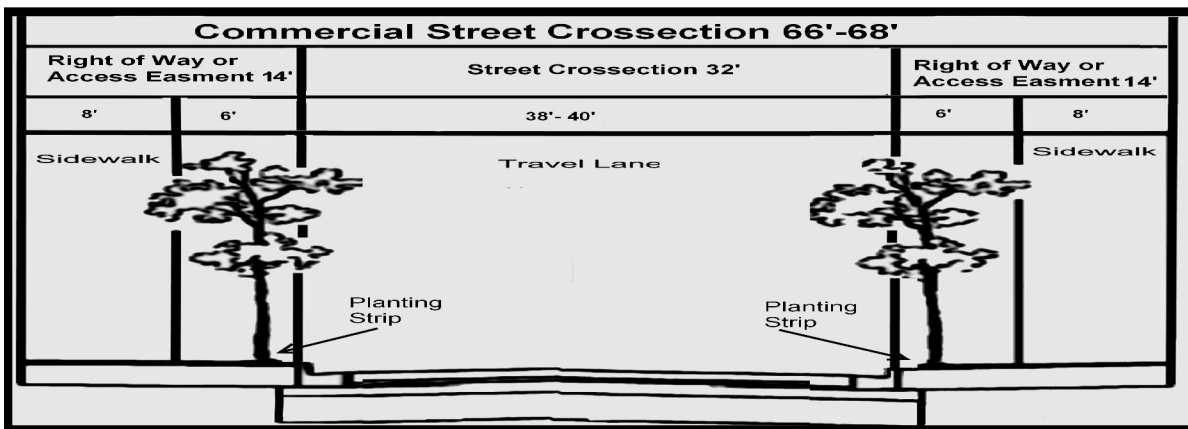


Figure 18: Cross Section Local and Major Local Commercial Street Design

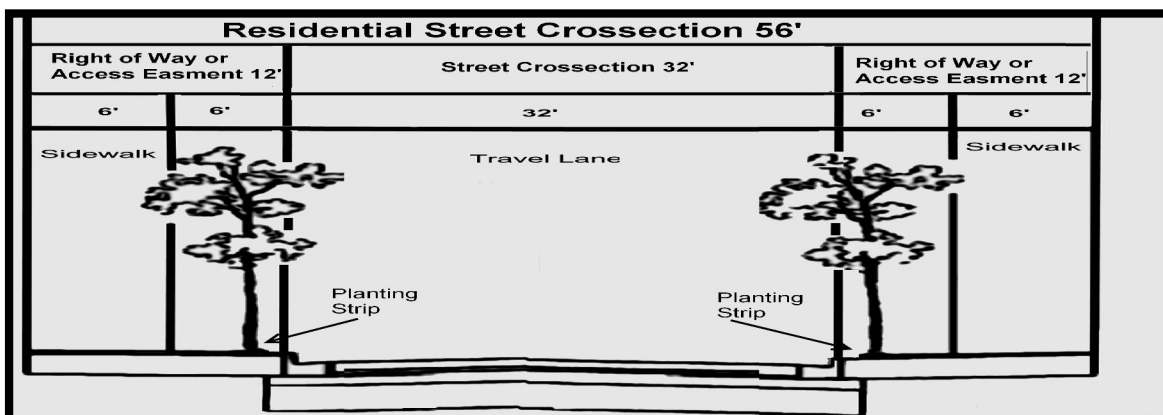


Exhibit A. Sawmill Sector Plan Amendment

g.4. Driveway Access.

Curb cuts on residential streets will have a 15-foot maximum width.

g.5. Sawmill Road Mountain to 12th Street and 15th Street from Mountain to Sawmill.

Existing Conditions: Sawmill Avenue between 12th Street and 15th Street and 15th Street from Sawmill to Mountain Road form a shortcut from 12th Street to Mountain Road used by motorists during rush hours to avoid the congested intersection at 12th and Mountain.

Improved Roadway Design: Sawmill Road will be closed west of the alleyway that is parallel to 12th Street. Closing the street will only occur upon written approval from all property owners with lands adjacent to Sawmill Road. The Sawmill and 15th Street rights-of-way will be maintained at existing widths and the existing paved cross sections of these roadways shall not be widened. The existing turning radii may be improved to facilitate emergency vehicle access.

2.h Local Street Designs.

Local streets shall follow the standards established in the DPM and shall not exceed 32-feet from curb to curb.

2.i Alley Designs.

Alleyways shall have a minimum width of 22-feet with at least a 20-foot of clear travel way comprised of a 16-foot lane and two 2-foot shoulders on both sides. The alley shall have a paved permeable surface, including, crusher fines, paver stones, or permeable asphalt. The shoulders shall be constructed of load-bearing pavers, crushable landscaping, or other permeable paving materials. Any garage or structure adjoining an alley shall have a security light directed into the alley.

Figure 19: Standard Alley Design

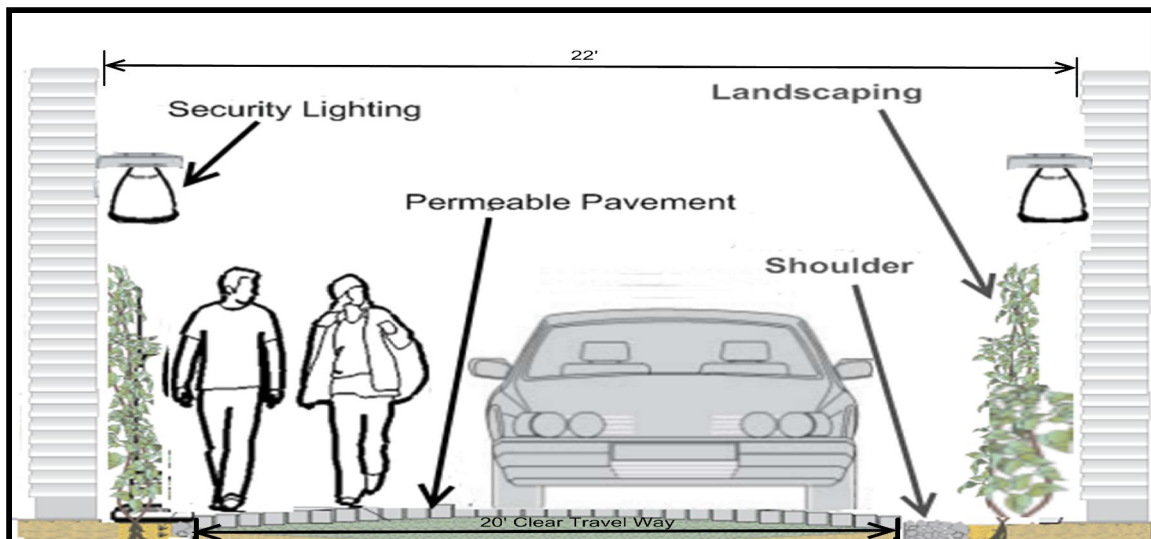


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2.j Traffic Impact Studies.

Traffic impact studies for a development or site plan within the Western Focus Area shall be based on the assumption that the street system is built to the standards set forth in the regulations.

2.k Exemptions.

The Environmental Planning Commission (EPC) and/or the Design Review Board (DRB) shall review and consider exemptions to these regulations on a case-by-case basis. If site conditions for a project render implementing these regulations overly burdensome the EPC or DRB may wave these regulations but shall review any proposed amended cross sections to ensure that the intent of these regulations has been met and safe and convenient mobility is provided to cyclists, motorists and pedestrians

2.l Recommendations: Street Connectivity.

To provide efficient and coherent connectivity without creating cut-through traffic in residential neighborhoods, the following proposed street network locates roadways within the railroad spur rights-of-way and where parcel boundary lines abut each other. The site plans for current development proposals are also part of the network, including roads proposed for the Sawmill Land Trust's Arbolera de Vida Subdivision and the Sawmill Artisan Village. These roadways will be in addition to streets developed in response to the regulations set forth in 2.e. of this task.

Approved Street Connections: Street connections and improvements proposed in the Sawmill Village Site plan include creating a new connection between 12th Street and Rio Grande by extending the segment of Bellamah Avenue that is west of 12th Street to a point parallel to the 18th Street right-of-way and extending 18th Street, from the existing intersection of Bellamah (the segment east of Rio Grande and west of 18th Street) north to the proposed extension of Bellamah that is proposed in the Artisan Village Site Plan (see Figure 3). The site plan approval also includes constructing a gravel cross section that extends Zearing Avenue approximately 430 feet northeast of 19th Street.

The site plan for the Arbolera de Vida Subdivision includes constructing Mill Pond Road from Aspen Road to Aqua Fresca Avenue which is currently in construction and funded by the Quarter Cent Transportation Tax. The approved site plan also includes construction of a gravel cross section for a segment of Aqua Fresca Avenue between Mill Pond Road and Mis Abuelitos Drive.

Recommended Street Connections: A network could be created to provide a connection between Rio Grande Boulevard and 1st Street using the rail spur rights-of-way. This network would draw traffic away from the segments of Mountain Road and Bellamah Avenue that pass through residential neighborhoods and place it onto routes in the commercial areas. The City should negotiate with the New Mexico Department of Transportation (NMDOT) to either assume ownership of the spur rights-of-way or acquire an easement. The right of way for the spur between Haines and Aspen would become a major local roadway. The western termini would be where the spur intersects with Aspen about 650 west of 12th Street and an eastern termini as far east as 1st Street.

Exhibit A. Sawmill Sector Plan Amendment

Figure 20: Connectivity and Property Ownership Western Focus
 I.3. Adoption of Bike, Facilities:

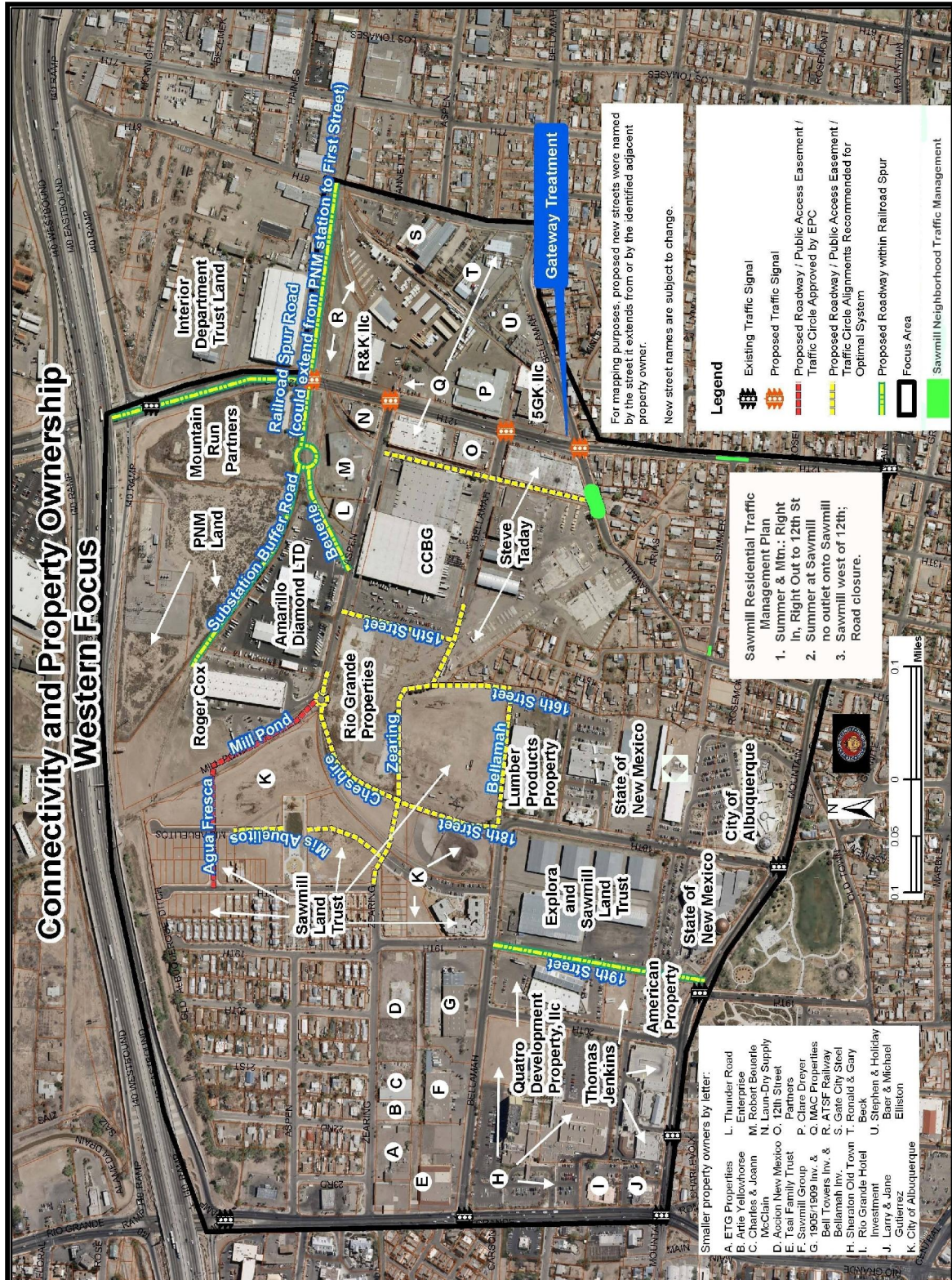


Exhibit A. Sawmill Sector Plan Amendment

A narrow lot that is located just inside the western boundary of the CCBG property (UPS warehouse) could be used as right-of-way for a local street to provide a direct connection between Aspen and Bellamah. This will also increase the street frontage along the west boundary of the CCBG and the Rio Grande Property's site. Two hundred fifty feet west of where this road would intersect Aspen is a rail spur right of way that connects to the rail spur where the major local road would be built. This rail spur right of way could be developed to link to the proposed major local road. Both this road and the proposed road along the west side of the UPS warehouse would create a connection from Bellamah to the major local road.

To provide a connection from Rio Grande Boulevard to 2nd Street, through the northern part of the sector plan area, 18th Street could be extended (from where it intersects with the Bellamah extension that will be built through the Sawmill Artisan Village,) (see Figure 3) north to the intersection of Aspen and Mill Pond Road. This would allow motorists on Rio Grande Boulevard to travel east on Bellemah to 18th Street, then north on this connection to Aspen, east on Aspen to the new Major Local Road and then, east onto 2nd Street.

The south side of the intersection of Bellamah Road and 19th Street is the remnant of a railroad spur that extends from Bellamah Road to Mountain Road. This spur would become a segment of 19th Street extending that roadway to Mountain Road. This link creates a new access to Mountain Road from the John Baron-Burg Park Addition (in the Northwest Quadrant of the Western Focus Area) and Sawmill Village. It would also improve pedestrian connections between these neighborhoods and the residential areas south of Mountain Road. The existing easement under this spur may not have the required width for City right of way standards. This issue could be addressed by acquiring easements from the property owners on the east side of the site. The City could acquire easements from the owners of the Blue Lynx site when it is redeveloped by incorporating an easement into the site plans. The Museum has a storm pond on its western perimeter that if used for the easement could be placed below the new roadway. The Museum and the Blue Lynx owners benefit by gaining the street frontage created when the 19th Street extension is constructed

A traffic management plan conducted by the City's Traffic Engineering Operations Division is proposed for the Sawmill Neighborhood residential area located south of Sawmill Road, north of Mountain, east of the Explora Museum and west of 12th. Two local streets, Sawmill Road and Summer Avenue become cut through routes for motorists trying to avoid congestion at the intersection of 12th and Mountain during rush hours. The example plan in figure 20 shows a configuration that deletes access through the neighborhood to both the east to north bound morning commuters and the south to west bound evening commuters. Though, access is maintained for residents and visitors traveling to the neighborhood.

2.m Improved Transit, Bicycle Routes and Multi-Use Trail Locations and Design.

The arterial streets in the Sawmill Wells Park Neighborhoods are used as the corridor for bus routes that ingress and egress the Alvarado Transit Center (ATC) to the Northeast Heights, the North Valley, and the Northwest Mesa. This provides an opportunity for a neighborhood circulator route. The route will serve the Sawmill, Wells

Exhibit A. Sawmill Sector Plan Amendment

Park, Old Town, Downtown, and the Near North Valley Neighborhoods. It will connect with routes on Lomas Boulevard, Central Avenue, Menaul Boulevard, Rio Grande, 2nd Street, 3rd Street, 4th Street, 5th Street, 6th Street, 12th Street, and could make an excursion to the ATC.

m.1 Existing Conditions.

Transit Routes: The existing circulator transit route within the Western Focus Area provides northbound local and commuter bus service on 12th and southbound service on Rio Grande Boulevard. The transit route in the Eastern Focus Area runs north bound on 5th Street and south bound on 6th Street. These routes do not fully serve the focus area because many origins and destinations are outside the typical one quarter mile walking distance preferred by transit riders, or the routes only provide service in one direction.

Bike Boulevard, Bike Lanes, and Bike Routes: The existing facilities include on-street bicycle lanes on Rio Grande Boulevard, a bicycle boulevard on Mountain Road, a bike route on 7th Street, a bike route along 15th Street to Bellamah, and east on Bellamah to 4th Street and a bike route on 12th Street from Bellamah/Sawmill Road to Mountain Road.

Bicycle Route Design: The bicycle routes, bike lanes and multi-use trail (paved trail for bikes and pedestrians) identified or proposed in Figure 22 are either based on existing proposals in the Albuquerque Bicycle Facilities Plan and the Long Range Bikeway System Map in the Metropolitan Transportation Plan (MTP) or are recommended, via this sector plan for adoption into these two documents. These routes will improve connectivity from the Sawmill Core and the Museums, Old Town and Tiquex Park the Museum District, the I-40 bike trail east of Rio Grande to the bike routes on 2nd Street and 3rd Street and the bike lanes on 12th Street north of I-40.

Figure 21: Protected Bike Crossing Aspen at Rio Grande

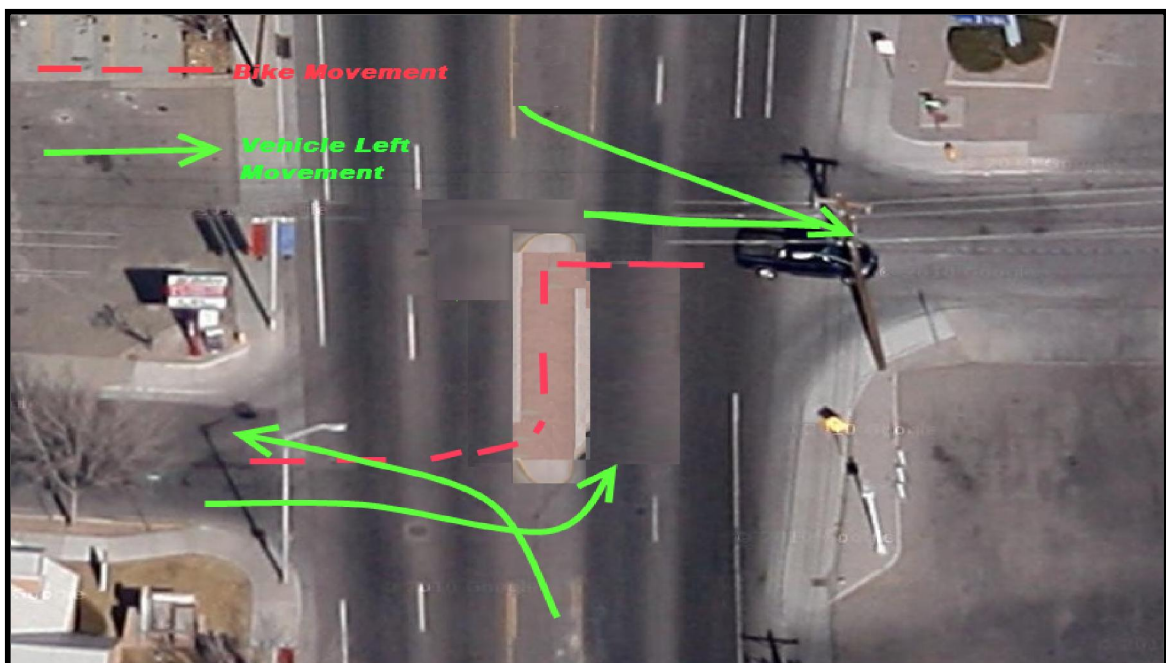


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Figure 22 Bicycle System and Recreation Trails

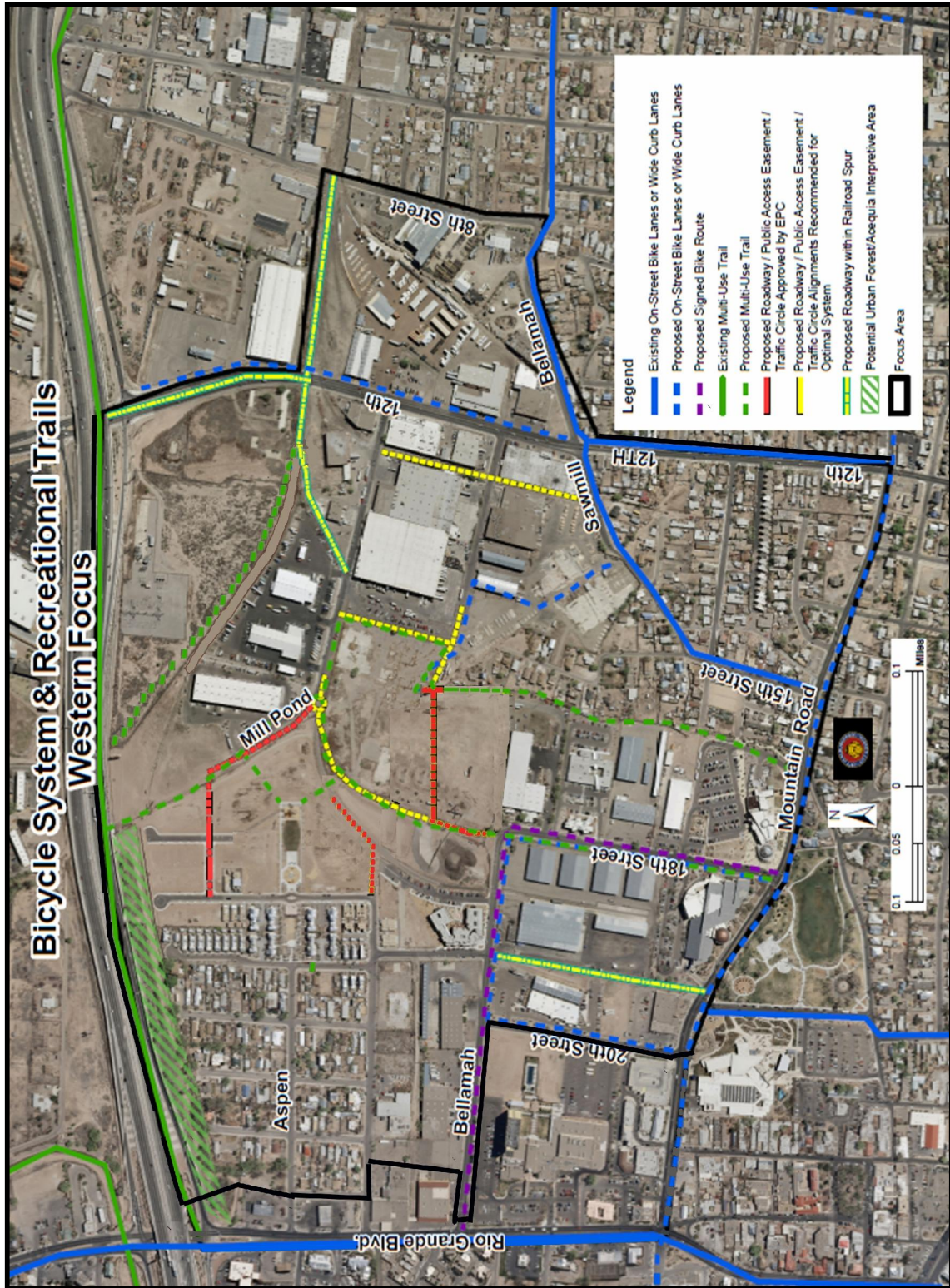


Exhibit A. Sawmill Sector Plan Amendment

m.2. Recommendations: Transit, Bicycle Routes and Multi-Use Trail Locations and Design.

Multi-Use Trails: There is a multi-use trail that is parallel to the south right-of-way of I-40 from Rio Grande Boulevard to 7th Street. A segment of the trail is located on Aspen Avenue from Rio Grande Boulevard west about 700-feet to where the I-40 trail (west of Rio Grande Boulevard) intersects with the street. Three hundred feet north of Aspen, on the east side of Rio Grande, the trail commences again along the southern right-of-way of I-40. One means to connect these two segments would be to install a protected bike crossing on Rio Grande at Aspen. Cyclists could proceed east on Aspen for 400-feet then north on 22nd Street, across the Albuquerque Ditch to the I-40 trail.

On the east side of Rio Grande the trail runs east to 7th street where it ends. The I-40 trail could be extended as far east as the NMDOT Main Rail Line by constructing a segment along the western boundary of the Public Service Company of New Mexico (PNM) transformer that is east of Mill Pond Road to the rail spur that runs from the transformer station to the main line. From this location the trail could run north along the rail line to I-40. This series of connections create a trail from 1st Street extending as far west as Unser Boulevard. There is also a short segment of a trail on the east side of the Explora Museum that is constructed on the old right-of-way for the Acequia Madre.

Figure 23: Rail Spur Trail Corridor.

Rail Spur Location Looking West from 6th Street.



Rail Spur Location Looking East from 12th Street



Google Street View

Exhibit A. Sawmill Sector Plan Amendment

m.3. Acequia Madre Trail.

A multi-use trail could be constructed along the abandoned right-of-way of the Acequia Madre from I-40 to Mountain Road to link Tiquex Park to the Bike Trail adjacent to I-40. This would provide a buffer between the institutional uses west of Old Town Road and the single family residential uses east of the road. A trail segment already exists along the eastern perimeter of the Explora Museum property. To extend this segment of the trail to the Sawmill Artisan's Village the City should negotiate with the New Mexico Natural History Museum, and the property owners north of the Museum's land to acquire right-of-way or an easement. The trail would continue north of Sawmill Village along the eastern and northern edges of that development to a point about 300-feet south of the Arborela De Vida Subdivision then west to a point south of the Arborela De Vida Commons, and then north to the Commons. From there it proceeds east to Mill Pond Road and north on Mill Pond to the trail adjacent to I-40. Amenities should be located along the trails including benches, shade structures and trash receptacles.

Figure 24 Acequia Madre Trail



Google Street View
Constructed Section of Old Acequia Madre adjacent to Explora Parking Lot



Google Street View
Segment of Trail Area north of Explora Museum and south of the Artisan Village.

m.4. Bike Lanes.

The City will per the Current Bicycle Facilities Plan will stripe bike lanes on 12th Street from Sawmill Road to I-40. A potential extension of this route would be to continue east on Aspen to 19th Street and south to Mountain Road using the proposed extension of 19th Street between Bellamah and Mountain Road. A second route could be striped on 15th Street from Mountain Road to Sawmill Road, continuing east on Bellamah to Los Tomases Drive, and north on Los Tomases to Aspen and west on Aspen to the bike routes

Exhibit A. Sawmill Sector Plan Amendment

on 2nd Street and 3rd Street. A Third potential set of lanes would be on Mountain Road from 14th Street to 4th Street.

3. A new Section 3 is created for Chapter 6 of the Sawmill/Wells Park Sector Plan to Regulate Off Street Parking Development.

3. Regulations: Off-Street Parking.

Parking facilities will be designed to avoid separating buildings with large parking fields that form a discontinuous building pattern. These large fields discourage shoppers and other users from walking between businesses. The fields also become heat islands that increase ambient temperature and decrease the volume of humidity in the area.

3.a General.

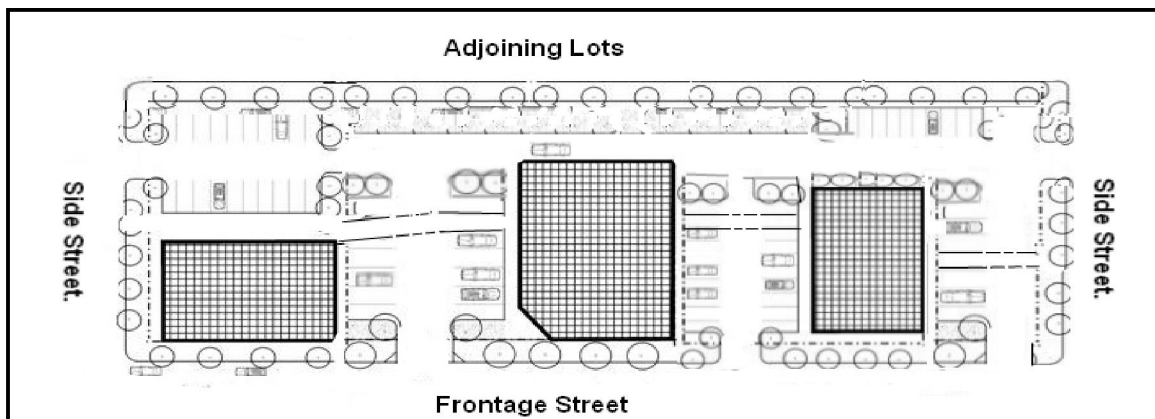
a.1. Off-street parking facilities shall be provided either in parking structures that are integrated into office and/or commercial structures or in discontinuous parking fields. The facilities shall be located and designed to reduce the visual impact of the lots and to encourage walking.

3.b Surface Parking Module Design.

b.1. As conditions allow surface parking lots shall be located on the side or the rear of buildings and dispersed in modules rather than aggregated into one parking field. Parking modules shall be separated by a minimum of 20-feet and have a maximum of 50 standard size parking spaces. Modules shall be connected through drive lanes and sidewalks. When two different modules are on separate properties the lots separating the modules should provide land for structures, open space, or pedestrian paths and plazas.

b.2. Parking areas fronting a street shall be a maximum 100-feet in length and 32- feet in depth.

Figure 25: Retail/Office Center with Parking Lots Dispersed in Modules



3.c Parking Structure Design.

c.1. Structures shall be wrapped within liner buildings along the ground floor or located behind the site's principle building. The entrances shall not exceed 30-feet in width along the liner building façade.

Exhibit A. Sawmill Sector Plan Amendment

Figure 26: Liner Building/Lofts around Parking Structure



[flickr.com/photos/shoppingbag](https://www.flickr.com/photos/shoppingbag)

4. **A new section 12 is created to provide guidance for implementing community exhibits and trails.**

12 Recommendations; Acequia Madre and John Baron-Burg Ditch Exhibit, and Urban Forest.

12.a Historic Conditions.

The farming settlements in the Rio Grande Valley historically depended upon the irrigation canals, known as the acequias. The main ditch in the acequia system was the Acequia Madre one of which flowed through the Sawmill/Wells Park area and made Old Town's settlement possible. The banks of these canals were used as equestrian and pedestrian routes between the farming communities located along the channel and as meeting places for friends and family.

12.b Acequia Madre Interactive Exhibit.

The City will form a committee to develop and implement an interactive exhibit about the Acequia Madres in the Rio Grande Valley. The committee will develop recommendations for the exhibit's location, design, funding and implementation. The following agencies would be invited to provide a representative: the Museum of Albuquerque, the New Mexico Museum of Natural History, the Explora Museum, the Rio Grande Botanical Park, the Old Town Merchants, the Sawmill and Wells Park Neighborhood Associations, the Sawmill Community Land Trust, the Middle Rio Grande Conservancy District (MRGCD) and the local Acequia Associations.

12.c Albuquerque Ditch Working Exhibit.

Existing Conditions. A working segment of the Albuquerque Ditch extends along the north boundary of the John Baron -Burg Neighborhood. The Ditch is located within a 60-foot strip of vacant land bounded to the north by I-40 to the east by the proposed Mill Pond Road extension and to the west by the Rio Grande Boulevard Right-of-Way The eastern half of this property is privately owned and the western half belongs to the NMDOT. The MRGCD has a 5-foot easement on both sides of the channel.

Exhibit A. Sawmill Sector Plan Amendment

Exhibit Design: The New Mexico Department of Transportation's vacant parcel should be converted to an urban forest/bosque. The City and the State have converted similar vacant parcels into urban forests. One such forest is located along I-40's southern right-of-way between Carlisle Boulevard and Lafayette Drive. A second forest is located along the southern I-40 right-of-way between San Pedro Road and Louisiana Boulevard. The forests are planted with drought tolerant flora and have walking trails and bike trails and benches. The forest between Louisiana and San Pedro also has a dog park.

The City should work with the private property owner of the eastern parcel to determine if a private conservation and drainage easement would be possible for the property. This would allow the City to expand any potential urban forest as far east as Mill Pond Road to connect with the community garden proposed for the Arborela De Vida Subdivision. A gravel trail with interpretive stations should be constructed within the urban forest explaining the role of irrigation channels in the area's development.

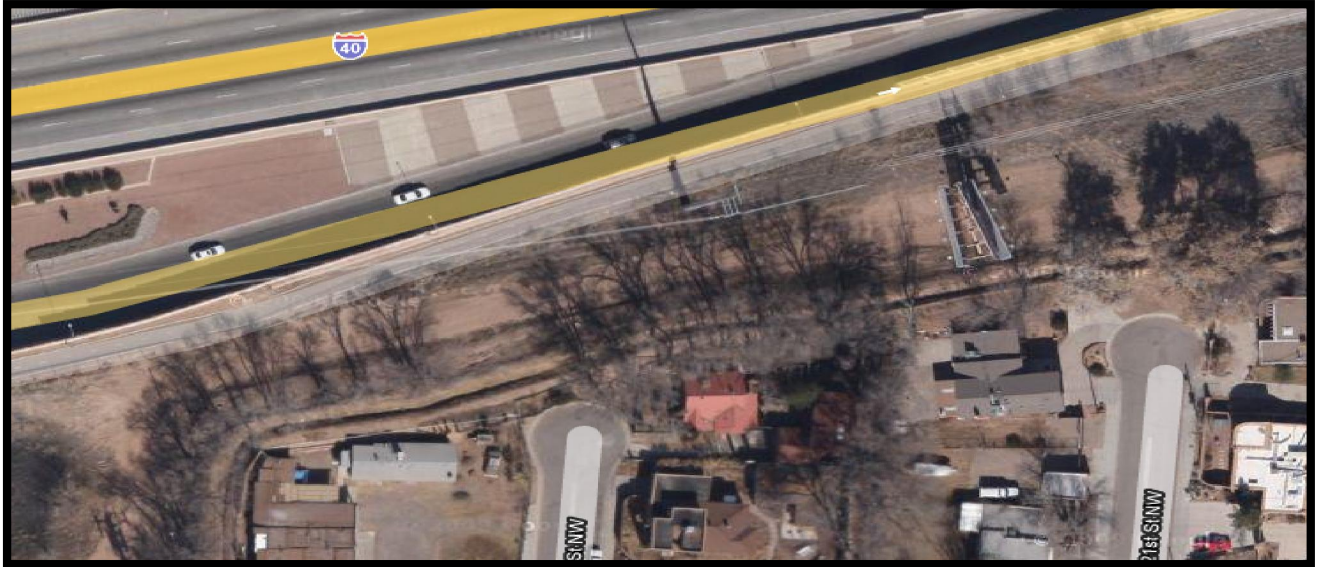
Figure 27, Albuquerque Potential Urban Forest/Acequia Interpretive Area Land Ownership.



Between Corrales and Ellison Roads along the south side of Cabezon Road is a conservancy district channel which was relocated to the middle of the property and reconstructed as a wide shallow meandering stream with small rapids. This type of landscape/waterscape could also be applied in the vacant area north of the John Baron Burg Park.

Exhibit A. Sawmill Sector Plan Amendment

Figure28: Albuquerque Ditch Channel within NMDOT
Property between 20th and 21st Street



Google Street View

Figure 29: Tom Bolack Urban Forest Between San Pedro
and Louisiana at Interstate 40.



Council Services
Tom Bolack Urban Forest Storm Drain

s



Council Service
Tom Bolack Urban Forest Dog Park

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Figure 30: Reconstructed Cabezon Channel
Between Corrales and Ellison Roads



Google Street View.



Council Services

5. **A new Section 13 of Chapter 6 is created to provide funding mechanisms to implement public projects. (sub headings numbered as Section 13)**

13. Regulations and Recommendations Funding Mechanisms. Funding sources must be identified to implement the streetscapes, construct the roadways, transit, and the trails facilities proposed for the focus area, and develop an interactive exhibit of the acequia system.

13.a Regulations: City Funding Responsibilities. The City should use capital-implementation program funds, state funds and federal funds to design, engineer and construct within the curb line the recommended streetscape for existing roadways.

13.b Regulations: Private Funding Responsibilities. New roadways and streetscapes shall either be constructed by the parties executing the project, or funded through a Tax increment Finance District (TIFD) or a Tax Increment Development District (TIDD).

13.c Recommendations: Establish a Tax Increment Finance District (TIF). Funding from this mechanism would be used for the construction, landscaping and maintenance of streetscapes, acquiring rights-of-way as necessary, developing street cross-sections, constructing parking structures, and expanding infrastructure capacity.

6. **A new Section of Chapter 7 pertaining to land use and building form recommendations is created to follow the Issues and Analysis Section.**

6. a Recommendations: Western Focus Area Land Uses and Building Forms.

a.1. Existing Land Use Conditions: The Western Focus Area is divided into stable and transitional areas. In the early 1990s, the focus area began redeveloping with residential and mixed uses, prompting interest in further developing vacant land and redeveloping existing structures.

Exhibit A. Sawmill Sector Plan Amendment

The **stable areas** will remain largely unchanged in terms of land uses or zoning. This includes the museums and Tiguex Park; the existing commercial districts, and residential neighborhoods located in the area's northwest and southeast quadrants. Also included, are industrial uses, such as the United Parcel Service (UPS) distribution center, the PNM Regional Transformer (south of I-40), and industrial office and warehouse properties.

The **Transitional areas** include the redeveloping properties such as those projects within the lands of the Sawmill Community Land Trust, including the Arbolera de Vida single-family project, senior housing, Sawmill Lofts, and a local economic development project. Also the ACCIÓN New Mexico-Arizona-Colorado Regional Headquarters at the corner of 18th Street and Zearing Avenue includes both offices and a small-business development center. Development of the Village will require expanding the detention pond located north of Bellamah Road and west of 18th Street.

The Rio Grande Properties industrial site just to the north of Sawmill Village could redevelop as either an industrial or non-industrial site. The Bureau of Indian Affairs /General Service Administration property located in the Southeast Quadrant of the I-40 and 12th Street interchange, comprised of vacant land and low intensity industrial and warehouse uses, is designated a Department of Interior Trust Land and therefore exempt from City regulations. There is also a proposal to develop a neighborhood shopping center at the Southwest Quadrant of the I-40 and 12th Street. The vacant parcels and structures between Bellamah and Aspen, east of Rio Grande Boulevard and west of 20th Street could also transition from commercial uses to mixed commercial and residential uses.

a.2. The existing zoning designations with the within the Western Focus Area are:

- C-2** — Community Commercial.
- S-R** — Sawmill Residential: includes single-family residential (attached/detached) and grandfathered non-residential uses.
- S-DR** — Sawmill Developing Residential: single-family residential, apartments and limited neighborhood commercial located in the John Baron Burg Park Subdivision.
- S-MRN** — Sawmill Mountain Road Neighborhood: single-family residential (attached/detached) limited neighborhood commercial.
- S-MI** — Sawmill Mixed Industrial: light industrial, limited wholesale distribution, warehousing, live/work commercial and new residential.
- S-I** — Sawmill Industrial: light manufacturing, light industrial and community commercial located on arterial streets.
- SU-1** — Sawmill Special Use: includes museums.

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Figure 31: Stable Land Uses & Properties Expected to Redevelop Western Focus Area

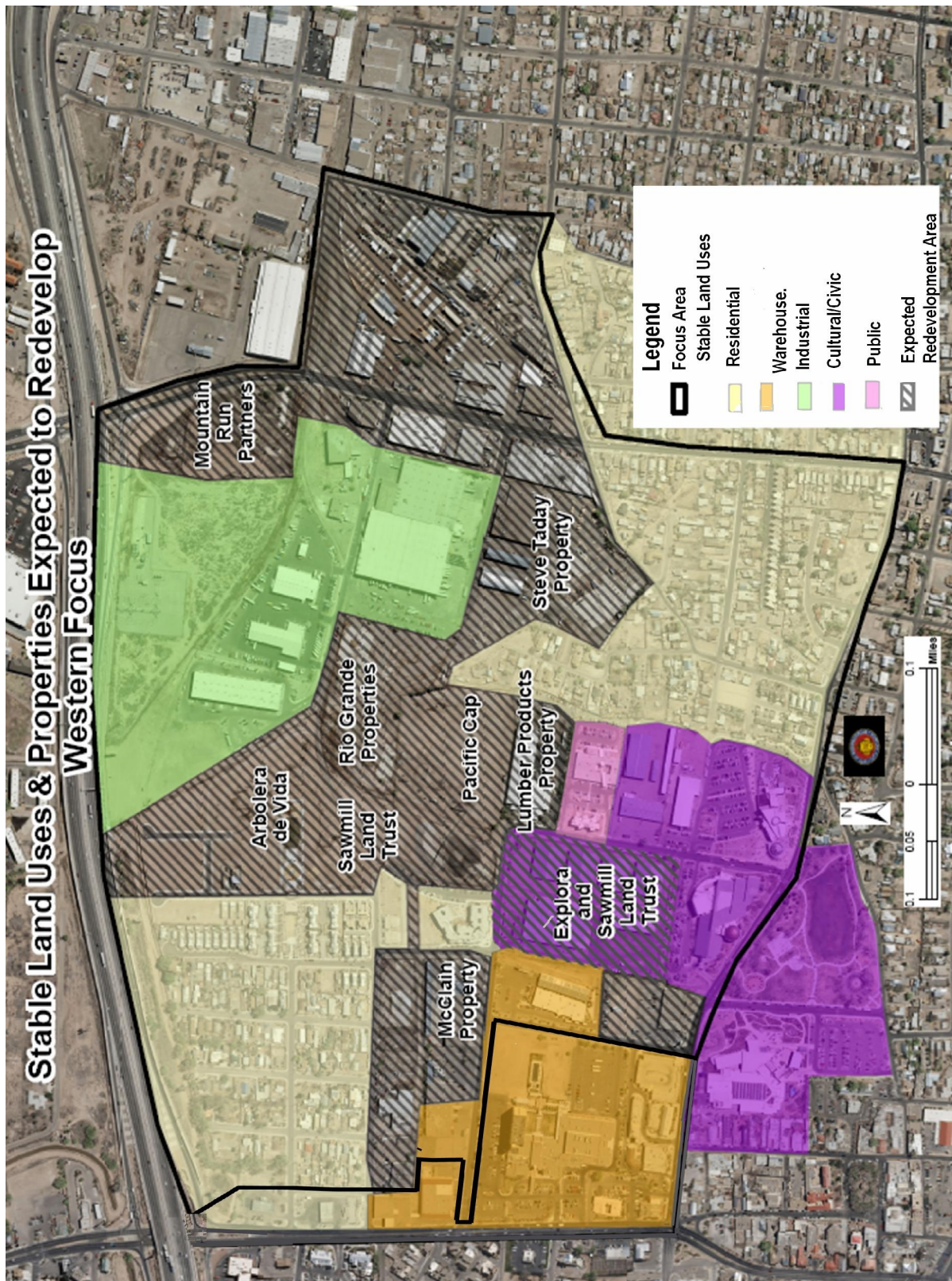


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6.b Recommended Sawmill Village Future Changes to Land Uses and Building Forms (No zone changes are scheduled to be enacted as a result of this amendment to the Sawmill/Wells Park Sector Plan).

Sawmill Village includes most of the land in the Western Focus Area excluding the existing single family areas, the CCBG property (UPS Warehouse) and the industrial lands north of Aspen, east of Mill Pond Road and west of the 12th Street Corridor. The recommendations and assumptions below are a discussion of potential land uses that could further the redevelopment of the Sawmill Village Area when integrated with the connectivity improvements set forth in this amendment.

These recommendations are specific to the Sawmill Village, including the 18th and 12th Streets Corridor. The land uses will overlay existing zones and are a mix of uses, intensities, building forms, and site plans. The uses within the center corridor and eastern gateway include a residential edge, a transit corridor frontage, a medium intensity zone, and the village center. Outside the Village Center and the 12th Street Corridors, the mix includes urban residential zones, industrial zones, and cultural and civic zones.

b.1. Village Center: This includes the lands generally north of Bellamah Road west of the UPS Warehouse and the Taday properties, south of Aspen, east of the Sawmill Lofts and southeast of the Arbolera de Vida development.

Proposed Land Uses. A village center site plan has been approved by EPC and is currently under construction and will be a cluster of mixed-use projects that include condominiums, rental and townhouse units, retail, office spaces, restaurants, and plazas. The center will connect to the surrounding street system through Bellamah Avenue to the east and 18th Street to the south.

Building Forms. The southern half of the Village Center is comprised of up to three-story mixed-use or live-work buildings with ground floor commercial and with one to two stories of residential above. Though, currently a senior housing project is under construction in the northern half of this part of the Village. The residential density will be at or under 20 dwelling units per acre. A range of residential and non-residential building types would also be appropriate for this section excluding single-family detached dwellings and duplex buildings. The front façades in the Central core would shade the sidewalk and enhance the pedestrian environment using portals, awnings, shop fronts, stoops, forecourts, and cantilevered shed roofs.

b.2. 18th Street Corridor. This corridor from Sawmill Village to the museums and Tiguex Park, connects the Old Town Civic and Cultural District to the mixed-use developments proposed for the Village and the single family subdivisions that are northwest and southeast of the eastern focus area. It is zoned S-MI and SU-1 which contains industrial, manufacturing, commercial services, and public/institutional land uses.

Land use types. 18th Street is envisioned as a pedestrian-oriented street that encourages walking and bicycling. The uses would include residential, commercial, entertainment and civic enterprises. These enterprises could be personal services, office space, specialty retail, restaurants, bars, theaters, museums, art galleries, dance studios, and residences. The Blue Lynx property on the southwest corner of Bellamah and Wyoming could be divided into smaller, pedestrian-scale blocks. With a mix of uses it could become a village within the Sawmill Area.

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Figure 32: Land Use Assumptions Western Focus Area

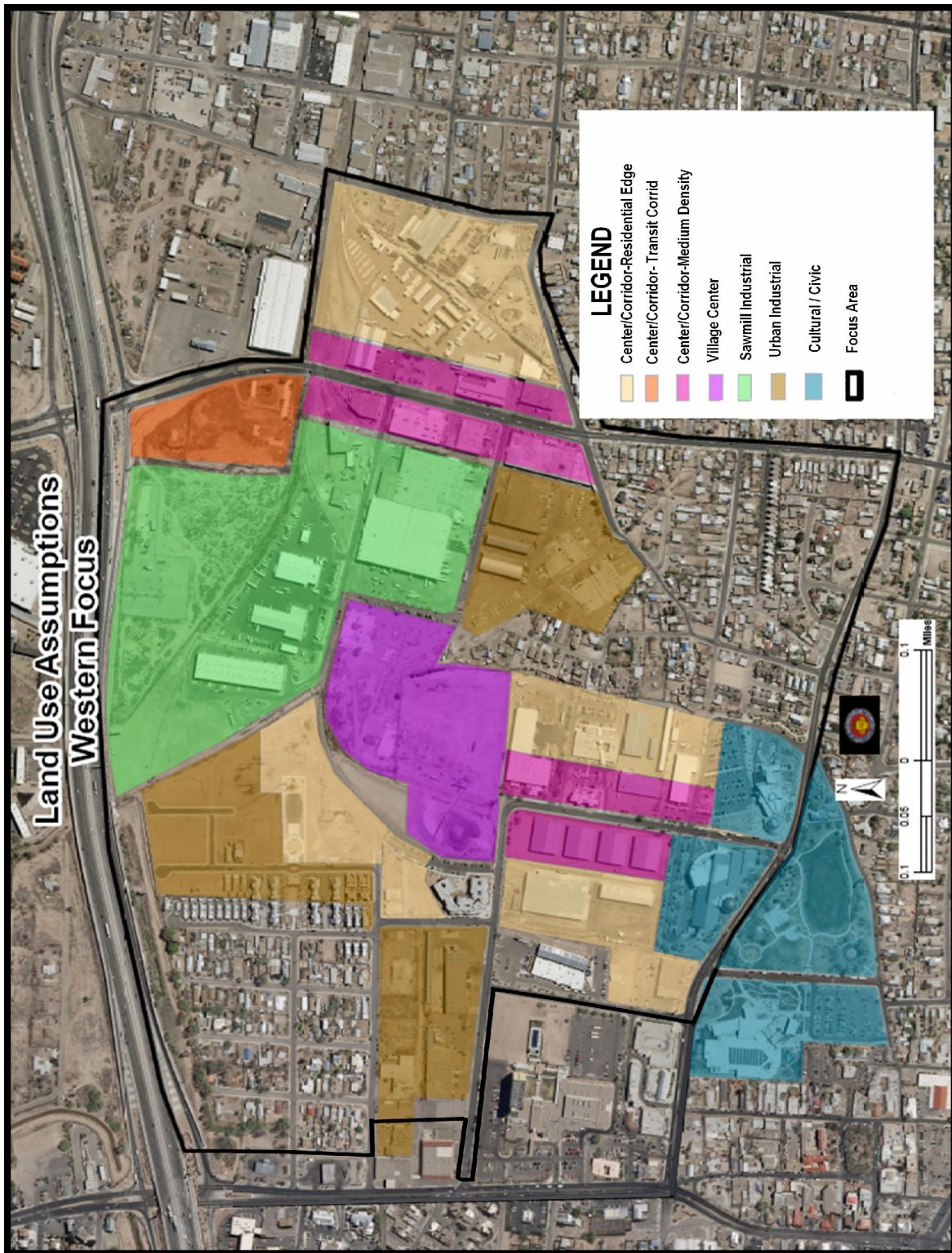
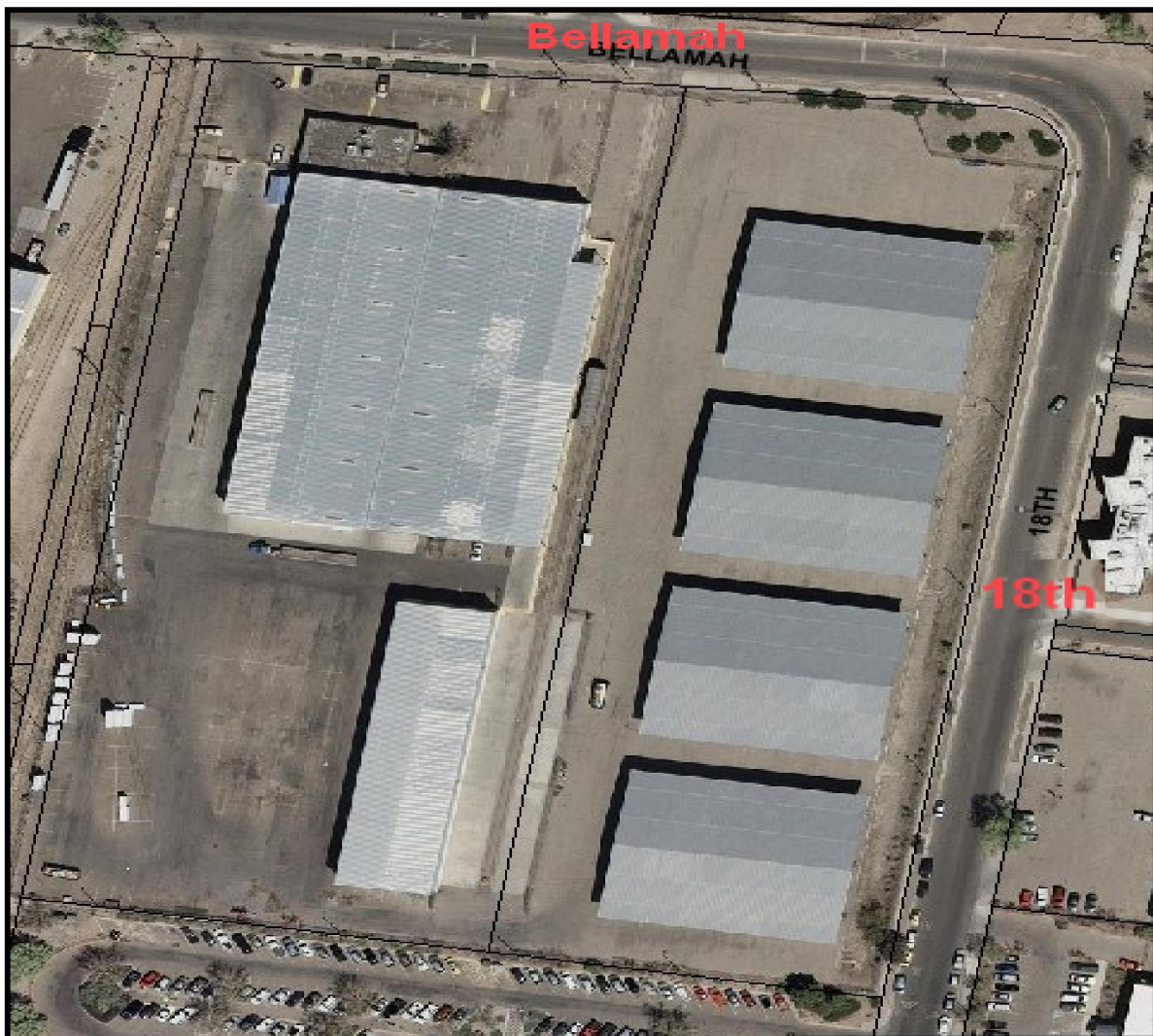


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Building forms. The 18th Street Corridor would be a compact built environment with an emphasis on pedestrian orientation and car accessibility. Possible building types include podium apartments, flex buildings, and live-work residences. These would be vertical mixed-use structures with at least two stories above the street level story. Building forms include commercial storefronts on the ground floor and dwelling units or a combination dwelling units/office spaces above. The front façades would include portals, stoops, forecourts, cantilevered shed roofs, and awnings that shade the sidewalk to enhance the pedestrian environment.

Figure 33:. Blue Lynx Property 18th Street and Bellamah Road



COA/GIS/Tegis

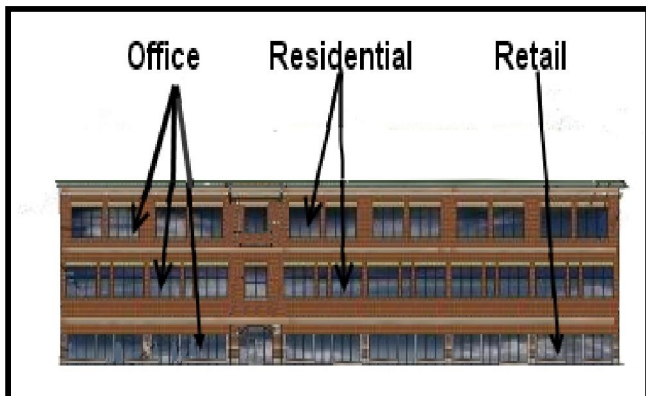
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Figure 34 Structure Types



<http://hubbarddevelopmentconsulting.com>

Podium Apartments: Commercial or mixed-use buildings with apartments above a first floor. Structures are usually a minimum 19-feet high from floor to ceiling and are configured for a mix of uses.



Flex Buildings: Units designed to house a combination of commercial, office, institutional, and residential uses.

Live Work Residences: Live-work Residences: Units built for habitation and work.



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Mixed Use Vertical Buildings



www.vil.woodridge.il.us/econ_dev

b.3. 12th Street Corridor.

The 12th Street Corridor from I-40 to Sawmill Road is the central gateway to the Sawmill/Wells Park Neighborhood. The parcels adjacent to the roadway are used for wholesale warehouses, manufacturing, and commercial services. The zoning in the corridor is a mix of Sawmill-Industrial and Sawmill-Manufacturing Zones. The corridor straddles the eastern and western focus areas and is a gateway into the Sawmill/Wells Park and Downtown areas, and the redevelopment projects occurring on the former Albuquerque Indian School site

Corridor Improvements.

Twelfth Street south of the railroad spur right-of-way located between Aspen and Haines Avenue, would transition to a walkable, transit-oriented, vertically mixed-use area with a mix of residential uses. Structures would include retail and office uses on the ground floor retail and residential or live-work units on the upper floors. Existing buildings with architectural merit would be adaptively reused.

Twelfth Street will continue to serve industrial uses and the street section; the building forms and streetscapes will be compatible with the industrial land uses. The corridor would potentially be the commercial nucleus for the surrounding neighborhoods and businesses. The densities and intensities of the built environment would be transit-supportive and would include retail stores and offices, and higher density housing.

Building Forms (Façades Adjacent to the Public Right-of-Way).

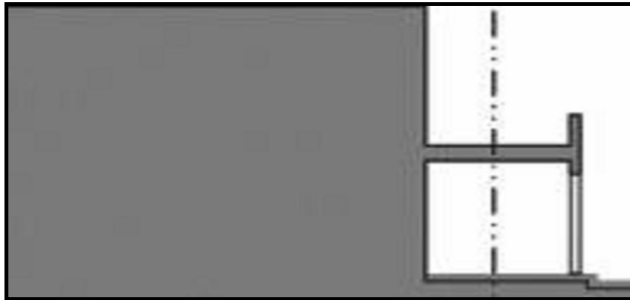
New and renovated structures would become vertical mixed-use buildings containing at least two stories above ground and not to exceed four stories. Commercial storefronts or commercial uses would be on the ground floor and dwelling units or combination dwelling units and office spaces on the upper floors. The main entrances to the buildings would be from 12th Street.

The front façades would enhance the pedestrian environment and shade the sidewalk using portals, awnings, shop fronts, stoops, forecourts, and cantilevered shed roofs. The portals would be attached to a building façade and could encroach to within 8 feet of the curb, including supporting columns. Portals on commercial uses would have 8 -

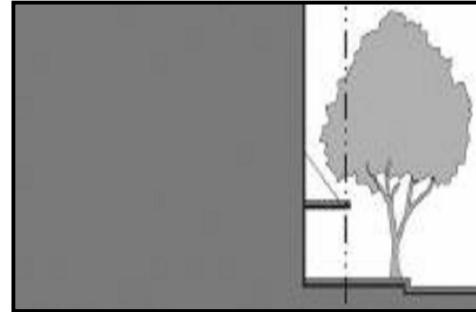
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feet clear in all directions. The awning or cantilevered shed roof would have a minimum extent of 6-feet from the building façade.

Figure 35 Façade Types



Front Façade with Portico over Sidewalk and outside the Right-of-way



Front Façade with Canopy both within and over Sidewalk outside of Right-of-way

b.4. Residential, Commercial, and Employment Areas.

The north, east and west edges of the Central Core are near established residential neighborhoods that will remain residential. The edge areas adjacent to the Village Center and 18th Street Corridor will transition from lower density residential areas to mixed-uses.

Land use types: The Central Core's edges would include low to moderate density housing, and live-work units. The Core's inner tracts would include a range of residential, commercial and employment uses, excluding light industrial uses. It should not be as intensely developed as the Village Center and 18th Street Corridor.

Building forms: Residential uses would range from single-family detached homes to courtyard apartments. The tracts closer to the Village Center, the 18th Street Corridor, or Mountain Road would have the same building form as the 18th Street Corridor, but would be lower in density. Buildings located between 18th and 19th Streets would be "double fronted" in design.

Double Fronted Retail



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